

# **ESTIMATED TIMED AGENDA**

## **CALIFORNIA TRANSPORTATION COMMISSION**

<http://www.catc.ca.gov>

**August 6, 2013  
Del Mar, California**

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### **Monday, August 5, 2013**

**6:30 PM**                      **Commissioners' Dinner**  
**Pacifica Del Mar Restaurant**  
**1555 Camino Del Mar**  
**Del Mar, CA**

### **Tuesday, August 6, 2013**

**9:00 AM**                      **Commission Meeting**  
**Hilton San Diego/Del Mar**  
**Salons A, B, C and D**  
**15575 Jimmy Durante Drive**  
**Del Mar, CA**

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**NOTICE:** Times identified on the following agenda are estimates only. The Commission has the discretion to take up agenda items out of sequence and on either day of the two-day meeting, except for those agenda items bearing the notation "TIMED ITEM." TIMED ITEMS which may not be heard prior to the Time scheduled but may be heard at, or anytime after the Time scheduled. The Commission may adjourn earlier than estimated on either day.

A copy of this meeting notice and agenda will be posted 10 days prior to the meeting and related book items will be posted 5 days prior to the meeting on the California Transportation Commission Website: [www.catc.ca.gov](http://www.catc.ca.gov)

Questions or inquiries about this meeting may be directed to the Commission staff at (916) 654-4245, 1120 N Street (MS-52), Sacramento, CA 95814. If any special accommodations are needed for persons with disabilities, please contact Deborah McKee at (916) 654-4245. Requests for special accommodations should be made as soon as possible but at least five days prior to the scheduled meeting.

Persons attending the meeting who wish to address the California Transportation Commission on a subject to be considered at this meeting are asked to complete a Speaker Request Card and give it to the Executive Assistant prior to the discussion of the item. If you would like to present handouts/written material to the California Transportation Commission at the meeting, please provide a minimum of 25 copies labeled with the agenda item number.

\* "A" denotes an "Action" item; "I" denotes an "Information" item; "B" denotes a California State Transportation (CalSTA) Agency item; "C" denotes a "Commission" item; "D" denotes a "Department" item; and "R" denotes a Regional Agency item.

**FREQUENTLY USED TERMS:** California Transportation Commission (Commission or CTC), California Department of Transportation (Department or Caltrans), Regional Improvement Program (RIP), Interregional Improvement Program (IIP), State Transportation Improvement Program (STIP), State Highway Operation and Protection Program (SHOPP), Traffic Congestion Relief Program (TCRP), Public Transportation Account (PTA), Clean Air and Transportation Improvement Act of 1990 (Proposition 116), High Speed Passenger Train Bond Program (Proposition 1A), Highway Safety, Traffic Reduction, Air Quality, and Port Security Bond Act of 2006 (Proposition 1B), Corridor Mobility Improvement Account (CMIA), State Route 99 Bond Program (RTE or SR 99), Local Bridge Seismic Retrofit Account (LBSRA), Trade Corridors Improvement Fund (TCIF), Highway-Railroad Crossing Safety Account (HRCSA), State-Local Partnership Program (SLPP), Traffic Light Synchronization Program (TLSP), Letter of No Prejudice (LONP), Environmental Phase (PA&ED), Design Phase (PS&E), Right of Way (R/W), Fiscal Year (FY)

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**Next regularly scheduled CTC Meeting is on Tuesday, October 8, 2013 in Modesto (Subject to change)**

Tab #	Item Description	Ref. #	Presenter	Status*	
<b><u>GENERAL BUSINESS</u></b>					
1	Roll Call	1.1	James Ghielmetti	I	C
2	Approval of Minutes for June 11, 2013	1.2	James Ghielmetti	A	C
3	Executive Director's Report	1.3	Andre Boutros	A	C
4	Commission Reports	1.4	James Ghielmetti	A	C
5	Commissioners' Meetings for Compensation	1.5	James Ghielmetti	A	C
<b><u>CALIFORNIA STATE TRANSPORTATION AGENCY REPORT</u></b>					
6	Report by Agency Secretary and/or Deputy Secretary	1.6	Brian Kelly	I	B
<b><u>CALTRANS REPORT</u></b>					
7	Report by Caltrans' Director and/or Deputy Director	1.7	Malcolm Dougherty	I	D
<b><u>UNITED STATES DEPARTMENT OF TRANSPORTATION REPORT</u></b>					
8	Report by US Department of Transportation	1.11	Vincent Mammano	I	R
<b><u>LOCAL REPORTS</u></b>					
9	Report by Regional Agencies Moderator	1.8	Adriann Cardoso	I	R
10	Report by Rural Counties Task Force Chair	1.9	Sharon Scherzinger	I	R
11	Report by Self-Help Counties Coalition Chair	1.10	Andy Chesley	I	R
<b><u>POLICY MATTERS</u></b>					
12	State and Federal Legislative Matters	4.1	Laura Pennebaker	A	C
13	Budget and Allocation Capacity Update	4.2	Mitchell Weiss Steven Keck	A	D
14	2014 STIP Guidelines Hearing	4.5	Mitchell Weiss	I	C
15	Adoption of 2014 State Transportation Improvement Program Guidelines Resolution G-13-07	4.6	Mitchell Weiss	A	C
16	Adoption of 2014 Fund Estimate Resolution G-13-08	4.7	Mitchell Weiss Steven Keck	A	C/ D
17	Los Angeles Accelerated Regional Transportation Improvements (ARTI) Public Private Partnership Proposal	4.14	Nizar Melehani	I	D
18	California Transportation Infrastructure Priorities	4.3	Brian Kelly	I	B
19	Bay Bridge Update	4.18	Stephen Maller	I	C
20	Buy America Update	4.15	Stephen Maller Brent Green	I	D
<b><u>INFORMATION CALENDAR</u></b>			Stephen Maller		
21	<b><u>Informational Reports on Allocations Under Delegated Authority</u></b> -- Emergency G-11 Allocations (2.5f.(1)): \$3,200,000 for three projects. -- SHOPP Safety G-03-10 Allocations (2.5f.(3)): \$34,422,000 for eight projects.	2.5f.		I	D
22	Monthly Report on Projects Amended into the SHOPP by Department Action	3.1		I	D
23	Status of Construction Contract Award for State Highway Projects, per Resolution G-06-08	3.2a.		I	D
24	Monthly Status of Construction Contract Award for Local Assistance STIP Projects, per FY 2005-06 Allocation Plan and Criteria and Resolution G-06-08	3.2b.		I	D
25	Monthly Report on Local and Regional Agency Notices of Intent to Expend Funds on Programmed STIP Projects Prior to Commission Allocation per SB 184	3.4		I	C
26	Annual Analysis by the State Treasurer of Bonding Capacity (GARVEE Bonds) of the Federal Transportation Funds	4.13		I	C
<b><u>CONSENT CALENDAR</u></b>			Stephen Maller		
27	The City of Inglewood proposed to amend the TLSP baseline agreement for the La Brea Avenue project (Project 6758) in Los Angeles County, to update the project schedule. Resolution TLSP-PA-1314-01	2.1c.(6)		A	D

Tab #	Item Description	Ref. #	Presenter	Status*	
28	Submittal of Notice of Preparation for Comments 04-CC-680 Construct HOV Lane on I-680 in the city of San Ramon. (EA 3A860 ) (NOP)	2.2a.(1)		A	D
29	Submittal of Notice of Preparation for Comments 11-SD-67, PM 6.7/19.0 Construct safety improvements along an existing 12.3 mile segment of SR-67 near the city of Poway (PPNO 0670) (NOP)	2.2a.(2)		A	D
30	Submittal of Draft Environmental Document for Comment: 12-ORA-405, PM 9.3/24.2, 07-LA-405, PM 0.0/1.2, 12-ORA-22, PM R0.7/R3.8, 12-ORA-22, PM R0.5/R0.7, 12-ORA-73, PM R27.2/R27.8, 12-ORA-605, PM 3.5/R1.6 07-LA-605, PM R0.0/R1.2 San Diego Freeway (I-405) Improvement Project Roadway improvements on a portion of I-405 in and near the city of Long Beach. (EA 0H1000) (SDEIR)	2.2b.		A	D
31	Approval of Projects for Future Consideration of Funding and/or Future Consideration for Route Adoption:  08-SBd-18, PM 53.12/53.87 State Route 18 Safety Road Widening and Installation of Left-Turn Pocket Project Roadway widening and improvements on a portion of SR 18 in the city of Big Bear. (ND) (PPNO 0188C) (SHOPP) Resolution E-13-58  02-Tri-299, PM 12.2/12.9 Collins Curve Improvement Project Roadway improvements on a portion of SR-299 near the town of Burnt Ranch. (MND) (PPNO 3438) (SHOPP) Resolution E-13-59  01-Men-271, PM 7.12/7.2 Leggett Maintenance Station Groundwater Remediation Project. Installation of groundwater remediation equipment on a section of SR 271 near the town Leggett. (ND) (PPNO 4510) (SHOPP) Resolution E-13-60  06-Tul-190, PM 0.0/8.0 State Route 190 Rehabilitation Project. Roadway improvements on a portion of SR-190 in and near the city of Tipton. (MND) (PPNO 6508) (SHOPP) Resolution E-13-61  10-Sta-4, PM 6.3/6.5 Rockaway Shoulder Widening and Rumble Strips Project. Roadway improvements on a portion of SR-04 near the community of Farmington. (MND) (PPNO 0287) (SHOPP) Resolution E-13-62  3-Pla-193, PM 4.4/5.5 (Route Adoption) State Route 193 Curve Improvement Project. Roadway improvements on a portion of SR-193 near the city of Lincoln. (MND) (PPNO 5580) (SHOPP) Resolution E-13-63 <i>(Related Item under Tab 32.)</i>	2.2c.(1)		A	D

Tab #	Item Description	Ref. #	Presenter	Status*	
32	A Route Adoption as a State Highway 03-Pla-193-PM 4.4/5.5 From 0.1 mile west of Clark Tunnel Road to 1.0 mile east of Clark Tunnel Road, in the County of Placer Resolution HRA 13-02 <i>(Related Item under Tab 31.)</i>	2.3a.		A	D
33	Three Relinquishment Resolutions – -- 04-SM-1-PM 38.4/40.0 Right of way along Route 1 superseded by the Devil's Slide Tunnel realignment, in the county of San Mateo. Resolution R-3878  -- 05-Mon-101-PM 85.5/85.7 Right of way along Route 101 at Airport Boulevard, De La Torre and Moffett Streets, in the city of Salinas. Resolution R-3879  -- 05-Mon-101-PM 85.3/85.5 Right of way along Route 101 at De La Torre Street, in the county of Monterey. Resolution R-3880	2.3c.		A	D
34 8 Ayes	23 Resolutions of Necessity Resolutions C-21077 through C21081, C-21083 through C-21095 and C-21097 through C-21101	2.4b.		A	D
35	Director's Deeds Items 1 through 15 Excess Lands - Return to State: \$6,817,900 Return to Others: \$0	2.4d.		A	D
36	Financial Allocation Amendment: Reduce the original SHOPP/TCIF allocation for construction by \$9,508,000, from \$42,300,000 to \$32,792,000, for Project 12 (I-80 Eastbound Cordelia Truck Scales Relocation project [PPNO 5301R]) in Solano County. Resolution FP-13-14, Amending Resolution FP-10-33 Resolution TCIF-AA-1314-02	2.5b.(5a)		A	D
37	Financial Allocation Amendment: Reduce the original SHOPP/TCIF allocation for construction by \$7,209,000, from \$48,959,000 to \$41,750,000, for Project 5 (I-580 Eastbound Truck Climbing Lane project [PPNO 0104]) in Alameda County. Resolution FP-13-15 Amending Resolution FP-10-33 Resolution TCIF-AA-1314-03	2.5b.(5b)		A	D
38	Financial Allocation Amendment: Reduce the original SR 99 Corridor allocation for construction by \$5,479,000, from \$52,000,000 to \$46,521,000, for the Atwater-Merced Expressway Phase 1A project (PPNO 5264A) in Merced County, and revise the project funding plan. Resolution R99-AA-1314-01 Amending Resolution R99-A-1213-05 Resolution R99-PA-1314-01 Amending Resolution R99-P-1213-06	2.1c.(2)/ 2.5g.(2)		A	D
39	Financial Allocation Amendment: Reduce the original TCIF allocation for construction by \$4,079,000 from \$37,638,000 to \$33,559,000 for Project 88 (Baldwin Avenue Grade Separation [PPNO TC88]), in Los Angeles County. Resolution TCIF-AA-1314-01, Amending Resolution TCIF-A-1112-12	2.5g.(5e)		A	D
40	Financial Allocation: \$426,000 for the City of Inglewood - La Brea Avenue TLSP project in Los Angeles County. Resolution TLS1B-A-1314-01	2.5g.(7)		A	D

Tab #	Item Description	Ref. #	Presenter	Status*	
41	Financial Allocation Amendment: Reduce the original HRCSA allocation for construction by \$1,534,213, from \$15,293,000 to \$13,758,787, for the Hageman Road Grade Separation project (EA H013BA) in Kern County, to reflect contract award savings. Resolution GS1B-AA-1314-01 Amending Resolution GS1B-AA-1112-005	2.5g.(9)		A	D
42	Technical correction to Resolution SLP1B-A-1213-18, originally approved March 5, 2013, allocating \$30,827,000 for 47 locally administered SLPP project, off the State Highway System. A technical correction is needed for Project 22 (Broadway and McFadden Avenue Rehabilitation) to revise the Project ID number.	2.9a.		A	D
43	Technical correction to Resolution FP-08-42, originally approved on April 15, 2009, for \$10,000,000 for 36 EEM Program projects. A technical correction is need for Project 6 – Middle Yuba River to revise the project recipient in the vote box from the Trust for Public Land to Bear Yuba Land Trust.	2.9b.		A	D
44	Technical correction to Resolution GS1B-A-1112-005, originally approved May 23, 2012, for \$12,157,000 for two locally administered HRCSA projects. A technical correction is need for Project 2 - North Spring Street Grade Separation project in Los Angeles County to revise the Project ID number.	2.9c.		A	D
45	Adoption of the Rate for Local Government Matching of California Aid to Airports Program (CAAP) Resolution G-13-09	4.11		A	D
46	Approval of the Capital Improvement Plan Element of the California Aviation System Plan	4.12		A	D
<b>END OF CONSENT CALENDAR</b>					
<b>AIRSPACE LEASES</b>					
47	Airspace Lease - Request to directly negotiate with the San Diego Unified Port District	2.4c.	Stephen Maller Brent Green	A	D
<b>PROGRAM STATUS</b>					
48	2013 Report of State Transportation Improvement Program (STIP) Balances, County and Interregional Shares	3.3	Laurel Janssen	I	C
49	Proposition 1B Semi-Annual Status Report	3.5	Stephen Maller	A	C
<b>POLICY MATTERS</b>					
50	Trade Corridor Improvement Fund Program – Policy Utilize Program Savings Resolution TCIF-P-1314-03	4.10	Stephen Maller	A	C
51	Trade Corridors Improvement Fund-Program Amendment Resolution TCIF-P-1314-02	4.8	Stephen Maller	A	C
52	Proposition 1B Intercity Rail Improvement Program Amendment Resolution ICR1B-P-1314-01, Amending Resolution ICR1B-P-1112-01	4.17	Juan Guzman Bill Bronte	A	D
53	Notice of the FFY 2011-12 Federal Transit Administration Section 5310 Elderly & Disabled Transit Program Resolution G-13-10	4.20	Juan Guzman	I	C
<b>Environmental Matters</b>					
<b>Environmental Matters – Approval of Projects for Future Consideration of Funding, Route Adoption or New Public Road Connection (Final Negative Declaration or EIR)</b>					
54	Approval of Project for Future Consideration of Funding: 07-LA-5, PM R45.4/R59.0 I-5 HOT Lane Project. Addition of a High Occupancy Toll Lane to a portion of I-5 in and near the city of Valencia. (SFEIR) (EA 2332E) Resolution E-13-64	2.2c.(2)	Laura Pennebaker Katrina Pierce	A	D

Tab #	Item Description	Ref. #	Presenter	Status*	
55	Approval of Project for Future Consideration of Funding: 05-Mon-156, PM R1.60/T5.2, 05-Mon-101, PM 94.6/96.8 Route 156 West Corridor Project. Roadway improvements on a portion of SR-156 in and near the city of Prunedale. (FEIR) (PPNO 0057C) (STIP) Resolution E-13-65	2.2c.(3)	Laura Pennebaker Katrina Pierce	A	D
56	Approval of Project for Future Consideration of Funding: 04-SCI-152, PM 0.14/5.20 Hecker Pass Safety Improvement Project. Roadway improvements at five locations along a portion of SR-152 near the city of Gilroy. (FEIR) (PPNO 0483J) (SHOPP) Resolution E-13-66	2.2c.(4)	Laura Pennebaker Katrina Pierce	A	D
<b>Amendments for Action</b>					
57	The Siskiyou County Local Transportation Commission proposes to amend the 2012 STIP to program \$300,000 in RIP funds from Siskiyou County's unprogrammed share balance for construction on a new project, Angel Maple Operational Improvements (PPNO 3530), in Siskiyou County. STIP Amendment 12S-049	2.1a.	Mitchell Weiss Rachel Falsetti	A	D
<b>Proposition 1B TCIF Project Amendments for Action</b>					
58	The Alameda Corridor East Construction Authority proposes to amend the TCIF baseline agreement for Project 15 (San Gabriel Valley Grade Separation Program [PPNO TC15]) in Los Angeles County to update the delivery schedule, cost and funding plan. Resolution TCIF-P-1314-01 Amending Resolution TCIF-P-1112-45	2.1c.(5)	Stephen Maller Katie Benouar	A	D
<b>Financial Allocations for SHOPP Projects</b>					
59	Financial Allocation: \$129,498,000 for 32 SHOPP projects, programmed, in FY 12-13, as follows: --\$117,422,000 for 27 SHOPP projects. --\$12,076,000 for five projects amended into the SHOPP by Departmental action. Resolution FP-13-01	2.5b.(1)	Juan Guzman Rachel Falsetti	A	D
60	Financial Allocation: \$53,792,000 for 11 SHOPP projects, programmed in FY 13-14, as follows: --\$39,792,000 for nine SHOPP projects. --\$14,000,000 for two projects amended into the SHOPP by Department action. Resolution FP-13-02	2.5b.(2)	Juan Guzman Rachel Falsetti	A	D
61	Advance Financial Allocation: \$1,369,000 for one SHOPP project in Los Angeles County, programmed in FY 2014-15. Resolution FP-13-13	2.5b.(4)	Juan Guzman Rachel Falsetti	A	D
<b>Financial Allocations for SHOPP/TCIF Projects</b>					
62	Financial Allocation: \$73,433,000 for SHOPP/TCIF Project 4 (I-880 Reconstruction – 29 <sup>th</sup> and 23 <sup>rd</sup> Avenues Overcrossing project [PPNO 0044C]) in Alameda County. Resolution FP-13-03 Resolution TCIF-A-1314-01	2.5b.(3a)	Stephen Maller Rachel Falsetti	A	D
63	Financial Allocation: \$15,000,000 for SHOPP/TCIF Project 94 (San- ta Clara – US 101 Freeway Performance Initiative project [PPNO 0449R]) in Santa Clara County. Resolution FP-13-04 Resolution TCIF-A-1314-02	2.5b.(3b)	Stephen Maller Rachel Falsetti	A	D

Tab #	Item Description	Ref. #	Presenter	Status*	
<b>Financial Allocations for STIP Projects</b>					
64	Financial Allocation: \$29,935,000 for three State administered STIP projects, on the State Highway System. Contributions from other sources: \$648,000. Resolution FP-13-06	2.5c.(1)	Mitchell Weiss Rachel Falsetti	A	D
65	Financial Allocations: \$16,925,000 for 24 locally administered STIP projects off the State Highway System, as follows: --\$4,768,000 for eight STIP projects. --\$7,342,000 for three STIP Transportation Enhancement projects. --\$4,815,000 for 13 STIP Planning, Programming, and Monitoring projects. Resolution FP-13-07	2.5c.(3a)	Mitchell Weiss Rachel Falsetti	A	D
<b>Supplemental Financial Allocations for Locally Administered STIP Projects</b>					
66	Supplemental Financial Allocation: \$183,000 for the Evergreen Road Bridge Replacement locally administered STIP project (PPNO 2379) in Tehama County, off the State Highway System. Resolution FP-13-08	2.5c.(3b)	Mitchell Weiss Denix Anbiah	A	D
67	Supplemental Financial Allocation: \$50,000 for the Branscomb Road Pedestrian Bridge locally administered STIP TE project (PPNO 4517) in Mendocino County, off the State Highway System. Resolution FP-13-09	2.5c.(3c)	Mitchell Weiss Denix Anbiah	A	D
<b>Advance Financial Allocations for STIP Projects</b>					
68	Advance Financial Allocation: \$2,468,000 for two locally administered STIP TE projects, off the State Highway System, programmed in FY 15-16. Contributions from other sources: \$ 3,700,000. Resolution FP-13-__	2.5c.(4)	Mitchell Weiss Denix Anbiah	A	D
<b>Financial Allocations for Local Alternative Transportation Improvement Program Projects</b>					
69	Financial Allocation: \$8,100,000 for the Hayward Route 238 Street Improvement, Local Alternative Transportation Improvement Program project, on the State Highway System. Resolution FP-13-11	2.5c.(5)	Laurel Janssen Rachel Falsetti	A	D
<b>Financial Allocations for Supplemental Funds</b>					
70	Financial Allocation: \$260,000 in supplemental funds for the previously voted SHOPP Major Damage Restoration (PPNO 0527) project in Imperial County to close-out the construction contract. The current SHOPP allocation is \$1,400,000. This request for \$260,000 results in an increase of 18.6 percent over the current allocation. Resolution FA-13-04	2.5e.(2)	Mitchell Weiss Laurie Berman	A	D
71	Financial Allocation: \$50,000 in supplemental funds for the previously voted SHOPP Major Damage Restoration (PPNO 1082) project in San Diego County to close-out the construction contract. The current SHOPP allocation is \$450,000. This request for \$50,000 results in an increase of 11.1 percent over the current allocation. Resolution FA-13-05	2.5e.(3)	Mitchell Weiss Laurie Berman	A	D
72	Financial Allocation: \$4,000,000 in STIP supplemental funds for the State Route 24 Caldecott Tunnel Fourth Bore project (PPNO 0057A) in Alameda and Contra Costa Counties. This request for \$4,000,000 results in an increase of 148.1 percent over the programmed budget. Resolution FA-13-06	2.5e.(4)	Mitchell Weiss Bijan Sartipi	A	D

Tab #	Item Description	Ref. #	Presenter	Status*	
<b>Financial Allocations for Proposition 1B TCIF Projects</b>					
73	Financial Allocation: \$8,855,000 for the locally administered TCIF Project 64 (Lenwood Road Railroad Grade Separation project [PPNO 1135]) in San Bernardino County, off the State Highway System. Contributions from other sources: \$22,878,000. Resolution TCIF-A-1314-04	2.5g.(5a)	Stephen Maller Rachel Falsetti	A	D
74	Financial Allocation: \$39,519,000 for the locally administered TCIF Project 40 (Lakeview Avenue Grade Separation project [PPNO TC40]) in Orange County, off the State Highway System. Contributions from other sources: \$60,244,000. Resolution TCIF-A-1314-05	2.5g.(5b)	Stephen Maller Rachel Falsetti	A	D
75	Financial Allocation: \$10,880,000 for the locally administered TCIF Project 2 (Richmond Rail Connector Rail project [PPNO 0241B]) in Contra Costa County. Contributions from other sources: \$11,770,000. Resolution TCIF-A-1314-06	2.5g.(5c)	Stephen Maller Rachel Falsetti	A	D
<b>Financial Allocation for Multi-Funded Project with STIP/SHOPP/TCIF Program Funds</b>					
76	Financial Allocation: \$35,412,000 for the State administered TCIF Project 89 -WB I-80 to SR 12 (West) Connector and Green Valley Road Interchange Improvements (PPNO 5301L) multi-funded project in Solano County, on the State Highway System. Contributions from other sources: \$29,448,000. Resolution FP-13-12 Resolution TCIF-A-1314-07	2.5g.(5d)	Stephen Maller Rachel Falsetti	A	D
<b>Proposition 116 Rail Program Project Approvals/ Amendments for Action</b>					
77	Proposition 116 Project Application Amendment for the Transportation Agency for Monterey County (TAMC) Rail Extension to Monterey County project (PPNO 1155) – PUC Section 99638. Resolution PA-13-01, Amending PA-10-03 <i>(Related Items under Tabs 78)</i>	2.1d.	Juan Guzman	A	C
<b>Financial Allocation Amendment for Local Proposition 116 Projects</b>					
78	Financial Allocation Amendment: Reduce the original Proposition 116 allocation of \$6,247,813 by \$4,917,837, to \$1,329,976, and to re-allocate \$300,000 for PA&ED; \$300,000 for PS&E; and \$729,976 for R/W for the Rail Extension to Monterey County project (PPNO 1155) in Monterey County - PUC 99638. Resolution BFA-13-01; Amending Resolution BFP-09-03 <i>(Related Item under Tab 77)</i>	2.6b.	Juan Guzman Jane Perez	A	D
<b>Financial Allocations/Amendments for TCRP Projects</b>					
79	Financial Allocation: \$38,142,000 for three Tier 1 TCRP projects in Los Angeles County. Resolution TFP-13-01	2.6e.(1)	Juan Guzman Rachel Falsetti	A	D
80	Financial Allocation: \$40,000,000 for construction of Tier 1 TCRP Project 1.2 (BART to San Jose- Phase 1; Extend BART Warm Springs to Berryessa [PPNO 2147D]) in Santa Clara County. Resolution TFP-13-02	2.6e.(2)	Juan Guzman Rachel Falsetti	A	D
<b>Aeronautics Financial Matters</b>					
81	Financial Allocation for FY 2013-14 Aeronautics Set-Aside to Match Federal Airport Improvement Program Grants Resolution FDOA-2013-01	2.7	Mitchell Weiss Dennis Jacobs	A	D
<b>Time Extension Requests per CTC Resolution G-06-08, Resolution G-06-20, STIP Guidelines, Section 65 – Timely Use of Funds / Proposition 116 Waiver Requests / Miscellaneous Requests</b>					
<b>Request to Extend the Period of Project Allocation</b>					
82	Request to extend the period of project allocation for nine SHOPP projects totaling \$60,224,000. Waiver 13-33	2.8a.	Juan Guzman Rachel Falsetti	A	D

Tab #	Item Description	Ref. #	Presenter	Status*	
<b><u>Request to Extend the Period of Contract Award</u></b>					
83	Request to extend the period of contract award for three locally-administered STIP projects totaling \$1,227,000, per Resolution G-06-08 Waiver 13-34	2.8b.(1)	Juan Guzman Denix Anbiah	A	D
84	Request to extend the period of contract award for one SHOPP project to upgrade Roadside Rest Area along Route 15 in San Bernardino County for \$11,273,000, per Resolution G-06-08. Waiver 13-35	2.8b.(2)	Juan Guzman Rachel Falsetti	A	D
85	Request to extend the period of contract award for one locally administered SLPP project to construct new interchange on US Highway 50 in El Dorado County for \$1,000,000, per Resolution G-06-08. Waiver 13-36	2.8b.(3)	Juan Guzman Rachel Falsetti	A	D
86	Request to extend the period of contract award for one locally administered SLPP project to construct sidewalks and raised median on Route 62 in San Bernardino County for \$778,000, per Resolution G-06-08. Waiver 13-37	2.8b.(4)	Juan Guzman Rachel Falsetti	A	D
<b><u>Request to Extend the Project Development Expenditures</u></b>					
87	Request to extend the period of project development expenditures for the Lathrop Road Westerly Grade Separation project in San Joaquin County, per Resolution G-06-08. Waiver 13-39	2.8d.(1)	Juan Guzman Jane Perez	A	D
88	Request to extend the period of project development expenditure for the Brooktrails Second Access project (PPNO 4099P) in the Mendocino County for \$459,000, per STIP Guidelines Waiver 13-41	2.8d.(3)	Juan Guzman Denix Anbiah	A	D
<b><u>OTHER MATTERS / PUBLIC COMMENT</u></b>		6.			
3:00 PM	<b><u>Adjourn</u></b>				

Tab #	Item Description	Ref. #	Presenter	Status*
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	<p><b><u>Highway Financial Matters</u></b></p> <p>\$ 273,092,000 Total SHOPP/Minor Requested for Allocation            \$ 57,428,000 Total STIP Requested for Allocation            \$ 95,092,000 Total Proposition 1B Bond Requested for Allocation            \$ 233,000 Total Supplemental Funds Requested for Allocation (Local Agencies)            \$ 4,310,000 Total Supplemental Funds Requested for Allocation (Department)            \$ 430,155,000 Sub-Total Project Funds Requested for Allocation</p> <p><u>\$ 37,622,000</u> Delegated Allocations            \$ 467,777,000 Sub-Total, Highway Project Allocations</p> <p><u>\$ 401,780,000</u> Contributions from Other Sources            \$ 869,557,000 Total Value</p> <p>Total Jobs Created: 15,652 <i>(Includes Direct, Indirect, and Induced)</i></p> <p>(\$ 11,092,213) Total Proposition 1B Bond De-Allocations Requested            (\$ 16,717,000) Total SHOPP Requested for De-allocation</p>			
	<p><b><u>Mass Transportation Financial Matters</u></b></p> <p><u>\$ 78,142,000</u> Total TCRP Requested for Allocation            \$ 78,142,000 Total TCRP Allocations</p> <p>Total Jobs Created: 1,404 <i>(Includes Direct, Indirect, and Induced)</i></p> <p>\$ 1,329,976 Total Proposition 116 Requested for Re-allocation            (\$ 4,917,837) Total Proposition 116 Requested for De-Allocation</p>			

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(1) SHOPP Projects</b>		<b>Resolution FP-13-01</b>		
1 \$5,000,000 Mendocino 01-Men-128 14.3/40.6	Near Boonville, from west of Mill Creek Bridge to east of Beebe Creek Bridge. <u>Outcome/Output:</u> Rehabilitate existing culverts, replace deteriorated culverts and place standard drainage inlet and outlet structures at 51 locations to improve drainage.	01-0200 SHOPP/12-13 \$5,000,000 0100000136 4 378164	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.151	\$100,000 \$4,900,000
2 \$4,500,000 Mendocino 01-Men-128 34.5/35.5	Near Boonville, from Shearing Creek Bridge to 0.7 mile west of Maple Creek Bridge. <u>Outcome/Output:</u> Stabilize embankment, install cast-in-place steel reinforced ground anchor wall system and rock slope protection (RSP) damaged by heavy rainfall.	f 01-4463 SHOPP/12-13 \$10,329,000 0100000351 4 476604	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.031	\$90,000 \$4,410,000
3 \$4,700,000 Humboldt 01-Hum-169 13.6/23.4	Near Redwood National Park, from 0.8 mile west of Peewan Creek Bridge to 1.0 mile east of Cappell Creek Bridge at various locations. <u>Outcome/Output:</u> Widen roadway and install metal beam guardrail at eight locations to reduce the potential for collisions and reduce the severity of run-off-road collisions.	01-2028 SHOPP/12-13 \$5,636,000 0100000219 4 450904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$94,000 \$4,606,000
4 \$27,314,000 Placer 03-Pla-80 8.1/37.8	In and Near Loomis, at various locations from Brace Road to Margra Road. <u>Outcome/Output:</u> Raise six overcrossing structures and replace roadway structural section beneath one underpass and two overcrossing structures to meet vertical clearance requirements for permit vehicles	03-5095 SHOPP/12-13 \$27,134,000 0300000473 4 3E1004	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.322	\$546,000 \$26,768,000
5 \$7,996,000 Alameda 04-Ala-580 46.0/46.5	In Oakland, at MacArthur Boulevard Bridge No. 33-0061L/R. <u>Outcome/Output:</u> Remove unsound concrete and patch with rapid set concrete, construct polyester concrete overlay and replace joint seal assemblies to maintain structural integrity, and reduce the risk to lives and properties.	04-0159N SHOPP/12-13 \$49,290,000 0412000346 4 1A6714	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.110	\$160,000 \$7,836,000
6 \$475,000 Marin 04-Mrn-1 0.9/1.0	Near the city of Marin, from Ross Drive to Tennessee Avenue. <u>Outcome/Output:</u> Replace corrugated metal pipe culverts with plastic pipe culverts, construct new drainage inlets, fill-in existing drainage ditch and replace with paved curb and gutter with drainage inlets and wing-wall damaged by heavy rainfall.	04-0312L SHOPP/12-13 \$880,000 0400020144 4 4S7704	2012-13 302-0042 SHA 20.20.201.131	\$475,000
7 \$1,150,000 Marin 04-Mrn-1 31.2	Near Point Reyes Station, at Petaluma Road. <u>Outcome/Output:</u> Stabilize roadway embankment and construct a continuous tangent cast in drilled hope pile wall to prevent further erosion problems to the embankment as a result of high-water rain events during the winter rainy seasons.	04-0322C SHOPP/12-13 \$1,100,000 0400001238 4 4S4504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$23,000 \$1,127,000
8 \$2,102,000 San Francisco 04-SF-Var. Var.	In San Francisco and Santa Clara Counties on various routes and various locations. <u>Outcome/Output:</u> Construct curb ramps and island passageways to meet the Americans with Disabilities Act (ADA) requirements.	04-0273M SHOPP/12-13 \$11,619,000 0400001152 4 4A6304	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.361	\$42,000 \$2,060,000
9 \$6,994,000 San Francisco 04-SF-280 R5.2/R6.0	In San Francisco, at Southern Freeway Viaduct Bridge No. 34-0046. <u>Outcome/Output:</u> Replace deteriorating hinges to maintain structural integrity, and reduce the risk to lives and properties.	04-0609K SHOPP/12-13 \$9,004,000 0400001138 4 4A5104	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.110	\$140,000 \$6,854,000

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(1) SHOPP Projects</b>		<b>Resolution FP-13-01</b>		
10 \$1,939,000 Santa Clara 04-SCI-9 4.2	Near Saratoga, 0.7 mile west of Sanborn Road. <u>Outcome/Output:</u> Stabilize slope, construct a tie-back retaining wall and install new drainage system damaged by heavy rainfall.	04-0392C SHOPP/12-13 \$2,780,000 0400001202 4 4S0504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$39,000 \$1,900,000
11 \$2,936,000 Solano 04-Sol-12 22.7/23.7	Near Rio Vista, at Currie, McCloskey and Azevedo Roads; also from Azevedo Road to Liberty Island Road. <u>Outcome/Output:</u> Construct left turn pockets and widen shoulders to reduce collisions and minimize collision severity involving fixed objects and provide a clear recovery zone off the traveled way.  Additional contributions: \$8,000,000 Office of Traffic Safety (OTS) federal grant.	04-8085A SHOPP/12-13 \$9,116,000 0400000832 4 2A6204	2012-13 302-0890 FTF 20.20.201.015	\$2,936,000
12 \$10,578,000 Sonoma 04-Son-12 9.6	In Sebastopol, at the Laguna de Santa Rosa Bridge (No. 20-0035). <u>Outcome/Output:</u> Replace one bridge that is rapidly deteriorating to maintain structural integrity, reduce the risk to lives and properties, and to comply with the Bridge Inspection Report recommendation.	04-0756F SHOPP/12-13 \$11,167,000 0400000482 4 1A2904	2012-13 302-0890 FTF 20.20.201.111	\$10,578,000
13 \$17,240,000 Santa Barbara 05-SB-101 22.3/23.0	In Goleta from 0.2 mile east to 0.7 mile west of the Fairview Avenue Overcrossing. <u>Outcome/Output:</u> Replace existing concrete culverts with bridges so as to increase the hydraulic flow capacity for two creeks and to minimize the possibility of future roadway flooding.	05-0707 SHOPP/12-13 \$17,169,000 0500000055 4 0G0704	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.150	\$345,000 \$16,895,000
14 \$1,077,000 Kern 06-Ker-119 Var	In Kern, Fresno, and Tulare Counties on Routes 33, 46, 63, 119, 216, and 223 at various locations. <u>Outcome/Output:</u> Extend culverts to outside highway clear recovery zone and upgrade guardrail at six different locations to reduce the frequency and severity of traffic collisions.	06-6462 SHOPP/12-13 \$2,097,000 0600000206 4 0J9304	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$22,000 \$1,055,000
15 \$319,000 Tulare 06-Tul-245 20.4	Near Badger, at Cottonwood Creek. <u>Outcome/Output:</u> Place rock slope protection at one location to stop and prevent further erosion of the embankment supporting the roadway.	06-6569 SHOPP/12-13 \$406,000 0600020698 4 0N3904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$37,000 \$282,000
16 \$2,678,000 Los Angeles 07-LA-1 8.5/8.8	In the city of Los Angeles, near Wilmington, at 0.1 mile west of the Dominguez Channel Bridge. <u>Outcome/Output:</u> Replace retaining wall system to restore storm damage.	07-4437 SHOPP/12-13 \$3,500,000 0712000064 4 3X3904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$54,000 \$2,624,000
17 \$1,500,000 Los Angeles 07-LA-5 14.9/16.8	In the city of Los Angeles, from Indiana Street to Boyle Avenue. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and inert material to reduce erosion and stabilize slopes over six acres of treatment area.	07-4060 SHOPP/12-13 \$2,033,000 0700000506 4 272404	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$30,000 \$1,470,000

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(1) SHOPP Projects</b>		<b>Resolution FP-13-01</b>		
18 \$1,000,000 Los Angeles 07-LA-5 25.2/27.0	In the city of Los Angeles, near Glendale, from 0.5 mile south of the Colorado Freeway Extension to Zoo Drive. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and other storm water quality measures to reduce erosion and stabilize slopes over 13 acres of treatment area.  (EA 27750, PPNO 4197 combined with EA 27760, PPNO 4198 and with EA 27770, PPNO 4199 for construction under EA 2777U, Project ID 0712000151)	07-4197 SHOPP/12-13 \$1,100,000 070000533 4 277504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$20,000 \$980,000
19 \$1,000,000 Los Angeles 07-LA-5 25.2/27.0	In the cities of Los Angeles and Glendale, northbound from Zoo Drive to Route 134, also westbound Route 134 from the Los Angeles River to Forest Lawn Drive. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and other storm water quality measures to reduce erosion and stabilize slopes over 13 acres of treatment area.  (EA 27760, PPNO 4198 combined with EA 27750, PPNO 4197 and with EA 27770, PPNO 4199 for construction under EA 2777U, Project ID 0712000151)	07-4198 SHOPP/12-13 \$1,200,000 070000534 4 277604	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$20,000 \$980,000
20 \$1,100,000 Los Angeles 07-LA-5 25.2/27.0	In the cities of Los Angeles and Glendale, southbound from Zoo Drive to Route 134, also eastbound Route 134 from Los Angeles River to Forest Lawn Drive. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and other storm water quality measures to reduce erosion and stabilize slopes over 13 acres of treatment area  (EA 27770, PPNO 4199 combined with EA 27750, PPNO 4197 and with EA 27760, PPNO 4198 for construction under EA 2777U, Project ID 0712000151)	07-4199 SHOPP/12-13 \$1,200,000 070000535 4 277704	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$22,000 \$1,078,000
21 \$1,928,000 Los Angeles 07-LA-5 R54.4/R54.8	In Santa Clarita, from Rye Canyon Road to Route 126 at the Castaic Truck Inspection Facility. <u>Outcome/Output:</u> Upgrade weigh station by replacing signs, CCTV, public address system, electrical equipment, weight and height gage equipment, signal lights, parking lot lights, and computer room cooling system. Upgrade plumbing, replace leaking roofs, replace damaged concrete driveway and apron slabs, and reconstruct asphalt shoulders.	07-4219 SHOPP/12-13 \$1,931,000 0700020197 4 278804	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.321	\$39,000 \$1,889,000
22 \$2,211,000 Los Angeles 07-LA-60 20.6	Near Industry and Rowland Heights, at the westbound off ramp to Nogales Street. <u>Outcome/Output:</u> Widen westbound off-ramp from two lanes to three lanes to improve traffic operations and reduce congestion.	07-3592 SHOPP/12-13 \$2,606,000 0700021079 4 4H9004	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.310	\$44,000 \$2,167,000
23 \$950,000 Los Angeles 07-LA-110 23.5/23.9	In the city of Los Angeles, at the northbound interchange with Route 101. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and other storm water quality measures to reduce erosion and stabilize slopes over 4.4 acres of treatment area.  (EA 27490, PPNO 4151 combined with EA 27590, PPNO 4172 for construction under EA 2759U, Project ID 0713000205)	07-4151 SHOPP/12-13 \$1,250,000 070000517 4 274904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$19,000 \$931,000
24 \$1,000,000 Los Angeles 07-LA-110 23.5/23.9	In the city of Los Angeles, at the southbound interchange with Route 101. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and other storm water quality measures to reduce erosion and stabilize slopes over 4.4 acres of treatment area.  (EA 27590, PPNO 4172 combined with EA 27490, PPNO 4151 for construction under EA 2759U, Project ID 0713000205)	07-4172 SHOPP/12-13 \$1,300,000 070000524 4 275904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$20,000 \$980,000

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(1) SHOPP Projects</b>		<b>Resolution FP-13-01</b>		
25 \$4,389,000 Riverside 08-Riv-111 1.5	At 1.5 miles north of the Imperial County line near the Salton Sea, at Salton Creek Bridge No. 56-0236. <u>Outcome/Output:</u> Replace bridge.	08-0086A SHOPP/12-13 \$7,398,000 0800000714 4 449104	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.112	\$88,000 \$4,301,000
26 \$486,000 San Bernardino 08-SBd-40 R18.0	Near Newberry Springs, 0.4 mile west of the National Trails Highway at Crest Wash Bridge No.54-0717L/R. <u>Outcome/Output:</u> Regrade streambed and place rock slope protection for both right and left bridges.	08-0207H SHOPP/12-13 \$489,000 0800020179 4 435414	2012-13 302-0042 SHA 20.20.201.119	\$486,000
27 \$4,860,000 Orange 12-Ora-55 17.0/R17.7	In the cities of Orange and Anaheim, at Santiago Boulevard and Nohl Ranch Road. <u>Outcome/Output:</u> Improve slope stability by removing soil from top of the slope.	12-3577A SHOPP/12-13 \$4,780,000 1200000188 4 0H2084	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$97,000 \$4,763,000

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(1) Projects Amended into the SHOPP by Department Action</b>		<b>Resolution FP-13-01</b>		
28 \$1,300,000 Placer 03-Pla-80 0.9/1.0	Near Auburn and Colfax, on Routes 49, 80, and 193 at various locations. <u>Outcome/Output:</u> Replace metal beam guardrail with concrete barrier at 9 locations to reduce repair costs and exposure to traffic of maintenance personnel.	03-4783 SHOPP/12-13 \$1,680,000 0312000283 4 3F4104	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$26,000 \$1,274,000
29 \$7,000,000 Los Angeles 07-LA-5 0.0/7.6	In La Mirada, Santa Fe Springs, Norwalk, and Downey, from Orange County line to 0.7 mile north of Route 605. <u>Outcome/Output:</u> Replace median barrier, signs, lighting, and ramp meters, and improve drainage.	07-4156 SHOPP/12-13 \$7,000,000 0700001835 4 215954	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.121	\$140,000 \$6,860,000
30 \$308,000 Los Angeles 07-LA-405 6.0/6.5	In Long Beach and Signal Hill, from Atlantic Avenue to Wardlow Road. <u>Outcome/Output:</u> Construct or upgrade pedestrian facilities at 20 locations to comply with Americans with Disabilities Act (ADA) standards.	07-4561 SHOPP/12-13 \$400,000 0700021239 4 1W3204	2012-13 302-0042 SHA 20.20.201.378	\$308,000
31 \$3,000,000 Los Angeles 07-LA-710 6.7/R15.9	At various locations in Long Beach, Compton, Paramount and Lynwood, from 0.1 mile south of Route 1 to 0.2 mile north of Route 105. <u>Outcome/Output:</u> Construct sand filters, infiltration basins and other Best Management Practices (BMPs) to remove storm water pollutants and meet permit requirements issued by the regulatory agencies.	07-3868A SHOPP/12-13 \$3,000,000 0713000045 4 259014	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$60,000 \$2,940,000
32 \$468,000 Riverside 08-Riv-74 6.9/10.2	Near Lake Elsinore, east of El Cariso Road. <u>Outcome/Output:</u> Install metal beam guardrail at steep embankment areas to enhance safety and reduce the number and severity of collisions.	08-0050K SHOPP/12-13 \$566,000 0800020127 4 0M4204	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$11,000 \$457,000

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(2) SHOPP Projects</b>				
<b>Resolution FP-13-02</b>				
1 \$1,525,000 Mendocino 01-Men-101 46.2/R84.6	Near Willits, from 0.3 mile north of Baechtel Creek Bridge to 0.1 mile south of Cummings Road. <u>Outcome/Output:</u> Rehabilitate drainage system components at 33 locations that have reached the end of their useful lives to reduce maintenance costs and personnel exposure to traffic.	01-0181A SHOPP/13-14 \$3,175,000 0100000156 4 402804	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.151	\$30,000 \$1,495,000
2 \$27,000,000 Shasta 02-Sha-299 30.3/40.7	Near Bella Vista, from 0.3 mile east of Intermountain Road to 0.3 mile west of Backbone Ridge Road. <u>Outcome/Output:</u> Rehabilitate 22.1 lane miles of roadway to improve the ride quality, prevent further deterioration of the road surface, minimize the costly roadway repairs and extend the pavement service life.	02-3017 SHOPP/13-14 \$27,000,000 0200000262 4 360704	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.120	\$540,000 \$26,460,000
3 \$1,250,000 El Dorado 03-ED-49 31.1	Near Auburn, at the Rattlesnake Bar Road Intersection. <u>Outcome/Output:</u> Construct left-turn lanes to reduce collisions and improve safety to allow through traffic to pass left-turn vehicles.	03-3118 SHOPP/13-14 \$1,250,000 0300020538 4 2F1504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.010	\$25,000 \$1,225,000
4 \$990,000 San Mateo 04-SM-35 13.1	Near Skylonda, at north Bear Gulch Road. <u>Outcome/Output:</u> Stabilize embankment and construct soldier pile wall damaged by heavy rainfall.	04-0636R SHOPP/13-14 \$1,158,000 0412000015 4 3G0204	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$20,000 \$970,000
5 \$1,713,000 Kern 06-Ker-5 10.4/R15.8	Near Grapevine, from 0.2 mile north of Grapevine Undercrossing to 0.3 mile north of Route 99 . <u>Outcome/Output:</u> Repair damaged pavement by replacing broken slabs to maintain traffic operation and safety.	06-6571 SHOPP/13-14 \$1,764,000 0600020697 4 0N3604	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$34,000 \$1,679,000
6 \$4,792,000 Riverside 08-Riv-10 R57.6/R60.9	In and near Coachella, from Route 86S to 0.5 mile east of the Coachella Canal. <u>Outcome/Output:</u> Provide preventative rehabilitation treatments to 13.2 lane miles and construct two ramp terminal sections. Project will extend pavement service life and ride quality.	08-0015K SHOPP/13-14 \$6,778,000 0800000337 4 0K2304	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.121	\$96,000 \$4,696,000
7 \$668,000 San Bernardino 08-SBd-18 53.5	Near Big Bear City, at Big Tree Drive. <u>Outcome/Output:</u> Improve safety by constructing left-turn lanes in both directions and widen westbound shoulder to reduce the number and severity of traffic collisions.	08-0188C SHOPP/13-14 \$803,000 0800000278 4 0J0104	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.010	\$26,000 \$642,000
8 \$1,178,000 San Joaquin 10-SJ-12 R4.4	Near Terminous, at Little Potato Slough Bridge, and near Manteca, at the westbound Route 120 to northbound Route 5 Overhead Connector. <u>Outcome/Output:</u> Replace bridge bearing pads and joint seals to maintain bridge operation and extend bridge service life.	10-7364 SHOPP/13-14 \$2,745,000 1000000043 4 0G3504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.110	\$24,000 \$1,154,000
9 \$676,000 San Diego 11-SD-94 32.8/40.3	Near Barrett Junction, at various locations from 1.0 mile west of Cochera via Drive to 1.0 mile west of Potrero Valley Road. <u>Outcome/Output:</u> Enhance safety by reconstructing guardrail and upgrading end treatments to reduce the number and severity of collisions.	11-0659 SHOPP/13-14 \$1,351,000 1100000346 4 287904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$14,000 \$662,000

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(2) Projects Amended into the SHOPP by Department Action</b>				
<b>Resolution FP-13-02</b>				
10 \$7,000,000 Fresno 06-Fre-Var Var	In Fresno and Madera Counties, on Routes 5, 41, 99, and 145 at various locations. <u>Outcome/Output:</u> Repair electrical systems damaged by theft and vandalism to restore traffic operations including highway lighting, traffic signals, pumping plants and irrigation.	06-6664 SHOPP/13-14 \$7,000,000 0613000265 4 0Q5404	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$140,000 \$6,860,000
11 \$7,000,000 Fresno 06-Fre-Var Var	In Fresno County, on Routes 168 and 180 at various locations. <u>Outcome/Output:</u> Repair electrical systems damaged by theft and vandalism to restore traffic operations including highway lighting, traffic signals, pumping plants and irrigation.	06-6663 SHOPP/13-14 \$7,000,000 0613000266 4 0Q5504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$140,000 \$6,860,000

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(3a) Multi-Funded Projects Funded with SHOPP and Proposition 1B TCIF</b>				
<b>Resolution FP-13-03 Resolution TCIF-A-1314-01</b>				
1 \$73,433,000 Alameda 04-Ala-880 28.4/29.2	<b>I-880 Reconstruction – 29<sup>th</sup> and 23<sup>rd</sup> Avenues Overcrossing.</b> In Oakland, from 0.2 mile south of 29 <sup>th</sup> Avenue Overcrossing to 0.3 mile north of 23 <sup>rd</sup> Avenue Overcrossing. <u>Outcome/Output:</u> Reconstruct Overcrossings, improve on and off ramps, and construct sound walls to improve traffic flow during peak hours and enhance safety. (TCIF Project 4)  (The TCIF/SHOPP allocation is split as follows: \$10,867,000 for construction engineering and \$62,133,000 for construction capital. The SHOPP allocation for construction engineering is \$433,000.)  (SHOPP funded Trade Corridor Improvement Fund (TCIF) project.)	04-0044C SHOPP/12-13 CON ENG \$11,300,000 CONST \$62,133,000 0400000160 4 0A7104	001-0042 SHA 2012-13 302-0042 SHA 302-0890 FTF 20.20.201.310	\$11,300,000 \$1,243,000 \$60,890,000

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(3b) Multi-Funded Projects Funded with SHOPP and Proposition 1B TCIF</b>				
<b>Resolution FP-13-04 Resolution TCIF-A-1314-02</b>				
1 \$15,000,000 Santa Clara 04-SCI-101 26.7	<b>Santa Clara – US 101 Freeway Performance Initiative Project.</b> Near Edenvale, from San Benito County line to Route 85. <u>Outcome/Output:</u> Install ramp metering and traffic operation system to minimize gridlock of the highway system, decrease travel time and improve mobility. (TCIF Project 94)  (SHOPP funded Trade Corridor Improvement Fund (TCIF) project.)	04-0449R SHOPP/12-13 \$18,349,000 0400020304 4 153304	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.315	\$300,000 \$14,700,000

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID AdvPhase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(4) SHOPP Projects (ADVANCEMENTS) Resolution FP-13-13</b>				
1 \$1,369,000 Los Angeles 07-LA-210 R6.8/R7.2	In the city of Los Angeles, from 0.2 mile east of Van Nuys Boulevard to 0.1 mile west of Terra Bella Street. <u>Outcome/Output:</u> Enhance safety by replacing existing metal beam guardrail at eastbound shoulder with concrete barrier to reduce the number and severity of collisions.	07-4495 SHOPP/14-15 \$1,500,000 0712000067 4 290904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$27,000 \$1,342,000

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(5a) Allocation Amendment – Multi-Funded Projects with SHOPP and Proposition 1B TCIF Resolution FP-13-14, Amending Resolution FP-10-33 Resolution TCIF-AA-1314-02</b>				
1 <del>\$42,300,000</del> <b>\$32,792,000</b> Solano 04N-Sol-80 13.3/15.7	<b>I-80 Eastbound Cordelia Truck Scales Relocation.</b> Near Fairfield, at the EB Cordelia Truck Scale. <u>Outcome/Output:</u> Relocate and expand truck scale facility and relocate and realign ramps to improve CHP truck inspection operation and improve freeway efficiency and safety for vehicular traffic. (TCIF Project 12)  (SHOPP funded Trade Corridor Improvement Fund (TCIF) project.)  (Contributions by others \$14,900,000)  <b><u>Amend Resolution FP-10-33 to de-allocate \$9,508,000 in SHOPP/TCIF CONST.</u></b>	04-5301R SHOPP/11-12 <del>\$49,800,000</del> <b>\$32,792,000</b> 0400000153 4 0A5354	2009-10 302-0890 FTF 20.20.201.321	<del>\$42,300,000</del> <b>\$32,792,000</b>

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(5b) Allocation Amendment – Multi-Funded Projects funded with SHOPP and Proposition 1B TCIF Resolution FP-13-15, Amending Resolution FP-10-33 Resolution TCIF-AA-1314-03</b>				
1 <del>\$48,959,000</del> <b>\$41,750,000</b> Alameda 04N-Ala-580 4.7/8.2	<b>I-580 Eastbound Truck Climbing Lane.</b> In Livermore, from North Flynn road to Greenville Road. <u>Outcome/Output:</u> Construct truck climbing lane and rehabilitate pavement to enhance the movement of goods, improve freeway safety and operations, and to relieve traffic congestion. (TCIF Project 5)  (SHOPP funded Trade Corridor Improvement Fund (TCIF) project.)  <b><u>Amend Resolution FP-10-33 to de-allocate \$7,209,000 in SHOPP/TCIF CONST.</u></b>	04-0104 SHOPP/10-11 <del>\$63,000,000</del> <b>\$41,750,000</b> <del>0400001403</del> <b>0400020643</b> 4 4A9704 4A07U4	2009-10 302-0042 SHA 302-0890 FTF 20.20.201.310	<del>\$979,000</del> <b>\$835,000</b> <del>\$47,980,000</del> <b>\$40,915,000</b>

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> County Dist-Co-Rte Postmile	Project Title Location Project Description Project Support Expenditures	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(1) State Administered STIP Projects on the State Highway System</b>				<b>Resolution FP-13-06</b>
1 \$14,900,000 Department of Transportation <u>DNLTC</u> Del Norte 01-DN-199 20.5/25.7	<p>Realignment &amp; Widening at Patrick Creek Narrows. Near Gasquet, on Route 199 at Patrick Creek. Shoulder widening and bridge Replacement.</p> <p>Final Project Development (RIP) Support Estimate: \$4,080,000 Programmed Amount: <u>\$3,512,000</u> Adjustment: \$ 0 (&lt; 20%)</p> <p>Final Right of Way (RIP) Right of Way Estimate: \$ 350,000 Programmed Amount: <u>\$1,346,000</u> Adjustment: \$ 996,000 (Credit)</p> <p>(RIP CON ENG increase because of \$334,000 to come from Del Norte regional shares balance.)</p> <p>(Future Consideration of Funding under Resolution E-13-46; June 2013.)</p> <p>(Time extension for FY 12-13 CON expires on August 31, 2013.)</p> <p>(Contributions from other sources: \$648,000.)</p> <p><u>Outcome/Output:</u> Roadway will be upgraded to STAA Route to accommodate the larger trucks traveling from the Oregon border to Route 101.</p>	<p>01-1047 RIP / 12-13 CON ENG <del>\$1,566,000</del> <b>\$1,900,000</b> CONST \$13,000,000 0100000371 4 479404</p>	<p>001-0042 SHA 001-0890 FTF 2012-13 301-0042 SHA 301-0890 FTF 20.20.075.600</p>	<p>\$38,000 \$1,862,000 \$260,000 \$12,740,000</p>
2 \$1,018,000 Department of Transportation <u>SBCAG</u> Santa Barbara 05-SB-101 83.2/83.8	<p>Union Valley Parkway Interchange Planting. In Santa Maria, from 0.9 mile north of Clark Avenue to 0.7 mile south of Santa Maria Way. Install mitigation landscaping.</p> <p>Final Project Development Support Estimate: \$341,000 Programmed Amount: <u>\$305,000</u> Adjustment: \$0 (&lt; 20%)</p> <p>Final Right of Way Right of Way Estimate: \$5,000 Programmed Amount: <u>\$5,000</u> Adjustment: \$0 (&lt; 20%)</p> <p>(CONST savings of \$32,000 to return to Santa Barbara County regional shares.)</p> <p>(Future Consideration of Funding under Resolution E-12-12; March 2012.)</p> <p><u>Outcome/Output:</u> Install 2.9 acres of mitigation planting.</p>	<p>05-4638Y RIP/13-14 CON ENG \$400,000 CONST <del>\$650,000</del> <b>\$618,000</b> 0512000105 4 46381</p>	<p>001-0042 SHA 2012-13 301-0042 SHA 20.20.075.600</p>	<p>\$400,000 \$618,000</p>

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC County Dist-Co-Rte Postmile	Project Title Location Project Description Project Support Expenditures	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(1) State Administered STIP Projects on the State Highway System</b>		<b>Resolution FP-13-06</b>		
3 \$14,017,000	12th Avenue Interchange on SR 198. In Hanford at 12th Avenue. Reconstruct interchange.	06-4348 RIP/13-14 CON ENG	2012-13 101-0042 SHA	\$46,000
Department of Transportation KCAAG Kings 06-Kin-198 R16.5/R17.2	Final Project Development Support Estimate: \$4,799,000 Programmed Amount: \$3,715,000 Adjustment: \$1,084,000 (Debit)	\$2,279,000 CONST <del>\$15,491,000</del> <b>\$11,738,000</b> 0600000488	001-0890 FTF 2012-13 301-0042 SHA	\$2,233,000
	Final Right of Way Right of Way Estimate: \$2,297,000 Programmed Amount: \$1,608,000 Adjustment: \$ 689,000 (Debit)	4 487504	301-0042 SHA 301-0890 FTF 20.20.075.600	\$235,000 \$11,503,000
<p>(CONST saving for \$3,753,000 to return to Kings County regional shares.)</p> <p>(Future Consideration of Funding under Resolution E-10-22; April 2010.)</p> <p><u>Outcome/Output:</u> Reconstruct interchange to increase capacity, reduce congestion, and improve safety.</p>				

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(3a) Locally Administered STIP Projects off the State Highway System</b>		<b>Resolution FP-13-07</b>		
1 \$10,000	Construction of Traffic Signal System at Fern Street and Walnut Drive. In Cutten, at Walnut Drive and Fern Street. Construct traffic signal system.	01-2258 RIP/13-14 PA&ED \$10,000	2012-13 101-0042 SHA 20.30.600.620	\$10,000
County of Humboldt HCAOG 01-Humboldt	<u>Outcome/Output:</u> Improve safety to motorist, pedestrians, and bicyclist who travel through the intersection from nearby schools, parks, and residential areas.	0100020175		
2 \$225,000	Route A15 Reconstruction-Phase II. In Portola, on Route A15 from Colorado Street to Commercial Street.  (Future Consideration of Funding – Resolution E-13-48, April 2013.)	02-2480 RIP/13-14 PS&E \$148,000 R/W \$77,000	2012-13 101-0042 SHA 20.30.600.621	\$225,000
City of Portola Plumas CTC 02-Plumas	<u>Outcome/Output:</u> Reconstruct 0.6 existing lane mile according to an approved rehabilitation plan.	0200020123		
3 \$2,050,000	Alma Street Rehabilitation. In Mount Shasta City, from Chestnut Street to Rockfellow Drive. Remove failed concrete and asphalt roadway, and replace with new asphalt roadway.	02-2453 RIP/13-14 CONST \$2,050,000	2012-13 101-0042 SHA 20.30.600.621	\$2,050,000
City of Mount Shasta SCLTC 02-Siskiyou	<u>Outcome/Output:</u> Rehabilitate to extend the useful life of the roadway by 10-15 years and improve vehicular pedestrian and bicyclist safety.	0200000417		
4 \$501,000	Town of Fort Jones Roadway Rehabilitation. In Fort Jones, on Fort Jones, on Marble View Avenue, Oak Mill Drive, Diggles Street, Douglas Street and Hamilton Street. Rehabilitate roadway.	02-2454 RIP/13-14 CONST \$501,000	2012-13 101-0042 SHA 20.30.600.621	\$501,000
Town of Fort Jones SCLTC 02-Siskiyou	<u>Outcome/Output:</u> Construct and rehabilitate failed and deteriorated sections of roadway and help prevent most costly full reconstruction later. Extend the useful lives of the facilities by at least 10 years and improve vehicular safety.	0200000422		

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(3a) Locally Administered STIP Projects off the State Highway System</b>				<b>Resolution FP-13-07</b>
5 \$600,000 City of Tulelake SCLTC 02-Siskiyou	Tulelake Street Rehabilitation. In Tulelake, on F Street from 4th Street to Main Street and Modoc Street and from A Street to 1st Street. Rehabilitate roadway.  (Construction increase of \$200,000 to come from Siskiyou County unprogrammed share balance.)  <u>Outcome/Output:</u> Rehabilitate approximately 4,000 linear feet of roadway using a one-inch leveling course with geotextile fabric and two-inch AC overlay. Extend the life of the facility by an expected 10 or more years.	02-2471 RIP/13-14 CONST <del>\$400,000</del> <b>\$600,000</b> 0200000423	2012-13 101-0042 SHA 20.30.600.621	\$600,000
6 \$1,200,000 City of Weed SCLTC 02-Siskiyou	Black Butte Drive and Vista Drive Rehabilitation. In Weed, on Black Butte Drive between Shastina Drive and Vista Drive, and on Vista Drive between Shastina Drive and Black Butte Drive. Rehabilitate roadway.  (Future Consideration of Funding – Resolution E-09-82, August 2008.)  <u>Outcome/Output:</u> Increase turn lane storage from frontage road and rehabilitate failed and cracked roadways.	02-2448 RIP/13-14 CONST \$1,200,000 0200000426	2012-13 101-0042 SHA 20.30.600.621	\$1,200,000
7 \$60,000 City of Yreka SCLTC 02-Siskiyou	Foothill Drive Project. In Yreka, on Foothill Drive from Kleaver Street easterly to city limit. Rehabilitate existing pavement, and install new asphalt pavement surface.  <u>Outcome/Output:</u> This project will reconstruct failed pavement structure and rehabilitate roadways to improve vehicular safety and extend roadway life by at least 10 years. This project will also extend bike lanes in accordance with the City of Yreka Bicycle Transportation Plan to improve bicycle safety along foothill drive.	02-2452 RIP/13-14 PS&E \$60,000 0213000109	2012-13 101-0042 SHA 20.30.600.621	\$60,000
8 \$122,000 Inyo County Inyo LTC 09-Inyo	West Bishop Roadway Reconstruction. In West Bishop, on various residential streets. Reconstruct roadways.  (SB 184 Submittal effective July 1, 2013.)  <u>Outcome/Output:</u> Provide greater motorist safety. Extend the life of 3 miles of existing roadway.	09-2599 RIP/13-14 PS&E \$122,000 0913000022	2012-13 101-0042 SHA 20.30.600.621	\$122,000

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(3a) Locally Administered STIP Transportation Enhancement Projects off the State Highway System</b>				<b>Resolution FP-13-07</b>
9 \$6,963,000 City of Sacramento SACOG 03-Sacramento	Sacramento City College Pedestrian Overcrossing. In the City of Sacramento, between the Sacramento City College RT light rail station and the Curtis Park Village development: construct a bicycle and pedestrian overcrossing over the railroad tracks.  (Future Consideration of Funding – Resolution E-13-49, July 2011.)  <u>Outcome/Output:</u> Construction of pedestrian and bicycle overcrossing to facilitate access to light rail station from neighborhoods currently restricted by railroad barrier and to allow light rail passengers to access neighborhoods to the east.	03-6577 RIP TE/13-14 CONST \$6,963,000 0300020206	2012-13 101-0890 FTF 20.30.600.731	\$6,963,000

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Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(3a) Locally Administered STIP Transportation Enhancement Projects off the State Highway System</b>				<b>Resolution FP-13-07</b>
10 \$329,000 County of Marin MTC 04-Marin	Sir Francis Drake Blvd Westbound Class II Bike Lane. Near the town of Fairfax in Marin County. Reconstruct and/or widen westbound shoulder on Sir Francis Drake Blvd from intersection of Baywood Canyon to top of White's Hill Road.  <u>Outcome/Output:</u> Increased bicycle use on Sir Francis Drake Blvd.	04-2127Q (Marin) RIP TE/13-14 CONST \$35,000  (Solano) RIP TE/13-14 CONST \$294,000 0400021116	2012-13 101-0890 FTF 20.30.600.731	\$329,000
11 \$50,000 Inyo County Inyo LTC 09-Inyo	Ed Powers Bike Lanes. Near West Bishop, on Ed Powers Road, from Route 168 to Route 395. Construct Class II bike lanes.  <u>Outcome/Output:</u> Provide a safer and less-congested alternative bicycle route along 2.4 miles of roadway.	09-2598 RIP TE/13-14 PS&E \$50,000 0913000021	2012-13 101-0890 FTF 20.30.600.731	\$50,000

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(3a) Local STIP Planning, Programming and Monitoring Projects</b>				<b>Resolution FP-13-07</b>
12 \$150,000 Humboldt County Association of Governments HCAOG 01-Humboldt	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	01-2002P RIP/13-14 CONST \$150,000 0113000131	2012-13 101-0042 SHA 20.30.600.670	\$150,000
13 \$64,000 Lake County/City Area Planning Council Lake CCAPC 01-Lake	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	01-3002P RIP/13-14 CONST \$64,000 0100020431	2012-13 101-0042 SHA 20.30.600.670	\$64,000
14 \$140,000 Mendocino Council of Governments MCOG 01-Mendocino	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	01-4002P RIP/13-14 CONST \$140,000 0113000129	2012-13 101-0042 SHA 20.30.600.670	\$140,000
15 \$81,000 Nevada County Transportation Commission Nevada CTC 03-Nevada	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	03-0L83 RIP/13-14 CONST \$81,000 0313000289	2012-13 101-0042 SHA 20.30.600.670	\$81,000
16 \$750,000 Alameda County Transportation Commission ACTC 04-Alameda	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	04-2179 RIP/13-14 CONST \$750,000 0413000390	2012-13 101-0042 SHA 20.30.600.670	\$750,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(3a) Local STIP Planning, Programming and Monitoring Projects</b>		<b>Resolution FP-13-07</b>		
17 \$259,000 Transportation Agency for Monterey County TAMC 05-Monterey	Planning, Programming and Monitoring	05-1165 RIP/13-14 CONST \$259,000 0513000170	2012-13 101-0042 SHA 20.30.600.670	\$259,000
18 \$350,000 Santa Barbara County Association of Governments SBCAG 05-Santa Barbara	Planning, Programming and Monitoring	05-1914 RIP/13-14 CONST \$350,000 0513000172	2012-13 101-0042 SHA 20.30.600.670	\$350,000
19 \$413,000 Ventura County Transportation Commission VCTC 07-Ventura	Planning, Programming and Monitoring	07-9002 RIP/13-14 CONST \$413,000 0713000465	2012-13 101-0042 SHA 20.30.600.670	\$ 413,000
20 \$1,200,000 San Bernardino Associated Governments SANBAG 08-San Bernardino	Planning, Programming and Monitoring (SB 184 Submittal effective July 1, 2013.)	08-9811 RIP/13-14 CONST \$1,200,000 0813000219	2012-13 101-0042 SHA 20.30.600.670	\$1,200,000
21 \$54,000 Calaveras Council of Governments Calaveras COG 10-Calaveras	Planning, Programming and Monitoring	10-C1950 RIP/13-14 CONST \$54,000 1013000230	2012-13 101-0042 SHA 20.30.600.670	\$54,000
22 \$200,000 San Joaquin Council of Governments SJCOG 10-San Joaquin	Planning, Programming and Monitoring	10-7952 RIP/13-14 CONST \$200,000 1013000233	2012-13 101-0042 SHA 20.30.600.670	\$200,000
23 \$300,000 Imperial County Transportation Commission ICTC 11-Imperial	Planning, Programming and Monitoring	11-7200 RIP/13-14 CONST \$300,000 1113000166	2012-13 101-0042 SHA 20.30.600.670	\$300,000
24 \$854,000 San Diego Association of Governments SANDAG 11-San Diego	Planning, Programming and Monitoring (SB 184 Submittal effective July 1, 2013.)	11-7402 RIP/13-14 CONST \$854,000 1113000174	2012-13 101-0042 SHA 20.30.600.670	\$854,000



2.5 Highway Financial Matters

Project #	Allocation Amount	Recipient	Project Title	PPNO	Budget Year	Amount by
RTPA/CTC	Location	Phase	Item #	Fund Type	Fund Type	Fund Type
District-County	Project Description	Prgm'd Amount	Program Code	Program Code	Program Code	Fund Type
<b>2.5c.(4) Locally Administered STIP Transportation Enhancement Projects off the State Highway System (ADVANCEMENTS)</b>						
<b>Resolution FP-13-__</b>						
1	\$1,897,000	City of Clovis FCOG 06-Fresno	Sunnyside/Shepherd Trail Head Rest Area. In Clovis, at the southwest corner of Sunnyside Avenue and Shepherd Avenues. Construct a trail head/rest area.  (Future Consideration of Funding – Resolution E-13-26, May 2013.)  (Funded from Fresno County FY 2015-16 TE Reserve PPNO B002.)  <u>Outcome/Output:</u> This project is designed as a streetscape improvement benefiting bicyclists and pedestrians.	06-B002S RIP TE/15-16 CONST <b>\$1,897,000</b> 0613000156	2012-13 101-0890 FTF 20.30.600.731	\$1,897,000
<b>THE DEPARTMENT RECOMMENDS THIS ITEM BE DEFERRED AT THIS TIME.</b>						
2	\$571,000	City of Long Beach LACMTA 07-Los Angeles	Downtown Long Beach Pine Avenue Streetscape Improvements. In Long Beach on Pine Avenue between <del>Seaside Way and Anaheim Street. Shoreline Drive and 8th Street, 3rd Street between Pacific Avenue and Long Beach Boulevard, Broadway between Pacific Avenue and Long Beach Boulevard, and 1st Street between Pacific Avenue and Elm Avenue.</del> Streetscape improvements.  (CONST allocation funded from FY 2015-16 TE Reserve PPNO B002.)  (RIP TE Construction savings of \$2,318,000 to be returned to Los Angeles County regional share balance.)  (Contributions from other sources: \$3,700,000.)  <u>Outcome/Output:</u> The improvements will include pedestrian lighting, crosswalk enhancements, diagonal crosswalks, street furniture, bike racks, street trees, landscaping and bollards to facilitate street closure for community events. It also includes removal of obstructions from the walkway to improve pedestrian mobility. The project will support local and Metro transit stations, employment areas, business districts, and major activity nodes.	07-4542 RIP TE/15-16 CONST <del>\$2,889,000</del> <b>\$571,000</b> 0713000405	2012-13 101-0890 FTF 20.30.600.731	\$571,000
<b>THE DEPARTMENT RECOMMENDS THIS ITEM BE DEFERRED AT THIS TIME.</b>						

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC County Dist-Co-Rte Postmile	Project Title Location Project Description Project Support Expenditures	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(5) Local Alternative Transportation Improvement Program Projects on the State Highway System Resolution FP-13-11</b>				
1 \$8,100,000 City of Hayward MTC Alameda 04-Ala-238 9.31/14	Route 238 Corridor Improvements. In Hayward, on Route 238 from Industrial Parkway to the I-580 ramp near Apple Avenue. Construct street improvements including pavement, curb, gutter, sidewalk, medians, streetlights, signals and utilities.  (This project was included in the State Route 238 Local Alternative Transportation Improvement Program [LATIP]) approved by the Commission in May 2010.)  (Future Consideration of Funding – Resolution E-10-41, May 2010.)  (Agency has accrued expenditures in excess of \$8,100,000.)  <u>Outcome/Output:</u> Construct various street improvements.	04-0095E XXXX / 12-13 CONST \$8,100,000 0400000427 4CONL 155312	2010-11 501-0942 SHA 20.20.XXX.XXX	\$8,100,000
<b>ALLOCATION IS CONTINGENT ON AVAILABILITY OF SUFFICIENT FUNDS</b>				

Project # Allocation Amount Recipient County Dist-Co-Rte Postmile	Location Project Description Reason for Supplemental Funds	PPNO Program Funding Year Item # Fund Type Program Codes Project ID Adv Phase EA	State Federal Current Amount by Fund Type	State Federal Additional Amount by Fund Type	State Federal Revised Amount by Fund Type
<b>2.5e.(2) Supplemental Funds for Previously Voted Projects Resolution FA-13-04</b>					
1 \$260,000 Department of Transportation Imperial 11-Imp-111 44.7	Near Calipatria at the "Z" Drain Bridge (Bridge No. 58-0153). A heavy rainstorm on July 13, 2012 caused severe erosion and undermining of the supporting bridge piles. Temporary falsework is necessary to stabilize the bridge and water has to be diverted away from the bridge. <u>Outcome/Outputs:</u> This project is to build two box culverts with wing walls on both the inlet and outlet sides and place rock slope protection on the upstream and downstream sides of the new culverts.  Supplemental funds needed to close-out the contract.  Total Revised Amount: \$1,660,000.	11-0527 SHOPP 2012-13 302-0042 SHA 20.20.201.130 1113000031 4 415804 20.20.201.130	\$1,400,000	\$260,000	\$1,660,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient County Dist-Co-Rte Postmile	Location Project Description Reason for Supplemental Funds	PPNO Program Funding Year Item # Fund Type Program Codes Project ID Adv Phase EA	State Federal Current Amount by Fund Type	State Federal Additional Amount by Fund Type	State Federal Revised Amount by Fund Type
<b>2.5e.(3) Supplemental Funds for Previously Voted Projects</b>					<b>Resolution FA-13-05</b>
1 \$50,000 Department of Transportation San Diego 11-SD-78 2.3	Near Oceanside, at 0.8 mile east of El Camino Real. On March 16, 2012, a sinkhole occurred in the median adjacent to the Number 1 lane. The sinkhole was filled with slurry to stabilize the travel lane. Subsequent close circuit video and ground penetrating radar survey of a metal drainage pipe at this location revealed that due to severe corrosion of the pipe, the slurry inadvertently flowed into the pipe reducing its capacity by 60 percent. <u>Outcome/Outputs:</u> This project is to excavate the old metal culvert pipe and replace it with a new reinforced concrete pipe (RCP).  Supplemental funds needed close-out the contract.  Total Revised Amount: \$500,000	11-1082 SHOPP 2012-13 302-0042 SHA 20.20.201.130 1113000028 4 415904 20.20.201.130	\$450,000	\$50,000	\$500,000

Project # Allocation Amount Recipient RTPA/CTC Dist-Co-Rte Postmile	Project Title Location Project Description	PPNO Program/ Budget Item # Fund Type Program Codes Project ID Adv Phase EA	State Federal Current Amount by Fund Type	State Federal Additional Amount by Fund Type	State Federal Revised Amount by Fund Type
<b>2.5e.(4) Supplemental Funds for Previously Voted Projects</b>					<b>Resolution FA-13-06</b>
1 \$2,796,000 Department of Transportation MTC Alameda/Contra Costa 04N-Ala/CC-24 Ala 5.3/ CC 1.3	On Route 24 in Alameda and Contra Costa Counties. Route 24/Caldecott Tunnel Fourth Bore (Segment 1) – Construct 2-lane fourth bore north of existing bores.  <u>Outcome/Outputs:</u> When combined with other segments (PPNO 0057G and 0057I), the overall Route 24/Caldecott Tunnel Fourth Bore project will result in daily vehicle hours of delay savings of about 10,368 hours.  Supplemental Funds needed to complete construction engineering activities.  Contribution from other sources: \$45,815,000 (\$17,600,000 [ARRA-Regional], \$28,215,000 [Local funds]).  Total Revised Amount for construction engineering: \$51,311,000.	04-0057A IIP 001-0042 SHA 501-0890 ARRA 0400002022 3 264134	\$2,700,000  \$0	\$0 \$2,796,000	\$2,700,000  \$2,796,000

2.5 Highway Financial Matters

Project# Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv. Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5f. Informational Report – Emergency G-11 Allocations (2.5f.(1))</b>				
1 \$300,000 Yolo 03-Yol-50 2.6/3.2	In West Sacramento, at the Sacramento River Viaduct (Bridge No. 24-0004R). Inspection of the structure detected potential defects in the pin and hanger assemblies in two locations at Span 12 and Span 16. This project is necessary to install temporary shoring to stabilize the bridge at the two locations. A follow-up project will be requested to make permanent repairs.  Initial G-11 Allocation 06/11/13: \$300,000	03-8783 SHOPP/12-13 0313000296 4 3F9704  Emergency	2011-12 302-0042 SHA 20.20.201.130	\$300,000
2 \$400,000 Los Angeles 07-LA-1 8.3	In Long Beach near the Hobson Avenue Overhead. On May 5, 2013, a sinkhole was discovered in the Number 1 lane of the Pacific Coast Highway (PCH) prompting the closure of the lane for further investigation and repair. An opening in the asphalt concrete was found to be the cause of soil erosion and the massive void under the pavement. This project is to provide traffic control as necessary, and excavate, shore, refill, compact, and re-pave the sinkhole area.  Initial G-11 Allocation 05/10/13: \$400,000	07-4641 SHOPP/12-13 0713000420 4 4X0704  Emergency	2011-12 302-0042 SHA 20.20.201.130	\$400,000
3 \$2,500,000 Ventura 07-Ven-1 2.6/10.2	Near Camarillo, from Deer Creek Road to Las Posas Road. The Camarillo Springs wildfire started on May 2, 2013. The wildfire burned and damaged vegetation, roadway signs and highway fencing. This project is to place guardrail to protect the roadway from post-fire falling rocks and debris flows, protect drainage system, replace damaged roadway signs, replace damaged highway fencing, and repair wire mesh and cable anchored covered hillside.  Initial G-11 Allocation 05/21/13: \$2,500,000	07-4640 SHOPP/12-13 0713000419 4 4X0604  Emergency	2011-12 302-0042 SHA 20.20.201.130	\$2,500,000

Project # Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Prgm'd Amount Project ID Adv. Phase EA	Budget Year Item # Fund Type Program Codes	Amount by Fund Type
<b>Informational Report – SHOPP Safety-Resolution G-03-10 Delegated Allocations (2.5f.(3))</b>				
1 \$910,000 Humboldt 01-Hum-101 86.3/87.9	Near Arcata, from the 11 <sup>th</sup> Street Overcrossing to the Arcata Overhead. <u>Outcome/Output:</u> Install cable median barrier to reduce the frequency of cross median collisions within the project limits and improve safety.  Allocation date: 05/29/2013	01-2330 SHOPP/12-13 \$980,000 0112000009 4 0B1004	2011-12 302-0890 FTF 20.20.201.010	\$910,000
2 \$813,000 El Dorado 03-ED-193 18.6/18.8	Near Georgetown, at Chicken Flat Road. <u>Outcome/Output:</u> Realign compound curve, increase roadway super-elevation, construct paved shoulders and clear vegetation to reduce run-off the road collisions.  Allocation date: 06/12/2013	03-3626 SHOPP/12-13 \$1,625,000 0300001113 4 1F3304	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	\$16,000 \$797,000
3 \$6,145,000 Santa Clara 04-SCI-9 2.5/7.0	Near Saratoga, from 2.5 miles north of Route 35 to 6 <sup>th</sup> Street. <u>Outcome/Output:</u> Upgrade lanes and shoulders, improve superelevation to improve roadway geometrics, increase sight distance and increase clear recovery zone to reduce the number and severity of cross centerline collisions.  Allocation date: 07/10/2013	04-0385F SHOPP/12-13 \$8,746,000 0400000822 4 2A4304	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	\$123,000 \$6,022,000

2.5 Highway Financial Matters

Project # Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Prgm'd Amount Project ID Adv. Phase EA	Budget Year Item # Fund Type Program Codes	Amount by Fund Type
<b>Informational Report – SHOPP Safety-Resolution G-03-10 Delegated Allocations (2.5f.(3))</b>				
4 \$18,394,000 Santa Clara 04-SCI-152 0.0/5.2	In Santa Clara, from Hecker pass to Uvas Creek. <u>Outcome/Output:</u> Upgrade lanes and shoulders, overlay pavement, remove trees, construct retaining walls, and add left-turn lane to reduce the number and severity of cross centerline collisions.  Allocation date: 07/10/2013	04-0483J SHOPP/12-13 \$24,826,000 0400000813 4 2A2504	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	\$368,000 \$18,026,000
5 \$3,584,000 Santa Barbara 05-SB-154 R7.8/R8.3	Near Santa Ynez, at the intersection with State Route 246. <u>Outcome/Output:</u> Construct rural roundabout to reduce the frequency and severity of traffic collisions.  Allocation date: 06/13/2013	05-2267 SHOPP/12-13 \$3,421,000 0500000471 4 0T0004	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	\$72,000 \$3,512,000
6 \$992,000 Los Angeles 07-LA-105 R0.5	In the city of Los Angeles, near LAX and El Segundo, on the eastbound on-ramp from southbound Route 1. <u>Outcome/Output:</u> Safety improvements to address the number and severity of wet pavement collisions.  Allocation date: 07/10/2013	07-4508 SHOPP/12-13 \$985,000 0712000121 4 4T5704	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.010	\$20,000 \$972,000
7 \$620,000 San Bernardino 08-SBd-38 5.2/5.5	Near Redlands and Mentone, from 0.2 mile east of Amethyst Street to Mountain View Lane. <u>Outcome/Output:</u> Improve safety by constructing left-turn lane to address the number and severity of collisions  Allocation date: 06/21/2013	08-0204V SHOPP/12-13 \$765,000 0800000481 4 0M4504	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	\$12,000 \$608,000
8 \$2,964,000 San Joaquin 10-SJ-26 18.5/19.0	Near Linden, from west of Sandstone Creek Bridge to Shelly Road. <u>Outcome/Output:</u> Realign two horizontal roadway curves and replace the Sandstone Creek bridge to reduce the frequency and severity of traffic collisions.  Allocation date: 05/22/2013	10-0264 SHOPP/12-13 \$2,789,000 1000000271 4 0T1604	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	\$59,000 \$2,905,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC County Dist-Co-Rte Postmile	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Item # Fund Type	Amount by Fund Type
<b>2.5g.(2) Allocation Amendment - Proposition 1B –Locally Administered Route 99 Projects on the State Highway System</b>			<b>Resolution R99-AA-1314-01</b> Amending Resolution R99-A-1213-05	
1 \$52,000,000 <del>\$46,521,000</del> Merced County MCAG Merced 10-Mer-99 19.5/20.7	<b>Atwater-Merced Expressway Phase 1A.</b> Near Atwater, from 1.0 mile south of Buhach Road to 0.1 mile north of Buhach Road. Widen to 6-lane freeway, including demolition of Buhach Road interchange, and construct new interchange.  (Concurrent SR 99 project baseline amendment under Resolution R99-PA-1314-01; August 2013.)  (SR 99 project funding broken down as <del>\$45,000,000</del> <b>\$39,521,000</b> for CONST and \$7,000,000 for CON ENG.)  (Future Consideration of Funding under Resolution E-11-59; August 2011.)  <u>Outcome/Output:</u> Daily travel time savings: 5,022 hours. Peak period time savings: 301,320 minutes.  <b><u>Amend Resolution R99-A-1213-05 to de-allocate \$5,479,000 SR 99 CONST to reflect contract award savings.</u></b>	10-5264A SR-99/12-13 CONST <del>\$52,000,000</del> <b>\$46,521,000</b> 1000000045 4CONL 0G4404	2011-12 304-6072 SR-99 20.20.722.000	<del>\$52,000,000</del> <b>\$46,521,000</b>

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(5a) Proposition 1B – Locally Administered TCIF Projects off the State Highway System</b>			<b>Resolution TCIF-A-1314-04</b>	
1 \$8,855,000 San Bernardino Associated Governments SANBAG 08-San Bernardino	<b>Lenwood Road Railroad Grade Separation.</b> In the city of Barstow. Construct a grade separation for BNSF lines at Lenwood Road (TCIF Project 64).  (The TCIF allocation is split as follows: \$500,000 for construction engineering and \$8,355,000 for construction capital.)  (Contributions from other sources: \$22,878,000.)  <u>Outcome/Output:</u> This project will eliminate the at-grade crossing, mitigate the impact of freight movement in the communities, eliminate gate down time, increase travel reliability, eliminate potential conflicts between vehicular and train traffic, increase safety and improve air quality.  <b>ALLOCATION IS CONTINGENT UPON APPROVAL OF A BUDGET REVISION BY THE DEPARTMENT OF FINANCE.</b>	08-1135 TCIF/13-14 CONST \$8,855,000 0800020269	2012-13 104-6056 TCIF 20.30.210.300	\$8,855,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(5b) Proposition 1B – Locally Administered TCIF Projects off the State Highway System</b>			<b>Resolution TCIF-A-1314-05</b>	
1 \$39,519,000 Orange County Transportation Authority OCTA 12-Orange	<b>Lakeview Avenue Grade Separation.</b> In Placentia at the Lakeview Avenue at-grade crossing. Construct overpass of the BNSF mainline tracks, including a connection road from Orangethorpe Avenue to the new overpass of Lakeview Ave. (TCIF Project 40)  (Future Consideration of Funding – Resolution E-10-74, July, 2010.)  (Related TCIF Programming Amendment under Resolution TCIF-P-1213-42; March 2013.)  (Related TCIF Baseline Amendment under Resolution TCIF-P-1213-44; March 2013.)  (The TCIF allocation is split as follows: \$6,241,000 for construction engineering and \$33,278,000 for construction capital.)  (Contributions from other sources: \$60,244,000.)  <u>Outcome/Output:</u> This project will decrease in traffic congestion and travel time. The elimination of potential collision points will improve goods movement and provide greater driver safety.  <b>ALLOCATION IS CONTINGENT UPON APPROVAL OF A BUDGET REVISION BY THE DEPARTMENT OF FINANCE.</b>	12-TC40 TCIF/13-14 CONST \$39,519,000 1212000004	2013-14 104-6056 TCIF 20.30.210.300	\$39,519,000

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(5c) Proposition 1B – Locally Administered TCIF Rail Projects</b>			<b>Resolution TCIF-A-1314-06</b>	
1 \$10,880,000 Department of Transportation MTC 04-Contra Costa	<b>Richmond Rail Connector.</b> (TCIF Project 2) Located between the cities of San Pablo and Richmond. The project will construct a rail connector on BNSF's Stockton Subdivision and UP's Martinez Subdivision. The at-grade rail connector between the two lines will allow BNSF trains access to UP's Martinez Subdivision rather than travel through the center of the city of Richmond for a more direct route to and from the Port of Oakland.  (Original programming under Resolution TCIF-P-0708-01; April 2008.)  (Future Consideration of Funding - Resolution E-13-41; May 2013.)  (The TCIF allocation is split as follows: \$880,000 for construction engineering and \$10,000,000 for construction capital.)  (Contribution from other sources: \$11,770,000)  <u>Outcome/Output:</u> Project will improve freight velocity to and from the Port of Oakland with reduced traffic delay in the city of Richmond	04-0241B TCIF/12-13 CONST \$10,880,000 0012000218 S	2012-13 304-6056 TCIF 30.20.723.000	\$10,880,000



2.5 Highway Financial Matters

Project #	Allocation Amount	Recipient Agency	Dst-County	RTPA/MPO	Corridor Name / Project Location	Program Prgm'd Amt	Project ID	Adv Phase	EA	Budget Year	Item #	Fund Type	Amount by Fund Type
<b>2.5g.(7) Proposition 1B - Traffic Light Synchronization Program (TLSP)</b>						<b>Resolution TLS1B-A-1314-01</b>							
1	\$426,000	City of Inglewood	LACMTA	07-Los Angeles	City of Inglewood – La Brea Avenue. <u>Outcome/Output:</u> This TLSP project is expected to provide traffic congestion relief by improving travel times over 17 percent for the corridor of 19 signals from 64 <sup>th</sup> Street to 104 <sup>th</sup> Street.	TLSP	\$426,000	0712000233	4	2012-13	104-6064	TLSP	\$426,000

Project #	Allocation Amount	Recipient	RTPA/CTC	District-County	Project Title	Project Description	EA	Program / Year	Phase	Prgm'd Amount	Project ID	Budget Year	Item #	Fund Type	Program Code	Amount by Fund Type
<b>2.5g.(9) Allocation Amendment - Proposition 1B – Locally Administered HRCSA Projects</b>							<b>Resolution GS1B-AA-1314-01</b> Amending Resolution GS1B-AA-1112-005									
1	<del>\$15,293,000</del> <b>\$13,758,787</b>	County of Kern	KCOG	06-Kern	<b>Hageman Road/BNSF Grade Separation.</b>	In Kern County, on Hageman Road and Allen Road. Construct grade separation to re-establish a direct connection of Allen Road, eliminating circuitous routing to Hageman Road and continue to the north providing continuity to the traveling public. A railroad underpass on Hageman Road and Allen Road are both required to replace the current crossing. Replace an at-grade crossing.  (CEQA – SE, July 2009)  <u>Outcome/Output:</u> The project will eliminate public safety hazards; alleviate traffic congestion and degradation of air quality.  <u>Amend Resolution GS1B-AA-1112-005 to de-allocate an additional \$1,534,213 in HRCSA CONST due to overall project cost savings.</u>	H013BA	HRCSA/09-10	CONST	<del>\$15,293,000</del> <b>\$13,758,787</b>	0000020467	2007-08	104-6063	HRCSA	20.30.010.400	<del>\$15,293,000</del> <b>\$13,758,787</b>

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	Dist-PPNO Program/Year PA# PUC Code Prgm'd Amount Project ID EA Adv Phase	Budget Year Fund Type Program Code	Allocation Amount
<b>2.6b. Allocation Amendment - Proposition 116 - Locally Administered Rail Projects</b>				
				<b>Resolution BFA-13-01, Amending Resolution BFP-09-03</b>
1 \$6,247,813 <del>\$1,329,976</del> Transportation Agency for Monterey County TAMC 05-Monterey	<b>Rail Extension to Monterey County.</b> Extend Capital Corridor passenger rail service from San Jose to Salinas and make improvements at three stations.  (Future Consideration of Funding – Resolution E-06-28; August 2006.)  (Concurrent Programming Amendment under Resolution PA-13-01; August 2013.)  <u>Outcome/Output:</u> Complete environmental document, final engineering and property acquisition.  <u>Amend Resolution BFP-09-03 to reallocate \$300,000 to PA&amp;ED; \$300,000 to PS&amp;E, leave \$729,976 for R/W and de-allocate \$4,917,837 for future Proposition 116 CONST allocation.</u>	05-1155 P116/13-14 PA-13-01 PUC 99638(a) <b>PA&amp;ED</b> <b>\$300,000</b> <b>PS&amp;E</b> <b>\$300,000</b> R/W <del>\$6,247,813</del> <b>\$729,976</b> 0014000001 R1316C S  <b>P116/15-16</b> <b>CON</b> <b>\$4,917,837</b>	2013-14 P116 30.10.070.625	<del>\$6,247,813</del> <b>\$1,329,976</b>

Project # Allocation Amount Implementing Agency District-County	BREF # and Project Description Description of Allocation	Item # Fund Type	Amount by Fund Type
<b>2.6e.(1) Traffic Congestion Relief Program Allocations</b>			
<b>Resolution TFP-13-01</b>			
1 \$8,000,000 Los Angeles County Metropolitan Transportation Authority 07- Los Angeles	<b>Project #39 – Route 405 – Add Carpool Lane from Route 10 to Route 101 (Northbound) (PPNO 0851G)</b>  Allocate \$8,000,000 per approved TCRP Allocation Plan.  <u>Output/Outcome:</u> Construct 10 miles of HOV lane Northbound from Route 10 to Route 101.  <b>This is a Tier 1 project.</b>	Chapter 91 of the Statutes of 2000  889-3007 TCRF	\$8,000,000
2 \$10,309,000 Los Angeles County Metropolitan Transportation Authority 07 – Los Angeles	<b>Project 40 – Interstate 10; between Route 605 and Route 57 project (PPNO 0306H)</b>  Allocate \$10,309,000 per approved TCRP allocation plan.  <u>Output/Outcome:</u> Construction for Interstate 10 between Route 605 and Route 57.  <b>This is a Tier 1 project.</b>	Chapter 91 of the Statutes of 2000  889-3007 TCRF	\$10,309,000
3 \$19,833,000 Los Angeles County Metropolitan Transportation Authority 07 – Los Angeles	<b>Project 42 – I-5 Widening; Orange County Line to Route 605 (PPNO 2808)</b>  Allocate \$19,833,000 per approved TCRP allocation plan.  <u>Output/Outcome:</u> Widen the I-5 Corridor from the Orange County Line to the Route 5/Route 605 junction: add HOV and Mixed-flow lanes from .02 miles south of Artesia Avenue to 0.2 mile north of the Florence Avenue Overcrossing to eliminate the northbound bottleneck.  <b>This is a Tier 1 project.</b>	Chapter 91 of the Statutes of 2000  889-3007 TCRF	\$19,833,000

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Implementing Agency District-County	BREF # and Project Description Description of Allocation	Item # Fund Type	Amount by Fund Type
<b>2.6e.(2) Traffic Congestion Relief Program Allocations</b>		<b>Resolution TFP-13-02</b>	
1 \$40,000,000 Santa Clara Valley Transportation Authority 04 – Santa Clara	<b>Project 1.2 – BART to San Jose – Phase 1; extend BART from Warm Springs to Berryessa.</b>  Allocate \$40,000,000 for Construction.  (Future Consideration of Funding – Resolution E-08-19, October 2008.) <u>Output/Outcome:</u> Construct BART extension from Warm Springs (Fremont) to Berryessa (San Jose).  <b>This is a Tier 1 project.</b>	Chapter 91 of the Statutes of 2000  601-3007 TCRF	\$40,000,000

**PUBLIC DISTRIBUTION**

1.1

CALIFORNIA TRANSPORTATION COMMISSION

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Mr. Carl Guardino, Vice Chair  
Silicon Valley Leadership Group  
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San Jose, CA 95110

Mr. Bob Alvarado  
Northern California Carpenters Regional Council  
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Ex Officio Members

The Honorable Mark DeSaulnier  
Member of the Senate  
State Capitol, Room 5035  
Sacramento, CA 95814

The Honorable Bonnie Lowenthal  
Member of the Assembly  
State Capitol, Room 3152  
Sacramento, CA 94814

Executive Director

Mr. Andre Boutros  
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# MINUTES

## CALIFORNIA TRANSPORTATION COMMISSION

<http://www.catc.ca.gov>

June 11, 2013  
Sacramento, California

11:00 AM

Commission Meeting  
Tsakopoulos Library Galleria  
828 I Street, Main Floor Galleria  
Sacramento, CA

11:00 AM	<b>GENERAL BUSINESS</b>				
1	Roll Call		1.1	James Ghielmetti	I C
	Chair Jim Ghielmetti	Present		Commissioner Jim Earp	Absent (Arrived at 11:02 AM)
	Commissioner Bob Alvarado	Present (Departed at 3:40 PM)		Commissioner Dario Frommer	Absent (Arrived at 11:24 AM)
	Commissioner Darius Assemi	Present		Commissioner Carl Guardino	Absent (Arrived at 11:06 AM)
	Commissioner Yvonne Burke	Present		Commissioner Fran Inman	Present
	Commissioner Lucetta Dunn	Present		Commissioner Joe Tavaglione	Present (Departed at 2:30 PM)
	<b>TOTAL</b>			<b>Present:</b>	
				<b>Absent:</b>	
	Senator Mark DeSaulnier, Ex-Officio			Absent (Arrived at 11:33 AM, departed at 12:05 PM)	
	Assembly member Bonnie Lowenthal, Ex-Officio			Absent (Arrived at 11:07 AM, departed at 12:00 PM)	

2	Approval of Minutes for May 7, 2013		1.2	James Ghielmetti	A C
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**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Tavaglione

**Second:** Dunn

**Vote result:** 7-0

**Absent:** Earp, Frommer, Guardino

3	Executive Director's Report		1.3	Andre Boutros	A C
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CTC Executive Director Andre Boutros discussed the changes to the 2013 CTC meeting calendar and also the proposed 2014 meeting dates.

**Recommendation:** Approval of 2013 calendar change and 2014 CTC Meeting Calendar

**Action Taken:** Approved

**Motion:** Tavaglione

**Second:** Alvarado

**Vote result:** 8-0

**Absent:** Frommer, Guardino

Mr. Boutros also recognized Susan Bransen as the new Chief Deputy Director for the CTC.

4	Commission Reports		1.4	James Ghielmetti	A C
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No Commissioner Reports were given.

5	Commissioners' Meetings for Compensation		1.5	James Ghielmetti	A C
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**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Dunn

**Second:** Tavaglione

**Vote result:** 8-0

**Absent:** Frommer, Guardino

Tab #	Item Description	Ref. #	Presenter	Status*	
<b><u>BUSINESS, TRANSPORTATION &amp; HOUSING AGENCY REPORT</u></b>					
6	Report by Agency Secretary and/or Deputy Secretary	1.6	Brian Kelly	I	B

Deputy Secretary for Transportation, Business, Transportation and Housing Agency, Brian Annis, gave a brief update on the new Transportation Agency, he added Caltrans would have an independent review of its performance and sustainability efforts and he discussed high speed rail.

<b><u>CALTRANS REPORT</u></b>					
7	Report by Caltrans' Director and/or Deputy Director	1.7	Malcolm Dougherty	I	D

Caltrans Director Malcolm Dougherty discussed Caltrans personnel changes, TIGER grants, congratulated Susan Bransen on her appointment and he is looking forward to working with the new Transportation Agency.

<b><u>UNITED STATES DEPARTMENT OF TRANSPORTATION REPORT</u></b>					
8	Report by US Department of Transportation	1.11	Vincent Mammano	I	R

Vincent Mammano congratulated the Southern California members of the National Freight Advisory Committee (Commissioner Fran Inman; Assemblymember Bonnie Lowenthal; Kristin Decas, Executive Director of Port of Hueneme; and USC Professor Genevieve Giuliano, Ph.D.); discussed TIGER, Map 21, and Buy America.

<b><u>LOCAL REPORTS</u></b>					
9	Report by Regional Agencies Moderator	1.8	Wil Ridder	I	R

Wil Ridder updated on recent topics discussed by the RTPA including their workshop focusing on streamlining on federal funding, budget and allocation capacity, 2014 STIP Guidelines, announced new moderator Adriann Cardoso (OCTA) and Vice-Moderator Renee DeVer-Okie (SACOG) and he thanked staff for their assistance.

10	Report by Rural Counties Task Force Chair	1.9	Sharon Scherzinger	I	R
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Sharon Scherzinger briefly discussed the activities of the Rural Counties Task Force including STIP Guidelines and Buy America.

11	Report by Self-Help Counties Coalition Chair	1.10	Andy Chesley	I	R
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Andy Chesley announced he was looking forward to the Focus on the Future Conference in November 18-19 in San Diego. He also discussed Buy America and delivery of Prop 1B program.

<b><u>POLICY MATTERS</u></b>					
12	State and Federal Legislative Matters	4.1	Susan Bransen	A	C

CTC Chief Deputy Susan Bransen gave an update on current legislative bills.

**Recommendation:** Approval of staff report

**Action Taken:** Approved

**Motion:** Tavaglione

**Second:** Burke

**Vote result:** 10-0

**Absent:** None

*Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:*

State and Federal Legislative Matters

**YELLOW BOOK ITEM**

13	Budget and Allocation Capacity Update	4.2	Mitchell Weiss Steven Keck	I	D
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Caltrans, Division of Budgets, Chief Steven Keck gave an update on the Budget and Allocation Capacity via PowerPoint.

14	Draft 2014 STIP Fund Estimate	4.3	Mitchell Weiss Steven Keck	I	D
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Tab #	Item Description	Ref. #	Presenter	Status*
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CTC Deputy Director Mitchell Weiss and Caltrans, Division of Budgets, Chief Steven Keck gave update on the 2014 STIP Fund Estimate and announced the July 18<sup>th</sup> 2014 STIP Guidelines Hearing/Fund Estimate Workshop.

*Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:*

Draft 2014 STIP Fund Estimate

**YELLOW SUPPLEMENTAL ITEM  
and PINK REVISED ATTACHMENT**

15	Draft 2014 STIP Guidelines	4.4	Mitchell Weiss	A	C
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CTC Deputy Director Mitchell Weiss gave highlights of this cycle of the 2014 STIP Guidelines including amendment of permanent guidelines, proposing additional reporting, expanded reporting on completed projects, ITIP Environmental Impact and intercity Rail Projects.

*Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:*

Draft 2014 STIP Guidelines

**PINK BOOK ITEM**

16	Buy America Update	4.17	Susan Bransen Brent Green	I	D
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Caltrans Division of Right of Way and Land Surveys Chief, Brent Green, gave update on Buy America.

Public Speaker

- Valerie Winn - PG&E
- Rod Brewer - Southern California Edison
- Congressman Bill Thomas - Kern COG & Various Cities
- Raul Rojas - City of Bakersfield
- Ray Wolfe - SANBAG

17	Workgroup Update – California Transportation Infrastructure Priorities	4.18	Brian Kelly	I	B
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Deputy Secretary for Transportation, Business, Transportation and Housing Agency, Brian Annis, gave an update on the California Transportation Infrastructure priorities.

<b>INFORMATION CALENDAR</b>		Stephen Maller			
18	<b>Informational Reports on Allocations Under Delegated Authority</b> -- Emergency G-11 Allocations (2.5f.(1)): \$645,000 for two projects. -- SHOPP Safety G-03-10 Allocations (2.5f.(3)): \$8,621,000 for six projects. -- Minor G-05-05 Allocations (2.5f.(4)): \$1,315,000 for two District minor projects.	2.5f.		I	D

This Item was presented as part of the Information Calendar.

19	Monthly Report on Projects Amended into the SHOPP by Department Action	3.1		I	D
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This Item was presented as part of the Information Calendar.

20	Monthly Status of Construction Contract Award for State Highway Projects, per Resolution G-06-08	3.2a		I	D
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This Item was presented as part of the Information Calendar.

Tab #	Item Description	Ref. #	Presenter	Status*	
21	Monthly Status of Construction Contract Award for Local Assistance STIP Projects, per FY 2005-06 Allocation Plan and Criteria and Resolution G-06-08	3.2b		I	D

This Item was presented as part of the Information Calendar.

22	Quarterly Update on Implementation of the Recovery Act of 2009	3.3		I	D
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This Item was presented as part of the Information Calendar.

23	Monthly Report on Local and Regional Agency Notices of Intent to Expend Funds on Programmed STIP Projects Prior to Commission Allocation per SB 184	3.4		I	C
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This Item was presented as part of the Information Calendar.

24	Third Quarter FY 2012-13 – Finance Report	3.6		I	D
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This Item was presented as part of the Information Calendar.

25	Second Quarter – Balance Report and AB 1012 – “Use It or Lose It” provision for FFY 2011 Unobligated CMAQ and RSTP Funds	3.12		A	D
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This Item was presented as part of the Information Calendar.

26	2014 Facilities Infrastructure Plan (Five Year Capital Plan)	4.5		I	D
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This Item was presented as part of the Information Calendar.

27	Annual Review of Rate for Local Government Matching of Grants for the California Aid to Airports Program (CAAP)	4.11		I	D
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This Item was presented as part of the Information Calendar.

Changes to this item were listed on the pink “Changes to CTC Agenda” handout as follows:

Annual Review of Rate for Local Government Matching of Grants for the CAAP

--Revise Book Item, last part of “Summary” as: “...adopt the attached resolution at its ~~June~~ **August** 2013 meeting.”

28	Draft of Capital Improvement Plan Element of the California Aviation System Plan	4.12		I	D
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This Item was presented as part of the Information Calendar.

<b>CONSENT CALENDAR</b>			Stephen Maller		
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**Recommendation:** Approval of Consent Calendar with noted changes.

**Action Taken:** Approved with changes

**Motion:** Tavaglione

**Second:** Dunn

**Vote result:** 9-0

**Absent:** Assemi

29	The Lake County/City Area Planning Council proposes to amend the 2012 STIP to reprogram the Construction from Fiscal Year 2013-14 to FY 2014-15 for the South Main Street Rehabilitation (PPNO 3032R) and the Soda Bay Road Rehabilitation projects (PPNO 3033R) in Lake County. STIP Amendment 12S-017	2.1a.(2)		A	D
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This Item was presented and approved as part of the Consent Calendar.

Tab #	Item Description	Ref. #	Presenter	Status*	
30	Placer County is requesting an AB 3090 cash reimbursement to use local funds to replace \$5,168,000 in FY 2014-15 RIP funds for construction of the Kings Beach Commercial Core Improvement project (PPNO 4679), with later reimbursement in FY 2015-16, 2016-17 and 2017-18. STIP Amendment 12S-018 <i>(Related Item under Tab 164) 2.5g.(10c)</i>	2.1a.(3)		A	D

This Item was presented and approved as part of the Consent Calendar.

31	The Plumas County Transportation Commission proposes to amend the 2012 STIP to delete the Big Creek Road Rock Slope Protection project (PPNO 2232) and the Bucks Lake Road Pavement Rehabilitation (Frenchman Hill to Grizzly Creek Bridge) project (PPNO 2348) and add a new project, Bucks Lake Road Pavement Rehabilitation (Snake Lake Road to Slate Creek Road) (PPNO 2542) in Plumas County. STIP Amendment 12S-022	2.1a.(4)		A	D
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This Item was presented and approved as part of the Consent Calendar.

32	The Tuolumne County Transportation Council proposes to amend the 2012 STIP to delete RIP funding for construction and program \$244,000 RIP for Environmental in FY 2013-14, \$80,000 RIP for Design in FY 2014-15, and \$192,000 RIP for Right of Way in FY 2016-17 for the Mono Way Operational and Safety project (PPNO 0235) in Tuolumne County. STIP Amendment 12S-023	2.1a.(5)		A	D
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This Item was presented and approved as part of the Consent Calendar.

33	The Department and Santa Barbara County Association of Governments propose to amend the 2012 STIP to delay RIP funds for construction from FY 2013-14 to FY 2014-15 and reduce the scope of the Route 246 Passing Lanes project (PPNO 6400) in Santa Barbara County. STIP Amendment 12S-025	2.1a.(7)		A	D
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This Item was presented and approved as part of the Consent Calendar.

34	The Mendocino Council of Governments proposes to amend the 2012 STIP to delay \$3,150,000 in RIP construction from FY 2014-15 to FY 2015-16 for the East Side Potter Valley Road Widening and Reconstruction project (PPNO 4073P) in Mendocino County. STIP Amendment 12S-026	2.1a.(8)		A	D
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This Item was presented and approved as part of the Consent Calendar.

35	The Mendocino Council of Governments (MCOG) proposes to amend the 2012 STIP to de-program \$554,000 in Regional Improvement Program (RIP) funds from the Blosser Lane Elementary School Enhancements project (PPNO 4516) in Mendocino County. MCOG also proposes to program \$604,000 in RIP funds to two existing RIP Transportation Enhancement (TE) projects in Mendocino County; the Branscomb Road Bridge project (PPNO 4517) and the Ukiah Downtown Streetscape Improvement Phase 1 project (PPNO 4563). STIP Amendment 12S-027	2.1a.(9)		A	D
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Tab #	Item Description	Ref. #	Presenter	Status*
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This Item was presented and approved as part of the Consent Calendar.

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

The Mendocino Council of Governments (MCOG) proposes to amend the 2012 STIP to de-program ~~\$554,000~~ **\$604,000** in Regional Improvement Program (RIP) funds from the Blosser Lane Elementary School Enhancements project (PPNO 4516) in Mendocino County. MCOG also proposes to program ~~\$604,000~~ **\$554,000** in RIP funds to two existing RIP Transportation Enhancement (TE) projects in Mendocino County; the Branscomb Road Bridge project (PPNO 4517) and the Ukiah Downtown Streetscape Improvement Phase 1 project (PPNO 4563).

--Correct dollar amounts in Agenda Language. Book Item is correct.

36	The Department and the Alameda County Transportation Commission propose to amend the 2012 STIP to reprogram \$400,000 in RIP funds from PA&ED to PS&E for the Follow-up Landscaping contract (PPNO 0057J) of the State Route 24 Caldecott Tunnel 4 <sup>th</sup> Bore project in the Alameda and Contra Costa Counties. STIP Amendment 12S-028	2.1a.(10)		A	D
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This Item was presented and approved as part of the Consent Calendar.

37	The Department, the Solano Transportation Authority and the Napa County Transportation and Planning Agency propose to amend the 2012 STIP to delay, from FY 2013-14 to FY 2014-15, the delivery of the Follow-up Landscaping contract (PPNO 0367J) of the State Route 12 – Jameson Canyon Widening – Phase 2 project in Solano and Napa Counties. STIP Amendment 12S-029	2.1a.(11)		A	D
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This Item was presented and approved as part of the Consent Calendar.

38	The Department proposes to amend the 2012 STIP to delete the Oakley to Port Chicago Double Track (Segment 2, Phase 2) project (PPNO 2099) and program a new project – Stockton to Escalon (Segment 3) project (PPNO 2030A) in San Joaquin County. STIP Amendment 12S-030	2.1a.(12)		A	D
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This Item was presented and approved as part of the Consent Calendar.

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

STIP program amendment for the Oakley to Port Chicago Double Track (Segment 2, Phase 2) project (PPNO 2099) and the Stockton to Escalon (Segment 3) project (PPNO 2030A)

-- Correct Book Item, Page 2, "Stockton to Escalon Double Track (Segment 3) project (PPNP 2030A)" chart; change amounts from negative to positive as follows: ~~(20,500)~~ **20,500**.

39	The County of Lassen proposes to amend the 2012 STIP to de-program \$50,000 RIP Construction funds from the Riverside Drive Reconstruction and Class I Pedestrian/Bike Lane project (PPNO 2480) and to increase RIP PS&E from \$50,000 to \$100,000 for the Skyline Road Extension (Phase 2) project (PPNO 2121A) in Lassen County. STIP Amendment 12S-031	2.1a.(13)		A	D
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This Item was presented and approved as part of the Consent Calendar.

40	The Transportation Agency for Monterey County proposes to amend the 2012 STIP to delay RIP funding for construction from FY 2013-14 to FY 2014-15 for the Route 68 Safety and Operations Corral de Tierra project (PPNO 1813A) in Monterey County. STIP Amendment 12S-032	2.1a.(14)		A	D
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Tab #	Item Description	Ref. #	Presenter	Status*	
41	The Department and Transportation Agency for Monterey County propose to amend the 2012 STIP to move RIP funding for construction from FY 2013-14 to FY 2015-16 for the Coast Daylight/Caltrain Track Improvements project (PPNO 1971) in Monterey County. STIP Amendment 12S-033	2.1a.(15)		A	D

This Item was presented and approved as part of the Consent Calendar.

42	The Santa Barbara County Association of Governments proposes to amend the 2012 STIP to delay RIP funding for construction from FY 2013-14 to FY 2014-15 for the Fowler Road and Ekwill Street Extension project (PPNO 4611) in Santa Barbara County. STIP Amendment 12S-034	2.1a.(16)		A	D
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This Item was presented and approved as part of the Consent Calendar.

43	The Transportation Agency for Monterey County proposes to amend the 2012 STIP to delay RIP TE funding for construction from FY 2013-14 to FY 2014-15 and change the scope of the Castroville Bicycle/Pedestrian Path and Railroad Crossing project (PPNO 2296) in Monterey County. STIP Amendment 12S-035	2.1a.(17)		A	D
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This Item was presented and approved as part of the Consent Calendar.

44	The Modoc County Transportation Commission (MCTC) proposes to amend the 2012 STIP to program \$19,000 of RIP TE funds programmed in FY 2014-15 by Modoc County (PPNO 2437) to the construction phase in FY 2013-14 for the East Connector Road project (PPNO 2138) in Trinity County. STIP Amendment 12S-036	2.1a.(18)		A	D
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This Item was presented and approved as part of the Consent Calendar.

45	The Santa Cruz County Regional Transportation Commission proposes to amend the 2012 STIP to delay RIP funding for Right of Way from FY 2013-14 to FY 2014-15, and delay RIP funding for construction from FY 2014-15 to FY 2015-16, for the Watsonville-Harkins Slough Road Interchange project (PPNO 0413) in Santa Cruz County. STIP Amendment 12S-037	2.1a.(19)		A	D
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This Item was presented and approved as part of the Consent Calendar.

46	The Santa Cruz County Regional Transportation Commission proposes to amend the 2012 STIP to delay RIP TE funding for Environmental from FY 2013-14 to FY 2014-15, delay RIP TE funding for Design and Right of Way from FY 2014-15 to FY 2016-17, and delay RIP TE funding for construction from FY 2015-16 to FY 2016-17 for the Mar Vista Bike/Pedestrian Overcrossing project (PPNO 1968) in Santa Cruz County. STIP Amendment 12S-039	2.1a.(21)		A	D
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This Item was presented and approved as part of the Consent Calendar.

This Item was presented and approved as part of the Consent Calendar.

Tab #	Item Description	Ref. #	Presenter	Status*	
47	The Orange County Transportation Authority proposes to amend the 2012 STIP to delay \$224,000 in RIP PA&ED from FY 2013-14 to FY 2014-15 for the Route 405 Southbound Auxiliary Lane – University to Sand Canyon project (PPNO 4956) in Orange County. STIP Amendment 12S-041	2.1a.(23)		A	D

This Item was presented and approved as part of the Consent Calendar.

48	The County of Sacramento proposes to amend the 2012 STIP to increase the scope and cost of the Fair Oaks Boulevard, Phase 2 Improvements project (PPNO 6579) in Sacramento County. It is also proposed to delay \$1,600,000 in RIP TE construction from FY 2013-14 to FY 2014-15. STIP Amendment 12S-042	2.1a.(24)		A	D
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This Item was presented and approved as part of the Consent Calendar.

49	The Department and the Mendocino Council of Governments propose to amend the 2012 STIP to delay RIP funding from -FY 2013-14 to FY 2015-16 for construction of the Willits Bypass – Ryan Creek / Coho Salmon Mitigation project (PPNO 0125Y) in Mendocino County. STIP Amendment 12S-043	2.1a.(25)		A	D
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This Item was presented and approved as part of the Consent Calendar.

50	The Department, the City of Alturas and the Modoc County Transportation Commission propose to amend the 2012 STIP to reduce the scope of the Alturas Route 299 Improvements project (PPNO 3368), decreasing the programmed RIP funding by \$1,010,000, from \$3,244,000 to \$2,234,000 and removing \$1,052,000 of programmed RIP-TE funds. It is also proposed to program \$1,173,000 of RIP TE funds to a new Pedestrian Improvements project along the Alturas Central Business District (PPNO 2534) in Modoc County. STIP Amendment 12S-044	2.1a.(26)		A	D
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This Item was presented and approved as part of the Consent Calendar.

51	The Santa Barbara County Association of Governments proposes to amend the 2012 STIP to delete \$1,477,000 of RIP TE funding from Santa Barbara County’s TE Reserve (PPNO 1834) in FY 2013-14, and program the Cabrillo Boulevard Pedestrian Improvements project (PPNO 1834B) in Santa Barbara County in FY 2014-15. STIP Amendment 12S-045	2.1a.(27)		A	D
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This Item was presented and approved as part of the Consent Calendar.

52	The Mono County Local Transportation Commission proposes to amend the 2012 STIP to delete the RIP TE funding for Design and construction for the Waterford Avenue Gap Closure project (PPNO 2596), and change the scope and increase the RIP TE funding for Design and construction for the Mammoth Creek Gap Closure project (PPNO 2597) in Mono County. STIP Amendment 12S-046	2.1a.(28)		A	D
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This Item was presented and approved as part of the Consent Calendar.

Tab #	Item Description	Ref. #	Presenter	Status*	
53	The Santa Clara Valley Transportation Authority (VTA) is requesting to amend TCRP Project 7.2 – Extend CalTrain to Gilroy; Platform modification & Gilroy Storage Tracks to revise the project funding plan and de-allocate \$18,123,000 TCRP funds previously allocated to Construction. Furthermore, the VTA and the Transportation Authority of Monterey County (TAMC) are also requesting to add a new TCRP Project 7.3 – CalTrain Service Improvement Phase III; Connect Gilroy yard/station track to Union Pacific mainline track and allocate \$18,123,000 TCRP funds to PS&E (\$890,000) and Construction (\$17,233,000) for Project 7.3. Resolution TAA-12-11, Amending Resolution TAA-10-14 Resolution TFP-12-10, Amending Resolution TFP-07-08	2.1a.(32) /2.6e.(2)		A	D

This Item was presented and approved as part of the Consent Calendar.

54	Financial Allocation Amendment: Reduce the original TCIF allocation for construction support from \$7,567,000 to \$0, for Project 67 - State Route 905 (East of Route 805/905 Separation to East of Britannia Overcrossing project [PPNO 0703]) in San Diego County, and revise the project funding plan. Resolution TCIF-P-1213-77, Amending Resolution TCIF-P-0809-001B Resolution TCIF-AA-1213-19, Amending Resolution TCIF-AA-1112-03	2.1c.(5l)/ 2.5g.(5t)		A	D
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This Item was presented and approved as part of the Consent Calendar.

55	Submittal of Notice of Availability for Comments:  11- San Diego County Mid-Coast Corridor Transit Project Extension of the San Diego Trolley Blue Line for approximately 11 miles from Downtown to University City in San Diego County (DSEIR)	2.2b.(2)		A	C
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This Item was presented and approved as part of the Consent Calendar.

56	Approval of Projects for Future Consideration of Funding: 03-But-99 Rock Creek Bridge Widening Project Roadway and bridge improvements on SR-99 in Placer County. (MND) (PPNO 2427) (SHOPP) Resolution E-13-47	2.2c.(2)		A	D
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This Item was presented and approved as part of the Consent Calendar.

57	Approval of Project for Future Consideration of Funding: 02 – Plumas County Road A15 Reconstruction/Rehabilitation Project - Construction of various street improvements on Road A15 in the City of Portola (MND) (STIP) (PPNO 2480) Resolution E-13-48	2.2c.(3)		A	C
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This Item was presented and approved as part of the Consent Calendar.

Tab #	Item Description	Ref. #	Presenter	Status*	
58	Approval of Project for Future Consideration of Funding: 03 – Sacramento County Sacramento City College Pedestrian Overcrossing Project - Construction of Pedestrian Overcrossing in the City of Sacramento (MND) (STIP-TE) (PPNO 6577) Resolution E-13-49	2.2c.(4)		A	C

This Item was presented and approved as part of the Consent Calendar.

59	Approval of Project for Future Consideration of Funding: 03 – Sacramento County Franklin Boulevard Improvements Project - Construction of various street improvements on Franklin Boulevard between 47 <sup>th</sup> Avenue and the Sacramento City/County Line (MND) (STIP-TE) (PPNO 6580) Resolution E-13-50 <i>(Related Item under Tab 135.) 2.5c.(5)</i>	2.2c.(5)		A	C
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This Item was presented and approved as part of the Consent Calendar.

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

Approval of Project for Future Consideration of Funding for the Sacramento County Franklin Boulevard Improvements Project - Construction of various street improvements on Franklin Boulevard between 47<sup>th</sup> Avenue and the Sacramento City/County Line (PPNO 6580)

**PINK REPLACEMENT ITEM**

60	Approval of Project for Future Consideration of Funding: 07 – Los Angeles County 25 <sup>th</sup> Street East Alignment Project - widen and construct various street improvements on 25 <sup>th</sup> Street East at Avenue J and Lancaster Boulevard in the City of Lancaster (MND) (SLPP) Resolution E-13-51 <i>(Related Item under Tab 162) 2.5g.(10a)</i>	2.2c.(6)		A	C
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This Item was presented and approved as part of the Consent Calendar.

61	Approval of Project for Future Consideration of Funding: 08 – San Bernardino County 5 <sup>th</sup> Street Corridor Improvements Project – Widen and construct various street improvements on 5 <sup>th</sup> Street from Victoria Avenue to Palm Avenue in the City of Highland. (MND) (SLPP) Resolution E-13-52 <i>(Related Item under Tab 162) 2.5g.(10a)</i>	2.2c.(7)		A	C
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This Item was pulled from the Consent Calendar at the request of Commissioner Inman who had to recuse herself from action on the item. It was taken up after the Consent Calendar.

**Recommendation:** Approval of Items 61 and 63.

**Action Taken:** Approved

**Motion:** Tavaglione

**Second:** Frommer

**Vote result:** 8-0

**Absent:** Assemi

**Recuse:** Inman

62	Approval of Project for Future Consideration of Funding: 08 – San Bernardino County Base Line/Greenspot Road Traffic Safety and Bikeway Improvements Project – Construction of various street improvements, bicycle, and pedestrian amenities in the City of Highland. (MND) (SLPP) Resolution E-13-53 <i>(Related Item under Tab 162) 2.5g.(10a)</i>	2.2c.(8)		A	C
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This Item was presented and approved as part of the Consent Calendar.

Tab #	Item Description	Ref. #	Presenter	Status*	
63	Approval of Project for Future Consideration of Funding: 08 – San Bernardino County Redlands Boulevard/Alabama Street Intersection Improvement Project – Widen and realign Redlands Boulevard and Alabama Street in the City of Redlands (MND) (SLPP) Resolution E-13-54 <i>(Related Item under Tab 162.) 2.5g.(10a)</i>	2.2c.(9)		A	C

This Item was pulled from the Consent Calendar at the request of Commissioner Inman who had to recuse herself from action on the item. It was taken up after the Consent Calendar.

**Recommendation:** Approval of Items 61 and 63.

**Action Taken:** Approved

**Motion:** Tavaglione      **Second:** Frommer      **Vote result:** 8-0      **Absent:** Assemi      **Recuse:** Inman

64	Approval of Project for Future Consideration of Funding: 08 – San Bernardino County Yucca Loma Road/Yates Road/Green Tree Boulevard Transportation Improvement Project – Construct a new bridge across the Mojave River and improve existing roads between the Town of Apple Valley in unincorporated San Bernardino County and the City of Victorville (MND) (SLPP) Resolution E-13-55 <i>(Related Item under Tab 162.) 2.5g.(10a)</i>	2.2c.(10)		A	C
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This Item was presented and approved as part of the Consent Calendar.

65	Approval of Project for Future Consideration of Funding: 08 – Riverside County Perris Boulevard Improvements Project – Widen and construct various roadway improvements on Perris Boulevard in the City of Moreno Valley (MND) (SLPP) Resolution E-13-56 <i>(Related Item under Tab 162.) 2.5g.(10a)</i>	2.2c.(11)		A	C
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This Item was presented and approved as part of the Consent Calendar.

66	Approval of Project for Future Consideration of Funding: 10 – San Joaquin County Weber Avenue Streetscape Beautification Project – Installation of various streetscape improvements on Weber Avenue in the City of Stockton (MND) (STIP-TE) (PPNO 0018E) Resolution E-13-57 <i>(Related Item under Tab 136.) 2.5c.(5)</i>	2.2c.(12)		A	C
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This Item was presented and approved as part of the Consent Calendar.

67	Three Relinquishment Resolutions – -- 01-Men-101U-PM 99.49/99.79 Right of way along Route 101 near Confusion Hill, in the county of Mendocino. Resolution R-3875  -- 10-Mer-99-PM 23.81 Right of way along Route 99 at Olive Avenue, in the city of Atwater. Resolution R-3876  -- 10-Cal-4-PM R21.5 Right of way along Route 4 at Casey Street and Gardner Lane, in the city of Angels (Angels Camp). Resolution R-3877	2.3c		A	D
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Tab #	Item Description	Ref. #	Presenter	Status*
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This Item was presented and approved as part of the Consent Calendar.

68 8 Ayes	13 Resolutions of Necessity Resolutions C-21064 through C-21076	2.4b		A	D
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This Item was presented and approved as part of the Consent Calendar.

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

43 12 Resolutions of Necessity

Resolutions C-21064 through **C-21071 and C-21073 through** C-21076

➤ Resolution C-21072 (Jason S. Lee, et al.; 08-SBd-138; PM 2.90; Parcel 21561-1 - EA 3401U9) **Withdrawn prior to the CTC Meeting**

69	Director's Deeds Items 1 through 31 Excess Lands - Return to State: \$6,547,700 Return to Others: \$0	2.4d		A	D
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This Item was presented and approved as part of the Consent Calendar.

70	Financial Allocation Amendment: Reduce the original TCIF allocation for construction by \$1,809,000, from \$8,745,000 to \$6,936,000, from Project 24 - Tier 1; Pier F Support Yard project (PPNO TC24) and by \$10,784,000, from \$27,000,000 to \$16,216,000, from Project 25 – Ports Rail System; Track Realignment at Ocean Boulevard project (PPNO TC25), both in Los Angeles county, to reflect contract award savings. Resolution TCIF-AA-1213-18, Amending Resolution TCIF-A-1112-03	2.5g.(5s)		A	D
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This Item was presented and approved as part of the Consent Calendar.

71	Financial Allocation Amendment: Reduce the original TLSP allocation by \$462,959, from \$2,000,000 to \$1,537,041, for the San Bernardino Valley Coordinated Traffic Signal System – Tier 3 and 4 project in San Bernardino County, to reflect award savings. Resolution TLS1B-AA-1213-04, Amending Resolution TLS1B-A-1011-001	2.5g.(7)		A	D
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This Item was presented and approved as part of the Consent Calendar.

72	Financial Allocation Amendment: Reduce the original HRCSA allocation for construction by a combined total of \$6,444,000, from \$27,000,000 to \$20,556,000, from three HRCSA projects, all in San Joaquin County, to reflect contract award savings. Resolution GS1B-AA-1213-02, Amending Resolution GS1B-A-0910-004	2.5g.(9a)		A	D
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This Item was presented and approved as part of the Consent Calendar.

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

Financial Allocation Amendment for HRCSA projects

--Revise Attachment and Vote List for Project 1(Eight Mile Road (West)), Project 2 (Eight Mile Road (East)), and Project 3 (Lower Sacramento Road/UPRR Grade Separation, between Armor Drive and Marlette Road); correct Budget Year as ~~2007-08~~ **2008-09**.

Tab #	Item Description	Ref. #	Presenter	Status*	
73	Financial Allocation Amendment: Reduce the original HRCSA allocation for construction by \$1,578,440, from \$9,000,000 to \$7,421,560, from the G Street Undercrossing Project in Merced County, to reflect contract award savings. Resolution GS1B-AA-1213-03, Amending Resolution GS1B-A-0910-003	2.5g.(9b)		A	D

This Item was presented and approved as part of the Consent Calendar.

74	Financial Allocation Amendment: Reduce the original SLPP allocation by \$119,000, from \$472,000 to \$353,000, for the Dale Street Reconstruction and Idaho Street Reconstruction project, in Orange County. Resolution SLP1B-AA-1213-20, Amending Resolution SLP1B-A-1213-18	2.5g.(10f)		A	D
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This Item was presented and approved as part of the Consent Calendar.

75	Financial Allocation Amendment: Reduce the original SLPP allocation by \$165,000, from \$600,000 to \$435,000, for Jamboree Road Rehabilitation project, in Orange County. Resolution SLP1B-AA-1213-21 Amending Resolution SLP1B-A-1213-14	2.5g.(10g)		A	D
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This Item was presented and approved as part of the Consent Calendar.

76	Financial Allocation Amendment: Reduce the original SLPP allocation by \$43,000, from \$1,000,000 to \$957,000, for the Route 91 Auxiliary Lane project, in the Riverside County. Resolution SLP1B-AA-1213-22 Amending Resolution SLP1B-A-1011-01	2.5g.(10h)		A	D
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This Item was presented and approved as part of the Consent Calendar.

77	The Department and the Los Angeles County Metropolitan Transportation Authority propose to amend TCRP Project 42.0 – I-5 Carpool Lane from Orange County Line to I-605 Corridor project (PPNOs 4153, 2808, 4154, 4155 and 4156) in Los Angeles County, to redistribute \$3,210,000 in previously allocated TCRP funds from the Environmental phase to Design (\$508,000) and Right of Way (\$2,702,000). Resolution TFP-12-11, Amending TCPD-01-09	2.6e.(1)		A	D
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This Item was presented and approved as part of the Consent Calendar.

78	Technical correction to Resolution CMIA-A-1112-005, originally approved on August 10, 2011, for the I-5 South HOV Lane-Segment 1 (PPNO 4153) in Los Angeles County. A technical correction is needed to revise Project Development and Right of Way estimate amounts in the vote box.	2.9a		A	D
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This Item was presented and approved as part of the Consent Calendar.

79	Technical correction to Resolution CMIA-A-1112-030, originally approved on April 25, 2012, for the I-5 South HOV Lane-Segment 3 (PPNO 4154) and the I-5 South HOV Lane-Segment 4 (PPNO 4155) in Los Angeles County. A technical correction is needed to revise Project Development and Right of Way estimate amounts in the vote boxes.	2.9b		A	D
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Tab #	Item Description	Ref. #	Presenter	Status*	
80	Technical correction to Resolution FP-12-51, originally approved on May 7, 2013 for \$11,170,000 for 31 locally administered STIP projects. A technical correction is needed for Project 4 – Old Highway 99W Rehabilitation (North of Maxwell) project (PPNO 3186) to revise the Fund Type from federally funded to state funded.	2.9c		A	D

This Item was presented and approved as part of the Consent Calendar.

81	Approval of \$300,324,000 from the Proposition 1B Bond Program, for reimbursement to the State Highway Account, for six bond funded projects that were advanced using Recovery Act funding in accordance with AB 3x 20. Resolution G-13-04 Resolution FS-12-01	4.13		A	D
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This Item was presented and approved as part of the Consent Calendar.

<b>END OF CONSENT CALENDAR</b>					
<b>Airspace Leases</b>					
82	Request for approval to execute a long term airspace lease agreement with Jewelry Square Associates	2.4c.(1)	Stephen Maller Brent Green	A	D

**Recommendation:** Approval of Items 82-84

**Action Taken:** Approved

**Motion:** Guardino                      **Second:** Tavaglione                      **Vote result:** 10-0                      **Absent:** None

83	Request for approval to execute a long term airspace lease agreement with 888 Brannan LP	2.4c.(2)	Stephen Maller Brent Green	A	D
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**Recommendation:** Approval of Items 82-84

**Action Taken:** Approved

**Motion:** Guardino                      **Second:** Tavaglione                      **Vote result:** 10-0                      **Absent:** None

84	Request for approval to execute a long term airspace lease agreement with Euromotors, Inc	2.4c.(3)	Stephen Maller Brent Green	A	D
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**Recommendation:** Approval of Items 82-84

**Action Taken:** Approved

**Motion:** Guardino                      **Second:** Tavaglione                      **Vote result:** 10-0                      **Absent:** None

<b>PROGRAM STATUS</b>					
85	Third Quarter FY 2012-13 – Rail Operations Report	3.7	Juan Guzman Bill Bronte	I	D

Caltrans Division of Rail Chief Bill Bronte gave an update on the third quarter Rail Operations Report for FY 2012-13.

86	Third Quarter FY 2012-13 – Project Delivery Report	3.8	Stephen Maller James Davis	I	D
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CTC Deputy Director Stephen Maller presented the third quarterly Project Delivery Report for FY 2012-13.

Tab #	Item Description	Ref. #	Presenter	Status*	
87	Proposition 1B – Quarterly Reports --Corridor Mobility Improvement Account (3.9a.) --Route 99 Corridor (3.9b.) --Local Bridge Seismic Retrofit Program (3.9c.) --State-Local Partnership Program (3.9d.) --Traffic Light Synchronization Program (3.9e.) --Highway-Rail Crossing Safety Account (3.9f.) --Intercity Rail Improvement Program (3.9g.) --Trade Corridor Improvement Fund (3.9h.)	3.9	Stephen Maller Rachel Falsetti	I	D

Caltrans Office of California Transportation Commission Liaison Office Chief Greg Wong briefly discussed the quarterly reports for Proposition 1B,

88	Preliminary Close-Out Report on FY 2012-13 Right of Way Lump Sum Allocation	3.10	Stephen Maller Brent Green	I	D
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CTC Deputy Director Stephen Maller presented the Preliminary Close-Out Report on FY 2012-13 Right of Way Lump Sum Allocation.

89	Financial Allocation: \$195,104,000 for FY 2013-14 Right of Way Lump Sum Allocation. Resolution FM-12-04	2.5h.	Stephen Maller Brent Green	A	D
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**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Tavaglione

**Second:** Alvarado

**Vote result:** 10-0

**Absent:** None

90	Quarterly Report – Local Assistance Lump Sum Allocation for the period ending March 31, 2013	3.11	Laurel Janssen Denix Anbiah	I	D
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CTC Associate Deputy Director Laurel Jansen briefly discussed the quarterly report for the Local Assistance Lump Sum Allocation for the period ending March 31, 2013.

91	Financial Allocation: \$104,063,000 in State funds for FY 2013-14 Local Assistance Lump Sum Allocation. Resolution FM-12-05	2.5i.	Laurel Janssen Denix Anbiah	A	D
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**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Frommer

**Second:** Assemi

**Vote result:** 10-0

**Absent:** None

92	Preliminary Close-out Report on FY 2012-13 Minor Program Lump Sum Allocation	3.13	Juan Guzman Rachel Falsetti	I	D
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CTC Associate Deputy Director presented the Preliminary Close-out Report on FY 2012-13 Minor Program Lump Sum Allocation.

93	Financial Allocation: \$70,365,000 for FY 2013-14 Minor Program Lump Sum Allocation. Resolution FM-12-06	2.5j.	Juan Guzman Rachel Falsetti	A	D
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**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Earp

**Second:** Tavaglione

**Vote result:** 10-0

**Absent:** None

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

Financial Allocation for FY 2013-14 Minor Program Lump Sum Allocation

--Revise Attachment; Page 1, EA 4G350, correct the Route as 3 299.

Tab #	Item Description	Ref. #	Presenter	Status*	
94	Update on the Transportation Agency for Monterey County Operating and Maintenance Plan	3.14	Juan Guzman Debbie Hale	I	C

Transportation Agency for Monterey County Executive Director Debbie Hale gave an update on the Transportation Agency for Monterey County Operating and Maintenance Plan.

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

Update on the Transportation Agency for Monterey County Operating and Maintenance Plan

**YELLOW BOOK ITEM**

95	Status Update on Trade Corridor Improvement Fund (TCIF) Projects	3.5	Stephen Maller Kenneth Kao Patricia Chen	I	D/ R
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Kenneth Kao representing northern California and Anne Mayer representing southern California discussed their respective area's TCIF projects.

Public Speakers

Anne Mayer - Southern CA Consensus Group  
Juan Acosta - BNSF Railway

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

Status Update on TCIF Projects

**YELLOW BOOK ITEM**

<b>POLICY MATTERS</b>					
96	Trade Corridor Improvement Fund Program – Proposal to Utilize Program Savings	4.16	Stephen Maller	I	C

CTC Deputy Director presented the TCIF Program - Proposal to Utilize Program Savings.

Public Speaker

Darrell Johnson - CEO, Orange County Transportation Authority  
Anne Mayer - Southern California Consensus Group  
Kenneth Kao - MTC  
Paul Hubler - Alameda Corridor East Construction Authority

97	Trade Corridors Improvement Fund Program – Program Amendment: Remove from the program Project 58 - I10 Citrus Avenue Interchange and Project 65 - Vineyard Avenue Grade Separation; increase funding for Project 84 - Laurel Avenue Grade Separation, Project 64 - Lengwood Road Grade Separation, and Project 61 – South Milliken Avenue Grade Separation; adjust funding to Project 89 – I-80/680/12 Connector. Resolution TCIF-P-1213-79 <i>(Related Items under Tabs 149 &amp; 150) 2.5g.(5i) &amp; 2.5g.(5j)</i>	4.15	Stephen Maller	A	C
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**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Frommer

**Second:** Assemi

**Vote result:** 9-0

**Absent:** Tavaglione

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

Trade Corridors Improvement Fund Program – Program Amendment: Remove from the program Project ~~58~~ **57** - I10 Citrus Avenue Interchange and Project 65 - Vineyard Avenue Grade Separation; increase funding for Project 84 - Laurel Avenue Grade Separation, Project 64 - ~~Lengwood~~ **Lenwood** Road Grade Separation, and Project 61 – South Milliken Avenue Grade Separation; adjust funding to Project 89 – I-80/680/12 Connector.

--Revise Agenda Language.

--Revise Book Item, all pages; correct the project number for the I10 Citrus Avenue Interchange as Project ~~58~~ **57**

Tab #	Item Description	Ref. #	Presenter	Status*	
98	Updated Presidio Parkway Funding Plan	4.6	Stephen Maller Kome Ajise	A	D

**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Frommer                      **Vote result:** 9-0                      **Absent:** Tavaglione

<b>Financial Allocation for Multi-Funded Project with SHOPP/STIP/SLPP Program Funds – Presidio Parkway Project (Doyle Drive)</b>					
99	Advance Financial Allocation: \$9,680,000 for the SHOPP portion of the Presidio Parkway P3 milestone payment (Doyle Drive project), located in San Francisco County, programmed in FY 2015-16. Resolution FP-12-70	2.5b.(2b)	Stephen Maller Rachel Falsetti	A	D

**Recommendation:** Approval of Items 99 and 100

**Action Taken:** Approved

**Motion:** Dunn                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

100	Financial Allocation: \$91,366,000 for STIP/SLPP portion of the Presidio Parkway P3 milestone payment (Doyle Drive project [PPNO 0619A]) in San Francisco County. The allocation is: \$67,000,000 of STIP, \$19,366,000 of SLPP and an advance of \$5,000,000 of STIP, programmed in FY 2014-15. Resolution FP-12-69 Resolution SLP1B-A-1213-30	2.5g.(10e)	Stephen Maller Rachel Falsetti Denix Anbiah	A	D
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**Recommendation:** Approval of Items 99 and 100

**Action Taken:** Approved

**Motion:** Dunn                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

101	Advance Financial Allocation: \$600,000 for Resident Engineer Office for the Presidio Parkway P3 (Doyle Drive project), located in San Francisco County. Allocation is from the project’s SHOPP Risk Reserve programmed in FY 2015-16. Resolution FP-12-71	2.5b.(2c)	Stephen Maller Rachel Falsetti	A	C
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**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Assemi                      **Second:** Earp                      **Vote result:** 9-0                      **Absent:** Tavaglione

<b>POLICY MATTERS</b>					
102	State-Local Partnership Competitive Program Amendment Resolution SLP1B-P-1213-13	4.7	Laurel Janssen	A	C

**Recommendation:** Approval of Nelson Lane

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Guardino                      **Vote result:** 9-0                      **Absent:** Tavaglione

**Recommendation:** Approval of Remaining Projects - Elk Grove and Highland

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Frommer                      **Vote result:** 9-0                      **Absent:** Tavaglione

Public Speakers

- Tom Cosgrove – City of Lincoln
- Ray Leftwich – City Engineer, City of Lincoln
- Rick Carter – City of Elk Grove

Changes to this item were listed on the pink “Changes to CTC Agenda” handout as follows:

State-Local Partnership Formula Program Amendment

**PINK REVISED ITEM**



Tab #	Item Description	Ref. #	Presenter	Status*	
<b>Amendments for Action</b>					
107	The City of Thousand Oaks and the Ventura County Transportation Commission are requesting an AB 3090 cash reimbursement in order to use local funds to replace \$20,000,000 in FY 2015-16 Regional Improvement Program funds for construction of the Los Angeles County Line to Route 23 – US 101 Improvements Phase 1 project (PPNO 2291). The City and VCTC propose reimbursement of \$15,764,000 in FY 2015-16, with the remaining \$4,236,000 returning to Ventura County’s share balance. STIP Amendment 12S-013 <i>(Related Item under Tab 157.) 2.5g.(5q)</i>	2.1a.(1)	Mitchell Weiss Rachel Falsetti	A	D

**Recommendation:** Approval of Items 107 -109

**Action Taken:** Approved

**Motion:** Frommer

**Second:** Assemi

**Vote result:** 9-0

**Absent:** Tavaglione

Public Speaker

Cliff Finley - City of Thousand Oaks

108	The Tulare County Association of Governments proposes to amend the 2012 STIP to reduce the scope and decrease the construction budget of the Santa Fe Trail Connection project (PPNO 6565), and program the City of Woodlake Downtown Enhancements Phase 4 project (PPNO 6658), the Garden Avenue Pedestrian Walkway project (PPNO 6659), and the City of Dinuba Class II and Class III Bike Lanes project (PPNO 6660) in Tulare County. STIP Amendment 12S-024	2.1a.(6)	Mitchell Weiss Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 107 -109

**Action Taken:** Approved

**Motion:** Frommer

**Second:** Assemi

**Vote result:** 9-0

**Absent:** Tavaglione

*Changes to this item were listed on the pink “Changes to CTC Agenda” handout as follows:*

STIP program amendment for the Santa Fe Trail Connection project (PPNO 6565), the City of Woodlake Downtown Enhancements Phase 4 project (PPNO 6658) the Garden Avenue Pedestrian Walkway project (PPNO 6659), and the City of Dinuba Class II and Class III Bike Lanes project (PPNO 6660)

**YELLOW REPLACEMENT ITEM**

109	The Santa Cruz County Regional Transportation Commission proposes to amend the 2012 STIP to delay RIP TE funding for Design from FY 2013-14 to FY 2014-15 for the Monterey Bay Sanctuary Scenic Trail Network project (PPNO 1872) in Santa Cruz County. STIP Amendment 12S-038	2.1a.(20)	Mitchell Weiss Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 107 -109

**Action Taken:** Approved

**Motion:** Frommer

**Second:** Assemi

**Vote result:** 9-0

**Absent:** Tavaglione

*Changes to this item were listed on the pink “Changes to CTC Agenda” handout as follows:*

STIP program amendment for the Monterey Bay Sanctuary Scenic Trail Network project (PPNO 1872)

**YELLOW REPLACEMENT ITEM**

Tab #	Item Description	Ref. #	Presenter	Status*	
110	The Department and the Orange County Transportation Authority propose to amend the 2012 STIP for the Interstate 5 HOV Lane - South of Avenida Vista Hermosa to South of Pacific Coast Highway project (PPNO 2531E) to reduce RIP construction by \$10,000,000, from \$47,381,000 to \$37,381,000 and backfill with CMAQ funding. It is also proposed to reprogram the \$10,000,000 in RIP funds to the design phase of a new STIP project, Interstate 5 Widening – El Toro Road to State Route 73 (PPNO 2640B) in Orange County. STIP Amendment 12S-040	2.1a.(22)	Mitchell Weiss Rachel Falsetti	A	D

This Item was pulled from the agenda.

<b>Multi-Funded Project Amendment for Action</b>					
111	The Department and the Los Angeles County Metropolitan Transportation Authority (LACMTA) propose to amend the 2012 STIP, the CMIA Baseline Agreement, and TCRP Project #42 for the Route 5 Carpool Lane-Orange County Line to I-605 project (I-5 South Corridor project) in Los Angeles County to program an additional \$35,709,000 from Los Angeles County's share balance and to update the project funding plan and schedule for the corridor. STIP Amendment 12S-048 Resolution CMIA-PA-1213-18, Amending CMIA-PA-1011-015 Resolution TAA-12-08, Amending Resolution TAA-10-03	2.1a.(30)/ 2.1c.(1a)	Mitchell Weiss Rachel Falsetti	A	D

**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Burke

**Second:** Frommer

**Vote result:** 9-0

**Absent:** Tavaglione

*Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:*

Multi-funded project amendment for the Route 5 Carpool Lane-Orange County Line to I-605 project (I-5 South Corridor project)

**YELLOW REPLACEMENT ITEM**

<b>STIP Program/Project Amendments/Approvals for Notice</b>					
112	The Siskiyou County Local Transportation Commission proposes to amend the 2012 STIP to program \$300,000 in RIP funds from Siskiyou County's unprogrammed share balance for construction on a new project, Angel Maple Operational Improvements (PPNO 3530), in Siskiyou County. STIP Amendment 12S-049	2.1b.	Mitchell Weiss Rachel Falsetti	I	D

CTC Deputy Director Mitchell Weiss noticed this item and will be placed on the agenda at a future meeting.

<b>Proposition 1B TCIF Project Amendments for Action</b>					
113	The Department proposes to amend the TCIF baseline agreement for Project 2 (Richmond Rail Connector Project [PPNO TC02]) to update the delivery schedule, cost, and funding plan. Resolution TCIF-P-1213-64 Amending Resolution TCIF-P-1011-27B <i>(Related Item under Tab 163.) 2.5g.(10b)</i>	2.1c.(5a)	Stephen Maller Bill Bronte	A	D

**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp

**Second:** Assemi

**Vote result:** 9-0

**Absent:** Tavaglione

Tab #	Item Description	Ref. #	Presenter	Status*	
114	The Riverside County Transportation Commission and the County of Riverside propose to amend the TCIF baseline agreement for Project 46 (Sunset Lakeview Avenue Overcrossing [PPNO 1122]) to update the project delivery schedule, cost and funding plan. Resolution TCIF-P-1213-65 Amending Resolutions TCIF-P-1213-45 <i>(Related Item under Tab 145.) 2.5g.(5e)</i>	2.1c.(5b)	Stephen Maller Katie Benouar	A	D

**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

115	The Riverside County Transportation Commission and the County of Riverside propose to amend the TCIF baseline agreement for Project 48 (Avenue 56 Grade Separation on Yuma Subdivision of UPR Mainline [PPNO 1124]) to update the delivery schedule, cost and funding plan. Resolution TCIF-P-1213-68 Amending Resolution TCIF-P-1213-22 <i>(Related Item under Tab 147.) 2.5g.(5g)</i>	2.1c.(5c)	Stephen Maller Katie Benouar	A	D
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**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

116	The Riverside County Transportation Commission and the County of Riverside propose to amend the TCIF baseline agreement for Project 50 (Grade Separation at Clay Street Railroad Grade Crossing [PPNO 1126]) to update the delivery schedule, cost, and funding plan. Resolution TCIF-P-1213-69 Amending Resolution TCIF-P-1213-23 <i>(Related Item under Tab 144.) 2.5g.(5d)</i>	2.1c.(5d)	Stephen Maller Katie Benouar	A	D
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**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

117	The Riverside County Transportation Commission and the County of Riverside propose to amend the TCIF baseline agreement for Project 53 (Grade Separation at Magnolia Avenue Railroad Grade Crossing - BNSF [PPNO 1129]) to update the delivery schedule, cost, and funding plan. Resolution TCIF-P-1213-70 Amending Resolution TCIF-P-1213-25 <i>(Related Item under Tab 143.) 2.5g.(5c)</i>	2.1c.(5e)	Stephen Maller Katie Benouar	A	D
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**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

TCIF Project Amendment for the Magnolia Avenue Railroad Grade Crossing – BNSF (PPNO 1129)

--Revise Book Item, Page 3; expand the eighth Fund Source heading as follows: State Bond Funds – Prop 1B – Local Streets-SCO.

Tab #	Item Description	Ref. #	Presenter	Status*	
118	The San Bernardino Association of Governments and the City of Ontario propose to amend the TCIF baseline agreement for Project 61 (ACE South Milliken Avenue Grade Separation at UP Los Angeles [PPNO 1131]) to update the cost and the funding plan. Resolution TCIF-P-1213-71 Amending Resolution TCIF-P-1213-37 <i>(Related Item under Tab 150.) 2.5g.(5j)</i>	2.1c.(5f)	Stephen Maller Katie Benouar	A	D

**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

119	The San Bernardino Association of Governments proposes to amend the TCIF baseline agreement and TCRP application for Project 84 (Laurel Street/BNSF Grade Separation, TCRP 55.4, [PPNO 1141]) to update the project delivery schedule, cost, and funding plan. Resolution TCIF-P-1213-72 Amending Resolution TCIF-P-1011-16B Resolution TAA-12-12 Amending Resolution TA-10-02 <i>(Related Items under Tabs 149 &amp; 124.) 2.5g.(5i); &amp; 2.1c.(5m)</i>	2.1c.(5g)	Stephen Maller Katie Benouar	A	D
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**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

120	The Riverside County Transportation Commission and the City of Coachella propose to amend the TCIF baseline agreement for Project 85 (Avenue 52 Grade Separation [PPNO 1142]) to update the delivery schedule and cost. Resolution TCIF-P-1213-73 Amending Resolution TCIF-P-1213-30 <i>(Related Item under Tab 146.) 2.5g.(5f)</i>	2.1c.(5h)	Stephen Maller Katie Benouar	A	D
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**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

121	The Port of Los Angeles proposes to amend the TCIF baseline agreement for Project 86 (Alameda Corridor West Terminus Intermodal Railyard – West Basin Railyard Extension [PPNO TC86]) to update the delivery schedule and cost. Resolution TCIF-P-1213-74 Amending Resolution TCIF-P-1213-04B <i>(Related Item under Tab 155.) 2.5g.(5n)</i>	2.1c.(5i)	Stephen Maller Katie Benouar	A	D
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**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

122	The Port of Los Angeles proposes to amend the TCIF baseline agreement for Project 87 (Cargo Transportation Improvement Emission Reduction Program - Phase 2 [PPNO TC87B]) to update the delivery schedule, cost, and funding plan. Resolution TCIF-P-1213-75 Amending Resolution TCIF-P-1213-04B <i>(Related Item under Tab 155.) 2.5g.(5o)</i>	2.1c.(5j)	Stephen Maller Katie Benouar	A	D
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Tab #	Item Description	Ref. #	Presenter	Status*
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**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

123	The Department and the Solano Transportation Agency propose to amend the baseline agreement for TCIF Project 89 (WB I-80 to SR 12 [West] Connector and Green Valley Road Interchange Improvements project (PPNO 5301L) in Solano County to revise the project funding plan and delivery schedule. Resolution TCIF-P-1213-76, Amending Resolution TCIF-P-1112-038B <i>(Related Items under Tabs 161 &amp; 159) 2.5b.(4c) &amp; 2.5g.(5r)</i>	2.1c.(5k)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

124	The San Bernardino Association of Governments proposes to amend the TCIF baseline agreement for Project 64 (Lenwood Road Grade Separation [PPNO 1135] to update the cost and funding plan. Resolution TCIF-P-1213-78, Amending Resolution TCIF-P-1213-57 <i>Related Item under Tab 148.) 2.5g.(5h)</i>	2.1c.(5m)	Stephen Maller Katie Benouar	A	D
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**Recommendation:** Approval of Items 113-124

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Assemi                      **Vote result:** 9-0                      **Absent:** Tavaglione

<b>Financial Allocations for Minor Projects</b>					
125	Financial Allocation: \$1,000,000 for one Minor project located in the city of Oxnard from Pleasant Valley Road to Route 101, in Ventura County. Resolution FP-12-55	2.5a.	Juan Guzman Rachel Falsetti	A	D

**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Frommer                      **Second:** Dunn                      **Vote result:** 9-0                      **Absent:** Tavaglione

<b>Financial Allocations for SHOPP Projects</b>					
126	Financial Allocation: \$128,682,000 for 17 SHOPP projects, as follows: --\$127,569,000 for 14 SHOPP projects. --\$1,113,000 for three projects amended into the SHOPP by Departmental action. Resolution FP-12-56	2.5b.(1)	Juan Guzman Rachel Falsetti	A	D

**Recommendation:** Approval as amended

**Action Taken:** Approved as amended

**Motion:** Dunn                      **Second:** Burke                      **Vote result:** 9-0                      **Absent:** Tavaglione

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

Financial Allocations for SHOPP projects

--Revise Attachment and Vote List; renumber the last three projects as 44 15 (PPNO 01-4525)) 45 16 (PPNO 07-4559) and 46 17 (PPNO 08-0044K). Book Item is correct.

➤ Project 2 (PPNO 04-8085A) **Withdrawn prior to the CTC Meeting. Project cannot be allocated due to not meeting Buy America requirements.**

➤ Project 3 (PPNO 05-0707) **Withdrawn prior to the CTC Meeting. Project cannot be allocated due to not meeting Buy America requirements.**

Tab #	Item Description	Ref. #	Presenter	Status*	
127	Advance Financial Allocation: \$78,293,000 for seven SHOPP projects, programmed in FY 2013-14 and FY 2014-15. Contributions from other sources: \$3,668,000. Resolution FP-12-57	2.5b.(2a)	Juan Guzman Rachel Falsetti	A	D

**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Earp                      **Second:** Frommer                      **Vote result:** 9-0                      **Absent:** Tavaglione

<b>Financial Allocations for Design-Build Projects</b>					
128	Financial Allocation: \$46,800,000 for two SHOPP Design-Build projects. Resolution FP-12-58	2.5b.(3a)	Stephen Maller Rachel Falsetti	A	D

**Recommendation:** Approval of Items 128 and 129

**Action Taken:** Approved

**Motion:** Dunn                      **Second:** Frommer                      **Vote result:** 9-0                      **Absent:** Tavaglione

129	Advance Financial Allocation: \$158,000,000 for the Design-Build Route 215 Junction to Oak Hill Road road rehabilitation project located in San Bernardino County, programmed in FY 2015-16. Resolution FP-12-72	2.5b.(3b)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 128 and 129

**Action Taken:** Approved

**Motion:** Dunn                      **Second:** Frommer                      **Vote result:** 9-0                      **Absent:** Tavaglione

<b>Financial Allocations for STIP Projects</b>					
130	Financial Allocation: \$2,519,000 for the Route 91 Widening (PPNO 4598D) State administered STIP project, in Orange County, on the State Highway System. Resolution FP-12-60	2.5c.(1a)	Mitchell Weiss Rachel Falsetti	A	D

**Recommendation:** Approval of Items 130-133

**Action Taken:** Approved

**Motion:** Dunn                      **Second:** Burke                      **Vote result:** 9-0                      **Absent:** Tavaglione

131	Financial Allocation: \$7,222,000 for two State administered STIP TE projects, on the State Highway System. Resolution FP-12-61	2.5c.(1b)	Mitchell Weiss Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 130-133

**Action Taken:** Approved

**Motion:** Dunn                      **Second:** Burke                      **Vote result:** 9-0                      **Absent:** Tavaglione

132	Financial Allocation: \$1,151,000 for two locally administered STIP TE projects, on the State Highway System. Resolution FP-12-62	2.5c.(2a)	Mitchell Weiss Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 130-133

**Action Taken:** Approved

**Motion:** Dunn                      **Second:** Burke                      **Vote result:** 9-0                      **Absent:** Tavaglione

Tab #	Item Description	Ref. #	Presenter	Status*	
133	Advance Financial Allocation: \$693,000 for the Route 43 Corridor Beautification Phase 2 (PPNO 6618) locally administered STIP TE project, located in Kern County, on the State Highway System, programmed in FY 2013-14. Contributions from other sources: \$90,000. Resolution FP-12-63	2.5c.(2b)	Mitchell Weiss Rachel Falsetti	A	D

**Recommendation:** Approval of Items 130-133

**Action Taken:** Approved

**Motion:** Dunn                      **Second:** Burke                      **Vote result:** 9-0                      **Absent:** Tavaglione

134	Financial Allocations: \$26,278,000 for 33 locally administered STIP projects off the State Highway System, as follows: --\$7,017,000 for five STIP projects. --\$18,756,000 for 26 STIP TE projects. --\$505,000 for two STIP Planning, Programming, and Monitoring projects. Contributions from other sources: \$38,439,494. Resolution FP-12-64	2.5c.(3)	Mitchell Weiss Rachel Falsetti	A	D
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**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Dunn                      **Second:** Earp                      **Vote result:** 9-0                      **Absent:** Tavaglione

Public Speaker

David Wheaton - City of Citrus Heights

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

Financial Allocations for locally administered STIP projects off the SHS

--Revise Attachment and Vote List for Project 26 (PPNO 4544) as follows:

→ Correct Phase and Programmed Amount from as PS&E \$1,543,000 to **CONST \$1,543,000.**

→ Insert the following statement: **(2012 STIP inadvertently showed the programming as PS&E instead of CONST; June 2013.)**

➤ Project 21 (PPNO 07-4305) **Withdrawn prior to the CTC Meeting**

135	Advance Financial Allocation: \$4,411,000 for five locally administered STIP projects off the State Highway System, programmed in FY 2013-14. Resolution FP-12-__	2.5c.(4)	Mitchell Weiss Rachel Falsetti	A	D
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No action taken on this Item.

136	Advance Financial Allocation: \$9,066,000 for seven locally administered STIP Transportation Enhancement projects off the State Highway System, programmed in FY 2013-14, FY 2014-15, FY 2015-16 and FY 2016-17. Contributions from other sources: \$1,399,305. Resolution FP-12-66 <i>(Related Item under Tabs 59 &amp; 66.) 2.2c.(5) &amp; 2.2c.(12)</i>	2.5c.(5)	Mitchell Weiss Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 136-138

**Action Taken:** Approved

**Motion:** Dunn                      **Second:** Burke                      **Vote result:** 9-0                      **Absent:** Tavaglione

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

Advance Financial Allocation for locally administered STIP TE projects off the SHS.

--Revise Attachment and Vote List for Project 2 (PPNO 07-4540) to correct Outcome/Output as: ~~This project will improve the pedestrian and bicycle mobility on Asher Avenue and connect the residents of south Taft to the Rails to Trails project.~~ **This project is instrumental in the City's efforts to create a continuous and consistent streetscape which improves the visual and physical environment for all modes of travel on Atlantic Avenue.**

Tab #	Item Description	Ref. #	Presenter	Status*	
137	Advance Financial Allocation: \$9,643,000 for 18 local STIP Planning, Programming, and Monitoring projects, programmed in FY 2013-14. Resolution FP-12-67	2.5c.(6)	Mitchell Weiss Rachel Falsetti	A	D

**Recommendation:** Approval of Items 136-138

**Action Taken:** Approved

**Motion:** Dunn

**Second:** Burke

**Vote result:** 9-0

**Absent:** Tavaglione

*Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:*

Advance Financial Allocation for local STIP Planning, Programming, and Monitoring projects

--Revise Attachment and Vote List; to correct PPNO for Project 6 (PPM for Transportation Authority of Marin; Project ID 0413000381)) from ~~04-Various~~ to **04-2127C**.

138	Advance Financial Allocation: \$500,000 for the East 17 <sup>th</sup> Streetscape Improvements (PPNO 2134) locally administered STIP TE project in Orange County, off the State Highway System, programmed in FY 14-15. Contributions from other sources: \$116,300. Resolution FP-12-__	2.5c.(7)	Mitchell Weiss Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 136-138

**Action Taken:** Approved

**Motion:** Dunn

**Second:** Burke

**Vote result:** 9-0

**Absent:** Tavaglione

<b>Financial Allocations for Projects with Costs that Exceed 120 Percent of the Programmed Amount</b>					
139	Financial Allocation: \$8,800,000 for one SHOPP project with costs that exceed 20 percent of the programmed amount. 02-Sha-5 – SHOPP project in Shasta County. Current programmed amount is \$6,100,000 and the current estimate is \$8,800,000, for an increase of 44.3 percent over the programmed amount. Resolution FA-12-26	2.5d.	Mitchell Weiss John Bulinski	A	D

**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Earp

**Second:** Assemi

**Vote result:** 7-2

**Absent:** Tavaglione

<b>Financial Allocations for Supplemental Funds</b>					
140	Financial Allocation: \$810,000 in supplemental funds for the previously voted SHOPP Roadway Rehabilitation (PPNO 9421) project in Stanislaus County to complete construction. The current SHOPP allocation is \$5,194,000. This request for \$810,000 results in an increase of 15.6 percent over the current allocation. Resolution FA-12-27	2.5e.	Mitchell Weiss Carrie Bowen	A	D

**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Assemi

**Second:** Earp

**Vote result:** 9-0

**Absent:** Tavaglione

<b>Financial Allocations for Proposition 1B TCIF Projects</b>					
141	Financial Allocation: \$9,678,000 for the Pioneer Bluff Bridge (PPNO TC92) locally administered TCIF project, in Yolo County, off the State Highway System. Contributions from other sources: \$12,847,000. Resolution TCIF-A-1213-12	2.5g.(5a)	Stephen Maller Rachel Falsetti	A	D

**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Alvarado

**Second:** Burke

**Vote result:** 9-0

**Absent:** Tavaglione

Tab #	Item Description	Ref. #	Presenter	Status*	
142	Financial Allocation: \$5,800,000 for the Washington Boulevard Widening and Reconstruction (PPNO 3085) locally administered TCIF project, in Los Angeles County, off the State Highway System. Contributions from other sources: \$26,200,000. Resolution TCIF-A-1213-13	2.5g.(5b)	Stephen Maller Rachel Falsetti	A	D

Commissioner Inman recused herself from action on this item.

**Recommendation:** Approval to defer item

**Action Taken:** Approved deferral of item

**Motion:** Alvarado

**Second:** Earp

**Vote result:** 8-0

**Absent:** Tavaglione

**Recuse:** Inman

143	Financial Allocation: \$17,696,000 for the Magnolia Avenue Railroad Grade Separation (PPNO 1129) locally administered TCIF project, in Riverside County, off the State Highway System. Contributions from other sources: \$33,936,000. Resolution TCIF-A-1213-14 <i>(Related Item under Tab 117.) 2.1c.(5e)</i>	2.5g.(5c)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 143-147

**Action Taken:** Approved

**Motion:** Frommer

**Second:** Dunn

**Vote result:** 9-0

**Absent:** Tavaglione

144	Financial Allocation: \$13,247,000 for the Clay Street Railroad Grade Separation (PPNO 1126) locally administered TCIF project, in Riverside County, off the State Highway System. Contributions from other sources: \$17,559,000. Resolution TCIF-A-1213-15 <i>(Related Item under Tab 116.) 2.1c.(5d)</i>	2.5g.(5d)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 143-147

**Action Taken:** Approved

**Motion:** Frommer

**Second:** Dunn

**Vote result:** 9-0

**Absent:** Tavaglione

145	Financial Allocation: \$10,000,000 for the Sunset Avenue Grade Separation (PPNO 1122) locally administered TCIF project, in Riverside County, off the State Highway System. Contributions from other sources: \$24,764,000. Resolution TCIF-A-1213-16 <i>(Related Item under Tab 117.) 2.1c.(5e)</i>	2.5g.(5e)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 143-147

**Action Taken:** Approved

**Motion:** Frommer

**Second:** Dunn

**Vote result:** 9-0

**Absent:** Tavaglione

146	Financial Allocation: \$10,000,000 for the Avenue 52 Grade Separation on Yuma Subdivision of UPRR Mainline (PPNO 1142) locally administered TCIF project, in Riverside County, off the State Highway System. Contributions from other sources: \$19,866,000. Resolution TCIF-A-1213-17 <i>(Related Item under Tab 120.) 2.1c.(5h)</i>	2.5g.(5f)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 143-147

**Action Taken:** Approved

**Motion:** Frommer

**Second:** Dunn

**Vote result:** 9-0

**Absent:** Tavaglione

Tab #	Item Description	Ref. #	Presenter	Status*	
147	Financial Allocation: \$15,066,000 for the ACE: Avenue 56 Railroad Grade Separation (UP) (PPNO 1124) locally administered TCIF project, in Riverside County, off the State Highway System. Contributions from other sources: \$16,592,000. Resolution TCIF-A-1213-18 <i>(Related Item under Tab 115.) 2.1c.(5c)</i>	2.5g.(5g)	Stephen Maller Rachel Falsetti	A	D

**Recommendation:** Approval of Items 143-147

**Action Taken:** Approved

**Motion:** Frommer                      **Second:** Dunn                      **Vote result:** 9-0                      **Absent:** Tavaglione

148	Financial Allocation: \$8,885,000 for the Lenwood Road Railroad Grade Separation (PPNO 1135) locally administered TCIF project, in San Bernardino County, off the State Highway System. Contributions from other sources: \$22,878,000. Resolution TCIF-A-1213-19 <i>(Related Item under Tab 124.) 2.1c.(5m)</i>	2.5g.(5h)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval to defer Item

**Action Taken:** Approved deferral of Item

**Motion:** Burke                      **Second:** Frommer                      **Vote result:** 9-0                      **Absent:** Tavaglione

149	Financial Allocation: \$24,713,000 for the Laurel Street Grade Separation (PPNO 1141) locally administered TCIF project, in San Bernardino County, off the State Highway System. Contributions from other sources: \$35,142,000. Resolution TCIF-A-1213-20 <i>(Related Item under Tabs 119 &amp; 97) 2.1c.(5g) &amp; 4.15</i>	2.5g.(5i)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 149-150

**Action Taken:** Approved

**Motion:** Assemi                      **Second:** Burke                      **Vote result:** 8-0                      **Absent:** Alvarado, Tavaglione

150	Financial Allocation: \$28,213,000 for the ACE: South Milliken Avenue Railroad Grade Separation (PPNO 1131) locally administered TCIF project, in San Bernardino County, off the State Highway System. Contributions from other sources: \$53,803,000. Resolution TCIF-A-1213-21 <i>(Related Item under Tabs 97, 119 &amp; 162.) 4.15 &amp; 2.1c.(5g); 2.5g.(10a)</i>	2.5g.(5j)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 149-150

**Action Taken:** Approved

**Motion:** Assemi                      **Second:** Burke                      **Vote result:** 8-0                      **Absent:** Alvarado, Tavaglione

151	Financial Allocation: \$39,519,000 for the Lakeview Avenue Grade Separation (PPNO TC40) locally administered TCIF project, in Orange County, off the State Highway System. Contributions from other sources: \$60,244,000. Resolution TCIF-A-1213-22	2.5g.(5k)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval to defer Item

**Action Taken:** Approved deferral of Item

**Motion:** Burke                      **Second:** Guardino                      **Vote result:** 8-0                      **Absent:** Alvarado, Tavaglione

Tab #	Item Description	Ref. #	Presenter	Status*	
152	Financial Allocation: \$35,890,000 for the State College Boulevard Grade Separation (PPNO TC35) locally administered TCIF project off the State Highway System. Contributions from other sources: \$38,754,000. Resolution TCIF-A-1213-23	2.5g.(5l)	Stephen Maller Rachel Falsetti	A	D

**Recommendation:** Approval

**Action Taken:** Approved

**Motion:** Dunn

**Second:** Frommer

**Vote result:** 8-0

**Absent:** Alvarado, Tavaglione

153	Financial Allocation: \$10,880,000 for the Richmond Rail Connector (PPNO 0241B) locally administered TCIF Rail project, in Contra Costa County. Contributions from other sources: \$11,770,000. Resolution TCIF-A-1213-24	2.5g.(5m)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval to defer Item

**Action Taken:** Approved deferral of Item

**Motion:** Burke

**Second:** Assemi

**Vote result:** 8-0

**Absent:** Alvarado, Tavaglione

154	Financial Allocation: \$20,712,000 for the POLA Alameda Corridor West Terminus Intermodal Rail yard (TraPac Terminal On-Dock Rail yard) (PPNO TC86) locally administered TCIF Rail project, in Los Angeles County. Contributions from other sources: \$52,275,230. Resolution TCIF-A-1213-25 <i>(Related Item under Tab 121.) 2.1c.(5i)</i>	2.5g.(5n)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 154-158

**Action Taken:** Approved

**Motion:** Earp

**Second:** Dunn

**Vote result:** 8-0

**Absent:** Alvarado, Tavaglione

155	Financial Allocation: \$26,664,000 for the POLA Cargo Transportation Improvements – Emission Reduction Program– Phase 2 (TraPac Automation) (PPNO TC87B) locally administered TCIF Rail project, in Los Angeles County. Contributions from other sources: \$116,336,000. Resolution TCIF-A-1213-26 <i>(Related Item under Tab 122.) 2.1c.(5j)</i>	2.5g.(5o)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 154-158

**Action Taken:** Approved

**Motion:** Earp

**Second:** Dunn

**Vote result:** 8-0

**Absent:** Alvarado, Tavaglione

156	Financial Allocation: \$96,820,000 for the State Route 4 Crosstown Freeway Extension (PPNO 0284) locally administered TCIF project, in San Joaquin County, on the State Highway System. Contributions from other sources: \$37,220,000. Resolution TCIF-A-1213-27	2.5g.(5p)	Stephen Maller Rachel Falsetti	A	D
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**Recommendation:** Approval of Items 154-158

**Action Taken:** Approved

**Motion:** Earp

**Second:** Dunn

**Vote result:** 8-0

**Absent:** Alvarado, Tavaglione

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

Financial Allocation: \$96,820,000 for the State Route 4 Crosstown Freeway Extension (PPNO 0284) ~~locally~~ **State** administered TCIF project, in San Joaquin County, on the State Highway System. Contributions from other sources: \$37,220,000.

--Revise Agenda Language, Book Item, Attachment, and Vote List; correct as ~~locally~~ **State** administered and correct recipient as ~~San Joaquin Council of Governments~~ **Department of Transportation**.



Tab #	Item Description	Ref. #	Presenter	Status*	
161	Financial Allocation: \$8,500,000 for the 04-Solano-80 – I-80/SR 12 SHOPP/TCIF project in Solano County. (TCIF Project 89). Resolution FP-12-74 Resolution TCIF-A-1213-33	2.5b.(4c)	Stephen Maller Rachel Falsetti	A	D

**Recommendation:** Approval to defer Items 159-161

**Action Taken:** Approved deferral of Item

**Motion:** Dunn

**Second:** Assemi

**Vote result:** 8-0

**Absent:** Alvarado, Tavaglione

<b>Financial Allocations for Proposition 1B SLPP Projects</b>					
162	Financial Allocation: \$70,517,000 for 36 locally administered SLPP projects, off the State Highway System. Contributions from other sources: \$172,941,553. Resolution SLP1B-A-1213-26 <i>(Related Items under Tabs 60, 64, 62, 63, 65 &amp; 103) 2.2c.(6), 2.2c.(10), 2.2c.(8), 2.2c.(9), 2.2c.(11) and 4.8</i>	2.5g.(10a)	Laurel Janssen Denix Anbiah	A	D

Commissioner Inman recused herself from action on this item.

**Recommendation:** Approval on Project 121 and 122

**Action Taken:** Approved

**Motion:** Dunn

**Second:** Burke

**Vote result:** 8-0

**Absent:** Tavaglione

**Recuse:** Inman

**Recommendation:** Approval of Florin Elk Grove East Stockton, Nelson Lane, and Baseline Greenspot projects

**Action Taken:** Approved

**Motion:** Dunn

**Second:** Assemi

**Vote result:** 7-0

**Absent:** Alvarado, Tavaglione

**Recuse:** Inman

*Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:*

Financial Allocation for locally administered SLPP projects off the SHS.

**YELLOW BOOK ITEM  
PINK REPLACEMENT ITEM**

163	Financial Allocation: \$10,921,000 for the Downtown San Bernardino Passenger Rail locally administered SLPP Transit project, in San Bernardino County. Contributions from other sources: \$55,426,000. Resolution SLP1B-A-1213-27	2.5g.(10b)	Laurel Janssen Denix Anbiah	A	D
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**Recommendation:** Approval to Items 163-165

**Action Taken:** Approved

**Motion:** Frommer

**Second:** Dunn

**Vote result:** 8-0

**Absent:** Alvarado, Tavaglione

164	Financial Allocation: \$1,000,000 for the Kings Beach Commercial Core Improvement locally administered SLPP project in Placer County, on the State Highway System. Contributions from other sources: \$22,857,000. Resolution SLP1B-A-1213-28 <i>(Related Item under Tab 30) 2.1a.(3)</i>	2.5g.(10c)	Laurel Janssen Denix Anbiah	A	D
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**Recommendation:** Approval to Items 163-165

**Action Taken:** Approved

**Motion:** Frommer

**Second:** Dunn

**Vote result:** 8-0

**Absent:** Alvarado, Tavaglione

165	Financial Allocation: \$34,789,000 for two State administered SLPP projects, on the State Highway System. Contributions from other sources: \$42,344,000 Resolution SLP1B-A-1213-29	2.5g.(10d)	Laurel Janssen Denix Anbiah	A	D
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Tab #	Item Description	Ref. #	Presenter	Status*	
<p><b>Recommendation:</b> Approval to Items 163-165  <b>Action Taken:</b> Approved  <b>Motion:</b> Frommer                      <b>Second:</b> Dunn                      <b>Vote result:</b> 8-0                      <b>Absent:</b> Alvarado, Tavaglione</p>					
166	Financial Allocation: \$309,000 for the SR 210-Greenspot Road Improvements locally administered SLPP project, in San Bernardino County, on the State Highway System. Contributions from other sources: \$6,822,000. Resolution SLP1B-A-1213-31	2.5g.(10i)	Laurel Janssen Denix Anbiah	A	D

**Recommendation:** Approval  
**Action Taken:** Approved  
**Motion:** Frommer                      **Second:** Dunn                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

*Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:*

Financial Allocation for the SR 210-Greenspot Road Improvements locally administered SLPP project on the SHS **YELLOW BOOK ITEM**

167	Financial Allocation: \$600,000 for the Pleasant Valley/Patterson Drive Signalization State administered SLPP project, in El Dorado County, on the State Highway System. Contributions from other sources: \$1,550,000 Resolution SLP1B-A-1213-32	2.5g.(10j)	Laurel Janssen Denix Anbiah	A	D
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**Recommendation:** Approval of Items 167-168  
**Action Taken:** Approved  
**Motion:** Earp                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

168	Financial Allocation: \$1,000,000 for the I-15-Base Line Road Interchange locally administered SLPP project, in San Bernardino County, on the State Highway System. Contributions from other sources: \$29,722,000 Resolution SLP1B-A-1213-33	2.5g.(10k)	Laurel Janssen Denix Anbiah	A	D
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**Recommendation:** Approval of Items 167-168  
**Action Taken:** Approved  
**Motion:** Frommer                      **Second:** Dunn                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

*Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:*

Financial Allocation for the I-15-Base Line Road Interchange locally administered SLPP project on the SHS **YELLOW BOOK ITEM**

<b>Financial Allocations for STIP Transit Projects</b>					
169	Financial Allocation: \$290,000 for two locally administered STIP transit projects. Resolution MFP-12-09	2.6a.	Juan Guzman Jane Perez	A	D

**Recommendation:** Approval  
**Action Taken:** Approved  
**Motion:** Assemi                      **Second:** Dunn                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

<b>Aeronautic Financial Matters</b>					
170	Financial Allocation for FY 2013-14 Aeronautics Set-Aside to Match Federal Airport Improvement Program Grants Resolution FDOA-2012-02	2.7	Teresa Favila Dennis Jacobs	A	D

This Item was pulled from the agenda.

Tab #	Item Description	Ref. #	Presenter	Status*	
<b>Request to Extend the Period of Project Allocation</b>					
171	Request to extend the period of project allocation for the Capitol Light Rail Transit Extension Phase II – Track, Signal, Stations project (PPNO 2174E) for \$13,000,000 in Santa Clara County, per Resolution G-06-08. Waiver 13-22	2.8a.(1)	Juan Guzman Jane Perez	A	D

**Recommendation:** Approval of Items 171-181 as amended

**Action Taken:** Approved as amended

**Motion:** Assemi                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

Proj #	PPNO	County	Agency	Extension	Recommendations			Notes
				Request	Caltrans	CTC Staff		
1	04-2174E	SCL	SCVTA	20 months	20 months	20 months	Additional studies delayed NEPA approval	

172	Request to extend the period of allocation for 11 locally administered STIP projects for \$5,859,000, per STIP Guidelines Waiver 13-23	2.8a.(2)	Juan Guzman Denix Anbiah	A	D
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**Recommendation:** Approval of Items 171-181 as amended

**Action Taken:** Approved as amended

**Motion:** Assemi                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

Proj #	PPNO	County	Agency	Extension	Recommendations			Notes
				Request	Caltrans	CTC Staff		
1	01-0302I	HUM	Arcata	8 months	8 months	8 months	Additional environmental & public input	
2	01-2257	HUM	Humboldt County	6 months	6 months	6 months	Old survey data delayed RW & design	
3	01-2289	HUM	Blue Lake	4 months	4 months	4 months	Historic structures required redesign	
4	02-2399D	TRI	Trinity County	10 months	10 months	10 months	Additional environmental design delayed	
5	02-3124H	BUT	Butte County	14 months	14 months	14 months	Additional studies delayed NEPA, PS&E	
6	03-3194	YOL	Woodland	20 months	20 months	Deny	Uncertain, lapse and reprogram	
7	06-6555	KER	Taft	8 months	8 months	8 months	Coordination w/overlapping development	
8	09-2034	INY	Inyo County	18 months	18 months	18 months	To align with PS&E and CON timelines	
9	09-2586	INY	Inyo County	18 months	18 months	18 months	To align with PS&E and CON timelines	
10	09-2561	MNO	Mono County	6 months	6 months	6 months	Additional studies	

Tab #	Item Description				Ref. #	Presenter	Status*
11	75-6045K	SOL	Fairfield	12 months	12 months	12 months	delayed CEQA Lack of cert delayed C&M agreements
173	Request to extend the period of project allocation for the Sacramento Maintenance Facility project (PPNO 2095) in Sacramento County, for \$18,850,000, per STIP Guidelines Waiver 13-24				2.8a.(3)	Juan Guzman Bill Bronte	A D

**Recommendation:** Approval of Items 171-181 as amended

**Action Taken:** Approved as amended

**Motion:** Assemi                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

Proj #	PPNO	County	Agency	Extension	Recommendations		Notes
				Request	Caltrans	CTC Staff	
1	75-2095	SAC	Caltrans-Rail	20 months	20 months	20 months	Site selection depends on new HSR MOU

174	Request to extend the period of project allocation for the Stockton Station Relocation project (PPNO 2081) in San Joaquin County, for \$11,400,000 per STIP Guidelines Waiver 13-25				2.8a.(4)	Juan Guzman Bill Bronte	A D
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**Recommendation:** Approval of Items 171-181 as amended

**Action Taken:** Approved as amended

**Motion:** Assemi                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

Proj #	PPNO	County	Agency	Extension	Recommendations		Notes
				Request	Caltrans	CTC Staff	
1	75-2081	SJ	Caltrans-Rail	20 months	20 months	20 months	Site selection depends on new HSR MOU

175	Request to extend the period of project allocation for three locally administered STIP projects on the State Highway System totaling \$174,000, per STIP Guidelines. Waiver 13-26				2.8a.(5)	Juan Guzman Rachel Falsetti	A D
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**Recommendation:** Approval of Items 171-181 as amended

**Action Taken:** Approved as amended

**Motion:** Assemi                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

Proj #	PPNO	County	Agency	Extension	Recommendations		Notes
				Request	Caltrans	CTC Staff	
1	03-3782	GLE-5	Willows	12 months	12 months	12 months	To coordinate w/overlapping CT project
2	03-3783	GLE-5/32	Orland	12 months	12 months	12 months	To coordinate w/overlapping CT project
3	03-3915	GLE-5/162	Willows	12 months	12 months	12 months	To coordinate w/overlapping CT project

Tab #	Item Description	Ref. #	Presenter	Status*	
176	Request to extend the period of project allocation for two State administered STIP projects on the State Highway System totaling \$17,466,000, per STIP Guidelines. Waiver 13-31	2.8a.(6)	Juan Guzman Rachel Falsetti	A	D

**Recommendation:** Approval of Items 171-181 as amended

**Action Taken:** Approved as amended

**Motion:** Assemi                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

Proj #	PPNO	County	Agency	Extension	Recommendations		Notes
				Request	Caltrans	CTC Staff	
1	01-1047	DN-199	Caltrans-STIP	2 months	2 months	2 months	Environmental recirculated for public input
2	04-0789E	SON-101	Caltrans-STIP	6 months	6 months	6 months	Revising metric specs back to English

<b>Request to Extend the Period of Contract Award</b>					
177	Request to Extend the Period of Contract Award for three SHOPP projects for \$40,552,000, per Resolution G-06-08. Waiver 13-27	2.8b.(1)	Juan Guzman Rachel Falsetti	A	D

**Recommendation:** Approval of Items 171-181 as amended

**Action Taken:** Approved as amended

**Motion:** Assemi                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

Proj #	PPNO	County	Agency	Extension	Recommendations		Notes
				Request	Caltrans	CTC Staff	
1	03-3296	ED-50	Caltrans-SHOPP	6 months	6 months	6 months	Not awarded to low bidder due to protests
2	04-5302C	SOL-80	Caltrans-SHOPP	6 months	6 months	6 months	Low bidder non-responsive, to next bidder
3	07-0309N	LA-10	Caltrans-SHOPP	6 months	6 months	6 months	Property transfer delayed by DOF review

Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:

Request to extend the period of contract award for SHOPP projects

- Project 2 (PPNO 04-5302C)

**Withdrawn prior to the CTC Meeting**

178	Request to extend the period of contract award for the Perris Valley Line – Commuter Rail Extension project (PPNO 1114) for \$52,978,000 in Riverside County, per Resolution G-06-08. Waiver 13-32	2.8b.(2)	Juan Guzman Jane Perez	A	D
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**Recommendation:** Approval of Items 171-181 as amended

**Action Taken:** Approved as amended

**Motion:** Assemi                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

Tab #	Item Description	Ref. #	Presenter	Status*	
	<b>Request to Extend the Period of Project Completion</b>				
179	Request to extend the period of project completion for the Branching Out Urban Forestry project (PPNO 4901) in Los Angeles County, for \$889,000, per STIP Guidelines Waiver 13-28	2.8c.	Juan Guzman Denix Anbiah	A	D

**Recommendation:** Approval of Items 171-181 as amended

**Action Taken:** Approved as amended

**Motion:** Assemi                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

Proj #	PPNO	County	Agency	Extension	Recommendations			Notes
				Request	Caltrans	CTC Staff		
1	07-4091	LA	Los Angeles	9 months	10 months	10 months	High local participation requires more time	

*Changes to this item were listed on the pink "Changes to CTC Agenda" handout as follows:*

Request to extend the period of project completion for the Branching Out Urban Forestry project (PPNO 4904 **4091**) in Los Angeles County, for \$889,000, per STIP Guidelines. Waiver 13-28

--Correct PPNO in Agenda Language. Book Item is correct.

	<b>Request to Extend the Project Development Expenditures</b>				
180	Request to extend the period of project development expenditures for the Capitol Expressway Light Rail Extension Phase I - Eastridge Transit Center and Bus Improvements project (PPNO 2174B) in Santa Clara County, per Guidelines for Allocating, Monitoring and Auditing of Funds for Local Assistance Projects. Waiver 13-29	2.8d.(1)	Juan Guzman Jane Perez	A	D

**Recommendation:** Approval of Items 171-181 as amended

**Action Taken:** Approved as amended

**Motion:** Assemi                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

181	Request to extend the period of project development expenditure for six locally administered STIP projects totaling \$4,496,000, per STIP Guidelines Waiver 13-30	2.8d.(2)	Juan Guzman Denix Anbiah	A	D
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**Recommendation:** Approval of Items 171-181 as amended

**Action Taken:** Approved as amended

**Motion:** Assemi                      **Second:** Frommer                      **Vote result:** 7-0                      **Absent:** Alvarado, Burke Tavaglione

<b>OTHER MATTERS / PUBLIC COMMENT</b>					
5:00 PM	<b>Adjourn</b>				

Chair Ghielmetti turned the adjournment over to Commissioner Inman who adjourned the meeting in honor of Donna Lee Andrews at 4:02 PM.

\_\_\_\_\_  
ANDRE BOUTROS, EXECUTIVE DIRECTOR

\_\_\_\_\_  
DATE

**EXECUTIVE DIRECTOR'S REPORT**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.

**COMMISSION REPORTS**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.

# Memorandum

**To:** CHAIR AND COMMISSIONERS

**Date:** August 6, 2013

**From:** Andre Boutros  
Executive Director

**File:** 1.5  
Action

**Subject:** Meeting for Compensation for April 2013 (April 1-30)

Per Government Code Section 14509, each member of the California Transportation Commission (Commission) shall receive a compensation of one hundred dollars (\$100) per day, but not to exceed eight hundred dollars (\$800) for any commission business authorized by the commission during any month, when a majority of the commission approves the compensation by a recorded vote, plus the necessary expenses incurred by the member in the performance of the member's duties. The need for up to eight days per diem per month is unique to the commission in that its members must evaluate projects and issues throughout the state in order to prioritize projects for the state transportation improvement program. These responsibilities require greater time, attention, and travel than local or regional transportation entities which have responsibility only of individual portions of the program.

The following list of meetings is being submitted for Commission approval.

## *Additional Meetings:*

### **Darius Assemi**

- April 26 – Meeting with Carlos Yamzon and Rosa DeLeon Re: Palendale Project. Fresno

### **Yvonne Burke**

- April 5 – Attended Ground Breaking Ceremony for Nogales St. Grade Separation with Norma Macias, Cong Lowenthal and City Representative. City of Industry

### **Lucetta Dunn**

- April 1 – Meeting with Ontario Airport Authority Re: Ontario Airport Issues. Ontario
- April 17 – Meeting with Cori Williams, Townsend and Heather Stratman Re: HSR. Irvine
- April 25 – Meeting with Debbie Dillon, SCAG and Commissioner Inman Re: Planning and Leadership. Tustin
- April 30 – Teleconference with CTC Staff Re: Committee on Aeronautics. Irvine

### **Jim Earp**

- April 9 – Attended BTH CTIP Workgroup Meeting Re: Transportation Plan. Sacramento
- April 19 – Meeting with Daryl Halls and Jim Sperring Re: Jameson Canyon Project. Sacramento

### **James Ghielmetti**

- April 2 – Teleconference with Andre Boutros and Brian Kelly Re: SR-153. Pleasanton
- April 16 – Meeting with CTC Staff Re: Misc. CTC Business. Sacramento.
- April 30 – Meeting with Darryl Halls, Kate Miller and Janet Adams Re: Jameson Canyon. Pleasanton

### **Carl Guardino**

- April 3 – Teleconference with Debbie Hale Re: Transportation Agency for Monterey County. San Jose
- April 10 – Meeting with Supervisor Katy Tang Re: Regional And Local Transportation Priorities. San Francisco
- April 23 – Meetings with Several “Freshman” Legislators Re: Their Transportation Priorities. Sacramento
- April 25 – Speaker at Ca. Asphalt Association Re: Transportation Funding and Statewide Priorities. Ontario
- April 26 – Meeting with Debbie Hale and Eileen Goodwin of TAMC Re: Commuter Rail Extension. San Jose.

### **Fran Inman**

- April 1 – Meeting with John Barna and Mr. Tagami Re: Oakland Project. Oakland
- April 4 – Meeting with Fred Strong Re: Amtrak and Lossan Corridor. Pasadena
- April 5 – Attended Ground Breaking for San Juan Road Project. Salinas
- April 9 – Meeting with Seattle Ports Commissioner John Creighton, Beth Osborne and Greg Edwards Re: West Coast Freight Issues. Washington D.C.
- April 10 – Attended US Senate Subcommittee Freight Hearing Re: Expanding the Panama Canal. Washington D.C.
- April 19 – Teleconference with John Barna Re: Freight and Oakland Project. City of Industry
- April 23 – Speaker at Ca. Freight Advisory Committee Meeting. Sacramento
- April 24 - Teleconference Re: Haagen Smith CARB Panel. City of Industry
- April 30 - Teleconference with CTC Staff Re: Aeronautics Committee. City of Industry

### **Joseph Tavaglione**

- No meetings to report.

# Memorandum

**To:** CHAIR AND COMMISSIONERS

**Date:** August 6, 2013

**From:** Andre Boutros  
Executive Director

**File:** 1.5  
Action

**Subject:** Meeting for Compensation for May 2013 (May 1-30)

Per Government Code Section 14509, each member of the California Transportation Commission (Commission) shall receive a compensation of one hundred dollars (\$100) per day, but not to exceed eight hundred dollars (\$800) for any commission business authorized by the commission during any month, when a majority of the commission approves the compensation by a recorded vote, plus the necessary expenses incurred by the member in the performance of the member's duties. The need for up to eight days per diem per month is unique to the commission in that its members must evaluate projects and issues throughout the state in order to prioritize projects for the state transportation improvement program. These responsibilities require greater time, attention, and travel than local or regional transportation entities which have responsibility only of individual portions of the program.

The following list of meetings is being submitted for Commission approval.

### Regular Commission Meeting Activities:

- May 7 - Commission Meeting in San Francisco (Commissioners Tavaglione and Alvarado were absent. All other Commissioners in attendance all or part of the meeting)

### Additional Meetings:

#### **Darius Assemi**

- May 3 – Teleconference with CTC Staff. Re: Agenda Briefing. Fresno

#### **Lucetta Dunn**

- May 2 – Attended SCAG Regional Conference. Re: “Changing The Way We Pay For Transportation”. Palm Desert
- May 3 – Teleconference with CTC. Re: Agenda Briefing. Irvine
- May 3 - Teleconference with OCTA and Caltrans. Re: CTC Briefing. Irvine

- May 6 – Attended MTC Tour with CTC Staff. Los Angeles
- May 8 – Attended OCBC OCMoves Committee Meeting. Re: CTC Update. Irvine
- May 11 – Meeting With Bill Campbell. Re: TCA. Irvine
- May 14 – Meeting with Ryan Chamberlain. Re: Caltrans “Brown Bag Lunch”. Irvine
- May 15 – Meeting with Walter Rhodes of Edison and Caltrans. Re: Buy America. Rosemead
- May 20 – Meeting with Gary Toebben and Gina Marie Lindsey Re: LAWA and Ontario Airport. Los Angeles
- May 29 – Teleconference with CTC Susan Bransen and Brent Green. Re: Buy America. Irvine
- May 29 – Meeting with Dr. Tom Coats Re: “Lighter than Air Freight Transport for Traffic Relief”. Irvine
- May 30 – Speaker ACC-OC 2013 City Infrastructure Summit. Costa Mesa
- May 31 – Meeting with Alan Murphy, John Wayne Airport. Re: JWA and ONT. Irvine
- May 31 – Meeting with Commissioner Fan Inman and Jeff Kagan of SCE Re: Buy America. Irvine.

### **Jim Earp**

- May 2 – Attended Caltrans Fallen Workers Memorial. Sacramento
- May 6 – Attended MTC Tour with CTC Staff. Los Angeles
- May 28 – Meeting with Debbi Hale Re: Rail Project Funding. Sacramento.

### **James Ghielmetti**

- May 2 – Teleconference with CTC Staff. Re Chair Briefing. Sacramento
- May 2 – Attended Caltrans Fallen Worker’s Memorial Ceremony. Sacramento
- May 6 – Attended MTC tour with CTC Staff. Re: State of Rail in the LA Region. Los Angeles
- May 28 – Teleconference with Debbie Hale. Re: Rail Extension to Salinas. Pleasanton

### **Carl Guardino**

- May 2 –Teleconference with CTC Staff Re: Chair Briefing. San Jose
- May 6 – CTC Commissioners Dinner Re: CTC Meeting. Los Angeles
- May 13 – Meeting with Erica Martinez Re: Speaker Perez’s Team on Legislation. Sacramento
- May 28 – Meeting with SF Mayor Ed Lee’s Transportation Task Force Re: County and Region-Wide Transportation Funding Priorities. San Francisco
- May 28 – Teleconference with CTC Chair James Ghielmetti Re: State Legislative Process. San Jose

### **Fran Inman**

- May 1 – Attended SCAG Annual Meeting. Desert Hot Springs
- May 1 – Teleconference with Mike Miles Re: Upcoming CTC Meeting. City of Industry

- May 2 – Attended World Trade Week Event. Los Angeles
- May 3 – Teleconference with CTC Staff Re: Agenda Briefing. City of Industry
- May 6 – Attended MTC Tour with CTC Staff. Los Angeles
- May 13 – Speaker at CARB Haagen Smith Sustainable Freight Conference. Los Angeles
- May 14 – Teleconference with SCE Officials and Commissioner Dunn Re: Buy America. City of Industry
- May 15- Attended Metrans 2013 CITT State of the Trade & Transportation Industry Meeting. Long Beach
- May 15 – Attended Meeting with Commissioner Dunn and SCE Officials and Caltrans. Re: Buy America. Rosemead
- May 23 – Attended CTF Annual Awards Luncheon with CTC Staff. Sacramento
- May 31 – Teleconference with CTC staff, Caltrans and SCE Officials Re: Buy America. City of Industry

### **Joseph Tavaglione**

- May 2 – Teleconference with CTC Staff. Re: Chair Briefing. Riverside
- May 3 – Teleconference with CTC Staff. Re: Right of Way. Riverside
- May 9 – Attended 91-HOV Task Force Meeting. Riverside
- May 10 – Meeting with Anne Mayer. Riverside
- May 20 – Meeting with Basam Muallem. Re: 91 Freeway. Riverside

# Memorandum

**To:** CHAIR AND COMMISSIONERS

**Date:** August 6, 2013

**From:** Andre Boutros  
Executive Director

**File:** 1.5  
Action

**Subject:** Meeting for Compensation for June 2013 (May 31- June30)

Per Government Code Section 14509, each member of the California Transportation Commission (Commission) shall receive a compensation of one hundred dollars (\$100) per day, but not to exceed eight hundred dollars (\$800) for any commission business authorized by the commission during any month, when a majority of the commission approves the compensation by a recorded vote, plus the necessary expenses incurred by the member in the performance of the member's duties. The need for up to eight days per diem per month is unique to the commission in that its members must evaluate projects and issues throughout the state in order to prioritize projects for the state transportation improvement program. These responsibilities require greater time, attention, and travel than local or regional transportation entities which have responsibility only of individual portions of the program.

The following list of meetings is being submitted for Commission approval.

### *Regular Commission Meeting Activities:*

- June 11 - Commission Meeting in Sacramento (All Commissioners in attendance all or part of the meeting)

### *Additional Meetings:*

#### **Darius Assemi**

- June 7 – Teleconference with CTC Staff Re: Agenda Briefing. Fresno

#### **Yvonne Burke**

- June 7 – Teleconference with Monterey County Transportation Authority, Elousie Rodrigez and Debbie Hale Re: Review of Rail Request Item 53. Los Angeles

- June 7 – Teleconference with Metro Patricia Chen and Robert Naylor Re: CTC Agenda Items, 96 TCIF Funds. Los Angeles
- June 7 – Teleconference with CTC Director Andre Boutros and Commissioners Inman, Assemi and Earp Re: Guidelines for STIP, Buy America Status, 95 SoCal Coalition etc... Los Angeles
- June 10 – Teleconference With CalTrans Mike Miles Re: 405 an I5. Los Angeles
- June 11 – Attended Legislative Breakfast Meeting with CTC Staff and Legislators. Sacramento

### **Lucetta Dunn**

- June 4 – Teleconference with CTC Chair and Commissioner Inman Re: MAP 21 and Buy America. Irvine
- June 5 – Teleconference with OCTA and Caltrans Re: CTC Briefing. Irvine
- June 12 – Attended ACEC Annual Awards Banquet on Behalf of CTC. Newport Beach
- June 14 - Teleconference with Tamara Werkmeiser and Dina Rochford Re: HNTB. Irvine
- June 14 – Teleconference with Paul Granillo, IEEP and Gary Toebben Re: ONT and LAX. Irvine
- June 17 – Ontario Airport Authority Meeting Re: ONT. Ontario Airport
- June 16 – Teleconference with OCBC and ACEC Re: 405 Freeway. Irvine
- June 18 – Teleconference with Darrell Johnsons of OCTA Re: 405 Freeway. Irvine

### **Jim Earp**

- June 7 – Teleconference with Daryl Hills and Jim Spering Re: TCIF Deprogramming Policy. Sacramento
- June 10 – Attended WTS Reception Honoring Andre Boutros. Sacramento
- June 13 – Meeting with Janet Dawson Re: Caltrans Performance Standards. Sacramento

### **James Ghielmetti**

- June 3 – Teleconference with CTC Executive Director Andre Boutros Re: CTC Business. Pleasanton
- June 4 – Teleconference with Commissioners Dunn and Inman Re: Buy America. Pleasanton
- June 6 - Teleconference with CTC Staff and Commissioners Dunn and Inman Re: Buy America. Pleasanton
- June 7 – Teleconference with CTC Staff Re: Legislative Breakfast. Pleasanton
- June 7 – Teleconference with CTC Staff Re: Chair Briefing. Pleasanton
- June 10 – Teleconference with CTC Staff Re: Right of Way Briefing. Pleasanton
- June 11 - Attended Legislative Breakfast Meeting with CTC Staff and Legislators. Sacramento
- June 25 – Meeting with Bill Gray and Mike Evanhoe Re: SR152. Pleasanton

## **Carl Guardino**

- June 4 – Teleconference with Rusty Areias Re: Agenda Items for CTC Meeting. San Jose
- June 7 – Teleconference with CTC Staff Re: Agenda Briefing. San Jose
- June 11 – Attended Legislative Breakfast Meeting with CTC Staff and Legislators. Sacramento
- June 18 – Meeting with Anthony Rendon and Richard Roth Re; Transportation Priorities. Sacramento.
- June 19 – Attended CTC/MTC/VTA/County of Santa Clara/ City of San Jose meeting Re: Transportation Priorities. San Jose
- June 25 – Meeting with SF Mayor Ed Lee Re: Transportation Task Force. San Francisco
- June 27 – Meeting with Barclay’s Executives Re: P-3’s and Toll Roads. San Francisco

## **Fran Inman**

- June 4 – Teleconference with Lucy Dunn and Jim Ghielmetti Re: Buy America Challenges. City of Industry
- June 5 – Teleconference with Mike Miles Re: CTC Hearing Items. City of Industry
- June 6 – Teleconference with Caltrans and CTC Staff Re: Buy America. City of Industry
- June 7 – Teleconference with CTC Staff Re: Ca. Transportation Committee Hearing Agenda. City of Industry
- June 10 – Teleconference with METRO Re: CTC Hearing. City of Industry
- June 14 - Teleconference with Caltrans and CTC Staff Re: Buy America. City of Industry
- June 19 – Attended as Moderator, Panel for Future Ports/Strong Ports Re: Freight Panel. Long Beach
- June 25 – Attended US Transportation Meeting Re: National Freight Advisory Council and Buy America Provisions. Washington D.C.

## **Joseph Tavaglione**

- June 6 – Meeting with Basaam Muallem, Anne Mayer and Ray Wolf Re: 91 Project. Riverside
- June 7 – Teleconference with CTC Staff Re: Chair Briefing. Riverside
- June 10 – Teleconference with CTC Staff Re: Right of Way. Riverside.
- June 13 – HOV Task Force Meeting Re: 91 HOV. Riverside
- June 14 – Attended 10-West Dedication. Yucaipa
- June 25 – Meeting with CalTrans and RCTC Re: 91 Toll Lanes. Riverside
- June 26 – Attended WTS Luncheon. Fontana
- June 28 – Attended Devore Interchange Groundbreaking. Devore

# Memorandum

## Addendum

To: CHAIR AND COMMISSIONERS

Date: August 6, 2013

From: Andre Boutros  
Executive Director

File: 1.5  
Action

**Subject: \$100 PER DAY ATTENDANCE ALLOWANCE FEBRAURY (JAN 31-FEB 28)  
MEETINGS FOR COMPENSATION**

In accordance with the in-house procedure adopted for identifying Commission activities eligible for compensation pursuant to SB 2168, the following list of meetings is being submitted for Commission approval (Commissioners are allowed to be reimbursed for up to eight meetings per month):

**Additional Meetings:**

**James Earp**

- February 4 – Attended Assembly Transportation Committee Hearing. Sacramento
- February 25 – Attended Assembly Transportation Discussion Group. Sacramento
- February 26 – Meeting with Jody Jones and Tom Brennan Re: District 3 Request for Supplemental Funding. Sacramento
- February 27 – Teleconference with Dan Landon Re: Request for Supplemental Funding on SR49. Sacramento
- February 28 – Teleconference with Erica Martinez Re: AB1290. Sacramento

# Memorandum

## Addendum

To: CHAIR AND COMMISSIONERS

Date: August 6, 2013

From: Andre Boutros  
Executive Director

File: 1.5  
Action

Subject: **\$100 PER DAY ATTENDANCE ALLOWANCE MARCH MEETINGS FOR COMPENSATION**

In accordance with the in-house procedure adopted for identifying Commission activities eligible for compensation pursuant to SB 2168, the following list of meetings is being submitted for Commission approval (Commissioners are allowed to be reimbursed for up to eight meetings per month):

### **Additional Meetings:**

#### **James Earp**

- March 1 – Teleconference with CTC Staff Re: Agenda Briefing. Sacramento
- March 12 – Attended BT&H Briefing with Brian Kelly and Bill Gray Re: Proposed SR152 P-3 Project. Sacramento

#### **Joseph Tavaglione**

- March 1 – Meeting with Anne Mayer, Basam Muallem and Ray Wolf. Re: Pre-CTC Meeting. Riverside
- March 12 – Meeting with Ray Wolf. Re: Transportation Issues. Riverside
- March 18 – Attended Celebration of Life for Bimla Rhinehart. Sacramento.
- March 19 – Meeting with Andre Boutros. Re: CTC Issues. Sacramento

**REPORT BY THE CALIFORNIA STATE TRANSPORTATION  
AGENCY SECRETARY AND/OR UNDERSECRETARY**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.

**REPORT BY CALTRANS' DIRECTOR**  
**AND/OR DEPUTY DIRECTOR**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.

**REPORT BY UNITED STATES**  
**DEPARTMENT OF TRANSPORTATION**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.

**REPORT BY REGIONAL  
AGENCIES MODERATOR**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.

**REPORT BY RURAL COUNTIES**  
**TASK FORCE CHAIR**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.

**REPORT BY SELF-HELP COUNTIES**  
**COALITION MODERATOR**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 4.1  
Action

From: ANDRE BOUTROS  
Executive Director

Subject: **STATE AND FEDERAL LEGISLATION**

## **ISSUE:**

Should the California Transportation Commission (Commission):

1. Accept the Staff Report?
2. Provide direction to staff with respect to the legislation identified and monitored by staff? Attachment A identifies 29 bills that met the criteria approved by the Commission. Bills summarized below are highlighted for informational purposes based on their potential impacts to the Commission.
3. Provide recommendations to clarify the objectives of Senate Bill (SB) 416? SB 416, authored by Senator Liu and co-authored by Assemblymember Holden, would make changes governing the sale of surplus residential properties in the State Route (SR) 710 corridor and establish the SR 710 Rehabilitation Account allowing sale proceeds to be continuously appropriated for property repairs. Funds exceeding the amount needed for repairs or remaining after repairs are completed shall be transferred to the State Highway Account to be used to fund projects located in specified communities. Staff recommends modifications to clarify project eligibility and selection.

## **RECOMMENDATION:**

Staff recommends that the Commission:

1. Accept the staff report;
2. Provide direction to staff on legislation of interest to it; and
3. Provide comments to Senator Liu and Assemblymember Holden in response to Senate Bill (SB) 416 as presented in Attachment B.

## **SUMMARY:**

### ***State Legislative Calendar Update***

The Assembly reconvened from summer recess on August 5<sup>th</sup> and the Senate will reconvene on August 12<sup>th</sup>. August 16<sup>th</sup> is the last day for policy committees to meet and report bills, and August 30<sup>th</sup> is the last day for fiscal committees to meet and report bills.

***State Legislation – Bills of Interest***

The following bills are highlighted for informational purposes based on their potential impacts to the Commission:

**Assembly Bill 1290 (J. Perez) - Transportation Planning**

This bill was amended July 2<sup>nd</sup> in the Senate to (1) clarify the development process for sub-regional SCS reports in the SCAG region, (2) provide clarification that SCS reports are non-binding on future plans and funding decisions and are not considered alternatives for the purposes of environmental analysis, and (3) replace individual state agency reports to the Commission with a single, annual report provided by the Strategic Growth Council. AB 1290 is scheduled to be heard in the Senate Appropriations Committee on August 12<sup>th</sup>.

In summary this bill in its present form proposes to:

- Provide 2 additional voting members of the California Transportation Commission to be appointed by the Legislature, and for the Secretary of the Transportation Agency, the Chairperson of the State Air Resources Board, and the Director of Housing and Community Development to serve as ex officio members without a vote.
- Require each appointing power to make every effort to assure that expertise in the transportation community that has not traditionally been represented on the Commission is reflected in future appointments to the Commission with a particular emphasis on stakeholders involved and engaged in efforts to make the State's transportation system more sustainable.
- Provide that the Planning Committee is responsible for monitoring land use and transportation outcomes in accordance with regional Sustainable Communities Strategies.
- Require each Metropolitan Planning Organization to make a biennial report to the Commission beginning on or before October 15, 2014, describing progress and challenges in implementing the Sustainable Communities Strategy and in attaining greenhouse gas emissions reductions, (this report shall not be binding on future plans or funding decisions and shall not constitute an alternative under CEQA).
- Require the Strategic Growth Council to report annually to the Commission at a public hearing by August 15<sup>th</sup> on its statutory charge to identify and review activities and funding programs of its member agencies that may be coordinated to improve sustainability.
- Require the Commission to include in the Annual Report, the Commission and Strategic Growth Council's assessment of state progress in achieving greenhouse gas emissions reductions from land use and transportation planning.
- Require the Regional Transportation Improvement Plan to include a discussion of how the program relates to the Sustainable Communities Strategy.

*Recommended CTC Action: Monitor*

**Senate Bill 416 (Liu, co-author Assemblymember Holden)**

This bill makes changes to the Roberti Act governing the sale of surplus properties in the SR 710 corridor and creates the SR 710 Rehabilitation Account. This bill would require the Department to deposit proceeds from sales of SR 710 properties into the account and would continuously appropriate these funds for the purpose of providing specified repairs to the properties. The total funds maintained in the SR 710 Rehabilitation Account shall not exceed \$500,000. Funds in excess of \$500,000 shall be transferred to the State Highway Account in the State Transportation Fund to be used exclusively to fund eligible projects located in Pasadena, Alhambra, La Canada Flintridge, and the community of El Sereno in the City of Los Angeles. The bill specifies that the funds shall not be used to advance or construct the proposed North State Route 710 tunnel. In its current form the bill does not specify the definition of eligible projects or the process by which eligible projects are selected and approved for funding. To ensure that this legislation is well-positioned to achieve the objective of utilizing excess funding from the sale of SR 710 properties to fund transportation projects in the areas specified, modifications to the bill are necessary to (1) clarify the definition of eligible projects, and (2) clarify the process by which eligible projects are selected and approved for funding.

*Recommended CTC Action:* Provide clarification comments as presented in Attachment B and continue to monitor.

**Senate Bill 486 (DeSaulnier) – : Office of Strategic Assessment and Accountability**

SB 486 was substantially amended in the Assembly on July 3<sup>rd</sup> to no longer create the Office of Legal Compliance and Ethics. This bill in its current form would establish the Office of Strategic Assessment and Accountability within the Transportation Agency. Under the direction of the Deputy Secretary for Strategic Assessment and Accountability, this office would be responsible for ensuring the ongoing performance measurement, transparency, and public accountability of the Department of Transportation. The bill would require the office to issue reports measuring the Department's success in meeting performance benchmarks identified by the Secretary of Transportation, in consultation with the Department Director, and to compile and make the reports publicly available. The bill would require the Secretary, on or before July 1, 2014, and not less than quarterly thereafter, to report to the Commission on the Department's performance.

*Recommended CTC Action:* Monitor

**BACKGROUND:**

The Commission approved criteria to guide Commission staff in monitoring legislation and selecting bills that should be brought forward for Commission consideration. An over-arching criterion is that a bill must directly affect transportation on a statewide basis. Bills meeting one or more of the criteria, provided below, will be brought forward to the Commission for consideration.

- Funding/Financing - funding or a funding mechanism for transportation (capital and operations).
- Environmental Mitigation - implementation of greenhouse gas emissions reduction and transportation (e.g., AB 32), and/or involve the environmental process and transportation (e.g., CEQA).
- Planning - implementation of transportation and land use and planning (e.g., SB 375).
- Project Delivery - changes to the way transportation projects are delivered.

Additional criteria for bringing a bill forward include:

- Direct Impact to Commission - changes in Commission responsibility, policy impact or operations.
- Commissioner Request - recommended by a Commissioner for consideration by the Commission at its next regularly scheduled meeting.

The Commission adopted policy to 1) consider legislation in relation to its overall policy by topic area prior to taking a position on legislation addressing that topic; and 2) remain selective in its use of monitor, support or opposition on a bill. The rationale for a policy by topic area is it permits the Commission to address a suite of legislative proposals dealing with the same topic by commenting to the author(s) without necessarily taking a position. Rather than taking specific positions on bills in their initial state, the Commission can advise the Legislature on a bill's policy and/or technical aspects, as well as how it helps or hinders transportation. The intent of the Commission's comments is to alert the Author of the bill's impact on a policy and/or technical aspect related to transportation planning, programming, financing, mitigation, or project delivery.

Further direction will be provided to staff, by the Chair, on bills that meet the aforementioned criteria.

Attachment A - Status of State and Federal Legislation  
Attachment B – SB 416 Draft Letter & Bill Text

<b>Bill #</b>	<b>Author</b>	<b>Title</b>	<b>Subject</b>	<b>Description</b>	<b>Status</b>
<b><u>AB 14</u></b>	Lowenthal	State Freight Plan	Direct Impact to CTC Planning	This bill would require the Business Transportation and Housing Agency (Agency) to prepare a state freight plan with specified elements to govern the immediate and long-range planning activities and capital investments of the state with respect to the movement of freight. The bill also requires the Agency to establish a freight advisory committee which will include participation from the Commission. The initial state freight plan would be submitted to the Legislature, the Governor, and certain state agencies, including the Commission, by December 31, 2014, and updated every 5 years thereafter.	<b>Last Action</b> In Senate, read second time, to Third Reading July, 2, 2013 <b>Current Location</b> Senate Third Reading File <b>Commission Adopted Position</b> Support position adopted 5-7-13 Support letter issued 5-10-13
<b><u>AB 164</u></b>	Wieckowski	Infrastructure Financing	Funding/Financing	This bill would require a lease agreement between a governmental agency undertaking an infrastructure project and a private entity to include performance bonds as security to ensure the completion of the construction of the facility and payment bonds to secure the payment of claims of laborers, mechanics, and <i>materials suppliers</i> employed on the work under contract.	<b>Last Action</b> Assembly concurred with Senate amendments, to enrollment June 27, 2013 <b>Current Location</b> Enrollment to Governor
<b><u>AB 243</u></b>	Dickinson	Local Government: Infrastructure Financing Districts	Funding/Financing	This bill would authorize the creation of an infrastructure and revitalization financing district and the issuance of debt with 55% voter approval. The bill would authorize a district to finance projects in redevelopment project areas, former redevelopment project areas and former military bases if special conditions are met. The bill would authorize a district to fund various projects including: highways, interchanges, ramps and bridges, arterial streets, parking facilities and transit facilities.	<b>Last Action</b> In Senate, read second time, to Third Reading July 2, 2013 <b>Current Location</b> Senate Third Reading File
<b><u>AB 401</u></b>	Daly	Public Contracts: Design Build: Highway Route 405	Project Delivery	This bill would authorize the Orange County Transportation Authority Design Build Program which would authorize OCTA, until January 1, 2018, based on <i>either best value or lowest responsible bid</i> , to utilize the design-build procurement for the Interstate Highway 405 Improvement Project. Requires the Department of Transportation to provide inspection services. Requires the reimbursement of the Department of Industrial Relations for performing prevailing wage monitoring and enforcement of a public works project.	<b>Last Action</b> In Senate Transportation and Housing Committee, not heard July 9, 2013 <b>Current Location</b> Senate Transportation and Housing Committee

<b>Bill #</b>	<b>Author</b>	<b>Title</b>	<b>Subject</b>	<b>Description</b>	<b>Status</b>
<b>AB 466</b>	Quirk-Silva	Federal Transportation Funds	Funding/Financing	Existing law (Streets and Highways Code Section 182.7) provides for the allocation of federal Congestion Mitigation and Air Quality (CMAQ) funding from the State to metropolitan planning organizations and regional transportation planning agencies pursuant to a formula set forth in United States Code which was removed pursuant to MAP-21 and replaced with a lump sum allocation to be sub-allocated at the discretion of the State. This bill would require the Department to sub-allocate CMAQ funds pursuant to weighted population and air quality factors identical to those previously specified in federal law.	<b>Last Action</b> In Senate, read second time, to third reading July 2, 2013 <b>Current Location</b> Senate Third Reading File
<b>AB 528</b>	Lowenthal	State Rail Plan: <i>High Speed Rail Authority Business Plan</i>	Direct Impact to CTC Planning	This bill would revise the items required to be included in the State Rail Plan and the High Speed Rail (HSR) business plan. The bill would require the State Rail Plan to be submitted to the commission for advice 6 months prior to submitting the final State Rail Plan to the Transportation Agency for approval, and, on or before March 1, 2017, would require the approved State Rail Plan to be submitted to the Legislature, the Governor, and other specified entities. The bill would require the state rail plan to be updated, at a minimum, every 5 years. The bill would change, from January 1 to May 1 of each even-numbered year, the date by which the High-Speed Rail Authority is required to prepare, publish, adopt, and submit the HSR business plan to the Legislature.	<b>Last Action</b> In Senate, read second time, to third reading June 25, 2013 <b>Current Location</b> Senate Third Reading File
<b>AB 680</b>	Salas	<del>Transportation Funds-</del> Transportation: Interregional Road System	As amended, no longer impacting Funding/Financing	<del>This bill would make a technical, non-substantive change to provisions of existing law requiring funds in the State Highway Account to be programmed, budgeted, and expended to maximize the use of federal funds and according to specified sequence of priorities, and to provide information to the Legislature to substantiate the department's proposed capital outlay support budget. Existing law requires certain transportation funds made available for transportation capital improvement projects to be programmed and expended in specified amounts for interregional improvements and regional improvements. Existing law specifies the state highway routes that are included in the interregional road system and the state highway routes that are eligible interregional and intercounty routes. This bill would include State Highway Route 43 as an eligible interregional and intercounty route.</del>	<b>Last Action</b> In Senate Appropriations, to Suspend File July 1, 2013 <b>Current Location</b> Senate Appropriations Committee

<b>Bill #</b>	<b>Author</b>	<b>Title</b>	<b>Subject</b>	<b>Description</b>	<b>Status</b>
<b><u>AB 755</u></b>	Ammiano	Suicide Barriers	Project Delivery	This bill would provide that the construction or reconstruction of a bridge designed for use by motor vehicles shall not be eligible for federal fund apportioned to the state, funds made available from the Highway Users Tax Account, or toll bridge funds unless the planning process for the bridge project takes into account the need for a suicide barrier.	<b>Last Action</b> Passed from Senate Transportation & Housing Committee, as amended, to Appropriations July 2, 2013 <b>Current Location</b> Senate Appropriations Committee
<b><u>AB 1046</u></b>	Gordon	Department of Transportation: Innovative Delivery	Project Delivery	<del>States the intent of the Legislature to enact legislation that would facilitate the implementation of the master agreement executed by the Department of Transportation and the Santa Clara Valley Transportation Authority for the Innovative Delivery Team Demonstration Program. This bill would authorize the department's District 4 director to direct existing District 4 resources to the Innovative Delivery Team Demonstration Program and to authorize department staff to perform reimbursed work for projects on and off the state highway system within the boundaries of the County of Santa Clara pursuant to the master agreement, as defined, and accompanying work programs, as defined.</del>	<b>Last Action</b> Passed from Senate Transportation & Housing Committee to Appropriations July 9, 2013 <b>Current Location</b> Senate Appropriations Committee
<b><u>AB 1070</u></b>	Frazier	California Transportation Financing Authority	Funding/Financing	This bill relates to the Transportation Financing Authority Act and the Transportation Financing Authority. This bill would provide for the roles of the authority and an issuer of bonds under the act if the project sponsor, rather than the authority, is the issuer of bonds.	<b>Last Action</b> Passed from Senate Governance & Finance Committee to Appropriations Committee July 3, 2013 <b>Current Location</b> Senate Appropriations Committee

<b>Bill #</b>	<b>Author</b>	<b>Title</b>	<b>Subject</b>	<b>Description</b>	<b>Status</b>
<b><u>AB 1081</u></b>	Medina	Economic Development: Goods-Movement Infrastructure	Funding/Financing Planning	Existing law requires the Governor, in conjunction with the Governor's Budget, to submit annually to the Legislature a proposed 5-year infrastructure plan containing specified information concerning infrastructure needed by state agencies, public schools, public post secondary educational institutions and a proposal for funding the needed infrastructure. This bill would require the infrastructure plan to include information related to infrastructure identified by state and federal transportation authorities and recommendations for private sector financing as specified.	<b>Last Action</b> In Senate, read second time and amended, referred to Appropriations Committee July 9, 2013 <b>Current Location</b> Senate Appropriations Committee
<b><u>AB 1194</u></b>	Ammiano and V.M Perez	Safe Routes to School Program	Funding/Financing	This bill would provide that the program may fund both construction and noninfrastructure activities, as specified. The bill would require the program to be funded by an annual appropriation in the budget act of not less than \$46,000,000, consisting of federal and state transportation funds eligible to be expended for this purpose. The bill would require 20% of program funds to be used for noninfrastructure activities, as specified. The bill would authorize the transfer of the responsibility for selecting projects and awarding grants from the Department of Transportation to the California Transportation Commission, at the discretion of the Transportation Agency. The bill would require the Department of Transportation to employ a full time coordinator to administer the program. The bill would also delete references to a superseded federal transportation act.	<b>Last Action</b> In Senate Transportation and Housing Committee, not heard July 2, 2013 <b>Current Location</b> Senate Transportation and Housing Committee

<b>Bill #</b>	<b>Author</b>	<b>Title</b>	<b>Subject</b>	<b>Description</b>	<b>Status</b>
<b><u>AB 1290</u></b>	J. Perez	Transportation Planning	Direct Impact to CTC	(1) Provide 2 additional voting members of the California Transportation Commission to be appointed by the Legislature, and for the Secretary of the Transportation Agency, the Chairperson of the State Air Resources Board, and the Director of Housing and Community Development to serve as ex officio members without a vote. (2) Require <i>each appointing power</i> to make every effort to assure that expertise in the transportation community that has not traditionally been represented on the Commission is reflected in future appointments to the Commission with a particular emphasis on stakeholders involved and engaged in efforts to make the State's transportation system more sustainable. (3) Provide that the Planning Committee is responsible for monitoring land use and transportation outcomes in accordance with regional Sustainable Communities Strategies. (4) Require each Metropolitan Planning Organization to make a biennial report to the Commission describing progress and challenges in implementing the Sustainable Communities Strategy and in attaining greenhouse gas emissions reductions, beginning on or before October 15, 2014 ( <i>this report shall not be binding on future plans or funding decisions and the report shall not constitute an alternative under</i>	<b>Last Action</b> Passed from Senate Transportation and Housing Committee to Appropriations July 9, 2013 <b>Current Location</b> Senate Appropriations Committee
<b><u>ACA 8</u></b>	Blumenfield	Local Government Financing: Voter Approval	Funding/Financing	This measure would lower to 55% the voter-approval threshold for a city, county, or city and county to incur bonded indebtedness in the form of general obligation bonds to fund specified public improvements and facilities including transportation infrastructures, streets and roads, sidewalks, transit systems, highways, freeways etc.	<b>Last Action</b> In Senate, re-referred to Appropriations Committee June 27, 2013 <b>Current Location</b> Senate Appropriations Committee
<b><u>AJR 6</u></b>	Fox	Unmanned Aircraft Systems: Test Sites	Aeronautics	This measure would request the Federal Aviation Administration to consider California as one of the 6 planned test sites for unmanned aircraft systems and integration of those systems into the next generation air transportation system.	<b>Last Action</b> In Senate, read third time, adopted July 8, 2013 <b>Current Location</b> Enrollment

<b>Bill #</b>	<b>Author</b>	<b>Title</b>	<b>Subject</b>	<b>Description</b>	<b>Status</b>
<b><u>SB 1</u></b>	Steinberg	Sustainable Communities Investment Authority	Funding/Financing Planning	<p>This bill would authorize certain public entities of a Sustainable Communities Investment Area, as described, to form a Sustainable Communities Investment Authority (authority) to carry out the Community Redevelopment Law in a specified manner. The bill would require the authority to adopt a Sustainable Communities Investment Plan for a Sustainable Communities Investment Area and authorize the authority to include in that plan a provision for the receipt of tax increment funds provided that certain economic development and planning requirements are met. The bill would authorize the legislative body of a city or county forming an authority to dedicate any portion of its net available revenue, as defined, to the authority through its Sustainable Communities Investment Plan. The bill would require the authority to contract for an independent financial and</p>	<p><b>Last Action</b>                      Passed from Assembly Housing &amp; Community Development to Local Government Committee July 3, 2013  <b>Current Location</b>                      Assembly Local Government Committee</p>
<b><u>SB 15</u></b>	Padilla	Aviation: Unmanned Aircraft Systems	Aeronautics	<p>This bill would, under existing civil and criminal provisions, provide that engaging in the prohibited activities with devices or instrumentalities affixed to or contained within an unmanned aircraft system is included within the prohibitions. With respect to the criminal provisions, the bill would impose a state mandated local program by changing the definition of a crime. This bill would also provide that an unmanned aircraft system may not be equipped with a weapon. This bill would define "unmanned aircraft system" for all of these purposes. This bill would additionally require that an application for a search warrant specify if an unmanned aircraft system, as defined, will be used in the execution of the search warrant, and the intended purpose for which the unmanned aircraft system will be used.</p> <p>This bill would also state the intent of the Legislature to enact legislation that would establish appropriate standards for the use of unmanned aircraft systems.</p>	<p><b>Last Action</b>                      In Assembly Public Safety Committee, not heard July 2, 2013  <b>Current Location</b>                      Assembly Public Safety Committee</p>
<b><u>SB 33</u></b>	Wolk and Frazier	Infrastructure Financing Districts: Voter Approval	Funding/Financing	<p>This bill would revise provisions governing infrastructure financing districts. This bill would eliminate the requirement of voter approval for creation of the district and for bond issuance, and would authorize the legislative body to create the district subject to specified procedures. This bill would authorize the creation of such district subject to specified procedure and would authorize a district to finance specified actions and projects including: : highways, interchanges, ramps and bridges, arterial streets, parking facilities and transit facilities.</p>	<p><b>Last Action</b>                      Passed from Assembly Appropriations Committee July 3, 2013  <b>Current Location</b>                      Assembly Second Reading File</p>

<b>Bill #</b>	<b>Author</b>	<b>Title</b>	<b>Subject</b>	<b>Description</b>	<b>Status</b>
<b>SB 110</b>	<i>De Saulnier Steinberg</i>	<i>East Span, SFOBB Safety Review Task Force California Transportation Commission Guidelines</i>	Project Delivery	<del>This bill would establish specific procedures that the commission would be required to utilize when it adopts guidelines, except as specified, and would exempt the adoption of those guidelines from the requirements of the Administrative Procedures Act. Similar Bills: SB 1348 (Steinberg, 2010) — Vetted by Governor on September 30, 2010 and SB 126 (Steinberg, 2011) — Amended to relate to agriculture labor relations — This bill would establish the East Span, San Francisco-Oakland Bay Bridge Safety Review Task Force to access the anticipated seismic structural performance of the East Span of the bridge by conducting a series of specified reviews. Requires the Bay Area Toll Authority to reimburse the State Highway Account for all funds expended for purposes of the task force. Provides that the records of the task force are subject to public disclosure. Provides for indemnification of task force members.</del>	<b>Last Action</b> In Assembly, read second time and amended, referred to Appropriations Committee July 3, 2013 <b>Current Location</b> Assembly Appropriations Committee
<b>SB 416</b>	Liu	Surplus Residential Property	Funding/Financing	This bill makes changes to the Roberti Act governing the sale of surplus properties in the State to (SR) 710 corridor and creates the SR 710 Rehabilitation Account. This bill would require the Department to deposit proceeds from sales of SR 710 properties into the account and would continuously appropriate these funds for the purpose of providing specified repairs to the properties. The total funds maintained in the SR 710 Rehabilitation Account shall not exceed \$500,000, funds exceeding that amount shall be transferred to the State Highway Account in the State Transportation Fund to be used exclusively to fund eligible projects located in Pasadena, Alhambra, La Canada Flintridge, and the community of El Sereno in the City of Los Angeles. The bill specifies that the funds shall not be used to advance or construct the proposed North State Route 710 tunnel. In its current form the bill does not specify the definition of eligible projects or the process by which eligible projects are selected and approved for funding.	<b>Last Action</b> Passed from Assembly Committee on Housing and Community Development to Transportation July 3, 2013 <b>Current Location</b> Assembly Transportation Committee

Bill #	Author	Title	Subject	Description	Status
<b>SB 425</b>	DeSaulnier	Public Works Peer Review Act of 2013	Project Delivery	This bill would enact the Public Works Peer Review Act of 2013 and would allow a public agency, principally tasked with administering, planning, developing, and operating a public works project, to establish a specified peer review group, as defined, and would require the administering agency, if a peer review group is established, to draft a charter, published on the agency's website, related to the duties of the peer review group.	<p><b>Last Action</b>                      Passed from Assembly Committee on Accountability and Administrative Review to Appropriations July 3, 2013</p> <p><b>Current Location</b>                      Assembly Appropriations Committee</p>
<b>SB 486</b>	DeSaulnier	<del>Office of Legal Compliance and Ethics</del> -Office of Strategic Assessment and Accountability	Direct Impact to the Commission	<p><del>This bill would create the Office of Legal Compliance and Ethics (office) within the agency and require the director of the office to organize the office with the approval of the audit committee, as defined. Creates the Office of Strategic Assessment and Accountability, which would be responsible for ensuring the ongoing performance measurement, transparency, and public accountability of the Department. Requires the office to issue reports measuring the department's success in meeting performance benchmarks and to compile and make such reports publicly available and to report at least quarterly to the Commission beginning July 2014.</del></p>	<p><b>Last Action</b>                      In Assembly, read second time and amended, referred to Assembly Transportation Committee July 3, 2013</p> <p><b>Current Location</b>                      Assembly Transportation Committee</p>

<b>Bill #</b>	<b>Author</b>	<b>Title</b>	<b>Subject</b>	<b>Description</b>	<b>Status</b>
<b><u>SB 731</u></b>	Steinberg	Environment: California Environmental Quality Act	Environment	<p>This bill would enact the "CEQA Modernization Act of 2013" which would revise the California Environmental Quality Act to, among other things, provide greater certainty for smart infill development. States the intent of the Legislature to provide funds annually to the Strategic Growth Council for the purposes of providing planning incentive grants to local and regional agencies to implement Sustainable Communities Strategies. May 7th version adds a requirement for the Attorney General to annually submit to the Legislature a report containing specified information on CEQA litigation in the state, removes reference to funding from the Alternative and Renewable Fuel and Vehicle Technology Fund, and would require appropriation of funds by the Legislature for the Strategic Growth Council.</p>	<p><b>Last Action</b>                      Passed from Assembly Committee on Natural Resources, as amended, Local Government Committee                      July 1, 2013  <b>Current Location</b>                      Assembly Second Reading File</p>
<b><u>SB 788</u></b>	Senate Transportation and Housing Committee	Transportation	Direct Impact to CTC	<p>This bill would make changes to several sections of law relating to transportation. Specifically of interest to the Commission, this bill would allow the Commission to relinquish the following State Routes: 25, 68, 74, and 86. <del>This bill also specifies that the relinquishments must be done at no cost to the state unless the Commission makes a finding of need.</del></p>	<p><b>Last Action</b>                      Passed from Assembly Transportation Committee to Natural Resources Committee                      June 17, 2013  <b>Current Location</b>                      Assembly Natural Resources Committee</p>

<b>Bill #</b>	<b>Author</b>	<b>Title</b>	<b>Subject</b>	<b>Description</b>	<b>Status</b>
<b><u>SB 811</u></b>	Lara	State Highway Route 710	Project Delivery	<p>Imposes various requirements on the Department of Transportation with respect to the I-710 expansion project in the County of Los Angeles. The bill would require the lead agency, in consultation with all interested community organizations, to <i>include</i>, within the environmental review process for the project, alternatives to address the air quality, public health, and mobility impacts the project will have on neighboring communities, <i>including, in its entirety, Community Alternative 7, as defined, as a complete project alternative</i>. The bill would require the final environmental document approved by the lead agency to include an investment in identified mitigation measures <i>and community benefits</i> for the affected communities and the Los Angeles River. The bill would require the lead agency to submit a report in that regard to the Legislature at least 90 days prior to approving the final environmental document for the project, <i>and would require the Senate Committee on Transportation and Housing and the Assembly Committee on Transportation to hold a public joint hearing on the proposed final environmental document at least 60 days prior to approval of the final environmental document</i>.</p>	<p><b>Last Action</b>                      In Assembly Transportation Committee, not heard July 1, 2013  <b>Current Location</b>                      Assembly Transportation Committee                      Will be Heard in Committee August 12, 2013</p>
<b><u>HR 711</u></b>	Foxx	Federal Transportation Projects and Wage Requirements	Project Delivery	<p>This bill would enact the "Highway Trust Fund Reform Act of 2013" and would amend Titles 23 and 49 of the United States Code to repeal wage requirements applicable to laborers and mechanics employed on Federal-aid highway and public transportation construction projects</p>	<p><b>Last Action</b>                      Referred to Committee February 15, 2013  <b>Current Location</b>                      House Committee on Transportation and Infrastructure: referred to Subcommittee on Highways and Transit</p>
<b><u>HR 1419</u></b>	Hahn	Bridge Repair and Reconstruction Grants	Funding/Financing	<p>This bill would enact the "Bridge to Jobs Act" and would provide funding to each state to cover all the costs to repair or reconstruct a bridge determined by the Federal Highway Administration to be structurally deficient.</p>	<p><b>Last Action</b>                      Referred to Committee April 10, 2013  <b>Current Location</b>                      House Committee on Transportation and Infrastructure: referred to Subcommittee on Highways and Transit</p>

<b>Bill #</b>	<b>Author</b>	<b>Title</b>	<b>Subject</b>	<b>Description</b>	<b>Status</b>
<b><u>HR 1544</u></b>	Petri	Urban and Regional Rail Corridor Development	Funding/Financing	This bill would enact the "National High Performance Passenger Rail Transportation-Oriented Development Act of 2013" which would promote transportation-oriented development and encourage dedicated revenue sources for urban and regional rail corridor development.	<p><b>Last Action</b>                      Referred to Committee April 15, 2013</p> <p><b>Current Location</b>                      House Committee on Transportation and Infrastructure; referred to Subcommittees on Highways &amp; Transit and Railroads, Pipelines &amp; Hazardous Materials</p>
<b><u>S 208</u></b>	Feinstein	Helicopter Noise Reduction Regulations	Aeronautics	This bill would require the Federal Aviation Administration to prescribe regulations to reduce helicopter noise pollution in residential areas in Los Angeles County, California.	<p><b>Last Action</b>                      Referred to Committee February 4, 2013</p> <p><b>Current Location</b>                      Senate Committee on Commerce, Science, and Transportation</p>

JAMES C. GHIEMMETTI, Chair  
 CARL GUARDINO, Vice Chair  
 BOB ALVARADO  
 DARIUS ASSEMI  
 YVONNE B. BURKE  
 LUCETTA DUNN  
 JAMES EARP  
 DARIO FROMMER  
 FRAN INMAN  
 JOSEPH TAVAGLIONE

STATE OF CALIFORNIA

EDMUND G. BROWN Jr., Governor



SENATOR MARK DESAULNIER, Ex Officio  
 ASSEMBLY MEMBER BONNIE LOWENTHAL, Ex Officio

Andre Boutros, Executive Director

## CALIFORNIA TRANSPORTATION COMMISSION

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# DRAFT

August \_\_, 2013

The Honorable Carol Liu  
 Member of the Senate  
 State Capitol, Room 5097  
 P.O. Box 942849  
 Sacramento, CA 94249-0069

The Honorable Chris Holden  
 Member of the Assembly  
 State Capitol, Room 5119  
 P.O. Box 942849  
 Sacramento, CA 94249-0041

Re: Senate Bill 416, Surplus Residential Property

Dear Senator Liu and Assemblymember Holden:

The California Transportation Commission (Commission) advises the Administration and the Legislature in formulating and evaluating state policies and plans for California's transportation programs. As part of this charge, at its August 2013 meeting, the Commission considered Senate Bill 416 that proposes changes to the Roberti Act governing the sale of surplus properties in the State Route (SR) 710 corridor, including the creation of the SR 710 Rehabilitation Account to fund repairs of SR 710 properties. This legislation would require funds exceeding the amount of \$500,000 to be transferred to the State Highway Account to be used exclusively to fund eligible projects located in Pasadena, Alhambra, La Canada Flintridge, and the community of El Sereno in the City of Los Angeles.

In an effort to ensure that this legislation is well-positioned to achieve the objective of utilizing excess funding from the sale of SR 710 properties to fund transportation projects in the areas specified, the Commission encourages you to consider modifications to: (1) clarify the definition of eligible projects, and (2) clarify the process by which eligible projects are selected and approved for funding.

Please be assured that the Commission looks forward to working with you as this bill continues through the legislative process. If you have questions concerning the Commission's recommendations please do not hesitate to contact Andre Boutros, Commission Executive Director, at (916) 654-4245.

Senator Liu  
Assemblymember Holden  
August \_\_, 2013  
Page 2 of 2

Sincerely,

JAMES C. GHIEMMETTI  
Chair

c: Honorable Mark DeSaulnier, Senate Transportation Committee  
Honorable Bonnie Lowenthal, Assembly Transportation Committee  
California Transportation Commissioners

DRAFT

AMENDED IN ASSEMBLY JUNE 24, 2013

AMENDED IN SENATE MAY 28, 2013

AMENDED IN SENATE MAY 1, 2013

AMENDED IN SENATE APRIL 17, 2013

**SENATE BILL**

**No. 416**

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**Introduced by Senator Liu**  
(Coauthor: Assembly Member Holden)

February 20, 2013

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An act to amend Sections 54236 and 54237 of, and to add Sections 54237.3 and 54237.7 to, the Government Code, relating to surplus residential property, and making an appropriation therefor.

LEGISLATIVE COUNSEL'S DIGEST

SB 416, as amended, Liu. Surplus residential property.

Existing law declares the intent of the Legislature to preserve, upgrade, and expand the supply of housing to persons and families of low or moderate income, through the sale of surplus residential property owned by public agencies. Existing law establishes priorities and procedures that any state agency disposing of surplus residential property is required to follow, and defines relevant terms for these purposes, including "fair market value."

This bill would revise the definition of "fair market value" for purposes of the sale of surplus residential property, to reflect the existing "as is" condition of the property, taking into account any needed repairs.

Existing law requires single-family residences to be first offered to their present occupants, at an affordable price, as defined. Under existing law, the selling agency has the option of making repairs to the property required by lenders or government assistance programs, or providing

the occupants with a replacement dwelling, pursuant to a specified provision of law.

This bill would revise the procedures applicable to the sale of surplus residential properties not otherwise sold pursuant to existing procedures, to be offered to current and former tenants in good standing, respectively, and to purchasers who will be owner occupants. The bill additionally would require the selling agency to offer tenants in good standing of nonresidential properties to be given priority to purchase the property they occupy. The bill would authorize the Department of Transportation to offer a residence or property in an “as is” condition, at the request of a person with priority to purchase the residence or property in accordance with existing law.

This bill would require proceeds from sales of surplus residential property to be placed in the SR-710 Rehabilitation Account, created by the bill, and would continuously appropriate these funds for the purpose of providing specified repairs to the properties until the last of the properties is repaired, at which time the funds would be transferred to the State Highway Account, as specified.

Vote:  $\frac{2}{3}$ . Appropriation: yes. Fiscal committee: yes.  
State-mandated local program: no.

*The people of the State of California do enact as follows:*

1 SECTION 1. Section 54236 of the Government Code is  
2 amended to read:  
3 54236. (a) As used in this article, the term “offer” means to  
4 solicit proposals prior to sale in a manner calculated to achieve a  
5 sale under the conditions specified, and to hold the offer open for  
6 a reasonable period of time, which shall be no more than one year,  
7 unless the time is extended by the selling agency at its discretion,  
8 for a period to be specified by the selling agency.  
9 (b) As used in this article, the term “affordable price” means,  
10 in the case of a purchaser, other than a lower income household,  
11 the price for residential property for which the purchaser’s monthly  
12 payments will not exceed that portion of the purchasing  
13 household’s adjusted income as determined in accordance with  
14 the regulations of the United States Department of Housing and  
15 Urban Development, issued pursuant to Section 235 of the National  
16 Housing Act; and, in the case of a purchaser that is a lower income  
17 household, the price for residential property for which the

1 purchaser's monthly payments will not exceed that portion of the  
2 purchasing household's adjusted income as determined in  
3 accordance with the regulations of the United States Department  
4 of Housing and Urban Development issued pursuant to Section 8  
5 of the United States Housing Act of 1937.

6 (c) As used in this article, the term "single-family residence"  
7 means a real property improvement used, or intended to be used,  
8 as a dwelling unit for one family.

9 (d) As used in this article, the term "surplus residential property"  
10 means land and structures owned by any agency of the state that  
11 is determined to be no longer necessary for the agency's use, and  
12 that is developed as single-family or multifamily housing, except  
13 property being held by the agency for the purpose of exchange.

14 Surplus residential properties shall only include land and  
15 structures that, at the time of purchase by the state, the state had  
16 intended to remove the residences thereon and to use the land for  
17 state purposes.

18 (e) As used in this article, the term "displacement" includes,  
19 but is not limited to, persons who will have to move from surplus  
20 residential property that they occupy when it is sold by a state  
21 agency because they are unable to afford to pay the price that the  
22 state agency is asking for the residential property.

23 (f) As used in this article, the term "fair market value" shall  
24 mean fair market value as of the date the offer of sale is made by  
25 the selling agency pursuant to the provisions of this article and  
26 shall reflect the existing "as is" condition of the property, taking  
27 into account any repairs required to make the property safe and  
28 habitable. This definition shall not apply to terms of sale that are  
29 described as mitigation measures in an environmental study  
30 prepared pursuant to the Public Resources Code if the study was  
31 initiated before this measure was enacted.

32 (g) As used in this article, the term "affordable rent" means, in  
33 the case of an occupant person or family, other than a person or  
34 family of low or moderate income, rent for residential property  
35 that is not more than 25 percent of the occupant household's gross  
36 monthly income, and in the case of an occupant person or family  
37 of low or moderate income, rent for residential property that is not  
38 more than the percentage of the adjusted income of the occupant  
39 person or family as permitted under regulations of the United States  
40 Department of Housing and Urban Development issued pursuant

1 to Section 8 of the United States Housing Act of 1937, but not in  
2 excess of the market rental value for comparable property.

3 (h) As used in this article, the term “area median income” means  
4 median household income, adjusted for family size as determined  
5 in accordance with the regulations of the United States Department  
6 of Housing and Urban Development issued pursuant to Section  
7 235 of the National Housing Act, as amended (Public Law 90-448),  
8 for the standard metropolitan statistical area (SMSA), in which  
9 surplus residential property to be disposed of pursuant to this article  
10 is located, or the county in which the property is located, if it is  
11 outside an SMSA.

12 (i) As used in this article, the term “persons and families of low  
13 or moderate income” means persons and families who meet both  
14 of the following conditions:

15 (1) Meet the definition of persons and families of low or  
16 moderate income set forth in Section 50093 of the Health and  
17 Safety Code.

18 (2) Have not had an ownership interest in real property in the  
19 last three years.

20 (j) As used in this article, the term “lower income households”  
21 means lower income households as defined in Section 50079.5 of  
22 the Health and Safety Code.

23 SEC. 2. Section 54237 of the Government Code is amended  
24 to read:

25 54237. (a) Notwithstanding Section 11011.1, any agency of  
26 the state disposing of surplus residential property shall do so in  
27 accordance with the following priorities and procedures:

28 (1) First, all single-family residences presently occupied by  
29 their former owners shall be offered to those former owners at the  
30 appraised fair market value.

31 (2) Second, all single-family residences shall be offered,  
32 pursuant to this article, to their present occupants who have  
33 occupied the property two years or more and who are persons and  
34 families of low or moderate income.

35 (3) Third, all single-family residences shall be offered, pursuant  
36 to this article, to their present occupants who have occupied the  
37 property five years or more and whose household income does not  
38 exceed 150 percent of the area median income.

39 (4) Fourth, a single-family residence shall not be offered,  
40 pursuant to this article, to present occupants who are not the former

1 owners of the property if the present occupants have had an  
2 ownership interest in real property in the last three years.

3 (b) Single-family residences offered to their present occupants  
4 pursuant to paragraphs (2) and (3) of subdivision (a) shall be  
5 offered to those present occupants at an affordable price, which  
6 price shall not be less than the price paid by the agency for original  
7 acquisition, unless the acquisition price was greater than the current  
8 fair market value, and shall not be greater than fair market value.  
9 When single-family residences are offered to present occupants  
10 at a price that is less than fair market value, the selling agency  
11 shall impose terms, conditions, and restrictions to ensure that the  
12 housing will remain available to persons and families of low or  
13 moderate income and households with incomes no greater than  
14 the incomes of the present occupants in proportion to the area  
15 median income. The Department of Housing and Community  
16 Development shall provide to the selling agency recommendations  
17 of standards and criteria for these prices, terms, conditions, and  
18 restrictions. The selling agency shall provide repairs required by  
19 lenders and government housing assistance programs, or, at the  
20 option of the agency, provide the present occupants with a  
21 replacement dwelling pursuant to Section 54237.5.

22 (c) If single-family residences are offered to their present  
23 occupants pursuant to paragraphs (2) and (3) of subdivision (a),  
24 the occupants shall certify their income and assets to the selling  
25 agency. When single-family residences are offered to present  
26 occupants at a price that is less than fair market value, the selling  
27 agency may verify the certifications, in accordance with procedures  
28 utilized for verification of incomes of purchasers and occupants  
29 of housing financed by the California Housing Finance Agency  
30 and with regulations adopted for the verification of assets by the  
31 United States Department of Housing and Urban Development.  
32 The income and asset limitations and term of residency  
33 requirements of paragraphs (2) and (3) of subdivision (a) shall not  
34 apply to sales that are described as mitigation measures in an  
35 environmental study prepared pursuant to the Public Resources  
36 Code, if the study was initiated before this measure was enacted.

37 (d) All other surplus residential properties and all properties  
38 described in paragraphs (1), (2), and (3) of subdivision (a) that are  
39 not purchased by the former owners or the present occupants shall  
40 be then offered to housing-related private and public entities at a

1 reasonable price, which is best suited to economically feasible use  
2 of the property as decent, safe, and sanitary housing at affordable  
3 rents and affordable prices for persons and families of low or  
4 moderate income, on the condition that the purchasing entity shall  
5 cause the property to be rehabilitated and developed as limited  
6 equity cooperative housing with first right of occupancy to present  
7 occupants, except that where the development of cooperative or  
8 cooperatives is not feasible, the purchasing agency shall cause the  
9 property to be used for low and moderate income rental or  
10 owner-occupied housing, with first right of occupancy to the  
11 present tenants. The price of the property in no case shall be less  
12 than the price paid by the agency for original acquisition unless  
13 the acquisition price was greater than current fair market value  
14 and shall not be greater than fair market value. Subject to the  
15 foregoing, it shall be set at the level necessary to provide housing  
16 at affordable rents and affordable prices for present tenants and  
17 persons and families of low or moderate income. When residential  
18 property is offered at a price that is less than fair market value, the  
19 selling agency shall impose terms, conditions, and restrictions as  
20 will ensure that the housing will remain available to persons and  
21 families of low or moderate income. The Department of Housing  
22 and Community Development shall provide to the selling agency  
23 recommendations of standards and criteria for prices, terms,  
24 conditions, and restrictions.

25 (e) Any surplus residential properties not sold pursuant to  
26 subdivisions (a) to (d), inclusive, shall then be sold at fair market  
27 value, with priority given first to purchasers who are present tenants  
28 in good standing with all rent obligations current and paid in full,  
29 second to former tenants who were in good standing at the time  
30 they vacated the premises, and then to purchasers who will be  
31 owner occupants.

32 (f) Tenants in good standing of nonresidential properties shall  
33 be given priority to purchase, at fair market value, the property  
34 they rent, lease, or otherwise legally occupy.

35 SEC. 3. Section 54237.3 is added to the Government Code, to  
36 read:

37 54237.3. Notwithstanding the requirement to provide repairs  
38 in subdivision (b) of Section 54237, the Department of  
39 Transportation may offer a residence or property in an “as is”  
40 condition at the request of a person given priority to purchase

1 pursuant to paragraphs (2) and (3) of subdivision (a) of Section  
2 54237.

3 SEC. 4. Section 54237.7 is added to the Government Code, to  
4 read:

5 54237.7. The Department of Transportation shall deposit  
6 proceeds from sales pursuant to this article into the SR 710  
7 Rehabilitation Account, which is hereby created. Notwithstanding  
8 Section 13340, funds in the account are hereby continuously  
9 appropriated to the department without regard to fiscal years for  
10 the purpose of providing repairs required pursuant to subdivision  
11 (b) of Section 54237. *The total funds maintained in the account*  
12 *shall not exceed five hundred thousand dollars (\$500,000). Funds*  
13 *exceeding that amount shall be transferred to the State Highway*  
14 *Account in the State Transportation Fund to be used exclusively*  
15 *to fund eligible projects located in Pasadena, Alhambra, La*  
16 *Canada Flintridge, and the community of El Sereno in the City of*  
17 *Los Angeles. The funds shall not be used to advance or construct*  
18 *the proposed North State Route 710 tunnel. Any funds remaining*  
19 *in the SR-710 Rehabilitation Account on the date that final payment*  
20 *due for the last of the properties repaired has been made shall be*  
21 *transferred to the State Highway Account in the State*  
22 *Transportation Fund.*

**BUDGET AND ALLOCATION CAPACITY UPDATE**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.

**2014 STIP GUIDELINES HEARING**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.

**ADOPTION OF 2014 STATE TRANSPORTATION  
IMPROVEMENT PROGRAM GUIDELINES**

THIS ITEM WILL BE PRESENTED AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.

# Memorandum

To: CHAIR AND COMMISSION  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 4.7  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **ADOPTION OF THE 2014 STIP AND AERONAUTICS ACCOUNT FUND ESTIMATES:  
RESOLUTION G-13-08**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) to approve Resolution G-13-08 to adopt the Proposed 2014 State Transportation Improvement Program (STIP) Fund Estimate and approve the Proposed 2014 Aeronautics Account Fund Estimate.

## **ISSUE:**

Section 14525(a) of the Government Code (GC) requires the Commission to adopt the Fund Estimate in each odd year by August 15. Resolution G-13-08 and the Proposed 2014 Aeronautics Account Fund Estimate have been updated based on Commission and Commission staff recommendations, and include the state and federal funding available for programming over the respective fund estimate periods.

## **BACKGROUND:**

Sections 14524 and 14525 of the GC require the Department to present a STIP Fund Estimate to the Commission by July 15, and the Commission to adopt a Fund Estimate by August 15 of each odd-numbered year, respectively. The purpose of the Fund Estimate is to forecast all federal and state funds reasonably expected to be available for programming in the subsequent STIP. Each even-numbered year, the Commission is required to adopt a STIP based on the funding identified in the adopted Fund Estimate.

## Attachments:

Resolution G-13-08  
Summary of the Proposed 2014 STIP Fund Estimate

# **RESOLUTION G-13-08**

## **APPENDIX G – RESOLUTION TO ADOPT THE 2014 STIP FUND ESTIMATE**

### **CALIFORNIA TRANSPORTATION COMMISSION**

#### **ADOPTION OF THE 2014 FUND ESTIMATE**

- 1.1. WHEREAS, Sections 14524 and 14525 of the Government Code require the Department of Transportation (Department) to present, and the California Transportation Commission (Commission) to adopt, a biennial fund estimate to include and estimate all State and federal Funds reasonably expected to be available for the biennial State Transportation Improvement Program (STIP), including the amount that may be programmed in each county for regional improvement programs; and
- 1.2. WHEREAS, on January 8, 2013, the Department presented an overview of the fund estimate process and schedule; and
- 1.3. WHEREAS, on May 7, 2013, the Department presented, and the Commission approved the 2014 Fund Estimate assumptions; and
- 1.4. WHEREAS, on June 11, 2013, the Department presented to the Commission the Draft 2014 Fund Estimate; and
- 1.5. WHEREAS, on July 18, 2013, the Commission held a workshop on the Proposed 2014 Fund Estimate to consider public comment, and indicated that the adoption of the 2014 Fund Estimate would be scheduled for August 6, 2013; and
- 1.6. WHEREAS, on August 6, 2013, the Department will present to the Commission an updated, proposed 2014 Fund Estimate; and
- 1.7. WHEREAS, the proposed 2014 Fund Estimate identifies new program capacity of approximately \$1.2 billion in new highway STIP capacity, and over-programming of approximately \$379 million in the Public Transportation Account for the six-year period covering 2013-14 through 2018-19; and
- 1.8. WHEREAS, the Proposed 2014 Fund Estimate includes annual programming targets, adjusted for STIP amendments and allocations through June 2013.

- 2.1 NOW THEREFORE BE IT RESOLVED, that the California Transportation Commission does hereby adopt the 2014 STIP Fund Estimate, as presented by the Department on August 6, 2013, with programming in the 2014 STIP to be based on the statutory funding identified; and
- 2.2 BE IT FURTHER RESOLVED, that the Commission requests that the Department, in cooperation with Commission staff, distribute copies of the 2014 Fund Estimate to each regional agency and county transportation commission.



## **SUMMARY OF THE PROPOSED 2014 STATE TRANSPORTATION IMPROVEMENT PROGRAM FUND ESTIMATE**

**Edmund G. Brown, Jr.**  
Governor  
STATE OF CALIFORNIA

**Brian P. Kelly**  
Secretary  
California State Transportation Agency

**Malcolm Dougherty**  
Director  
Department of Transportation



*The 2014 State Transportation Improvement Program Fund Estimate Book is available online at  
<http://www.dot.ca.gov/hq/transprog/ctcliaison.htm>*

## EXECUTIVE SUMMARY

On August 6, 2013, the California Transportation Commission (Commission) adopted the 2014 State Transportation Improvement Program (STIP) Fund Estimate (FE). The STIP FE is a biennial estimate of all resources available for the state's transportation infrastructure over the next five-year period, and establishes the program funding levels for the STIP and the State Highway Operation and Protection Program (SHOPP). The 2014 STIP FE period covers state fiscal years 2014-15 through 2018-19.

### STIP Capacity

STIP projects add capacity to the state's transportation infrastructure. The 2014 STIP FE includes a total estimate of \$3.4 billion in program capacity over the five-year FE period. Program capacity represents the total value of projects that can be funded each year, and includes construction, right-of-way (R/W), and support. Support consists of preliminary engineering, planning, design, and construction engineering. The 2014 STIP FE displays a new, estimated STIP program capacity of almost \$1.2 billion over the FE period. For comparison, the 2012 STIP FE displayed a forecast of \$1.5 billion in new STIP program capacity over the same five-year period. As a result of the new STIP program capacity forecasted in the 2014 STIP FE, some projects currently programmed in the STIP may need to be delayed (reprogrammed into a later year).

- STIP capacity **does not** include federal commitments for Transportation Enhancements (TE) because Moving Ahead for Progress in the 21<sup>st</sup> Century (MAP-21) eliminated dedicated funding for TE.
- STIP capacity in the future will continue to depend primarily on retail prices and consumption of gasoline and diesel. Both of these sources are difficult to forecast with any certainty due to the current economic climate.

### SHOPP Capacity

SHOPP projects consist of major rehabilitation work on the State Highway System. The 2014 STIP FE forecasts SHOPP program capacity of \$11.4 billion over the five-year FE period. Similar to the STIP, SHOPP program capacity represents the total value of projects that can be funded each year, and includes construction, R/W, and support. New SHOPP capacity of over \$7.3 billion is estimated over the FE period. In comparison, the 2012 STIP FE displayed a forecast of \$6.0 billion in new SHOPP program capacity.

- The State Highway Account (SHA), which is the primary funding source of the SHOPP, has a fund balance that is highly volatile in nature. The cash balance in this account fluctuates daily.
- The SHOPP is constrained over the entire FE period. While the 2014 STIP FE forecasts an average of \$2.3 billion of SHOPP program capacity each year over the FE period, the annual SHOPP goal-constrained need is roughly \$8.2 billion as identified in the 2013 Ten-Year SHOPP Plan. As a result of the approximately \$5.9 billion annual shortfall, potential impacts may include delays of needed projects, an inability to fix new and/or ongoing deterioration of the highways, and cost increases over the FE period.

## ESTIMATED CAPACITY BY PROGRAM Fund Estimate Five-Year Period

<b>2014 STIP FE SHOPP Program Capacity (\$ in millions)</b>								
	2013-14	2014-15	2015-16	2016-17	2017-18	2018-19	5-Year Total	6-Year Total
<b>2014 STIP FE SHOPP Target Capacity</b>	<b>\$2,000</b>	<b>\$2,200</b>	<b>\$2,300</b>	<b>\$2,300</b>	<b>\$2,300</b>	<b>\$2,300</b>	<b>\$11,400</b>	<b>\$13,400</b>
2012 SHOPP Program	2,325	2,032	2,063	0	0	0	4,095	6,420
<b>New SHOPP Program Capacity</b>	<b>(\$325)</b>	<b>\$168</b>	<b>\$237</b>	<b>\$2,300</b>	<b>\$2,300</b>	<b>\$2,300</b>	<b>\$7,305</b>	<b>\$6,980</b>
<b>Cumulative Difference</b>	<b>(\$325)</b>	<b>(\$157)</b>	<b>\$80</b>	<b>\$2,380</b>	<b>\$4,680</b>	<b>\$6,980</b>		

Note: Individual numbers may not add to total due to independent rounding.

<b>2014 STIP FE STIP Program Capacity (\$ in millions)</b>								
	2013-14	2014-15	2015-16	2016-17	2017-18	2018-19	5-Year Total	6-Year Total
<b>2014 STIP FE Target Capacity</b>	<b>\$739</b>	<b>\$690</b>	<b>\$680</b>	<b>\$675</b>	<b>\$675</b>	<b>\$670</b>	<b>\$3,390</b>	<b>\$4,129</b>
2012 STIP Program	739	732	741	720	0	0	2,193	2,932
<b>New STIP Program Capacity</b>	<b>\$0</b>	<b>(\$42)</b>	<b>(\$61)</b>	<b>(\$45)</b>	<b>\$675</b>	<b>\$670</b>	<b>\$1,197</b>	<b>\$1,197</b>
<b>Cumulative Difference</b>	<b>\$0</b>	<b>(\$42)</b>	<b>(\$103)</b>	<b>(\$148)</b>	<b>\$527</b>	<b>\$1,197</b>		

Note: Individual numbers may not add to total due to independent rounding.

<b>2014 STIP FE PTA STIP Program Capacity* (\$ in millions)</b>								
	2013-14	2014-15	2015-16	2016-17	2017-18	2018-19	5-Year Total	6-Year Total
<b>2014 STIP FE PTA Target Capacity</b>	<b>\$25</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$25</b>
2012 PTA STIP Program	79	127	101	97	0	0	325	404
<b>New PTA STIP Capacity</b>	<b>(\$54)</b>	<b>(\$127)</b>	<b>(\$101)</b>	<b>(\$97)</b>	<b>\$0</b>	<b>\$0</b>	<b>(\$325)</b>	<b>(\$379)</b>

Note: Individual numbers may not add to total due to independent rounding.

\*Included in the overall STIP Program Capacity above.

**2014 STIP FUND ESTIMATE**  
**STATE HIGHWAY AND FEDERAL TRUST FUND ACCOUNTS**  
(\$ millions)

	2013-14	2014-15	2015-16	2016-17	2017-18	2018-19	5-Year Total	6-Year Total
<b>RESOURCES</b>								
Beginning Balance	\$445							\$445
Fuel Excise Taxes (Base)	\$1,777	\$1,781	\$1,784	\$1,785	\$1,783	\$1,783	\$8,916	\$10,693
Fuel Excise Taxes (Price-Based)	2,045	1,952	1,933	1,967	1,998	2,007	9,858	11,903
Net Weight Fees	0	0	0	0	0	0	0	0
Misc. Revenues	80	79	78	79	80	78	393	472
Loan Repayments from General Fund	50	135	100	0	0	0	235	285
Transportation Loans	250	(135)	(100)	0	0	0	(235)	15
Net Transfers - Others	(174)	(163)	(163)	(165)	(166)	(165)	(820)	(994)
Expenditures - Other Agencies	(97)	(94)	(101)	(103)	(102)	(105)	(504)	(602)
<b>Subtotal - State Resources</b>	<b>\$4,375</b>	<b>\$3,555</b>	<b>\$3,532</b>	<b>\$3,564</b>	<b>\$3,593</b>	<b>\$3,598</b>	<b>\$17,842</b>	<b>\$22,217</b>
Toll Bridge Seismic Retrofit Program	(\$300)	\$0	\$0	\$0	\$0	\$0	\$0	(\$300)
<b>Total State Resources</b>	<b>\$4,075</b>	<b>\$3,555</b>	<b>\$3,532</b>	<b>\$3,564</b>	<b>\$3,593</b>	<b>\$3,598</b>	<b>\$17,842</b>	<b>\$21,917</b>
Obligation Authority (OA)	\$3,157	\$3,157	\$3,157	\$3,157	\$3,157	\$3,157	\$15,785	\$18,942
August Redistribution	118	118	118	118	118	118	592	710
Other Federal Resources	(186)	(186)	(186)	(186)	(186)	(186)	(930)	(1,116)
<b>Total Federal Resources</b>	<b>\$3,089</b>	<b>\$3,089</b>	<b>\$3,089</b>	<b>\$3,089</b>	<b>\$3,089</b>	<b>\$3,089</b>	<b>\$15,446</b>	<b>\$18,536</b>
<b>TOTAL STATE &amp; FED RESOURCES</b>	<b>\$7,165</b>	<b>\$6,644</b>	<b>\$6,621</b>	<b>\$6,653</b>	<b>\$6,682</b>	<b>\$6,687</b>	<b>\$33,288</b>	<b>\$40,453</b>
<b>COMMITMENTS</b>								
<b>STATE OPERATIONS</b>	<b>(\$917)</b>	<b>(\$942)</b>	<b>(\$967)</b>	<b>(\$993)</b>	<b>(\$1,020)</b>	<b>(\$1,047)</b>	<b>(\$4,969)</b>	<b>(\$5,886)</b>
<b>MAINTENANCE</b>	<b>(\$1,269)</b>	<b>(\$1,297)</b>	<b>(\$1,325)</b>	<b>(\$1,354)</b>	<b>(\$1,384)</b>	<b>(\$1,415)</b>	<b>(\$6,775)</b>	<b>(\$8,043)</b>
<b>LOCAL ASSISTANCE (LA)</b>								
Oversight (Partnership)	(\$122)	(\$127)	(\$123)	(\$120)	(\$118)	(\$115)	(\$603)	(\$725)
State & Federal LA	(1,258)	(1,249)	(1,246)	(1,247)	(1,245)	(1,244)	(6,233)	(7,490)
<b>TOTAL LA</b>	<b>(\$1,380)</b>	<b>(\$1,377)</b>	<b>(\$1,370)</b>	<b>(\$1,367)</b>	<b>(\$1,363)</b>	<b>(\$1,359)</b>	<b>(\$6,836)</b>	<b>(\$8,216)</b>
<b>SHOPP CAPITAL OUTLAY SUPPORT (COS)</b>								
SHOPP Major	(\$568)	(\$439)	(\$245)	(\$125)	(\$74)	(\$29)	(\$911)	(\$1,480)
SHOPP Minor	(38)	(40)	(40)	(40)	(40)	(40)	(201)	(240)
Stormwater	(46)	(46)	(46)	(46)	(46)	(46)	(230)	(276)
<b>TOTAL SHOPP COS</b>	<b>(\$653)</b>	<b>(\$525)</b>	<b>(\$331)</b>	<b>(\$211)</b>	<b>(\$160)</b>	<b>(\$115)</b>	<b>(\$1,342)</b>	<b>(\$1,995)</b>
<b>SHOPP CAPITAL OUTLAY</b>								
Major capital	(\$1,538)	(\$151)	(\$58)	(\$15)	(\$6)	\$0	(\$229)	(\$1,767)
Minor capital	(63)	(63)	(69)	(67)	(67)	(67)	(333)	(396)
R/W Project Delivery	(37)	(30)	(30)	(7)	(7)	(7)	(81)	(118)
Unprogrammed R/W	(18)	(18)	(18)	(18)	(18)	(18)	(90)	(108)
GARVEE Debt Service	(11)	(11)	(11)	(11)	(11)	(11)	(57)	(68)
<b>TOTAL SHOPP CAPITAL OUTLAY</b>	<b>(1,668)</b>	<b>(273)</b>	<b>(186)</b>	<b>(118)</b>	<b>(109)</b>	<b>(103)</b>	<b>(789)</b>	<b>(2,457)</b>
<b>TOTAL SHOPP COMMITMENTS</b>	<b>(\$5,886)</b>	<b>(\$4,413)</b>	<b>(\$4,180)</b>	<b>(\$4,044)</b>	<b>(\$4,036)</b>	<b>(\$4,039)</b>	<b>(\$20,711)</b>	<b>(\$26,597)</b>
<b>STIP LA</b>								
STIP Off-System	(\$45)	(\$48)	(\$24)	(\$15)	(\$12)	(\$6)	(\$105)	(\$149)
Oversight (Partnership)	(36)	(37)	(36)	(35)	(34)	(33)	(176)	(211)
<b>TOTAL STIP LA</b>	<b>(\$80)</b>	<b>(\$86)</b>	<b>(\$60)</b>	<b>(\$49)</b>	<b>(\$46)</b>	<b>(\$39)</b>	<b>(\$280)</b>	<b>(\$361)</b>
<b>STIP COS</b>								
<b>STIP COS</b>	<b>(\$126)</b>	<b>(\$97)</b>	<b>(\$99)</b>	<b>(\$72)</b>	<b>(\$39)</b>	<b>(\$17)</b>	<b>(\$324)</b>	<b>(\$451)</b>
<b>STIP CAPITAL OUTLAY</b>								
STIP On-System	(\$414)	(\$333)	(\$166)	(\$69)	(\$18)	\$0	(\$585)	(\$999)
R/W Project Delivery	(129)	(111)	(57)	(34)	(8)	(8)	(218)	(347)
Unprogrammed R/W	(11)	(11)	(11)	(13)	(12)	(12)	(59)	(70)
GARVEE Debt Service	(73)	(73)	0	0	0	0	(73)	(146)
<b>TOTAL STIP CAPITAL OUTLAY</b>	<b>(\$626)</b>	<b>(\$528)</b>	<b>(\$234)</b>	<b>(\$116)</b>	<b>(\$38)</b>	<b>(\$20)</b>	<b>(\$935)</b>	<b>(\$1,561)</b>
<b>TOTAL STIP COMMITMENTS</b>	<b>(\$833)</b>	<b>(\$710)</b>	<b>(\$393)</b>	<b>(\$237)</b>	<b>(\$123)</b>	<b>(\$76)</b>	<b>(\$1,539)</b>	<b>(\$2,372)</b>
<b>TOTAL RESOURCES AVAILABLE</b>	<b>\$445</b>	<b>\$1,521</b>	<b>\$2,049</b>	<b>\$2,372</b>	<b>\$2,524</b>	<b>\$2,573</b>	<b>\$11,037</b>	<b>\$11,483</b>
<b>SHOPP TARGET CAPACITY</b>	<b>\$2,000</b>	<b>\$2,200</b>	<b>\$2,300</b>	<b>\$2,300</b>	<b>2,300</b>	<b>2,300</b>	<b>\$11,400</b>	<b>\$13,400</b>
<b>STIP TARGET CAPACITY</b>	<b>\$714</b>	<b>\$690</b>	<b>\$680</b>	<b>\$675</b>	<b>\$675</b>	<b>\$670</b>	<b>\$3,390</b>	<b>\$4,104</b>

Note: Individual numbers may not add to total due to independent rounding.

**2014 STIP FUND ESTIMATE**  
**PUBLIC TRANSPORTATION ACCOUNT**  
(\$ in thousands)

	2013-14	2014-15	2015-16	2016-17	2017-18	2018-19	5-Year Total	6-Year Total
<b>RESOURCES</b>								
Beginning Balance	\$391,169							\$391,169
Sales Tax on Diesel	610,777	601,344	601,753	602,162	602,572	602,982	3,010,813	3,621,590
SMIF Interest Earned	230	191	275	275	275	275	1,291	1,521
Transfer from Aeronautics Account	30	30	30	30	30	30	150	180
Loan Repayment from SHA	0	135,000	0	0	0	0	135,000	135,000
Loan Repayment from TDIF	2,054	0	0	0	0	0	0	2,054
Loan to High-Speed Rail (HSR)	(26,199)	0	0	0	0	0	0	(26,199)
Transfer from SHA (S&HC 194)	26,304	26,872	27,451	28,044	28,649	29,268	140,284	166,589
<b>TOTAL RESOURCES</b>	<b>\$1,004,366</b>	<b>\$763,437</b>	<b>\$629,509</b>	<b>\$630,511</b>	<b>\$631,526</b>	<b>\$632,555</b>	<b>\$3,287,538</b>	<b>\$4,291,904</b>
State Transit Assistance	(391,972)	(379,779)	(380,040)	(380,298)	(380,557)	(380,816)	(1,901,491)	(2,293,463)
<b>SUBTOTAL AVAILABLE RESOURCES</b>	<b>\$612,394</b>	<b>\$383,658</b>	<b>\$249,469</b>	<b>\$250,213</b>	<b>\$250,969</b>	<b>\$251,739</b>	<b>\$1,386,047</b>	<b>\$1,998,441</b>
<b>COMMITMENTS</b>								
<b>STATE OPERATIONS</b>								
Rail and Mass Transportation Support	(\$28,511)	(\$29,138)	(\$29,779)	(\$30,434)	(\$31,104)	(\$31,788)	(\$152,244)	(\$180,755)
Planning Staff and Support	(21,858)	(22,339)	(22,830)	(23,333)	(23,846)	(24,371)	(116,718)	(138,576)
California Transportation Commission	(1,403)	(1,434)	(1,465)	(1,498)	(1,531)	(1,564)	(7,492)	(8,895)
Institute of Transportation Studies	(980)	(980)	(980)	(980)	(980)	(980)	(4,900)	(5,880)
Public Utilities Commission	(5,434)	(5,554)	(5,676)	(5,801)	(5,928)	(6,059)	(29,017)	(34,451)
State Controller's Office	(19)	(19)	(20)	(20)	(21)	(21)	(101)	(120)
<b>TOTAL STATE OPERATIONS</b>	<b>(\$58,205)</b>	<b>(\$59,464)</b>	<b>(\$60,751)</b>	<b>(\$62,066)</b>	<b>(\$63,409)</b>	<b>(\$64,783)</b>	<b>(\$310,472)</b>	<b>(\$368,677)</b>
<b>INTERCITY RAIL</b>								
Intercity Rail and Bus Operations	(\$90,347)	(\$93,057)	(\$95,849)	(\$98,725)	(\$101,686)	(\$104,737)	(\$494,055)	(\$584,402)
Amtrak Funding Adjustment	(\$18,600)	(31,000)	(24,800)	(24,800)	(24,800)	(24,800)	(\$130,200)	(\$148,800)
San Joaquin Service Improvements (HSR Integration)	0	0	0	0	(27,210)	(28,026)	(55,236)	(55,236)
Coast Daylight - New Train Service	0	0	(5,000)	(5,150)	(5,305)	(5,464)	(20,918)	(20,918)
Capital Corridor - Service Change	0	0	0	0	0	0	0	0
Pacific Surfliner - Service Change	0	0	0	0	0	0	0	0
Heavy Equipment Overhaul	(16,800)	(16,800)	(16,800)	(16,800)	(17,800)	(17,800)	(86,000)	(102,800)
<b>TOTAL INTERCITY RAIL</b>	<b>(\$125,747)</b>	<b>(\$140,857)</b>	<b>(\$142,449)</b>	<b>(\$145,475)</b>	<b>(\$176,801)</b>	<b>(\$180,827)</b>	<b>(\$786,409)</b>	<b>(\$912,156)</b>
<b>LOCAL ASSISTANCE</b>								
Bay Area Ferry Operations/Waterborne	(\$3,148)	(\$3,179)	(\$3,211)	(\$3,243)	(\$3,276)	(\$3,309)	(\$16,219)	(\$19,367)
<b>TOTAL LOCAL ASSISTANCE</b>	<b>(\$3,148)</b>	<b>(\$3,179)</b>	<b>(\$3,211)</b>	<b>(\$3,243)</b>	<b>(\$3,276)</b>	<b>(\$3,309)</b>	<b>(\$16,219)</b>	<b>(\$19,367)</b>
<b>CAPITAL OUTLAY</b>								
STIP - Mass Transportation	(\$18,734)	(\$31,241)	(\$39,208)	(\$51,159)	(\$25,501)	(\$5,471)	(\$152,580)	(\$171,314)
STIP - Rail	(13,346)	(28,934)	(24,078)	(21,732)	(13,888)	(2,490)	(91,121)	(104,467)
<b>TOTAL CAPITAL OUTLAY</b>	<b>(\$32,081)</b>	<b>(\$60,175)</b>	<b>(\$63,286)</b>	<b>(\$72,891)</b>	<b>(\$39,389)</b>	<b>(\$7,961)</b>	<b>(\$243,701)</b>	<b>(\$275,781)</b>
<b>CASH AVAILABLE FOR PROGRAMMING</b>	<b>\$393,213</b>	<b>\$119,982</b>	<b>(\$20,227)</b>	<b>(\$33,462)</b>	<b>(\$31,906)</b>	<b>(\$5,140)</b>	<b>\$29,247</b>	<b>\$422,460</b>
<b>PTA STIP TARGET CAPACITY</b>	<b>\$25,000</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$25,000</b>

Note: Individual numbers may not add to total due to independent rounding.

**2014 FUND ESTIMATE  
AERONAUTICS ACCOUNT  
(\$ in thousands)**

	2013-14	2014-15	2015-16	2016-17	3-Year Total
<b>RESOURCES</b>					
Beginning Balance	\$7,370	\$1,268	\$984	\$707	
Adjustment for Prior Commitments <sup>1</sup>	(5,822)				
<b>ADJUSTED BEGINNING BALANCE</b>	<b>\$1,548</b>	<b>\$1,268</b>	<b>\$984</b>	<b>\$707</b>	<b>\$2,959</b>
Aviation Gas Excise Tax <sup>2</sup>	2,836	2,744	2,654	2,568	7,966
Jet Fuel Excise Tax <sup>2</sup>	2,618	2,771	2,933	3,105	8,809
Interest (SMIF)	22	22	21	20	62
Federal Trust Funds	436	446	455	465	1,366
Sale of Documents	1	1	1	1	4
Transfer to PTA Account	(30)	(30)	(30)	(30)	(90)
<b>TOTAL RESOURCES</b>	<b>\$7,432</b>	<b>\$7,221</b>	<b>\$7,019</b>	<b>\$6,836</b>	<b>\$21,076</b>
<b>STATE OPERATIONS</b>					
State Operations	(\$3,663)	(\$3,736)	(\$3,811)	(\$3,887)	(\$11,434)
State Controller (0840)	(8)	(8)	(8)	(9)	(25)
Financial Information System for California (8880)	(3)	(3)	(3)	(3)	(9)
<b>TOTAL STATE OPERATIONS</b>	<b>(\$3,674)</b>	<b>(\$3,748)</b>	<b>(\$3,822)</b>	<b>(\$3,899)</b>	<b>(\$11,469)</b>
<b>LOCAL ASSISTANCE</b>					
Grants to Local Agencies (Annual Credit Program)	(\$1,490)	(\$1,490)	(\$1,490)	(\$1,490)	(\$4,470)
Airport Improvement Program (AIP) Match	(1,000)	(1,000)	(1,000)	(1,000)	(3,000)
Acquisition & Development (A&D)	0	0	**	**	0
<b>TOTAL LOCAL ASSISTANCE</b>	<b>(\$2,490)</b>	<b>(\$2,490)</b>	<b>(\$2,490)</b>	<b>(\$2,490)</b>	<b>(\$7,470)</b>
<b>CASH AVAILABLE DURING FE PERIOD</b>	<b>\$1,268</b>	<b>\$984</b>	<b>\$707</b>	<b>\$447</b>	

Note: Numbers may not add due to rounding.

<sup>1</sup> Includes outstanding encumbrances.

<sup>2</sup> Excise tax revenues are based on the 2013-14 projection from the 2013-14 Governor's Budget and escalated each year from 2013-14 through 2016-17 per assumption Aero 2.

\*\* A&D for 2015-16 and 2016-17 will be determined when federal budget funding has been approved. Past action by the Commission dictates that AIP Match receives priority for available funds.

## COUNTY AND INTERREGIONAL SHARE ESTIMATES

The STIP consists of two broad programs, the regional program funded from 75 percent of new STIP funding and the interregional program funded from 25 percent of new STIP funding. The 75 percent regional program is further subdivided by formula into County Shares. County Shares are available solely for projects nominated by regions in their Regional Transportation Improvement Programs (RTIP). A detailed explanation of this methodology is included in the County Share portion of this document.

The 2014 STIP Fund Estimate (FE) indicates that there are negative program capacities for the Public Transportation Account (PTA) and the federal Transportation Enhancement Program (TE); therefore, programming targets for the PTA and TE are not needed for the 2014 STIP cycle. PTA funds in the STIP are severely limited and will remain so in the future, and the TE program has been eliminated in the new federal transportation act (MAP-21, Moving Ahead for Progress in the 21<sup>st</sup> Century Act) signed by the President on July 6, 2012. This means that many of the transit and TE projects currently programmed in the STIP will either have to be delivered with other funds (if the projects are eligible for other STIP fund types) or be unprogrammed. In particular, TE reserve amounts must be unprogrammed.

The following tables display STIP county and interregional shares and targets for the 2014 STIP.

### **Table 1. Reconciliation to County and Interregional Shares**

This table lists the net changes to program capacity from the 2014 STIP FE to the capacity used in the County and Interregional Shares. This table also separates the program capacity by PTA, non-PTA (the State Highway Account, Federal Trust Fund, and the Transportation Facilities Account), and Transportation Enhancements (TE) capacity. The table is based on Commission actions through June 30, 2013.

### **Table 2. Summary of Targets and Shares**

This table takes into account all county and interregional share balances through the June 2013 Commission meeting, as well as new statewide STIP capacity. For each county and the interregional share, the table identifies the following target amounts:

- Total Target: This target is determined by calculating the STIP formula share of all new capacity through 2018-19. The calculation of this target is shown in Table 3.
- Maximum: This target is determined by estimating the STIP formula share of all available new capacity through the end of the county share period in 2019-20. This represents the maximum amount that the Commission may program in a county, other than advancing future shares, pursuant to Streets and Highways Code Section 188.8(j), to a county with a population of under 1 million. The calculation of this target is shown in Table 4.

### **Table 3. Calculation of New Programming Targets and Shares - Total Target**

This table displays factors in the calculation of the Total Target.

- Net Carryover: These columns display the current share status, including STIP allocations and amendments through the June 23, 2013 Commission meeting. Positive numbers indicate unprogrammed shares, and negative numbers indicate shares that were advanced.
- 2014 STIP Target Through 2018-19: This section calculates the total target. The total target is the formula distribution of new capacity available through 2018-19 adjusted for carryover balances and lapses.
  - Formula Distribution: This is the 2014 STIP share through 2018-19. It is the formula distribution of program capacity available through 2018-19. The amount distributed is the new capacity less the unprogrammed shares, lapses, and the decrease in advances.
  - Add Back Lapses 11-12/12-13: This identifies the amount of projects lapsed in 2011-12 and 2012-13. These amounts are credited back in the 2014 STIP Fund Estimate to county and interregional shares for the four-year share period beginning 2016-17.
  - Net Share (Total Target): This is the 2014 STIP target through 2018-19. The Net Share (Total Target) is calculated by adding to the Formula Distribution the lapses and the Unprogrammed Balance or Balance Advanced. In cases where the distribution of new capacity is insufficient to cover prior advances (i.e., the Net Share would be less than zero), a zero appears in the Net Share column.
  - Net Advance: Numbers in this column represent advances against future capacity. This occurs when the distribution of new shares (through 2018-19) is insufficient to cover prior advances.

**Table 4. Calculation of New Programming Targets and Shares – Maximum**

This table calculates the maximum amount that the Commission may program in a county, other than advancing future shares, pursuant to Streets and Highways Code Section 188.8(j), to a county with a population of under 1 million.

- Net Carryover: These columns display the current share status, including STIP allocations and amendments through the June 23, 2013 Commission meeting. Positive numbers indicate unprogrammed shares, and negative numbers indicate shares that were advanced.
- 2014 STIP Share Through 2019-20: This section estimates the maximum target. This is the formula distribution of estimated new capacity available through 2019-20 adjusted for carryover balances and lapses.
  - Formula Distribution: This column estimates the STIP share of the estimated new capacity through the county share period ending in 2019-20. It is the formula distribution of estimated program capacity available through the county share period ending in 2019-20. The amount distributed is the new capacity less the unprogrammed shares, lapses, and the decrease in advances.
  - Add Back Lapses 11-12/12-13: This identifies the amount of projects lapsed in 2011-12 and 2012-13. These amounts are credited back in the 2014 STIP Fund Estimate to county and interregional shares for the four-year share period beginning 2016-17.
  - Net Share (Maximum): This target is the STIP share of all available new capacity through the end of the county share period in 2019-20. This represents the maximum

amount that the Commission may program in a county, other than advancing future shares, pursuant to Streets and Highways Code Section 188.8(j), to a county with a population of under 1 million. The Net Share (Maximum) is calculated by adding to the Formula Distribution the lapses and the Unprogrammed Balance or Balance Advanced. In cases where the distribution of new capacity is insufficient to cover prior advances (i.e., the Net Share would be less than zero), a zero appears in the Net Share column.

- Net Advance: Numbers in this column represent advances against future capacity. This occurs when the distribution of new shares (through 2019-20) is insufficient to cover prior advances.

#### **Table 5. Planning, Programming, and Monitoring (PPM) Limitations**

State law provides that up to 5% of a county share may be expended for planning, programming, and monitoring (PPM). This limitation is applied separately to each four-year county share period.

- Total: This section identifies the shares for the 2016-17 through 2018-19 share period, based upon the 2012, and 2014 Fund Estimates. These are the amounts against which the 5% is applied.
- 5% PPM Limitation: These are the PPM limitations for the 2016-17 through 2018-19 share period. The PPM limitations for the 2012-13 through 2015-16 are not shown here. They have not changed since the 2012 STIP.

## 2014 STIP FUND ESTIMATE

**Table 1 - Reconciliation to County and Interregional Shares**  
(\$ millions)

	2013-14	2014-15	2015-16	2016-17	2017-18	2018-19	5-Year Total	6-Year Total
<b>Public Transportation Account (PTA)</b>								
2014 FE PTA Target Capacity	\$25	\$0	\$0	\$0	\$0	\$0	\$0	\$25
<b>Total 2014 STIP FE PTA Target Capacity</b>	<b>\$25</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$25</b>
<b>2012 STIP Program <sup>1</sup></b>	<b>\$68</b>	<b>\$84</b>	<b>\$101</b>	<b>\$97</b>	<b>\$0</b>	<b>\$0</b>	<b>\$282</b>	<b>\$350</b>
Extensions	\$11	\$43	\$0	\$0	\$0	\$0	\$43	\$54
Delivered But Not Allocated	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Advances	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
<b>Net PTA STIP Program</b>	<b>\$79</b>	<b>\$127</b>	<b>\$101</b>	<b>\$97</b>	<b>\$0</b>	<b>\$0</b>	<b>\$325</b>	<b>\$404</b>
<b>PTA Capacity for County Shares</b>	<b>(\$54)</b>	<b>(\$127)</b>	<b>(\$101)</b>	<b>(\$97)</b>	<b>\$0</b>	<b>\$0</b>	<b>(\$325)</b>	<b>(\$379)</b>
<b>Cumulative</b>	<b>(\$54)</b>	<b>(\$181)</b>	<b>(\$282)</b>	<b>(\$379)</b>	<b>(\$379)</b>	<b>(\$379)</b>		
<b>SHA</b>								
2014 FE Non-PTA Target Capacity	\$798	\$774	\$691	\$686	\$686	\$681	\$3,518	\$4,316
2014 FE Non-PTA GARVEE Debt Service	(\$84)	(\$84)	(\$11)	(\$11)	(\$11)	(\$11)	(\$128)	(\$212)
TE State Match (Estimated program totals)	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
<b>Total 2014 STIP FE Non-PTA Capacity</b>	<b>\$714</b>	<b>\$690</b>	<b>\$680</b>	<b>\$675</b>	<b>\$675</b>	<b>\$670</b>	<b>\$3,390</b>	<b>\$4,104</b>
<b>2012 STIP Program <sup>1</sup></b>	<b>\$462</b>	<b>\$516</b>	<b>\$569</b>	<b>\$531</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,616</b>	<b>\$2,078</b>
Extensions	\$120	\$2	\$0	\$0	\$0	\$0	\$2	\$122
Delivered But Not Allocated	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Advances	\$0	(\$5)	\$0	\$0	\$0	\$0	(\$5)	(\$5)
<b>Net Non-PTA STIP Program</b>	<b>\$581</b>	<b>\$512</b>	<b>\$569</b>	<b>\$531</b>	<b>\$0</b>	<b>\$0</b>	<b>\$1,613</b>	<b>\$2,194</b>
<b>Non-PTA Capacity for County Shares</b>	<b>\$133</b>	<b>\$178</b>	<b>\$111</b>	<b>\$144</b>	<b>\$675</b>	<b>\$670</b>	<b>\$1,777</b>	<b>\$1,910</b>
<b>Cumulative</b>	<b>\$133</b>	<b>\$310</b>	<b>\$421</b>	<b>\$565</b>	<b>\$1,240</b>	<b>\$1,910</b>		
<b>Transportation Enhancements (TE)</b>								
2014 STIP FE TE Capacity (Federal)	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
TE State Match (Estimated program totals)	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
<b>Total 2014 STIP FE TE Capacity</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>
<b>2012 STIP Program <sup>1</sup></b>	<b>\$81</b>	<b>\$95</b>	<b>\$72</b>	<b>\$94</b>	<b>\$0</b>	<b>\$0</b>	<b>\$260</b>	<b>\$341</b>
Extensions	\$4	\$1	\$0	\$0	\$0	\$0	\$1	\$5
Advances	(\$6)	(\$3)	(\$1)	(\$1)	\$0	\$0	(\$6)	(\$12)
<b>Net TE</b>	<b>\$79</b>	<b>\$92</b>	<b>\$70</b>	<b>\$92</b>	<b>\$0</b>	<b>\$0</b>	<b>\$255</b>	<b>\$334</b>
<b>TE Capacity for County Shares</b>	<b>(\$79)</b>	<b>(\$92)</b>	<b>(\$70)</b>	<b>(\$92)</b>	<b>\$0</b>	<b>\$0</b>	<b>(\$255)</b>	<b>(\$334)</b>
<b>Cumulative</b>	<b>(\$79)</b>	<b>(\$171)</b>	<b>(\$241)</b>	<b>(\$334)</b>	<b>(\$334)</b>	<b>(\$334)</b>		
<b>Total Capacity</b>	<b>\$0</b>	<b>(\$42)</b>	<b>(\$61)</b>	<b>(\$45)</b>	<b>\$675</b>	<b>\$670</b>	<b>\$1,197</b>	<b>\$1,197</b>

**Notes:**

General note: Numbers may not add due to rounding.

<sup>1</sup> 2013 Orange Book

**2014 STIP Fund Estimate**  
**County and Interregional Shares**  
 Table 2. Summary of Targets and Shares  
 (,000)

County	2014 STIP Programming		
	Total Target	Maximum	TE Target
	Target through 2018-19	Estimated Share through 2019-20	Target through 2018-19
Alameda	32,031	49,551	0
Alpine	2,147	2,668	0
Amador	2,377	3,559	0
Butte	18,480	21,976	0
Calaveras	2,415	3,823	0
Colusa	2,407	3,343	0
Contra Costa	25,552	37,542	0
Del Norte	0	0	0
El Dorado LTC	0	0	0
Fresno	15,872	29,067	0
Glenn	3,483	4,463	0
Humboldt	423	3,946	0
Imperial	17,405	23,626	0
Inyo	18,461	23,303	0
Kern	28,350	46,137	0
Kings	0	0	0
Lake	7,520	9,050	0
Lassen	5,391	7,631	0
Los Angeles	167,168	273,126	0
Madera	0	0	0
Marin	0	0	0
Mariposa	3,111	4,027	0
Mendocino	6,720	10,009	0
Merced	19,080	23,412	0
Modoc	3,653	4,849	0
Mono	14,770	18,367	0
Monterey	14,102	20,338	0
Napa	6,606	8,763	0
Nevada	0	916	0
Orange	62,339	95,004	0
Placer TPA	0	0	0
Plumas	5,214	6,550	0
Riverside	66,804	95,687	0
Sacramento	46,577	63,174	0
San Benito	0	0	0
San Bernardino	51,066	84,274	0
San Diego	34,490	71,613	0
San Francisco	12,414	21,306	0
San Joaquin	23,713	32,708	0
San Luis Obispo	7,372	13,995	0
San Mateo	20,239	29,287	0
Santa Barbara	1,927	9,386	0
Santa Clara	17,074	37,888	0
Santa Cruz	5,534	9,118	0
Shasta	14,204	18,041	0
Sierra	2,251	2,885	0
Siskiyou	7,286	9,916	0
Solano	10,564	15,995	0
Sonoma	0	0	0
Stanislaus	14,697	21,351	0
Sutter	3,955	5,489	0
Tahoe RPA	2,981	3,795	0
Tehama	6,244	8,194	0
Trinity	3,016	4,399	0
Tulare	8,316	16,535	0
Tuolumne	11,245	12,774	0
Ventura	29,858	40,956	0
Yolo	13,148	16,353	0
Yuba	5,116	6,290	0
<b>Statewide Regional</b>	<b>905,168</b>	<b>1,386,455</b>	<b>0</b>
Interregional	292,229	460,942	0
<b>TOTAL</b>	<b>1,197,397</b>	<b>1,847,397</b>	<b>0</b>

	New Capacity
Statewide Flexible Capacity	1,909,730
Statewide PTA Capacity	(378,695)
Statewide TE Capacity	(333,638)
<b>Total STIP Capacity</b>	<b>1,197,397</b>

**2014 Fund Estimate  
County and Interregional Shares**

Table 3. Calculation of New Programming Targets and Shares - Total  
(\$1,000's)

County	Net Carryover		2014 STIP Share through 2018-19			
	Unprogrammed Balance	Balance Advanced	Formula Distribution	Add Back Lapses 11-12/12/13	Net Share (Total Target)	Net Advance
Alameda	2,000	0	30,031	0	32,031	0
Alpine	1,255	0	892	0	2,147	0
Amador	350	0	2,027	0	2,377	0
Butte	12,488	0	5,992	0	18,480	0
Calaveras	0	0	2,415	0	2,415	0
Colusa	673	0	1,604	130	2,407	0
Contra Costa	5,000	0	20,552	0	25,552	0
Del Norte	0	(11,560)	1,497	0	0	(10,063)
El Dorado LTC	0	(9,478)	4,203	0	0	(5,275)
Fresno	0	(8,176)	22,618	1,430	15,872	0
Glenn	1,802	0	1,680	1	3,483	0
Humboldt	0	(5,655)	6,038	40	423	0
Imperial	6,741	0	10,664	0	17,405	0
Inyo	9,824	0	8,299	338	18,461	0
Kern	0	(2,711)	30,488	573	28,350	0
Kings	0	(17,941)	4,474	0	0	(13,467)
Lake	4,665	0	2,623	232	7,520	0
Lassen	652	0	3,839	900	5,391	0
Los Angeles	0	(17,809)	181,619	3,358	167,168	0
Madera	0	(14,078)	4,162	0	0	(9,916)
Marin	0	(39,820)	5,617	245	0	(33,958)
Mariposa	1,541	0	1,570	0	3,111	0
Mendocino	1,081	0	5,639	0	6,720	0
Merced	11,655	0	7,425	0	19,080	0
Modoc	1,373	0	2,048	232	3,653	0
Mono	8,439	0	6,166	165	14,770	0
Monterey	0	(6,844)	10,690	10,256	14,102	0
Napa	2,678	0	3,698	230	6,606	0
Nevada	0	(4,118)	3,179	0	0	(939)
Orange	0	(1,653)	55,992	8,000	62,339	0
Placer TPA	0	(45,878)	7,625	0	0	(38,253)
Plumas	2,925	0	2,289	0	5,214	0
Riverside	15,380	0	49,508	1,916	66,804	0
Sacramento	17,630	0	28,447	500	46,577	0
San Benito	0	(6,819)	1,969	0	0	(4,850)
San Bernardino	0	(5,969)	56,920	115	51,066	0
San Diego	0	(29,142)	63,632	0	34,490	0
San Francisco	0	(2,827)	15,241	0	12,414	0
San Joaquin	7,957	0	15,418	338	23,713	0
San Luis Obispo	0	(4,624)	11,354	642	7,372	0
San Mateo	3,728	0	15,511	1,000	20,239	0
Santa Barbara	0	(12,288)	12,785	1,430	1,927	0
Santa Clara	0	(19,262)	35,676	660	17,074	0
Santa Cruz	0	(611)	6,145	0	5,534	0
Shasta	7,628	0	6,576	0	14,204	0
Sierra	1,043	0	1,087	121	2,251	0
Siskiyou	2,470	0	4,509	307	7,286	0
Solano	1,256	0	9,308	0	10,564	0
Sonoma	0	(21,840)	11,444	1,204	0	(9,192)
Stanislaus	3,292	0	11,405	0	14,697	0
Sutter	1,327	0	2,628	0	3,955	0
Tahoe RPA	1,585	0	1,396	0	2,981	0
Tehama	2,422	0	3,343	479	6,244	0
Trinity	586	0	2,370	60	3,016	0
Tulare	0	(6,022)	14,088	250	8,316	0
Tuolumne	8,626	0	2,619	0	11,245	0
Ventura	9,335	0	19,023	1,500	29,858	0
Yolo	6,739	0	5,494	915	13,148	0
Yuba	3,004	0	2,012	100	5,116	0
Statewide Regional	169,150	(295,125)	867,563	37,667	905,168	(125,913)
Interregional	0	(13,246)	289,188	16,287	292,229	0
<b>TOTAL</b>	<b>169,150</b>	<b>(308,371)</b>	<b>1,156,751</b>	<b>53,954</b>	<b>1,197,397</b>	<b>(125,913)</b>
Statewide Flexible Capacity					1,909,730	
Statewide PTA Capacity					(378,695)	
Statewide TE Capacity					(333,638)	
Total					1,197,397	

**2014 Fund Estimate  
County and Interregional Shares**

Table 4. Calculation of New Programming Targets and Shares - Maximum  
(\$1,000's)

County	Net Carryover		2014 STIP Share through 2019-20			
	Unprogrammed Balance	Balance Advanced	Formula Distribution	Add Back Lapses 11-12/12-13	Net Share (Maximum)	Net Advance
Alameda	2,000	0	47,551	0	49,551	0
Alpine	1,255	0	1,413	0	2,668	0
Amador	350	0	3,209	0	3,559	0
Butte	12,488	0	9,488	0	21,976	0
Calaveras	0	0	3,823	0	3,823	0
Colusa	673	0	2,540	130	3,343	0
Contra Costa	5,000	0	32,542	0	37,542	0
Del Norte	0	(11,560)	2,371	0	0	(9,189)
El Dorado LTC	0	(9,478)	6,655	0	0	(2,823)
Fresno	0	(8,176)	35,813	1,430	29,067	0
Glenn	1,802	0	2,660	1	4,463	0
Humboldt	0	(5,655)	9,561	40	3,946	0
Imperial	6,741	0	16,885	0	23,626	0
Inyo	9,824	0	13,141	338	23,303	0
Kern	0	(2,711)	48,275	573	46,137	0
Kings	0	(17,941)	7,084	0	0	(10,857)
Lake	4,665	0	4,153	232	9,050	0
Lassen	652	0	6,079	900	7,631	0
Los Angeles	0	(17,809)	287,577	3,358	273,126	0
Madera	0	(14,078)	6,590	0	0	(7,488)
Marin	0	(39,820)	8,894	245	0	(30,681)
Mariposa	1,541	0	2,486	0	4,027	0
Mendocino	1,081	0	8,928	0	10,009	0
Merced	11,655	0	11,757	0	23,412	0
Modoc	1,373	0	3,244	232	4,849	0
Mono	8,439	0	9,763	165	18,367	0
Monterey	0	(6,844)	16,926	10,256	20,338	0
Napa	2,678	0	5,855	230	8,763	0
Nevada	0	(4,118)	5,034	0	916	0
Orange	0	(1,653)	88,657	8,000	95,004	0
Placer TPA	0	(45,878)	12,073	0	0	(33,805)
Plumas	2,925	0	3,625	0	6,550	0
Riverside	15,380	0	78,391	1,916	95,687	0
Sacramento	17,630	0	45,044	500	63,174	0
San Benito	0	(6,819)	3,117	0	0	(3,702)
San Bernardino	0	(5,969)	90,128	115	84,274	0
San Diego	0	(29,142)	100,755	0	71,613	0
San Francisco	0	(2,827)	24,133	0	21,306	0
San Joaquin	7,957	0	24,413	338	32,708	0
San Luis Obispo	0	(4,624)	17,977	642	13,995	0
San Mateo	3,728	0	24,559	1,000	29,287	0
Santa Barbara	0	(12,288)	20,244	1,430	9,386	0
Santa Clara	0	(19,262)	56,490	660	37,888	0
Santa Cruz	0	(611)	9,729	0	9,118	0
Shasta	7,628	0	10,413	0	18,041	0
Sierra	1,043	0	1,721	121	2,885	0
Siskiyou	2,470	0	7,139	307	9,916	0
Solano	1,256	0	14,739	0	15,995	0
Sonoma	0	(21,840)	18,121	1,204	0	(2,515)
Stanislaus	3,292	0	18,059	0	21,351	0
Sutter	1,327	0	4,162	0	5,489	0
Tahoe RPA	1,585	0	2,210	0	3,795	0
Tehama	2,422	0	5,293	479	8,194	0
Trinity	586	0	3,753	60	4,399	0
Tulare	0	(6,022)	22,307	250	16,535	0
Tuolumne	8,626	0	4,148	0	12,774	0
Ventura	9,335	0	30,121	1,500	40,956	0
Yolo	6,739	0	8,699	915	16,353	0
Yuba	3,004	0	3,186	100	6,290	0
<b>Statewide Regional</b>	<b>169,150</b>	<b>(295,125)</b>	<b>1,373,703</b>	<b>37,667</b>	<b>1,386,455</b>	<b>(101,060)</b>
<b>Interregional</b>	<b>0</b>	<b>(13,246)</b>	<b>457,901</b>	<b>16,287</b>	<b>460,942</b>	<b>0</b>
<b>TOTAL</b>	<b>169,150</b>	<b>(308,371)</b>	<b>1,831,604</b>	<b>53,954</b>	<b>1,847,397</b>	<b>(101,060)</b>
Statewide Flexible Capacity					2,559,730	
Statewide PTA Capacity					(378,695)	
Statewide TE Capacity					(333,638)	
Total					1,847,397	

**2014 STIP FUND ESTIMATE**  
**County and Interregional Shares**  
Table 5 - Planning, Programming and Monitoring (PPM) Limitations  
(\$1,000's)

County	Total		5% PPM Limitation	
	2012 STIP FY 2016/17	2014 STIP 16/17-18/19	Total 16/17-18/19	FY 2016/17-2018-19
Alameda	20,348	30,031	50,379	2,519
Alpine	602	892	1,494	75
Amador	1,383	2,027	3,410	171
Butte	4,031	5,992	10,023	501
Calaveras	1,623	2,415	4,038	202
Colusa	1,081	1,604	2,685	134
Contra Costa	13,881	20,552	34,433	1,722
Del Norte	1,011	1,497	2,508	125
El Dorado LTC	2,806	4,203	7,009	350
Fresno	15,366	22,618	37,984	1,899
Glenn	1,132	1,680	2,812	141
Humboldt	4,066	6,038	10,104	505
Imperial	7,218	10,664	17,882	894
Inyo	5,617	8,299	13,916	696
Kern	20,698	30,488	51,186	2,559
Kings	3,035	4,474	7,509	375
Lake	1,769	2,623	4,392	220
Lassen	2,585	3,839	6,424	321
Los Angeles	122,728	181,619	304,347	15,217
Madera	2,810	4,162	6,972	349
Marin	3,792	5,617	9,409	470
Mariposa	1,058	1,570	2,628	131
Mendocino	3,799	5,639	9,438	472
Merced	5,004	7,425	12,429	621
Modoc	1,379	2,048	3,427	171
Mono	4,180	6,166	10,346	517
Monterey	7,227	10,690	17,917	896
Napa	2,497	3,698	6,195	310
Nevada	2,146	3,179	5,325	266
Orange	37,971	55,992	93,963	4,698
Placer TPA	5,140	7,625	12,765	638
Plumas	1,542	2,289	3,831	192
Riverside	33,370	49,508	82,878	4,144
Sacramento	19,227	28,447	47,674	2,384
San Benito	1,328	1,969	3,297	165
San Bernardino	38,336	56,920	95,256	4,763
San Diego	43,126	63,632	106,758	5,338
San Francisco	10,283	15,241	25,524	1,276
San Joaquin	10,407	15,418	25,825	1,291
San Luis Obispo	7,729	11,354	19,083	954
San Mateo	10,617	15,511	26,128	1,306
Santa Barbara	8,644	12,785	21,429	1,071
Santa Clara	24,115	35,676	59,791	2,990
Santa Cruz	4,164	6,145	10,309	515
Shasta	4,436	6,576	11,012	551
Sierra	732	1,087	1,819	91
Siskiyou	3,036	4,509	7,545	377
Solano	6,277	9,308	15,585	779
Sonoma	7,819	11,444	19,263	963
Stanislaus	7,718	11,405	19,123	956
Sutter	1,775	2,628	4,403	220
Tahoe RPA	942	1,396	2,338	117
Tehama	2,269	3,343	5,612	281
Trinity	1,595	2,370	3,965	198
Tulare	9,531	14,088	23,619	1,181
Tuolumne	1,780	2,619	4,399	220
Ventura	12,867	19,023	31,890	1,595
Yolo	3,691	5,494	9,185	459
Yuba	1,357	2,012	3,369	168
<b>Statewide</b>	<b>586,696</b>	<b>867,563</b>	<b>1,454,259</b>	<b>72,713</b>

Note: Limitation amounts include amounts already programmed.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 4.14  
Information Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Kome Ajise  
Deputy Director  
Planning and Modal Programs

Subject: **LOS ANGELES ACCELERATED REGIONAL TRANSPORTATION IMPROVEMENTS (ARTI) PROJECT – PUBLIC PRIVATE PARTNERSHIP PROPOSAL**

## **SUMMARY:**

The Los Angeles County Metropolitan Transportation Authority (Metro) and the California Department of Transportation (Department), collectively referred to as the Project Sponsors, will make a presentation on the project delivery options available for the procurement of the Accelerated Regional Transportation Improvements (ARTI) Project (the Project) as a candidate for a Public-Private Partnership (P3) project, as provided in Streets and Highway Code Section 143(c).

## **BACKGROUND:**

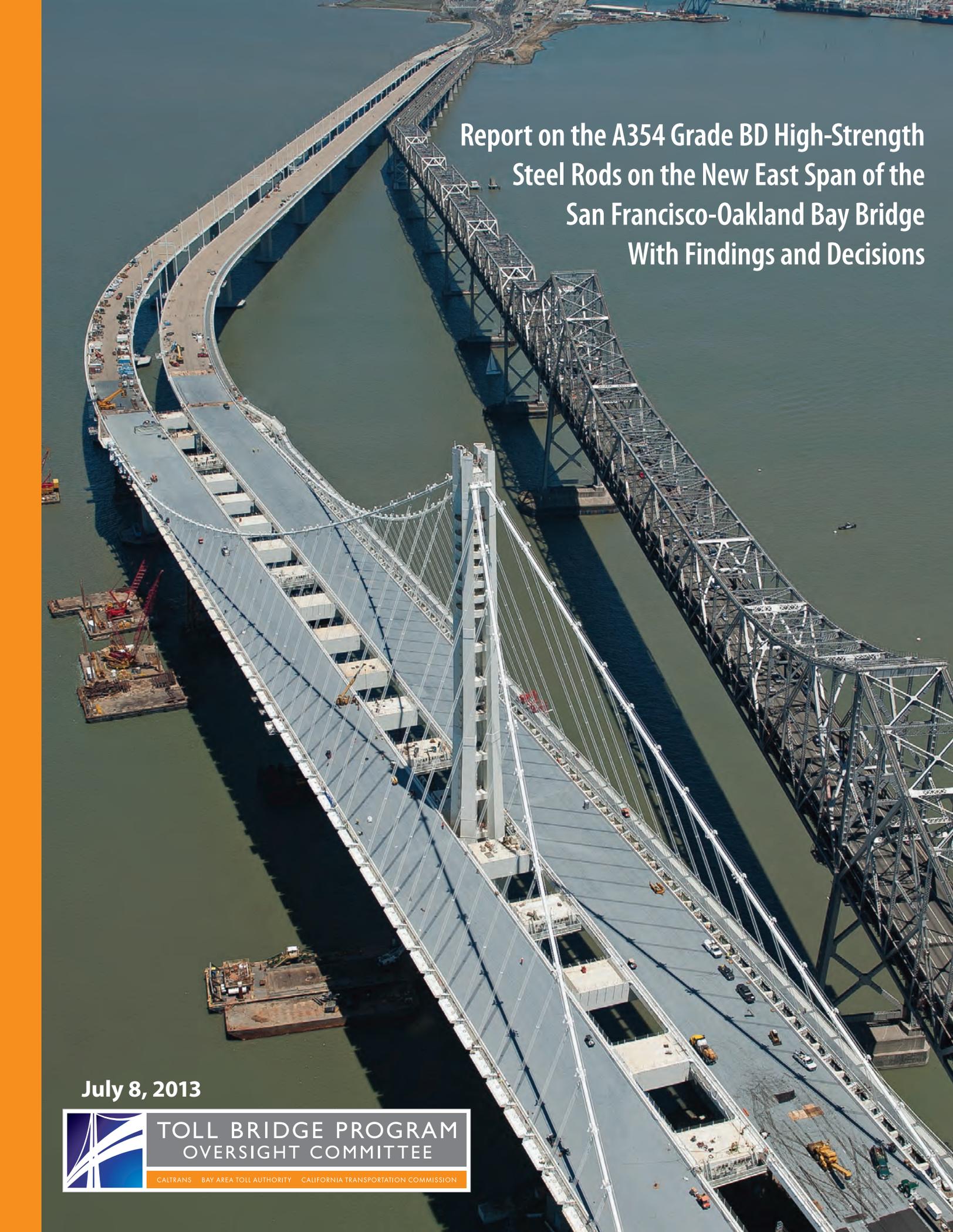
The ARTI Project consists of six individual Elements located in Los Angeles County. The six Elements are defined as follows:

1. I-5 North Capacity Enhancement;
2. I-5 North Pavement Rehabilitation;
3. SR-71 Gap Project, I-10 to Mission Boulevard;
4. SR-71 Gap Project, Mission Boulevard to Rio Rancho Road;
5. Soundwall Packages 10 and 10A; and
6. Soundwall Package 11.

The scope of work includes the Design, construction, financing, and fence-to-fence” Operation and Maintenance (O&M) of the existing and proposed improvements along the I-5 and SR-71 Elements. The I-5 North Capacity Enhancement Element includes O&M of the general purpose lanes and the High Occupancy Toll lanes, including structures and facilities and all field-side tolling equipment, for a period of 35 years, after substantial completion. The SR-71 Gap Elements, which consist of Elements 3 and 4, include O&M of the general purpose lanes and the High Occupancy Vehicle lanes, and all structures and facilities for a period of 35 years, after substantial completion. The scope also includes establishment of the means to operate, and manage the traffic on, these Elements through a Traffic Management Center outside of the Department’s Right of Way. Lastly, the scope includes the Design or Design update, as appropriate, and the construction of the Soundwall Elements, specifically Elements 5 and 6. The Developer will have no O&M responsibilities for the soundwall elements after construction is complete, but will provide a structural warranty for five years and will support establishment of landscaping for three years.

**CALIFORNIA TRANSPORTATION INFRASTRUCTURE**  
**PRIORITIES**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE CALIFORNIA  
TRANSPORTATION COMMISSION MEETING.



Report on the A354 Grade BD High-Strength  
Steel Rods on the New East Span of the  
San Francisco-Oakland Bay Bridge  
With Findings and Decisions

July 8, 2013



TOLL BRIDGE PROGRAM  
OVERSIGHT COMMITTEE

CALTRANS BAY AREA TOLL AUTHORITY CALIFORNIA TRANSPORTATION COMMISSION



On July 18, 2005, Governor Schwarzenegger and the State Legislature, through Assembly Bill 144 (AB 144), created the Toll Bridge Program Oversight Committee (TBPOC) to provide project oversight and project control for the Bay Area's Toll Bridge Seismic Retrofit Program, which includes the San Francisco-Oakland Bay Bridge East Span Replacement Project.

The TBPOC is composed of the Executive Director of the California Transportation Commission (CTC), the Director of the Department of Transportation (Caltrans), and the Executive Director of the Bay Area Toll Authority (BATA). The TBPOC's project oversight and control activities include: (a) reviews of contract bid documents and specifications, ongoing capital costs, significant change orders and claims; (b) implementation of a risk management program; and (c) resolution of project issues.

Current members are:

Steve Heminger, *Chair*  
*Executive Director,*  
*Bay Area Toll Authority*

Malcolm Dougherty  
*Director, California*  
*Department of Transportation*

Andre Boutros  
*Executive Director, California*  
*Transportation Commission*

# Report on the A354 Grade BD High-Strength Steel Rods on the New East Span of the San Francisco-Oakland Bay Bridge, With Findings and Decisions



**July 8, 2013**

Department of Transportation  
Office of the Director  
1120 N Street  
P.O. Box 942873  
Sacramento, CA  
94273-0001

**web** [baybridgeinfo.org](http://baybridgeinfo.org)



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## Executive Summary

The Toll Bridge Program Oversight Committee (TBPOC) — composed of the executive directors of the California Transportation Commission, Caltrans, and the Bay Area Toll Authority — is charged with project oversight and control of the Bay Area’s Toll Bridge Seismic Retrofit Program, which includes the new East Span of the San Francisco-Oakland Bay Bridge. As part of this charge, the TBPOC is investigating and resolving the challenge of the fractured A354 grade BD high-strength steel rods installed on the Self-Anchored Suspension (SAS) Bridge of the new East Span. When 32 of the 96 A354 grade BD high-strength anchor rods on shear keys S1 and S2 on Pier E2 failed in March 2013 after being tightened to their specified tension levels, the TBPOC launched an investigation into why these rods failed and whether the 2,210 other rods on the SAS Bridge also are at risk. The TBPOC directed its staff to investigate and report on what led to the failure of the 32 rods, what course of action is needed to address all the rods, and what implications the analysis, findings and recommendations from the investigation have on the TBPOC’s determination of the timing for opening the new East Span to traffic.

As part of the investigative process, the TBPOC has gathered and analyzed available project records pertaining to the design, specifications, fabrication and construction activities related to the A354 grade BD rods on the SAS Bridge, and synthesized the technical analysis into this report. Specifically, the TBPOC did the following:

- Conducted four workshops on April 17, May 1, May 15, and June 25, 2013;
- Met over 25 times in person or by phone;
- Consulted with industry experts, the Seismic Peer Review Panel, and the Federal Highway Administration Review Panel;
- Reviewed over 50 documents and over 5,000 pages of material;
- Briefed the Bay Area Toll Authority (BATA) and the BATA Oversight Committee on March 27, April 10, April 24, May 8, and May 29, 2013;
- Presented and responded to questions during the California Senate Transportation and Housing Committee hearing on May 14, 2013; and
- Briefed members of the Bay Area State Legislative Delegation on June 6, 2013.

## Three Investigation Questions

The TBPOC prepared this report in order to determine whether the issues pertaining to the A354 grade BD rods on the SAS Bridge have been satisfactorily addressed and, more importantly, to enable us to reach an informed decision on when the new East Span can open to traffic. The three key questions for this investigative report are:

1. What led to the failure of the A354 grade BD high-strength steel rods on shear keys S1 and S2, which were manufactured in 2008, on Pier E2 of the SAS Bridge?;

2. What retrofit strategy should be used to replace the lost clamping force of the rods?; and
3. What should be done about the other 2,210 A354 grade BD high-strength rods used elsewhere on the SAS Bridge?

## A354 Grade BD Rods on the SAS Bridge

The SAS Bridge of the new East Span contains a total of 17 different types of A354 grade BD rods at seven different locations, for a total of 2,306 rods. Table ES-1 summarizes the location, description and quantity of rods used for each of the 17 rod types, and Figure ES-1 shows the locations where these rods are used on the SAS Bridge.

Of the total 2,306 rods, 288 3-inch diameter A354 grade BD high-strength steel rods are located in Pier E2 (48 rods at each of the four shear keys and 24 rods at each of the four bearings – see Items #1 and #2 in Table ES-1). These 288 high-strength steel rods connect the shear keys and bearings to the top of the E2 pier cap. In addition, there are 544 rods connecting the shear keys and bearings to the orthotropic box girders (OBG's) above them — see Items #3 and #4 in Table ES-1. As noted in Table ES-1, these rods are at the highest tension levels on the SAS Bridge.

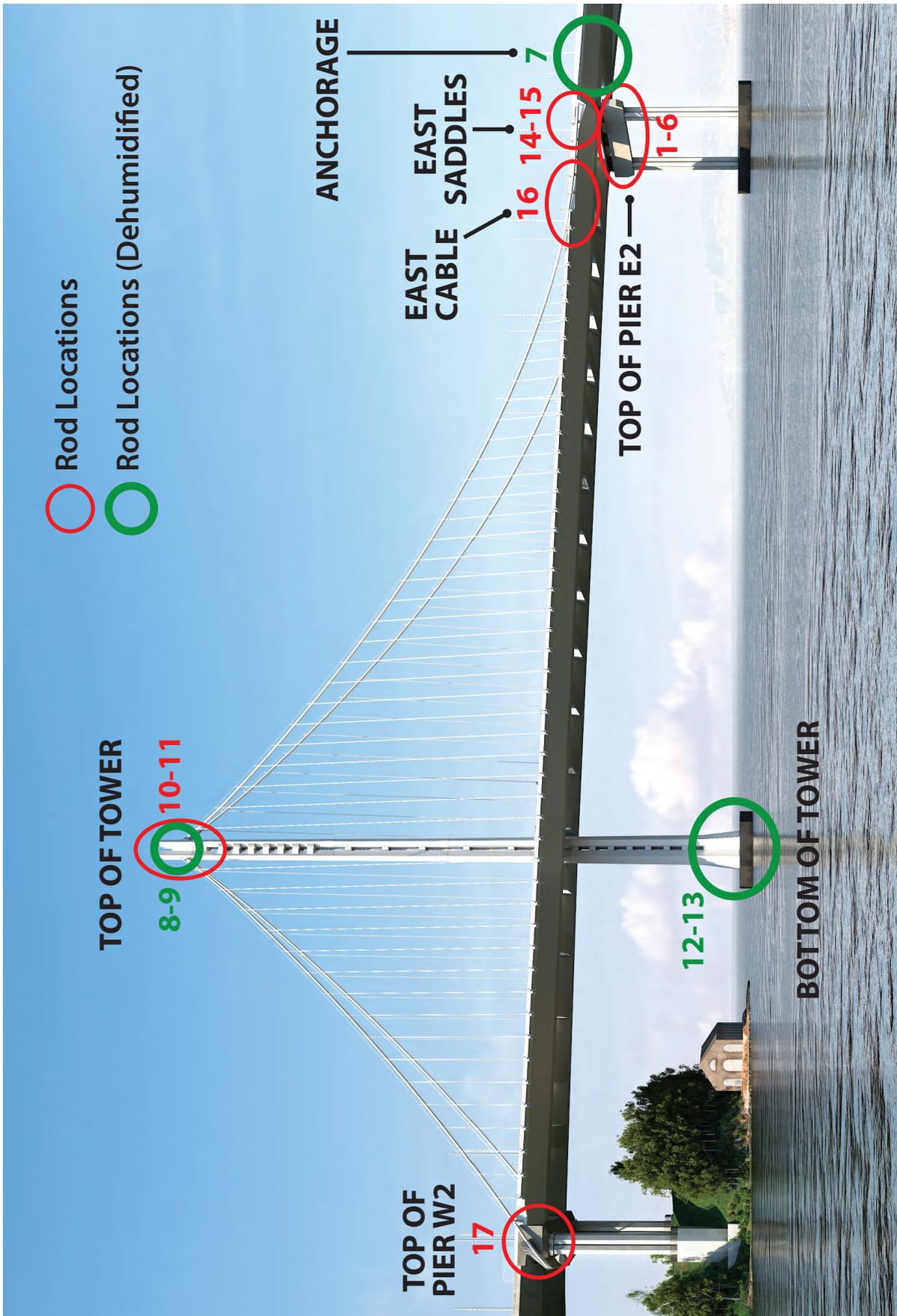
**Table ES-1 A354 Grade BD Rods on the SAS Bridge**

Item No.	Location	Component	Quantity Installed	Diameter (in)	Length (ft)	Tension (fraction of Fu*)
1	Top of Pier E2	Shear Key Anchor Rods (2008)	96	3	10-17	0.7
2		Bearing & Shear Key Anchor Rods	192	3	22-23	0.7
3		Shear Key Rods (top)	320	3	2-4.5	0.7
4		Bearing Rods (top)	224	2	4	0.7
5		Bearing Assembly	96	1	2.5	0.6
6		Bearing Retainer Ring Plate Assembly	336	1	0.2	0.4
7	Anchorage	Parallel Wire Strand (PWS) Anchor Rods	274	3.5	28-32	0.3
8	Top of Tower	Saddle Tie Rods	25	4	6-18	0.7
9		Saddle Turned Rods	108	3	1.5-2	0.5
10		Saddle Grillage	90	3	1	0.1
11		Outrigger Boom	4	3	2	0.1
12	Bottom of Tower	Tower Anchor Rods (Type 1)	388	3	26	0.5
13		Tower Anchor Rods (Type 2)	36	4	26	0.4
14	East Saddles	East Saddle Anchor Rods	32	2	3	0.1
15		East Saddle Tie Rods	18	3	5	0.2
16	East Cable	Cable Band Anchor Rods	24	3	10-11	0.2
17	Top of Pier W2	Bikepath Anchor Rods	43	1.2	1.5	TBD**
		<b>TOTAL QUANTITY</b>	<b>2,306</b>			

\*Fu = Design-specified minimum ultimate tensile strength. Numbers rounded to the nearest tenth.

\*\*Details for bike path support frame being redesigned to improve consistency with other design features of SAS.

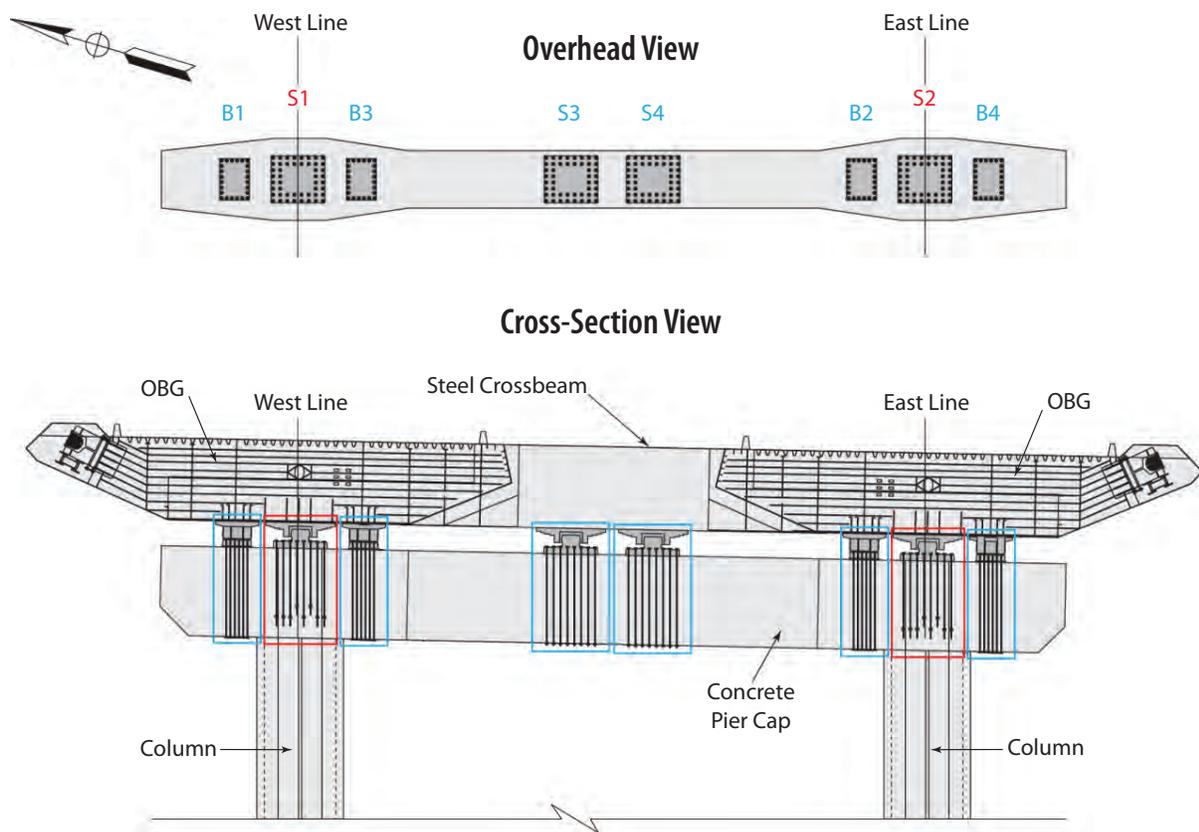
Figure ES-1 A354 grade BD rod locations on the SAS Bridge



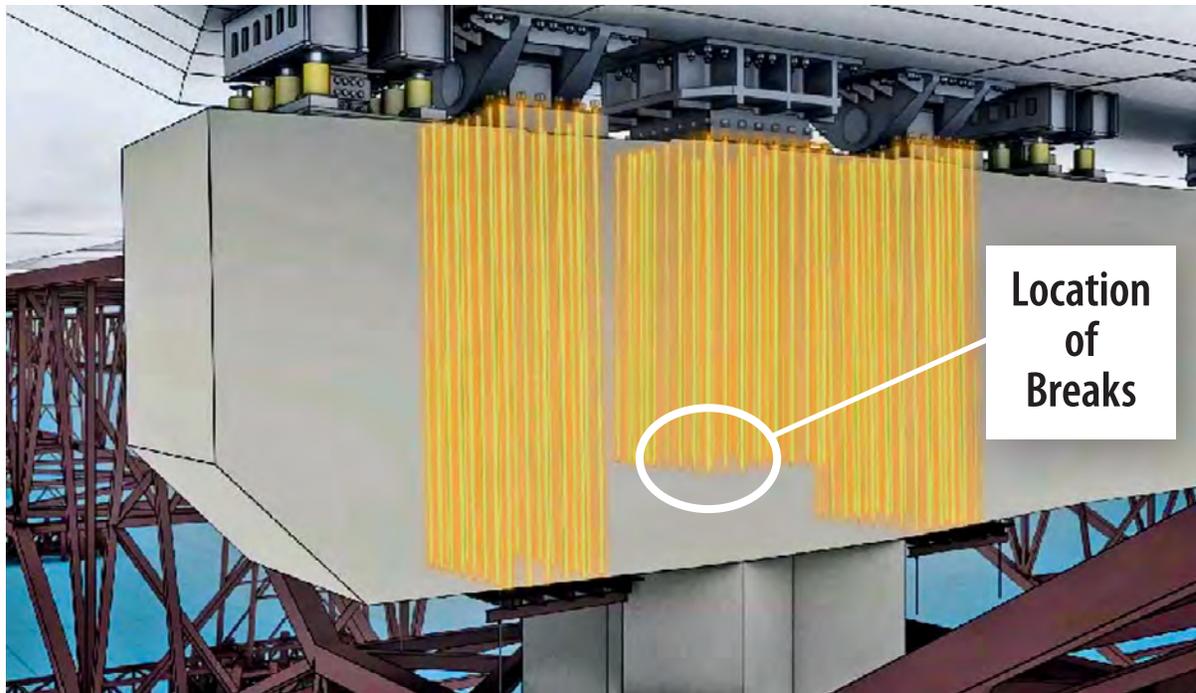
# Question 1: What Led to the Failure of the A354 Grade BD Steel Rods on Shear Keys S1 and S2 at Pier E2?

Ninety-six (96) high-strength steel rods are installed on the lower housing of shear keys S1 and S2 (Item #1 in Table ES-1) at Pier E2. These rods were fabricated by Dyson Corporation in Ohio between June 4, 2008 and September 6, 2008 and installed by American Bridge/Fluor Joint Venture, the bridge contractor for the SAS Bridge, in October 2008. Figure ES-2 illustrates Pier E2 and the location of the shear keys, bearings, and their high-strength steel rods. Figure ES-3 shows the location of the fractured rods.

**Figure ES-2 Bearings (B1-B4) and Shear Keys (S1-S4) in Pier E2**



**Figure ES-3: Location of Failed A354 Grade BD Anchor Rods**



On March 1, 2013, following load transfer of the weight of the OBG roadway decks from the temporary falsework onto the main cable, American Bridge/Fluor Joint Venture tensioned the anchor rods at shear key S2. Between March 2 and March 5, 2013, American Bridge/Fluor Joint Venture tensioned the anchor rods at shear key S1. In accordance with contract plans and submittals, the rods were initially jacked to 0.75 Fu (i.e., 75 percent of their specified minimum ultimate tensile strength). Due to seating losses as the load is transferred from the hydraulic jack to the nut, the load then settled to its final design load of 0.68 Fu.

Between March 8, 2013 and March 14, 2013, 32 out of the 96 rods were discovered to have fractured. By March 14, 2013, Caltrans decided to lower the tension of the remaining unbroken rods from the 0.68 Fu to 0.45 Fu to avoid further fractures and to allow for investigation of the cause of the failures. The tension level was reduced on all unbroken rods. If the tension had not been reduced, it is possible that more of these 2008 high-strength steel rods at shear keys S1 and S2 would have fractured.

A metallurgical investigative team, composed of a consultant to American Bridge/Fluor Joint Venture (Salim Brahimi), a Caltrans metallurgist (Rosme Aguilar), and a consultant to Caltrans who is also principal/founder of Christensen Materials Engineering (Conrad Christensen), was tasked with examining the cause of the failures of the 2008 high-strength steel rods (Item #1 in Table ES-1).

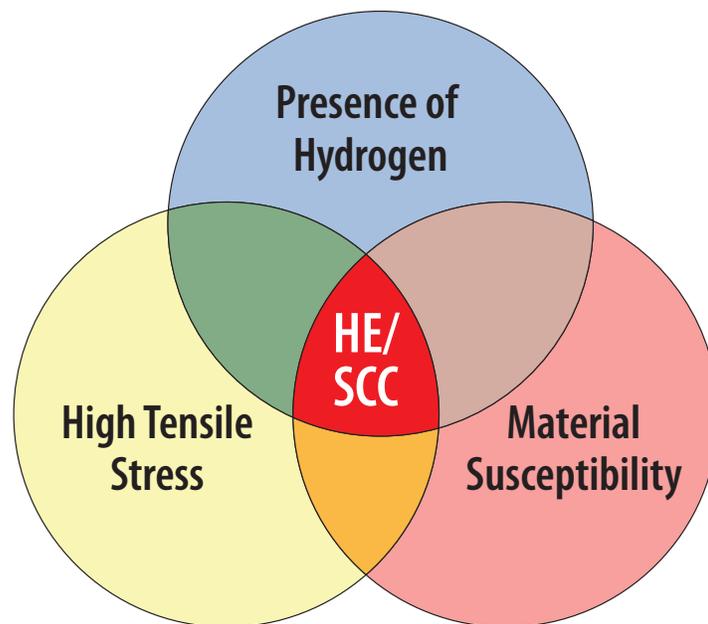
Based on its examination of two of the extracted high-strength steel rods, the metallurgical investigation team on April 23, 2013, found that the rods failed due to hydrogen embrittlement, which is the process by which metals become brittle and fracture following exposure to hydrogen. The team concluded the following:

1. *The anchor rods failed as a result of hydrogen embrittlement (HE), resulting from the applied tensile load and from hydrogen that was already present and available in the rod material as they were tensioned. The root cause of the failures is attributed to higher than normal susceptibility of the steel to hydrogen embrittlement.*
2. *The steel rods comply with the basic mechanical and chemical requirements of ASTM A354 grade BD.*
3. *The metallurgical condition of the steel was found to be less than ideal. More precisely, the microstructure of the steel is inhomogeneous resulting in large difference in hardness from center to edge, and high local hardness near the surface. As an additional consequence of the metallurgical condition, the material exhibits low toughness and marginal ductility. The combination of all of these factors has caused the anchor rods to be susceptible to HE failure.*
4. *Procurement of future A354 grade BD anchor rods should include a number of standard supplemental requirements to assure against HE failure. The appropriate specification of supplemental requirements is currently under review.*

## Summary of the TBPOC Investigation

Hydrogen embrittlement is the root cause for the failure of the A354 grade BD high-strength steel anchor rods at shear keys S1 and S2 (Item #1 in Table ES-1). As used in this report, hydrogen embrittlement is considered a short-term phenomenon that occurs in metals, including high-strength steel, when three conditions apply: a susceptible material, presence of hydrogen and high tensile stress (as shown in Figure ES-4). To trace what led to the rod failures, this summary calls out each of the three hydrogen embrittlement conditions, and then tracks the events and decisions that either caused or contributed to that condition. In their totality, these events and decisions led to the failure of the 2008 A354 grade BD rods in March 2013.

**Figure ES-4 Causes of Hydrogen Embrittlement (HE) or Stress Corrosion Cracking (SCC)**



## 1. Material Susceptibility

### Selection of A354 Grade BD Rods

The San Francisco-Oakland Bay Bridge was designated by Caltrans in October 1994 as an important “lifeline” structure because of its location along crucial transportation corridors. In short, this means that the Bay Bridge is to provide a high level of post-earthquake transportation service for emergency response and support for the safety and economic livelihood of the Bay Area. Combined with the West Span seismic retrofit, the retrofit of the west Yerba Buena Island viaduct and Yerba Buena Island tunnel, and the West Approach replacement, the replacement of the East Span would complete the lifeline connection across San Francisco Bay. Because of the Bay Bridge’s designation as a lifeline structure, Caltrans required that the East Span Replacement Project incorporate design elements that exceed the requirements of standard seismic bridge design. The East Span Replacement Project was designed to withstand massive seismic accelerations expected only reoccur once every 1,500 years. The bridge’s expected life span is 150 years, so there is approximately a 10 percent chance that such an earthquake would happen during its life span.

T.Y. Lin International/Moffatt & Nichol Design Joint Venture, the Engineer of Record, required the use of high-strength pre-tensioned rods and slip critical connections at Pier E2 to forge a strong physical bond at high-load locations on the SAS Bridge, taking into account bridge type, seismic design requirements, specified design loads and site-specific requirements (such as geology and geotechnical conditions). They selected A354 grade BD rods for use on the SAS Bridge as indicated in the SAS Design Criteria, which were finalized on July 15, 2002. Beyond the design requirements for a high-strength material, the decision to use A354 grade BD steel rods was also due to sole-source restrictions that discouraged use of proprietary rods, unless

it could be established that there were no alternatives. Alternative high-strength rods such as F1554 and A722 rods were available for consideration by bridge designers for use on the SAS Bridge but not pursued due to sole-source restrictions.

## Hot-Dip Galvanization

High-strength steels over 150 ksi possess a metallurgical structure that can have an affinity for hydrogen. The A354 grade BD high-strength steel rods for the SAS Bridge were hot-dipped galvanized to protect the steel from corrosion (except for Item #6 in Table ES-1). Hot-dip galvanization could make the A354 grade BD rod material susceptible to hydrogen embrittlement because the process requires the use of heat in which the fabricated steel is dipped into a bath of molten zinc at approximately 850°F. Too much heat could cause the release of internal hydrogen and when encapsulated in the zinc coating increases the risk of hydrogen embrittlement.

Correspondence between Caltrans and the T.Y. Lin International/Moffatt & Nichol Design Joint Venture in 2003 indicates that both parties were aware of the challenges with hot-dip galvanizing the A354 grade BD rods and the potential for hydrogen embrittlement. To avoid the problem, the initial specifications for the SAS Bridge contracts required the rods to be mechanically galvanized — a method of galvanizing that would subject the rods to less heat and less potential for hydrogen embrittlement — versus hot-dip galvanizing. However, a bidder inquiry at the time of advertisement of the East Pier/Tower (E2/T1) Marine Foundation Contract noted an inability to mechanically galvanize the large 3-inch and 4-inch diameter tower anchor rods. After further investigation, the general conclusion among both T.Y. Lin International/Moffatt & Nichol Design Joint Venture and Caltrans design staff was that the tower rods were too long and too heavy for the mechanical process.

In March 2003, SAS design staff learned that the Richmond-San Rafael Bridge Seismic Retrofit Project also included A354 grade BD rods that were galvanized for corrosion protection. The Richmond-San Rafael Bridge Seismic Retrofit Project had changed its requirement for mechanical galvanizing of A354 grade BD rods to hot-dip galvanizing (because of the size of the rods), with an explicit instruction to use dry blast cleaning in lieu of cleaning in a pickling solution prior to galvanizing. The rods on the Richmond-San Rafael Bridge project were installed, in many locations underwater, to a low-tension snug-tight fit, without any apparent problems. Based on Caltrans' experience on the Richmond-San Rafael Bridge, and by adding a requirement that certified test results be submitted for conformance to ASTM A143, the SAS Bridge design team and the Caltrans design oversight team appeared reassured that hot-dip galvanizing could be performed successfully while avoiding hydrogen embrittlement by requiring dry blast cleaning in lieu of pickling for the A354 grade BD high-strength rods. This led to the issuance of Addendum #3 to the E2/T1 Marine Foundation Contract in April 2003, which included these requirements.

There is little documented discussion regarding the variety of applications and far higher tension levels that would be placed on some of the high-strength rods on the SAS Bridge and potential alternative corrosion protection methods.

## Design and Contract Specifications

The Caltrans Bridge Design Specifications call for all ferrous bridge materials on a reinforced concrete bridge within 1,000 feet of a marine environment to be protected by hot-dip galvanizing or an equivalent protective method. Further, Caltrans Standard Special Provisions direct that high-strength fastener assemblies and other bolts attached to structural steel with nuts and washers shall be zinc-coated. For the A354 grade BD steel rods on the SAS Bridge, the T.Y. Lin International/Moffatt & Nichol Design Joint Venture selected galvanization for long-term corrosion protection. This choice was supported by the Caltrans design oversight team. The specifics on how and why galvanization was selected compared to other methods were not documented.

## Heat Treatment

The 2008 A354 grade BD rods used at Pier E2 were reported to have strength and hardness well above the minimum requirements of the specification. Also, when examined, the failed rods showed that the metallurgical structure was not uniform across the thickness of the rod and parts did not have the expected material properties. This indicates the steel production and heat treatment were not fully successful in achieving the desired uniform metallurgical structure and desired material properties. Further, Quality Assurance (QA) also noted that the 2008 rods were subjected to a second heat treatment, as the documentation for the first treatment could not be produced by the fabricator. It is not uncommon to perform a second heat treatment. However, in this case, given what is now known about the poor quality of the 2008 rod material, the second heat treatment may have further hardened and strengthened the material and contributed to the rods' susceptibility to hydrogen embrittlement.

## 2. High Tensile Stress

The failed A354 grade BD anchor rods (Top of Pier E2 – Item #1 in Table ES-1) were loaded to very high tension due to design requirements at the connections which, when combined with a susceptible metallurgical structure and low toughness, led to a high risk of failures through hydrogen embrittlement. Because the SAS Bridge project utilized specifications developed for galvanized A354 grade BD rods for the Richmond-San Rafael Bridge Seismic Retrofit Project that were only snug tight, these specifications did not fully take account the high tensile stresses and associated risk to be imposed on the Pier E2 anchor rods. The SAS Bridge specifications for the A354 grade BD rods did not limit the hardness and tensile strength nor did they require minimum toughness levels in the rod material.

## 3. Presence of Hydrogen

### Hydrogen Present in Rod Material

The metallurgical assessment of the failed A354 grade BD anchor rods (Item #1 in Table ES-1) concluded that they failed as a result of hydrogen embrittlement, resulting from the applied tensile load and from hydrogen that already was present and available in the rod material

as the rods were tensioned. The visual examinations found evidence that hydrogen-assisted cracks were present in the rods and propagated prior to failure. Furthermore, the presence and appearance of the cracks, and the delayed nature of the fractures, point to time-dependence of the failure mechanism, including hydrogen-assisted cracking. When the fracture surfaces were further examined, there were inter-granular fractures at, and near, the thread root. The rod material also was found to not be homogeneous, as evidenced by the presence of ferrite and pearlite in between layers of martensite. Additionally, while ASTM A354 grade BD specifies a maximum bulk hardness of Rockwell 39 HRC, the rods show large disparities in hardness from center to edge, indicating that the steel may not have had optimal through-thickness hardenability or that it was improperly heat treated. The rod material also lacked toughness, with low Charpy Impact values ranging from 13.5 to 17.7 ft-lb.

### Embedded Rods in Pier E2 Exposed to Environment

The failed A354 grade BD anchor rods installed at Pier E2 were manufactured by Dyson in Ohio in 2008, and were installed prior to the final concrete pour on December 5, 2008. These high-strength steel rods were embedded within the pier directly above the columns, and were sitting in ducts for five years before they were tensioned. During this five-year period, water was pumped out of the ducts a number of times at the request of Caltrans. Temporary drainage and sealing arrangements had not prevented the ingress and collection of rainwater, since it had not been anticipated that there would be such an extended period prior to completing the erection and grouting operation at Pier E2. The actual length of time during which water was present in these holes is unknown, but the presence of water may have been a contributing source of hydrogen contamination in the rods.

## Conclusion

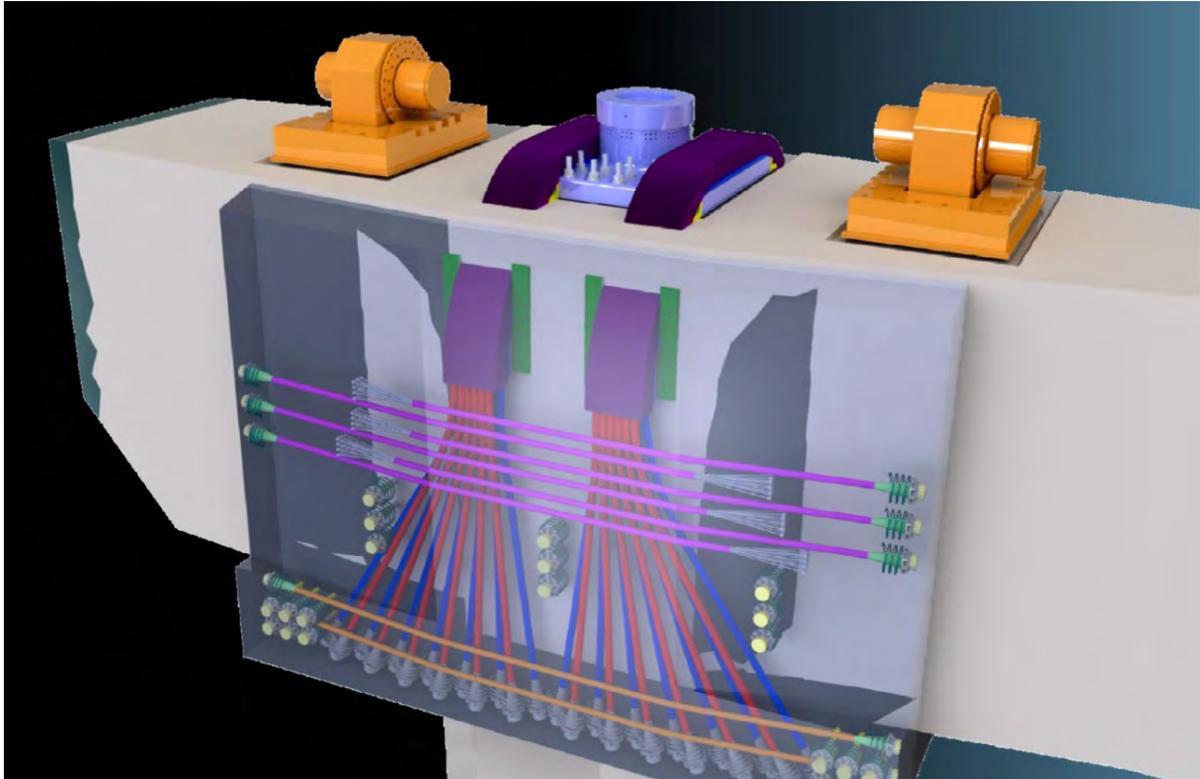
The A354 grade BD anchor rods installed on the lower housing of shear keys S1 and S2 failed due to hydrogen embrittlement. The three conditions of susceptible material, high tensile strength and the presence of hydrogen all were present, leading to crack extension and brittle fracture. The actions taken and decisions made on the design and specifications, fabrication, and construction activities are all contributing factors to the rod failures.

## Question 2: What retrofit strategy should be used to replace the lost clamping force of the rods?

The 2008 A354 grade BD rods installed in Pier E2 cannot be replaced. These rods were installed and embedded into the Pier E2 cap and are in-line with the vertical columns of the pier. In addition, the OBGs have been placed over the shear keys, further limiting access to the rods. Therefore, replacing these 96 rods would require significant destruction of the pier cap to allow for the removal of the 2008 rods and installation of replacement rods. Thus, the lost clamping force from the failure of the 2008 rods must be replaced in another fashion.

After review of three retrofit design options, on May 8, 2013, the TBPOC unanimously approved selection of the steel saddle retrofit option after finding that it would meet all design requirements and objectives of the project. As shown in Figure ES-5, it also applies a direct preload to the lower housing via the radial forces that are developed from the main vertical post-tensioning force being applied as intended in the original design. The project's Seismic Peer Review Panel also supported this option, and the American Bridge/Fluor Joint Venture indicated this option would be the easiest to construct and the fastest option to complete.

**Figure ES-5 Recent Rendering of Selected Steel Saddle Option**



## Question 3: What should be done about the other 2,210 A354 grade BD high-strength rods used elsewhere on the SAS Bridge?

### No Further Rod Failures from Hydrogen Embrittlement

A monitored, time-dependent, *in-situ* tensioning test was conducted on all remaining 192 rods to determine their susceptibility to hydrogen embrittlement. This tensioning test was conducted over a period of 30 days, which was considered sufficient time to ascertain whether ‘internal’ hydrogen was likely to embrittle the rods. Tensioning of the 192 rods was completed on April 9, 2013, at which time the 30-day *in-situ* test period began. The 30-day *in-situ* test period was completed on May 9, 2013 and resulted in no rod failures or evidence of hydrogen embrittlement. As of July 1, 2013, these rods continued to perform as designed.

As for the remaining 2,018 A354 grade BD rods, none have failed, and all have been under tension from 91 to 1,429 days as of July 1, 2013. Because hydrogen embrittlement is a time-dependent phenomenon, also dependent on the level of sustained tension, these rods have low risk of hydrogen embrittlement. In contrast, approximately 30 percent of the anchor rods in shear keys S1 and S2 failed just 3 to 10 days after tensioning to their design loads, and more might have failed if that tension level had been maintained.

### Longer-Term Risk of Stress Corrosion Cracking

Stress corrosion cracking is time-dependent — it occurs over years or decades of sustained tension and is based on the commencement and rate of corrosion. The longer-term concern is whether the remaining A354 grade BD rods are susceptible to stress corrosion cracking and, if so, when cracking may occur. Like hydrogen embrittlement, there are three factors that contribute to stress corrosion cracking — susceptible material, high tensile stress and hydrogen-related corrosion. Without any one of these three conditions, stress corrosion cracking will not occur.

Stress corrosion cracking develops in response to the tension the rod is placed under, its diameter, threads and the hardness of material. Individual rods with higher tension levels and hardness levels at or above 35 HRC should be further evaluated for risk to stress corrosion cracking.

Five tests — *in-situ* hardness test (Test I), Rockwell hardness test (Test II), Charpy V-Notch test for toughness and chemical composition (Test III), and two accelerated stress corrosion cracking tests (Townsend Test IV and Raymond Test V) — were designed to evaluate the risk of stress corrosion cracking. All tests, except for Tests IV and V, were completed by June 21, 2013. Tests II and III were conducted by independent laboratories in Texas and in Richmond, California. The results from Tests I, II and III verified the mechanical properties of the rods and categorized each rod by hardness.

Tests I, II and III for the other rods verified QC/QA test results and confirmed that the rods have low risk for near-term hydrogen embrittlement failures because the rods exhibit better metallurgical uniformity and improved toughness as compared to the failed 2008 rods. As

noted earlier, these rods have performed successfully under tension from a minimum of three months to a maximum of nearly four years.

For the longer-term stress corrosion cracking, there are a number of rods that exhibit surface hardness that is in excess of 35 HRC, a point at which there is increased risk for stress corrosion cracking under sustained high tension. However, based on the tests, these rods also exhibit better metallurgical uniformity and improved toughness. Further, many of the remaining rods are not subject to high sustained tension levels or are located in dehumidified or sealed areas that provided additional corrosion protection. Further, stress corrosion testing is underway as part of Tests IV and V that will provide important data for further analysis and remediation of the rods.

## Findings

Based on the information gathered and analysis in this investigative report, the TBPOC makes the following findings:

1. As noted in the joint Caltrans - American Bridge/Fluor Joint Venture metallurgical report dated May 7, 2013, "The [2008] anchor rods failed as a result of hydrogen embrittlement, resulting from the applied tensile load and from hydrogen that was already present and available in the rod material as they were tensioned. The root cause of the failures is attributed to higher than normal susceptibility of the steel to hydrogen embrittlement." However, that same report concluded that "the steel rods comply with the basic mechanical and chemical requirements of ASTM A354 grade BD," which was the basis of the rod specification selected by the designer and owner of the project.
2. The three factors contributing to the risk of failure due to hydrogen embrittlement are the presence of hydrogen, high tensile loads and the susceptibility of the material to hydrogen. The contract specifications for the East Span did not consider the unique requirements of the seven different rod locations on the SAS Bridge. One specification was inappropriately applied to all locations. In addition, it was inappropriate to adapt the fastener specification modified during the Richmond-San Rafael Bridge Retrofit Project, where the A354 grade BD galvanized rods were deployed underwater at low tension (snug tight), to the E2/T1 Marine Foundation and SAS Superstructure contracts for the new east span, where similar bolts were deployed above water and at considerably higher tension levels.
3. There was inadequate consideration to allow for sole-source specifications, utilizing alternative or specific mechanical properties of steel. In fact, proprietary Macalloy high-strength rods were specified for the pre-stressing rods in the W2 cap beam in the SAS special provisions. Investigation into other types of high-strength steel rods, even if they might have required sole-sourcing, appears to have been warranted.
4. There was inadequate consideration given to the combined effect of high-strength rod material requirements and corrosion protection. The fastener selection process was completed during design, and the corrosion protection specification was modified dur-

ing advertisement and construction. There was no subsequent return discussion to the fastener selection decision.

5. There was inadequate consideration of alternative corrosion protection treatments, given well-known concerns about the risk of hydrogen embrittlement from hot-dipped galvanizing of A354 grade BD rods. In particular, alternative treatments such as Geomet®, or greased and sheathed, or painted solutions should have been more fully considered depending on the various sizes and applications. A life cycle cost analysis should have been prepared for the various rod alternatives and the various methods of long-life corrosion protection.
6. The fastener specification for the E2/T1 Marine Foundations and SAS Superstructure contracts relied too heavily on generic ASTM standards and should have included special provisions reflecting a better understanding of the principles of the ASTM standards to guard against hydrogen embrittlement. In particular, the contracts should have more clearly addressed the following four requirements: 1) maximum steel hardness and through consistency, 2) minimum steel toughness, 3) magnetic particle testing, and 4) a time-dependent test of the rods under tension prior to their installation on the new bridge. As one peer review panelist noted: “National Standards are the minimum. You still need to do good engineering.”
7. The construction of Pier E2 should not have allowed for water to collect during the construction process. The collection of water in their support cylinders may have exacerbated the embrittlement of the 2008 high-strength steel rods. Because the rods were to be embedded in concrete, it was infeasible to remove and replace them. In the words of one engineer, “A good design should not be so sensitive to bad material.”
8. ASTM 143 required a hydrogen embrittlement test. The designer was aware of the potential of hydrogen embrittlement, but construction oversight technicians only tested rods with 1½-inch diameter or less. The large-diameter rods were not tested for hydrogen embrittlement and a Request for Information was not issued. Closer coordination was needed between design and construction staff.
9. It took a considerable amount of time including significant manual effort to assemble the QC/QA information for the SAS rods. In the case of the E2/T1 Marine Foundation contract, much of the information has not been located for a contract completed as recently as 2008. Such information is vital not only for an investigation of materials failure such as this, but for routine maintenance and major rehabilitation of the SAS over its 150-year design life.

## Responsible Parties

The design and construction of the Self-Anchored Suspension (SAS) Bridge of the new East Span involved several responsible parties:

- Caltrans is the owner and operator of the New East Span;
- T.Y. Lin International/Moffatt & Nichol Design Joint Venture is the Engineer of Record;

- American Bridge/Fluor Joint Venture is the contractor for the SAS Superstructure; and
- Kiewit/FCI/Manson Joint Venture is the contractor for the SAS E2/T1 Marine Foundation.

These parties are responsible for the actions that led to the following findings:

- T.Y. Lin International/Moffatt & Nichol Design Joint Venture, American Bridge/Fluor Joint Venture and Caltrans jointly share responsibility for Findings 1 and 7.
- T.Y. Lin International/Moffatt & Nichol Design Joint Venture and Caltrans jointly share responsibility for Findings 2, 3, 4, 5 and 6.
- American Bridge/Fluor Joint Venture and Caltrans jointly share responsibility for Finding 8.
- Caltrans is responsible for Finding 9.

## TBPOC Decisions and Actions

Based on the findings above and review of the 17 locations where A354 grade BD are located on the East Span, there are four categories into which this report classifies the 2,210 high-strength steel rods on the SAS Bridge:

1. Rods whose clamping capacity is to be replaced before opening the bridge to traffic;
2. Rods that are to be replaced after opening the bridge, as a precautionary measure to address concerns of longer-term stress corrosion;
3. Rods that are subject to mitigating actions, such as reduced tension, dehumidification or other corrosion protection systems; and
4. Rods that are acceptable for use, will meet performance expectations, and will undergo a regular inspection schedule.

Table ES-2 depicts a provisional approach for remediating the stress corrosion cracking potential of the various A354 grade BD rods on the SAS Bridge. These recommendations are provisional pending completion of the final tests (referred to as the Townsend Test and Raymond Test). In no case, however, do we expect the remaining tests to indicate that any rods, other than the failed Item #1 anchor rods, will need to be replaced before opening the new East Span to traffic. The risk of near-term hydrogen embrittlement has passed. The potential for longer-term stress corrosion cracking can be managed safely and effectively after the SAS is placed into service.

# Rod-by-Rod Resolution

**Table ES-2: Recommended Rod-by-Rod Resolution**

Location	Construction	Maintenance			
	Replace Before Opening	Replace After Opening	Reduce Tension	Augment Dehumidification	Accept and Monitor
E2	1. Shear Key Anchor Rods* (bottom)  *replaced by steel saddle retrofit	2. Bearing & Shear Key Anchor Rods (bottom) 3. Shear Key Rods (top) 4. Bearing Rods (top)			5. Bearing Assembly (bushings) 6. Bearing Retainer Ring Plate Assembly
Anchorage				7. PWS Anchor Rods	
Top of Tower		11. Outrigger Boom	8. Saddle Tie Rods 9. Saddle Turned Rods		10. Saddle Grillage
Bottom of Tower			12. Tower Anchor Rods (Type 1) 13. Tower Anchor Rods (Type 2)		
East Saddle					14. East Saddle Anchor Rods 15. East Saddle Tie Rods
East Cable					16. Cable Band Anchor Rod
W2					17. Bikepath Anchor Rods

Note: Dehumidification is already in place for the Top of Tower, Bottom of Tower and Main Cable Anchorage.

The rod-by-rod resolution displayed in Table ES-2 details the remediation strategy for each grouping of A354 grade BD rods. The “Replacement Before Opening” is self-explanatory. “Replace After Opening” and “Augment Dehumidification” are anticipated to occur before the end of 2014 to take advantage of the efficiencies offered by the existing contractor and the temporary work platforms that are still in place. Rods confirmed by T.Y. Lin International/Moffatt & Nichol Design Joint Venture, the Engineer of Record, as being appropriate for reduction in tension will be adjusted as soon as the load distribution ceases to change due to construction activities. The rods labeled “Accept and Monitor” do not require remediation and illustrate the fact that the original specification used for all 17 rod locations was only appropriate for fasteners installed under low tension. All high-strength rods will require routine and periodic maintenance.

## Revised Specifications for Replacement Rods

Additional high-strength steel rods are to be purchased to replace the 2010 rods on Pier E2 that have been selected for testing. The remediation strategy outlined above also will require procurement of additional high-strength steel rods. Caltrans has applied supplementary specifications for the rods identified for replacement, which limit the ultimate tensile strength, minimum toughness, maximum hardness and impose a tight tolerance on hardness, which will be measured at small intervals across the diameter, thereby ensuring homogeneous metallurgical structure. Caltrans also will be performing the time-dependent hydrogen embrittlement “pull test” required by ASTM F606 and the Townsend and Raymond Tests to determine the replacement rods’ susceptibility to stress corrosion cracking. Finally, alternative corrosion protection methods will be evaluated. The Toll Bridge Program Oversight Committee will review and approve all major actions regarding procurement of replacement rods.

## Maintenance Plan

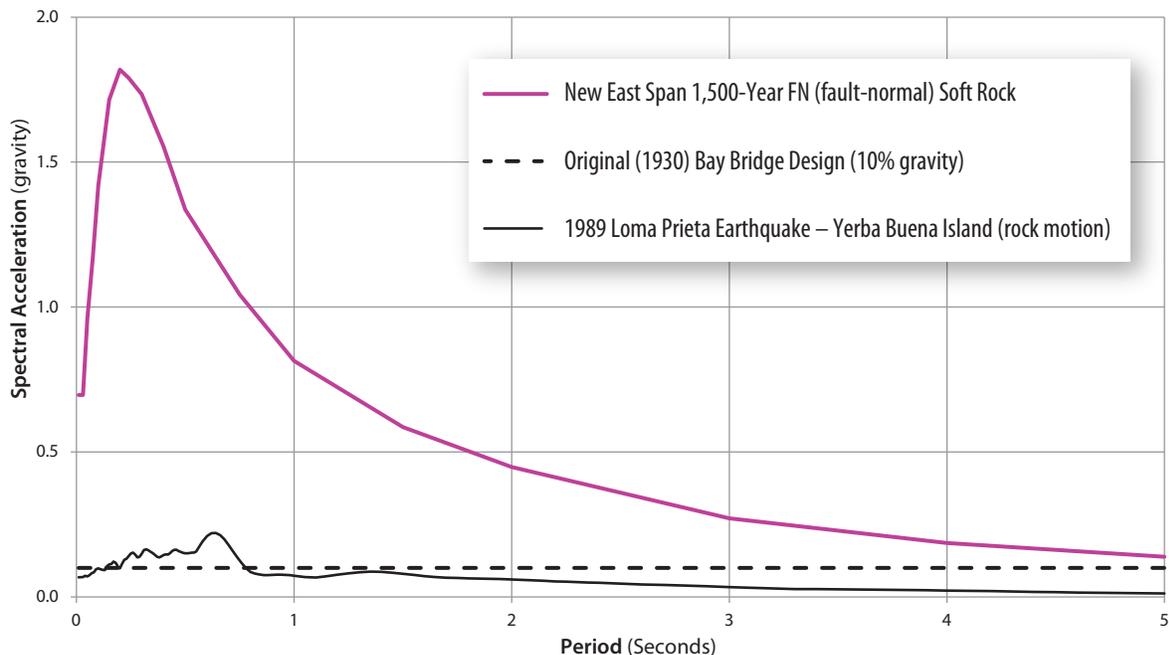
One of the tasks of the design team is to prepare Bridge Maintenance and Inspection Manuals for each of the major components of the East Span shown in Figure ES-1, as each component is completed. Each set of manuals will provide documentation on the design, documentation on the construction, load ratings, detailed inspection procedures for each major element, an initial “baseline” inspection and inventory, sources and reference material, and post-seismic inspection and repair procedures. The manuals are to be used primarily by personnel engaged by Caltrans to perform routine inspections, in-depth or special inspections, and routine maintenance on the East Span structures. Regarding the A354 grade BD rods, the maintenance plan for these elements of the SAS Bridge will include existing baseline information (test data, etc.), required monitoring and testing, inspection and testing methods to be employed, required intervals, required routine and periodic maintenance, protocols for notification and action when required, and actions required after an extreme event (earthquake, vessel collision, etc.).

## Bridge Opening

The TBPOC concludes that it is safe to open the new East Span after replacing the capacity lost by the failed 2008 rods. It is unnecessary to replace any of the remaining rods (Items #2 through #17) before the bridge opening since the risk of near-term hydrogen embrittlement has passed, and especially in light of the safety imperative of moving traffic off the seismically deficient existing East Span Bridge. While some rods are highly susceptible to longer-term stress corrosion cracking, ample evidence exists that none are at high risk of near-term fracture.

Ground accelerations have been plotted in Figure ES-6 comparing the design of the new East Span with the 1936 East Span and recorded Loma Prieta earthquake accelerations in 1989. The Loma Prieta earthquake was a 6.9-magnitude earthquake centered nearly 60 miles away from the Bay Bridge that still caused the partial collapse of a section of the existing cantilever structure. While the west spans of the Bay Bridge have been fully retrofitted, the east span of the bridge is still vulnerable until replaced.

**Figure ES-6 Comparison of Ground Accelerations**



# 1. Report Purpose

This report provides factual information on all A354 grade BD high-strength steel rods installed on the Self-Anchored Suspension (SAS) Bridge of the new East Span of the San Francisco-Oakland Bay Bridge. These A354 grade BD rods are quenched and tempered alloy steel elements that have a minimum specified tensile strength of 140 kilopounds per square inch (ksi) and a specified Rockwell hardness of 31 to 39 HRC for rods over 2½ inches in diameter. They meet the mechanical and chemical requirements defined in American Society for Testing and Materials (ASTM) A354 grade BD. The report presents the technical analysis, findings, and conclusions on what led to the failure of a portion of the A354 grade BD rods on the east pier of the SAS Bridge, as well as recommendations for addressing these and other rods used on the SAS Bridge.

The analysis focuses on three questions:

1. What led to the failure of the A354 grade BD high-strength steel rods on shear keys S1 and S2, which were manufactured in 2008, on Pier E2 of the SAS Bridge?;
2. What retrofit strategy should be used to replace the lost clamping force of the rods?; and
3. What should be done about the other 2,210 A354 grade BD high-strength rods used elsewhere on the SAS Bridge?

Based upon the findings of the investigation, this report classifies the high-strength rods into four categories:

1. Rods whose clamping capacity is to be replaced before opening the bridge to traffic;
2. Rods that are to be replaced after opening the bridge, as a precautionary measure to address concerns of longer-term stress corrosion;
3. Rods that are subject to mitigating actions, such as reduced tension, dehumidification, or other corrosion protection systems; and
4. Rods that are acceptable for use, will meet performance expectations, and will undergo a regular inspection schedule.

These rod-by-rod resolution recommendations are provisional pending completion of the final tests (referred to as the Townsend Test and Raymond Test).

Note that at the request of the Toll Bridge Program Oversight Committee (TBPOC), the Federal Highway Administration (FHWA) is conducting an independent review of the process and analysis supporting the conclusions reached in this report regarding questions 1 and 3 above. In addition, the project's independent Seismic Peer Review Panel has provided comments on the report, and will provide its written review to the TBPOC under separate cover.

## 2. Overview of San Francisco-Oakland Bay Bridge East Span Replacement Project

### Why a New East Span?

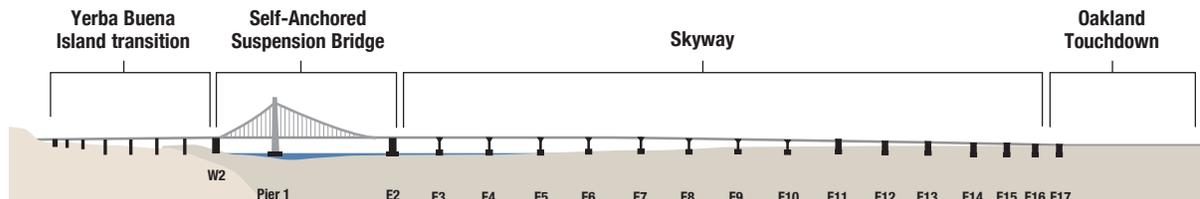
On October 17, 1989, the Loma Prieta earthquake caused a 50-foot, 250-ton section of the upper deck of the East Span of the San Francisco-Oakland Bay Bridge (Bay Bridge) to collapse onto the deck below. The earthquake reached a magnitude of 6.9 on the Richter scale — the largest earthquake the San Francisco Bay Area has experienced since the earthquake of 1906. The Loma Prieta earthquake left 63 people dead and 3,757 injured, and thousands of people were left homeless.

On January 29, 2002, construction began on the new East Span. The new East Span is 2.2 miles long on an alignment parallel to and north of the existing East Span. The original East Span will be demolished after the new East Span is opened to traffic. When completed, the new East Span will consist of four major sections (described in detail below), but will appear as a single unified structure. The new East Span will include two side-by-side bridge decks, each with five travel lanes and standard 10-foot-wide shoulders. Additionally, there will be a shared bicycle and pedestrian path located on the south side of the eastbound deck of the span.

# Components of the East Span

The new East Span of the Bay Bridge consists of four major components: 1) the Yerba Buena Island Transition Structures; 2) the Self-Anchored Suspension (SAS) Bridge; 3) the Skyway; and 4) the Oakland Touchdown approach. Figure 1 depicts the four components of the new East Span.

**Figure 1 Major Components of the East Span**



The **Yerba Buena Island Transition Structures** connect the SAS Bridge to the Yerba Buena Island tunnel, and will transition the East Span's side-by-side traffic to the upper and lower decks of the tunnel and the West Span. The new structures are made of cast-in-place reinforced concrete, with 13 supports (footings and columns).

The **Skyway** is a 1.2-mile-long elevated viaduct between the SAS Bridge and the Oakland Touchdown, with two parallel roadways that will each accommodate five lanes of traffic in each direction and two 10-foot-wide shoulders to help keep vehicles moving during a traffic incident. The Skyway has large pilings driven deep into a dense material known as the Alameda formation, and contains seismic safety devices that will enable the road decks to slide, rather than buckle, in the event of an earthquake. The Skyway's decks are composed of 452 pre-cast concrete segments, each standing three stories high and measuring 90 feet wide and 25 feet long.

The **Oakland Touchdown** connects the Skyway structure to the Oakland shoreline. These approaches are a combination of cast-in-place reinforced concrete structures and light-weight fill roadways that provide a gradual transition from the new bridge to the toll plaza.

# Self-Anchored Suspension (SAS) Bridge

The SAS Bridge, with its single 525-foot-tall steel tower, is 2,047 feet in length and like the other three East Span components is designed to withstand a massive earthquake. While traditional suspension bridges have twin cables connected to the roadway deck by smaller suspender cables, the SAS Bridge features a single continuous main cable that is anchored within the eastern end of the roadway, carried over the tower, wrapped around the two side-by-side decks at the western end, carried back over the tower, and then anchored again in the roadway at the eastern end.

## Tower

The steel tower is made up of four separate steel legs connected by shear link beams, which are designed to act like fuses and absorb most of the shock during an earthquake and to protect the tower from significant damage. The damaged beams can then be individually removed and replaced.

## Pier W2

The single main cable wraps around this pier, like a sling cradling a stone. Pier W2 holds down the cable and is supported by some of the largest foundation works ever constructed by Caltrans.

## Pier E2

Pier E2 is the first pier east of the main tower of the SAS Bridge, near the point where the twin steel orthotropic box girder (OBG) roadways of the SAS meet the concrete decks of the Skyway. Within the OBGs at this end are the anchorages for the single main cable that carries the weight of the bridge. The OBGs are connected to the pier by bearings and protected from seismic movement by shear keys. There are a total of four shear keys (S1 through S4) and four bearings (B1 through B4) at the top of Pier E2.

The SAS Bridge, together with the shear keys, has been designed to withstand a 1,500-year seismic event. The shear keys at Pier E2 are intended to transfer the forces from the combined superstructure (SAS Bridge and Skyway Bridge) into Pier E2 during a seismic event, the forces being both in the East-West and North-South directions.

Shear keys S1 and S2 are located at the centerlines of the OBGs directly above the pier columns. Shear keys S3 and S4 are located on the concrete pier cap between the OBG sections and under the steel crossbeam.

The four bearings, B1 through B4, are designed to perform the normal duty of providing fixity and accommodating thermal expansion of the OBGs during everyday use. In the event the shear keys fail during a major seismic event, the bearings also serve as a back-up system to transmit the seismic load.

## A354 Grade BD Rods on SAS Bridge

The SAS Bridge of the new East Span contains a total of 17 different types of A354 grade BD rods at seven different locations, for a total of 2,306 rods. These rods range in diameter from 1 inch to 4 inches. Table 1 summarizes the location, description and quantity of rods used for each of the 17 rod types, and Figure 2 shows the locations where these rods are used on the SAS Bridge.

Of the total 2,306 rods, 288 3-inch diameter A354 grade BD high-strength steel rods are located in Pier E2 (48 rods at each of the four shear keys and 24 rods at each of the four bearings — see Items #1 and #2 in Table 1). These 288 high-strength steel rods connect the shear keys and bearings to the top of the E2 pier cap. In addition, there are 544 rods connecting the shear keys and bearings to the OBGs above them — see Items #3 and #4 in Table 1. As noted in Table 1, these rods are at the highest tension levels on the SAS Bridge.

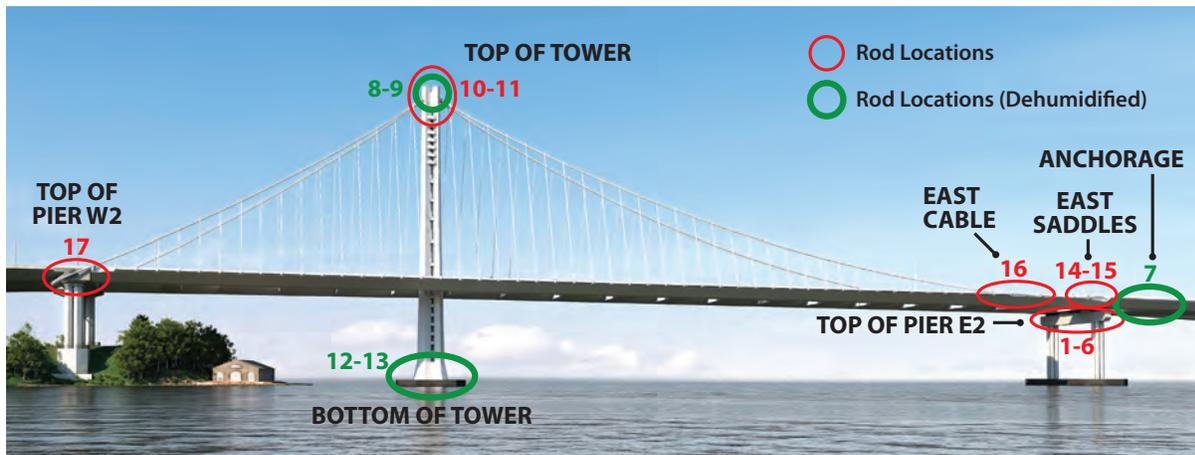
**Table 1 A354 Grade BD Rods on the SAS Bridge**

Item No.	Location	Component	Quantity Installed	Diameter (in)	Length (ft)	Tension (fraction of Fu*)
1	Top of Pier E2	Shear Key Anchor Rods (2008)	96	3	10-17	0.7
2		Bearing & Shear Key Anchor Rods	192	3	22-23	0.7
3		Shear Key Rods (top)	320	3	2-4.5	0.7
4		Bearing Rods (top)	224	2	4	0.7
5		Bearing Assembly	96	1	2.5	0.6
6		Bearing Retainer Ring Plate Assembly	336	1	0.2	0.4
7	Anchorage	Parallel Wire Strands (PWS) Anchor Rods	274	3.5	28-32	0.3
8	Top of Tower	Saddle Tie Rods	25	4	6-18	0.7
9		Saddle Turned Rods	108	3	1.5-2	0.5
10		Saddle Grillage	90	3	1	0.1
11		Outrigger Boom	4	3	2	0.1
12	Bottom of Tower	Tower Anchor Rods (Type 1)	388	3	26	0.5
13		Tower Anchor Rods (Type 2)	36	4	26	0.4
14	East Saddles	East Saddle Anchor Rods	32	2	3	0.1
15		East Saddle Tie Rods	18	3	5	0.2
16	East Cable	Cable Band Anchor Rods	24	3	10-11	0.2
17	Top of Pier W2	Bikepath Anchor Rods	43	1.2	1.5	TBD**
<b>TOTAL QUANTITY</b>			<b>2,306</b>			

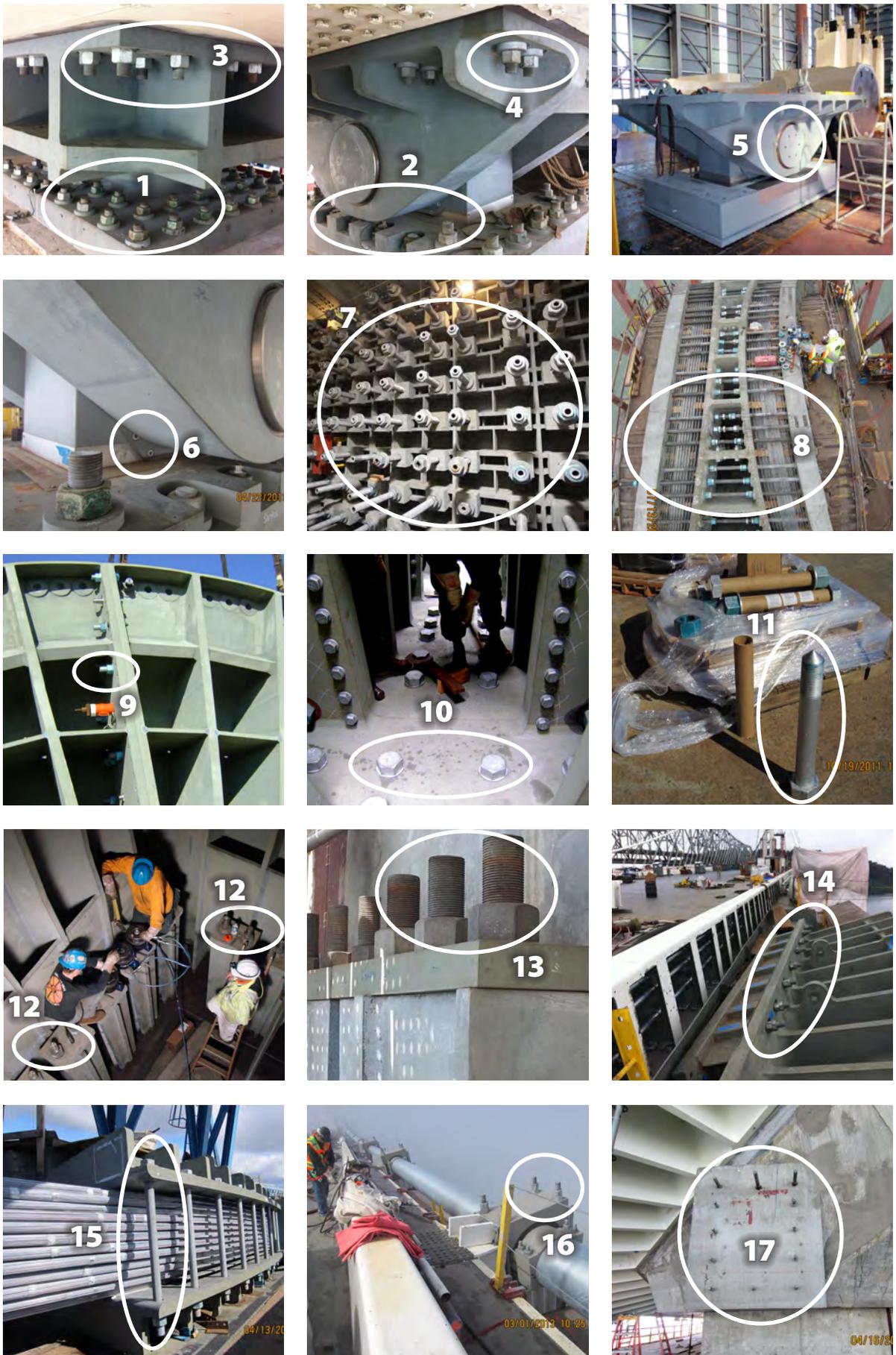
\*Fu = Design-specified minimum ultimate tensile strength. Numbers are rounded to the nearest tenth.

\*\*Details for bike path support frame being redesigned to improve consistency with other design features of SAS.

**Figure 2 A354 Grade BD Rod Locations on the SAS Bridge**

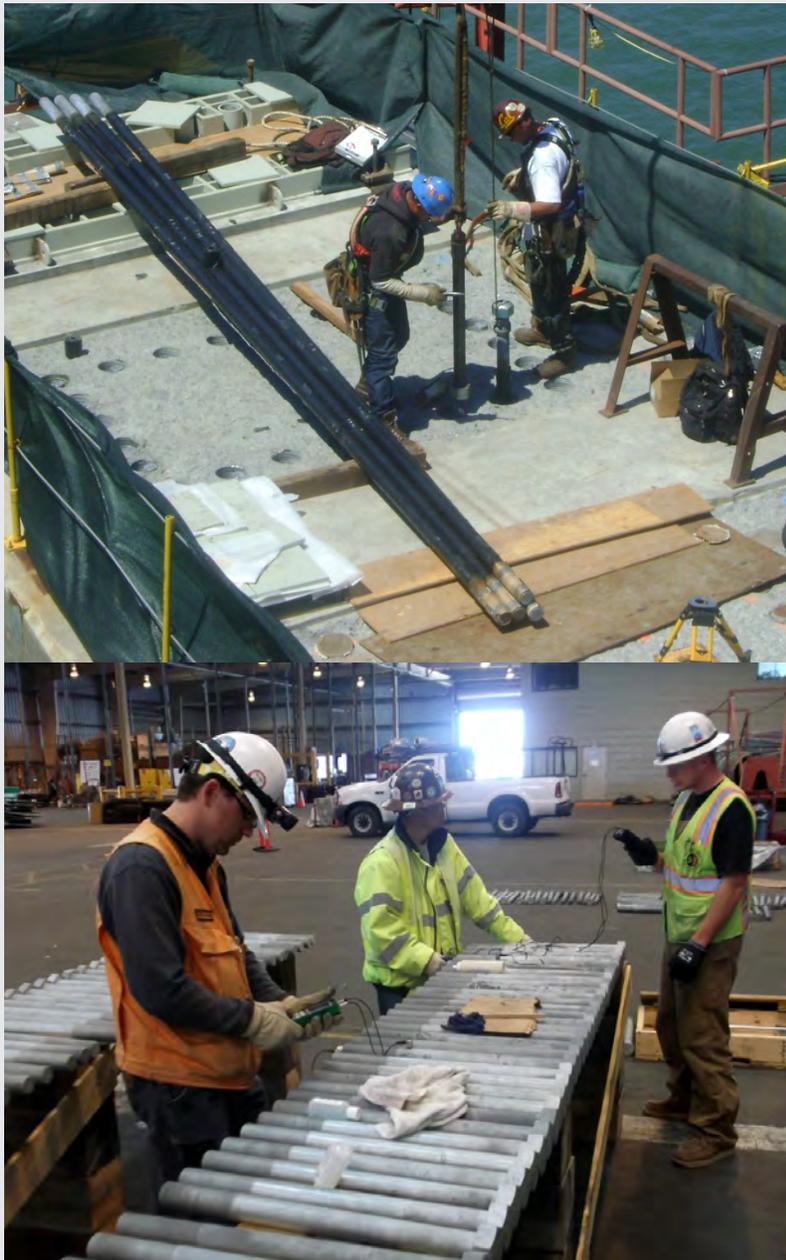


**Figure 2 A354 Grade BD Rod Locations on the SAS Bridge (continued)**



## Definition of Fasteners, Bolts and Rods

Fasteners are steel devices used to mechanically join objects together. Examples of fasteners include rivets, nuts, bolts, studs, screws, eyebolts, nails, threaded rods and washers. Bolts are fasteners that have a head on one end and threads on the other. Anchor bolts are threaded on one end and typically embedded in concrete on the other end, usually with a plate that the head or nut can bear against. Rods are fasteners with threads on each end and typically join items with the use of nuts on each end. For the SAS Bridge, the bolts and rods are made of quenched and tempered steel to ASTM standards that are intended for use in structural connections. For simplicity purposes, this report uses the standard term of “rod.” Shown below are construction photos of the Pier E2 shear key A354 grade BD rods (top) and the cable band bolts (bottom).



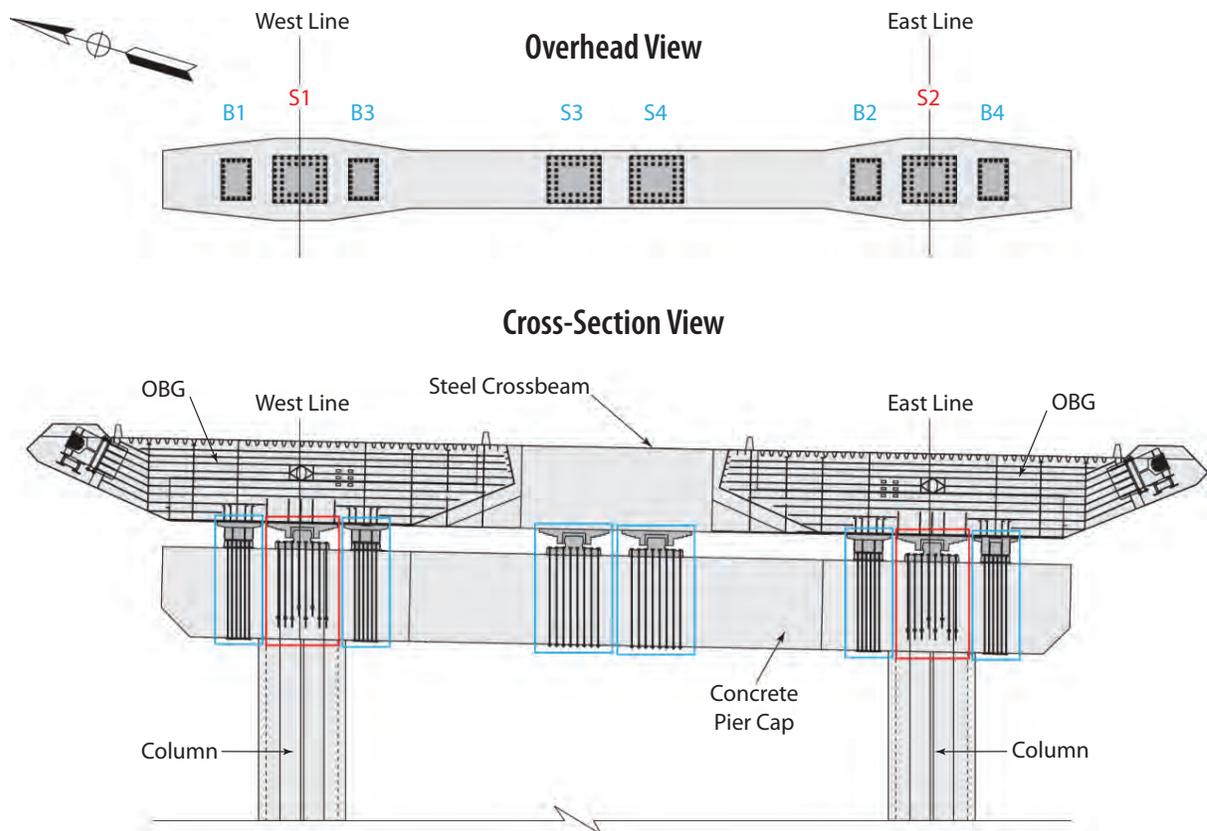
### 3. Background on Failure of the Pier E2 A354 Grade BD Shear Key Anchor Rods

#### Where Are the Failed Rods Located?

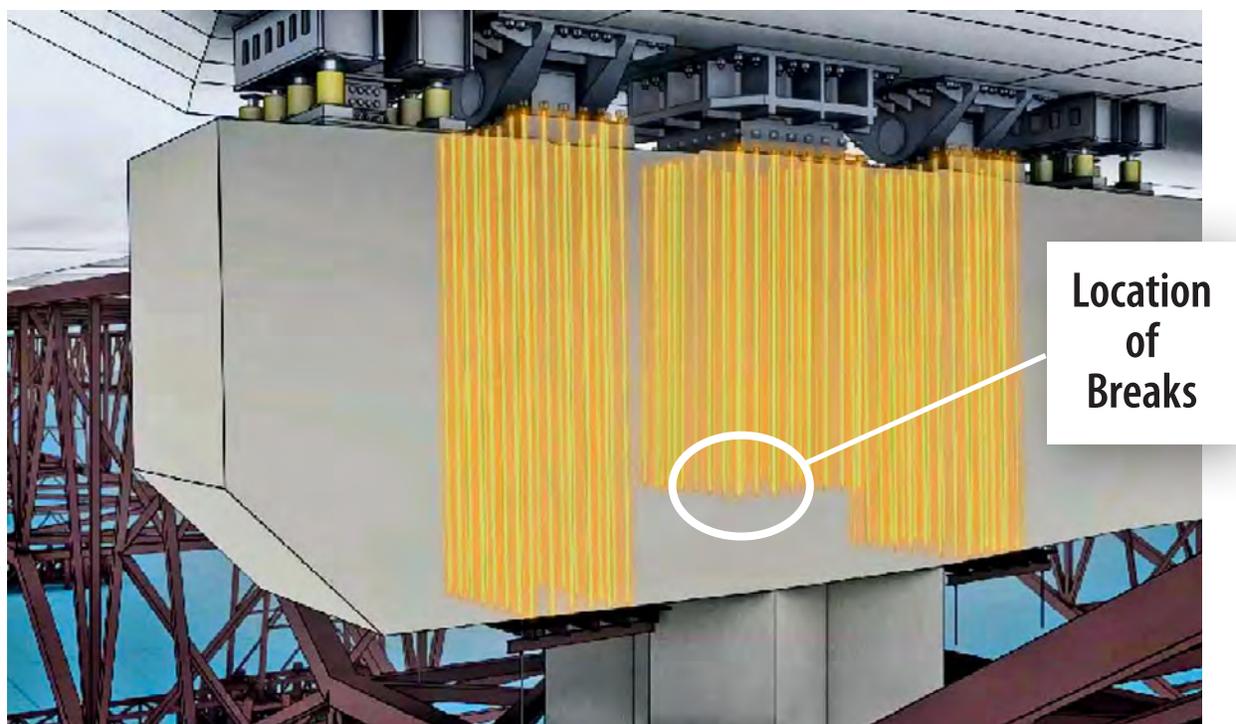
Ninety-six (96) high-strength steel rods are installed on the lower housing of shear keys S1 and S2 (Item #1 in Table 1) at Pier E2. These rods were fabricated by Dyson Corporation in Ohio between June 4, 2008 and September 6, 2008 and installed by American Bridge/Fluor Joint Venture, the bridge contractor, in October 2008.

Because of the location of shear keys S1 and S2, directly over the Pier E2 support columns, the design developed by the T.Y. Lin International/Moffatt & Nichol Design Joint Venture called for the rods to be embedded as the concrete pier cap was constructed. This rendered the lower portion of the rods and nuts inaccessible after installation. Figure 3 illustrates Pier E2 and the location of the shear keys, bearings, and their high-strength steel rods. Figure 4 shows the location of the fractured rods.

**Figure 3 Bearings (B1-B4) and Shear Keys (S1-S4) in Pier E2**



**Figure 4 Location of Failed A354 Grade BD Anchor Rods**



The E2 pier cap, including the embedded 2008 shear key anchor rods, was completed by early 2009. Due to the extended construction schedule, Pier E2 was completed three years before the roadway boxes were erected in place over the pier. This resulted in the anchor rods being exposed to the environment for an extended period of time before the next construction stage, which would tension and grout them in place. This open environment is shown in a Pier E2 construction progress photograph (Figure 5) taken soon after completion of the pier cap. There were no provisions made in the design by the T.Y. Lin International/Moffatt & Nichol Design Joint Venture or the installation procedures prescribed by American Bridge/Fluor Joint Venture to include water drainage or sufficient rain protection to prevent the ingress and accumulation of rainwater or other moisture in the anchor rod housings during this extended period.

## What Happened When the Rods Were Tensioned?

On March 1, 2013, following load transfer of the weight of the OBG roadway decks from the temporary falsework onto the main cable, American Bridge/Fluor Joint Venture tensioned the anchor rods at shear key S2. Between March 2 and March 5, 2013, American Bridge/Fluor Joint Venture tensioned the anchor rods at shear key S1. In accordance with contract plans and submittals, the rods were initially loaded to 0.75  $F_u$  (i.e., 75 percent of their specified minimum ultimate tensile strength). Due to seating losses as the load is transferred from the hydraulic jack to the nut, the load then settled to its final design load of 0.68  $F_u$ .

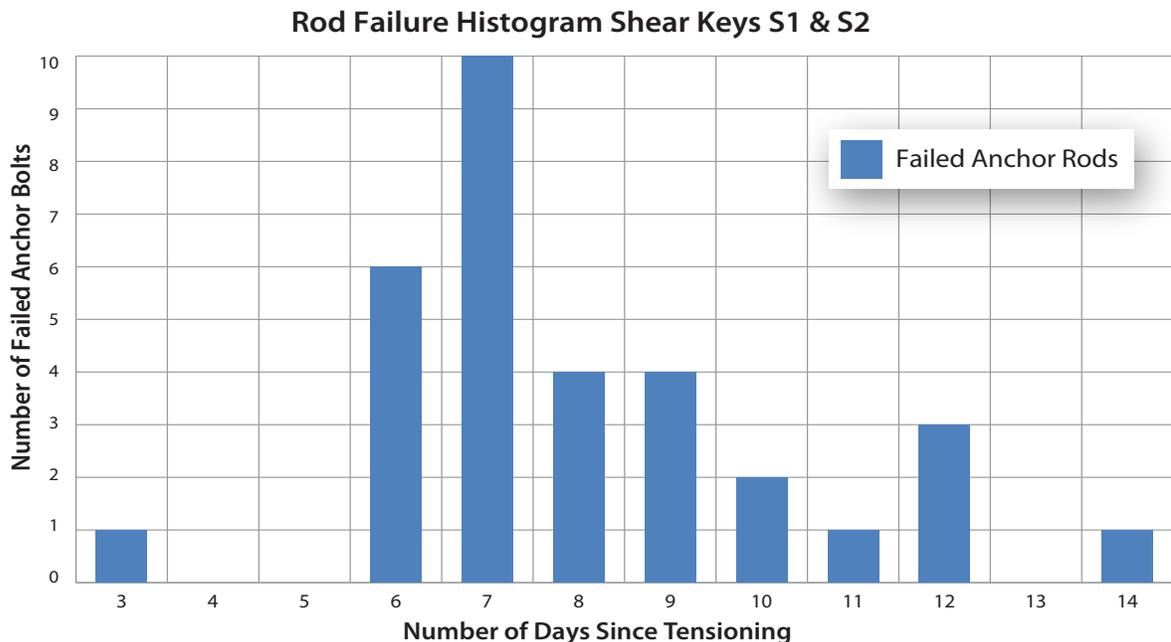
Between March 8, 2013 and March 14, 2013, 32 out of the 96 rods were discovered to have fractured. By March 14, 2013, Caltrans decided to lower the tension of the remaining unbroken rods from the 0.68  $F_u$  to 0.45  $F_u$  to avoid further fractures and to allow for investigation of the cause of the failures. The tension level was reduced on all unbroken rods. If the tension

**Figure 5 E2 Pier Cap Construction —**  
 (Center of photograph is the location of future shear key S1)



had not been reduced, it is possible that more of these 2008 high-strength steel rods at shear keys S1 and S2 would have fractured. A chart showing the number of rods that fractured after tensioning began (and the number of days it took them to do so) is shown in Figure 6. Most of these rods have since had their nuts removed and the threaded ends cut off in preparation for the installation of the steel saddle retrofit.

**Figure 6 Timeline of 2008 Anchor Rod Fractures After Stressing\***



\* The time axis shows the number of days after tensioning each individual rod was discovered fractured. Note that the tension in all non-fractured rods was reduced to 0.45 Fu after 14 days.

# What Were the Findings of the Metallurgical Analysis Conducted on the Failed Rods?

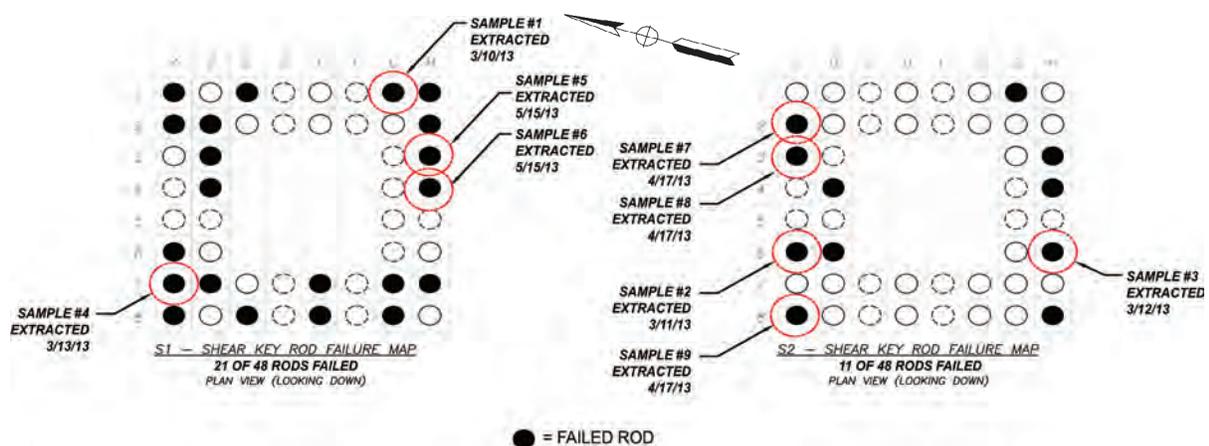
This section of the report provides a summary of the metallurgical analysis and testing performed on a sample of the failed 2008 rods.

A metallurgical investigative team, composed of a consultant to American Bridge/Fluor Joint Venture (Salim Brahim), a Caltrans metallurgist (Rosme Aguilar), and a consultant to Caltrans who is also principal/founder of Christensen Materials Engineering (Conrad Christensen), was tasked with examining the cause of the failures of the 2008 high-strength steel rods (Item #1 in Table 1). The full report of their findings is contained in Appendix H.13, but a summary is provided below and in Table 2.

The American Bridge/Fluor Joint Venture extracted nine of the 32 fractured rods. The metallurgical team concluded that a sample of nine rods was sufficient to yield reliable results about all the fractured rods based on ASTM F1470 sample sizes, and visual appearance of the fractured faces were found to be very similar. (Sample size required by ASTM F1470 is four rods.) Figure 7 illustrates the location of the 32 fractured rods and the nine extracted rods in shear keys S1 and S2, as listed below. The fractured rods were removed in multiple sections due to the small overhead clearance.

- 3/10/13: Shear Key S1 Location G1 (Sample #1)
- 3/11/13: Shear Key S2 Location A6 (Sample #2)
- 3/12/13: Shear Key S2 Location H6 (Sample #3)
- 3/13/13: Shear Key S1 Location A7 (Sample #4)
- 4/17/13: Shear Key S2 Locations A2, A3, A8 (Samples #7,8,9)
- 5/15/13: Shear Key S1 Locations H3, and H4 (Samples #5,6)

**Figure 7 Location of Fractured Rods in Shear Keys S1 and S2**



<sup>1</sup>Brahimi, Salim, Rosme Aguilar, and Conrad Christensen. "Metallurgical Analysis of Bay Bridge Broken Anchor Rods S1-G1 & S2-A6", May 7, 2013.

## Metallurgical Team

### **Rosme Aguilar, Branch Chief, Caltrans**

Mr. Aguilar is the Branch Chief of Caltrans' Structural Materials Testing Branch, responsible for quality assurance testing of structural materials product used in construction projects throughout the state. He has over 30 years of work experience as an Engineer, of which 23 of these years as a Transportation Engineer at Caltrans, 2 years as a Quality Assurance Auditor for the Technological Research Institute of the Venezuelan Petroleum Industry, and 5 years as a Researcher at a Venezuelan steel mill. Mr. Aguilar holds a Master of Science degree in Metallurgy and a Bachelor of Science degree in Metallurgical Engineering from the University of Utah. He is a licensed professional Civil Engineer in the State of California. His areas of expertise and responsibility are Quality Assurance and materials testing, but in addition he has performed or assisted in the performance of numerous materials characterization and failure analysis for Caltrans and other state agencies.

### **Salim Brahim, President, IBECA Technologies**

Mr. Brahim is a consultant to American Bridge/Fluor Joint Venture. He is the president of IBECA Technologies. He is a licensed member of the Quebec Order of Professional Engineers and has over 24 years of experience in the fastener industry. Mr. Brahim holds a Master of Science degree in Materials Engineering from McGill University in Montreal. He is the current chairman of the ASTM Committee F16 on Fasteners. He also serves on the ISO TC2 (Technical Committee on Fasteners), ASTM committees B08 (Coatings), E28 (Mechanical Testing), A01 (Steel), F07 Aerospace and Aircraft, Industrial Fasteners Institute (IFI) Standards and Technical Practices Committee, and the Research Council on Structural Connections (RCSC). Mr. Brahim is recognized and highly respected throughout the fastener industry as a leading expert in fastener manufacturing, fastener metallurgy, application engineering, corrosion prevention, failure analysis and hydrogen embrittlement.

### **Conrad Christensen, Principal/Founder, Christensen Materials Engineering**

Mr. Christensen is a consultant to Caltrans. He is the principal and founder of Christensen Materials Engineering, which provides laboratory testing and materials engineering services. He holds a Bachelor of Science degree in Materials Science and Engineering from the University of California at Berkeley. He is a licensed professional metallurgical engineer in the States of California and Nevada. He has over 32 years of experience as a metallurgist, specializing in materials testing and failure analysis. His areas of expertise include: microscopic evaluation and characterization of materials, optical microscopy, scanning electron microscopy and fracture analysis.

**Table 2 Summary of May 2013 Metallurgical Analysis of Fractured 2008 A354 Grade BD Rods** (Two Samples Analyzed)

Test	Description	Results
<b>1. Visual examination/ observations</b>	Anchor rod samples (2) inspected visually	<ul style="list-style-type: none"> <li>• Observations of both rods indicated an overall brittle appearance.</li> <li>• Evidence indicating that hydrogen-assisted cracks were present in both rods prior to failure.</li> <li>• Cracks initiated and extended from the thread root up to a depth of 0.6 inches in Rod S1-G1, and to a depth of 0.4 inches in Rod S2-A6.</li> <li>• Presence and appearance of cracks, and the delayed nature of the fractures, point to time-dependence of the failure mechanism.</li> <li>• Cracks developed and grew in both rods.</li> </ul>
<b>2. Scanning electron microscopy</b>	Fracture surfaces examined at high magnification with a scanning electron microscope (SEM) to further characterize the failure mechanism.	<ul style="list-style-type: none"> <li>• Observations revealed inter-granular fracture cracking at, and near, the thread root (i.e., crack origin). This indicates a number of brittle fracture mechanisms, including hydrogen-assisted cracking.</li> <li>• Gradually increasing mixed morphology was observed as the crack progressively grew and extended inward from the thread root.</li> <li>• Sudden fast fracture occurred when the crack reached a critical size.</li> <li>• Morphology across the final fast fracture zone was almost exclusively cleavage (brittle fracture mechanism).</li> </ul>
<b>3. Microstructural examination</b>	Cross-sections were cut from both rods and metallurgically prepared (i.e., mounted/potted, polished and etched) to examine the structure of the steel on a microscopic scale	<ul style="list-style-type: none"> <li>• Observations indicated the microstructure was generally tempered martensite, which is the normal structure associated with quenched and tempered American Iron and Steel Institute (AISI) 4140 steel.</li> <li>• However, in some areas, there was evidence of incomplete martensitic transformation, with presence of ferrite and pearlite alternating in banded layers between regions of fully transformed martensite. The banded nature of the microstructure is an indication that the material is not homogeneous.</li> </ul>

Test	Description	Results
<b>4. Hardness testing</b>	Rockwell hardness tests were conducted using a conical diamond indenter to correlate to the steel's tensile strength, wear resistance and ductility.	<ul style="list-style-type: none"> <li>• Results show variation in hardness from 25 Hardness Rockwell C (HRC) (center) to 39 HRC (outer diameter), indicating material not uniformly through-hardened. Completely uniform through-hardening is difficult to achieve in large diameter rods such as these; however, the large disparity in hardness from center to edge indicates the steel may not have had optimal through-thickness hardenability (i.e., optimal and uniform hardness throughout the thickness of the steel) or was improperly heat treated.</li> <li>• The mid-radius Rockwell C hardness values ranged from 32.5 to 36.2 HRC, which are in compliance with the A354 grade BD requirements of 31 to 39 HRC.</li> </ul>
<b>5. Tensile testing</b>	Performed on machined test specimens taken from near the outer diameter of each anchor rod.	<ul style="list-style-type: none"> <li>• Results indicate the material meets yield strength, tensile strength and elongation requirements for A354 grade BD, although elongation (i.e., ductility) was slightly above the minimum limit.</li> </ul>
<b>6. Charpy V-Notch Impact testing</b>	Performed on machined Charpy test specimens taken from near the outer diameter of each anchor rod to assess the toughness of the steel.	<ul style="list-style-type: none"> <li>• Results indicate the material lacks toughness, even when tested at room temperature. Further investigation is required to more fully assess the lack of toughness in the steel.</li> </ul> <p>Note: Charpy v-notch impact testing is not a requirement of ASTM A354. However, impact testing characterizes the toughness of the steel, which was called into question by the failures.</p>
<b>7. Chemical analysis</b>	Performed on samples of material from each anchor rod to determine chemical composition.	<ul style="list-style-type: none"> <li>• Results indicate the chemistry is consistent with AISI 4140 steel and meets the ASTM A354 grade BD requirements.</li> </ul>

Sections of Samples 1, 2, and 3 — Rod IDs S1-G1, S2-A6, and S2-H6 — were transported to the Christensen Materials Engineering lab in Alamo, California. The remaining six of the nine extracted fractured rods were transported to an American Bridge/Fluor facility at Pier 7 in Oakland, California, and have been tested, per the testing program described in this report.

Three extracted fractured rods were transported to the Christensen Materials Engineering lab. The laboratory observed, through visual examination, that all three fracture surfaces displayed similar characteristics, so two rods, Samples 1 and 2 (S1-G1 and S2-A6), were metallurgically analyzed and destructively tested from March 18, 2013 through April 11, 2013. Table 2 summarizes the different tests that were conducted and the results of each test, which included:

1. Visual examination/observations;
2. Scanning Electron Microscopy (SEM);
3. Microstructural examination;
4. Hardness testing;
5. Tensile testing;
6. Charpy V-Notch Impact testing; and
7. Chemical analysis.

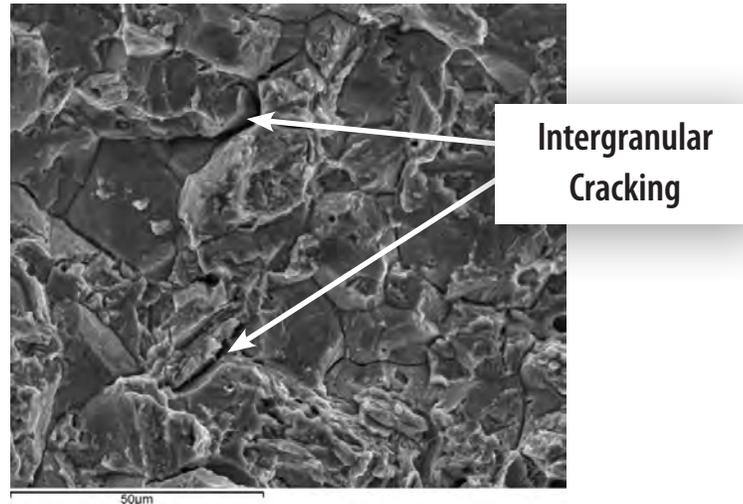
**1 Visual examination/observations:** Figure 8 is a photograph of fractured Rod S1-G1, after cleaning. The metallurgical team found that both rods had an overall brittle appearance and showed evidence of hydrogen-assisted cracks.

**Figure 8 Fracture Surface of Rod S1-G1 After Cleaning**



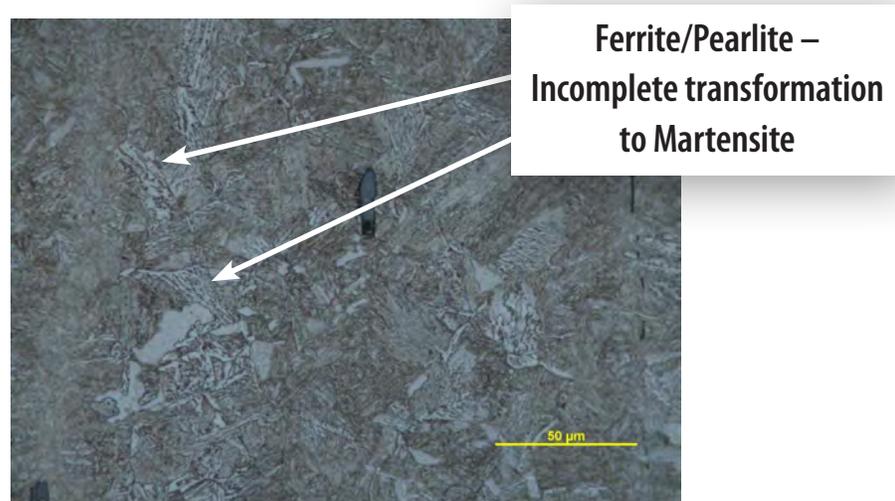
**2 Scanning Electron Microscopy (SEM):** Following visual observations, the fracture surfaces were examined at high magnification with a scanning electron microscope (SEM). Figure 9 is an SEM image of Rod S1-G1 at high magnification, which shows intergranular cracking at, and near, the crack origin. Intergranular cracking is a characteristic feature indicative of a number of brittle fracture mechanisms, including hydrogen-assisted cracking.

**Figure 9 SEM Image of Rod S1-G1 Showing Intergranular Fracture Features**



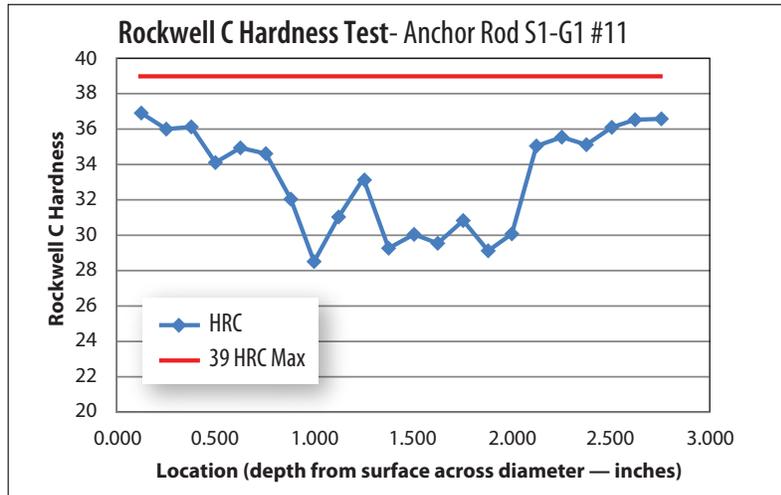
**3 Microstructural examination:** Following the SEM examination, cross-sections were cut from both rods and metallurgically prepared to examine the structure of the steel on a microscopic scale. The results of this examination (Figure 10) indicate the material is not homogeneous (i.e., not uniform in metallurgical structure across the examined sample of rod), as evidenced by the presence of ferrite and pearlite in between layers of martensite.

**Figure 10 Microstructural Examination Indicating Non-Homogeneous Material**

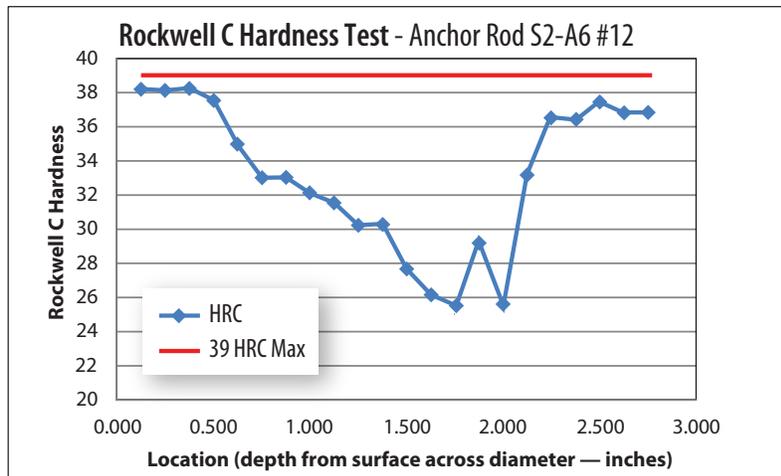


**4 Hardness testing:** The Rockwell C hardness test is a technique that assesses a material's tensile strength, wear resistance and ductility. Samples were machined at Christensen Materials Labs and tests were performed in Hayward, California by Anamet Inc., where Rockwell C hardness measurements were made across the diameter and at mid-radius locations of both rods. Figures 11 and 12 illustrate the results of the Rockwell hardness tests conducted on Rod S1-G1 and Rod S2-A6, respectively. The results of the Rockwell hardness test show variation in hardness, with the outer diameter approaching 39 HRC (high hardness is generally considered greater than 35 HRC). The center hardness drops to as low as 25 HRC, indicating the material was not uniformly through-hardened. The metallurgical report states that completely uniform through-hardening is difficult to achieve in large diameter rods such as these; however, the large disparity in hardness from center to edge indicates that the steel may not have had optimal through-thickness hardenability or was improperly heat-treated. ASTM A354 for grade BD specifies a maximum rod hardness of 39 HRC, as shown by the solid red line in Figures 11 (Rod S1-G1) and 12 (Rod S2-A6).

**Figure 11 Rockwell Hardness Test Results — Rod S1-G1**



**Figure 12 Rockwell Hardness Test Results — Rod S2-A6**



- 5 Tensile testing:** To measure the material's other properties, a tensile test was conducted, where the rods were subjected to a controlled tension until failure. Tensile testing was performed by Anamet, Inc. on two specimens taken from near the outer diameter of each fractured rod. The metallurgical team found that results indicated the material meets yield strength, tensile strength and elongation (i.e., ductility) requirements for A354 grade BD, although elongation was slightly above the minimum limit.
- 6 Charpy V-Notch Impact testing:** To assess the toughness of the steel, Charpy V-Notch impact testing was performed. A rectangular specimen with a 'V' shaped notch cut into the midpoint of the length is struck by a pendulum-mounted striker to determine the amount of energy absorbed by a material during a fracture. The metallurgical team found that results obtained for these rods ranged from 13.5 to 17.7 ft-lb when tested at 40° F. ASTM A354 does not have a Charpy V-Notch testing requirement, so as a useful comparison, the minimum requirement for general grade steel is usually 20 ft-lb when tested at room temperature and some steel grades have toughness requirements as high as 60 ft-lb (minimum). The results for the 2008 rods are low, demonstrating that these rods exhibit a lack of toughness.
- 7 Chemical Analysis:** Finally, to determine the chemical composition of the fractured rods, a chemical analysis was performed on samples of material from each anchor rod. The findings indicate the chemical composition of the rods meets the ASTM A354 grade BD requirements.

Based on its examination of two of the extracted high-strength steel rods (S1-G1 and S2-A6), the metallurgical investigation team on April 23, 2013, found that:

- 1) The chemical composition was compliant with the ASTM standards for A354 rods, even though the range of some of the tests placed individual test results outside of the specification but were statistically acceptable to the ASTM standards;
- 2) Despite meeting ASTM standards, the A354 grade BD material was susceptible to hydrogen embrittlement;
- 3) The material was not homogeneous (i.e., composed of elements that are not all of the same kind) with a mixture of ferrite, pearlite and transformed martensite banding providing varying mechanical properties (hence the wide range in test results);
- 4) There was evidence of elongated inclusions (i.e., the presence of particles in a long and thin pattern) laying in the same direction as the ferrite, pearlite and transformed martensite banding;
- 5) The hardness of the outer half-inch of the rods was significantly different than the inner two-inch core; therefore, the elasticity and distribution of the load within the material may vary; and
- 6) There was no evidence of surface corrosion near the fractures. (Subsequent visual examinations of the other six extracted rods confirmed this finding to be representative of all nine extracted rods.)

The metallurgical report's conclusions are quoted below:

- 1) *The anchor rods failed as a result of hydrogen embrittlement (HE), resulting from the applied tensile load and from hydrogen that was already present and available in the rod material as they were tensioned. The root cause of the failures is attributed to higher than normal susceptibility of the steel to hydrogen embrittlement.*
- 2) *The steel rods comply with the basic mechanical and chemical requirements of ASTM A354 grade BD.*
- 3) *The metallurgical condition of the steel was found to be less than ideal. More precisely, the microstructure of the steel is inhomogeneous resulting in large difference in hardness from center to edge, and high local hardness near the surface. As an additional consequence of the metallurgical condition, the material exhibits low toughness and marginal ductility. The combination of all of these factors has caused the anchor rods to be susceptible to HE failure.*
- 4) *Procurement of future A354 grade BD anchor rods should include a number of standard supplemental requirements to assure against HE failure. The appropriate specification of supplemental requirements is currently under review.*

## Did Other Factors Contribute to the Rod Failures?

The metallurgical report concluded that the primary cause for the failure was the susceptibility of the 2008 A354 grade BD rods to hydrogen embrittlement. The resultant microstructure of the rods was not homogeneous and the tensile strength significantly exceeded the minimum specified requirements. These properties are developed in the steel during the fabrication of the rod.

As covered later in this report, a number of other factors may also have contributed to the failure of the 2008 A354 grade BD rods. When combined with the microstructure not being homogeneous, these factors resulted in a very high failure rate of the 2008 rods. These other factors include:

- High Hardness — values greater than 35 HRC
- High Ultimate Strength — values 159–170 ksi (20% higher than minimum specified)
- High Tension Levels — 0.7 Fu
- Hot-Dip Galvanization
- Additional Heat Treatment
- An Embedded Rod Detail Exposed to the Environment

## What Is Hydrogen Embrittlement?

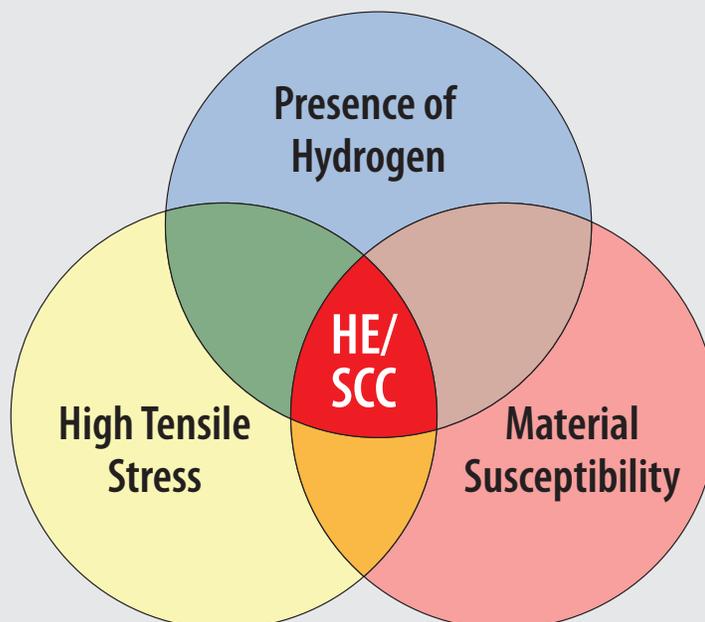
Hydrogen embrittlement (HE) is the process by which metals, including high-strength steel, become brittle and fracture following exposure to hydrogen. Excess hydrogen in a metal can migrate on an atomic level and accumulate, causing weakness/embrittlement of the material when under high stress. The embrittlement is time-dependent and typically occurs within days to a couple of weeks of stressing.

HE can seriously reduce ductility and load-bearing capacity, causing cracking and brittle failures at stresses below the yield stress of susceptible materials. High-strength steels exceeding a tensile strength of 150 ksi possess a metallurgical structure that has an affinity for hydrogen, which is increased through the application of heat usually during the manufacturing process, or when subjected to high levels of stress. There is also a risk of internal HE in high-strength steel rods having a specified minimum hardness of 33 HRC. While the specified hardness range for ASTM 354 grade BD bolts and rods is between 31 HRC and 39 HRC, ASTM F2329 emphasizes the risk of embrittlement for high-strength steel at 33 HRC and above.

The threaded section of a fastener assembly is most susceptible to hydrogen embrittlement due to the high stress concentration and the ability of hydrogen to migrate to this location.

The accompanying Venn diagram shows that when all three conditions apply (i.e., the presence of hydrogen, high tensile stress and a susceptible material), the metallurgical structure of the steel has a higher susceptibility to HE. The diagram also shows that these same conditions can cause a related phenomenon known as Stress Corrosion Cracking (SCC), which will be addressed later in this report.

### **Causes of Hydrogen Embrittlement (HE) or Stress Corrosion Cracking (SCC)**



## 4. Question 1: What Led to the Failure of the A354 Grade BD Steel Rods on Shear Keys S1 and S2 at Pier E2?

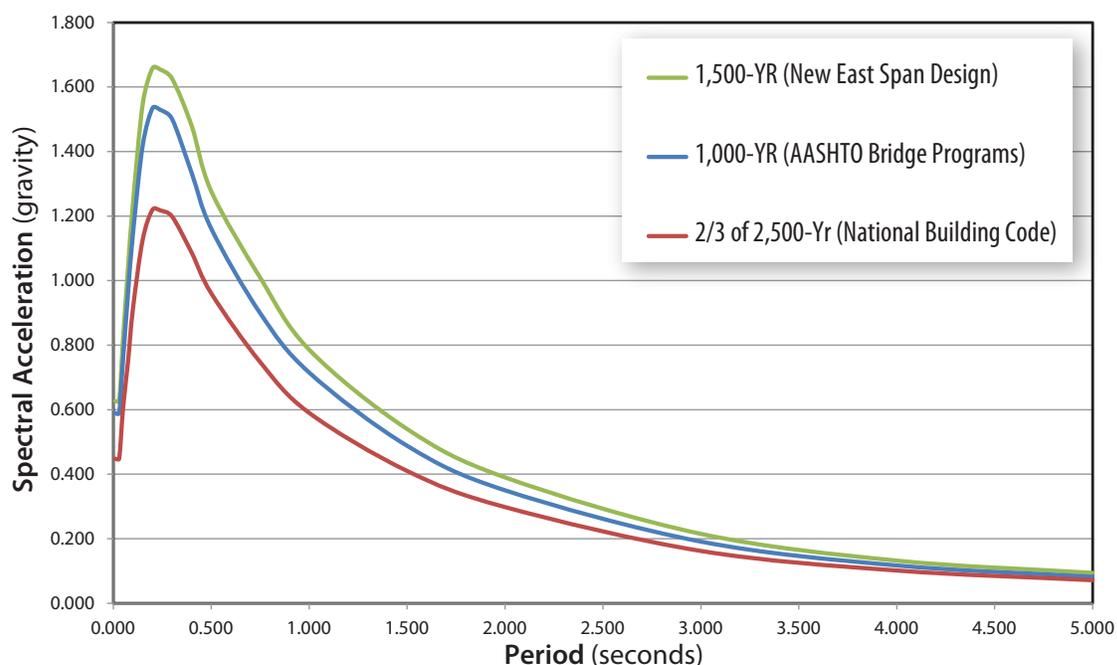
### Design & Specifications

#### Why Are High-Strength Steel Rods Required?

The San Francisco-Oakland Bay Bridge was designated by Caltrans in October 1994 as an important “lifeline” structure because of its location along crucial transportation corridors. In short, this means that the Bay Bridge is to provide a high level of post-earthquake transportation service for emergency response and support for the safety and economic livelihood of the Bay Area. Combined with the West Span seismic retrofit, the retrofit of the west Yerba Buena Island viaduct and Yerba Buena Island tunnel, and the West Approach replacement, the replacement of the East Span would complete the lifeline connection across San Francisco Bay.

Because of the Bay Bridge’s designation as a lifeline structure, Caltrans required that the East Span Replacement Project incorporate design elements that exceed the requirements of standard seismic bridge design. The East Span Replacement Project was designed to withstand massive seismic accelerations expected to only reoccur once every 1,500 years. The bridge’s expected life span is 150 years, so there is approximately a 10 percent chance that such an earthquake would happen during its life span. As indicated in Figure 13, the design ground motions from a 1,500-year return period earthquake are greater than design ground motions from the American Association of State Highway and Transportation Officials (AASHTO)’s current standard of a 1,000-year return period earthquake for highway bridges. They also exceed the standard set by the National Building Code for modern building construction.

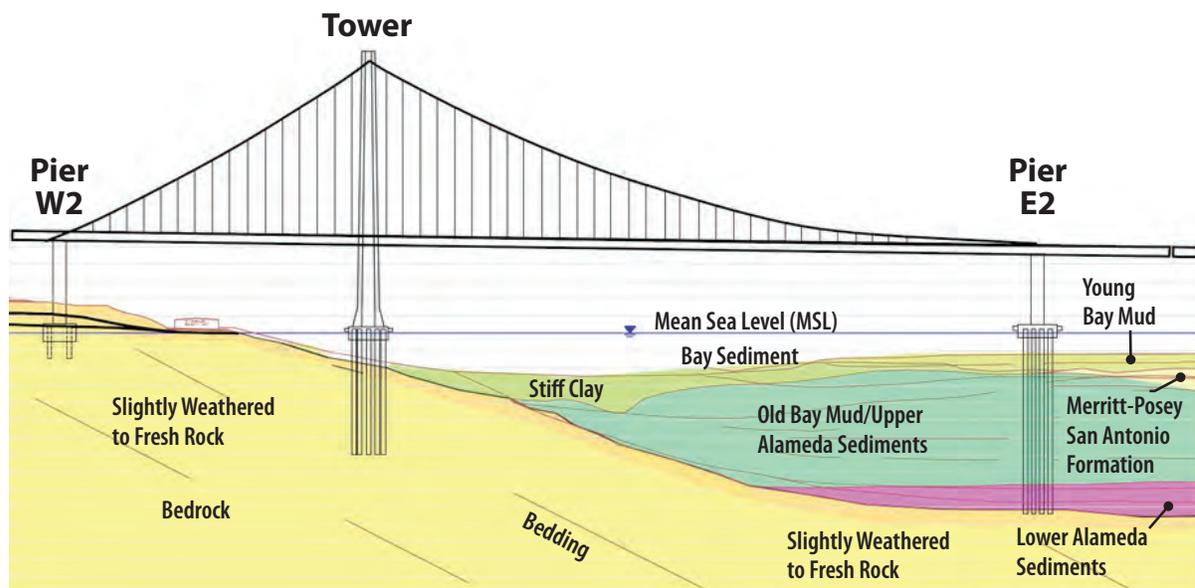
**Figure 13 Comparison of New East Span Design Ground Motions to Other Standards**



The geology and geotechnical conditions for the East Span Replacement Project were some of the challenges considered in the bridge design. As illustrated in Figure 14, the marine foundations of the main tower (T1) and west pier (W2) of the SAS Bridge are in bedrock, while the foundation of the east pier (E2) sits in bay sediments. Specifically, Pier E2 sits in the Alameda formation, which is the oldest of five formations that make up the bay sediments and is composed of layers of dense clays and sands. This means that the T1 and W2 piers will behave and shake differently than the E2 pier, if left unmitigated. To keep Pier E2 moving in harmony with the rest of the bridge during a seismic event, bridge designers determined a strong connection to the east pier was needed to withstand the high seismic loads.

To make these strong connections, and to ensure the lifeline seismic performance expected of the new east span, the T.Y. Lin International/Moffatt & Nichol Design Joint Venture's design required the use of high-strength steel rods at several locations on the SAS: Parallel Wire Strand (PWS) Anchor Rod, Cable Band Anchor Rods, East Saddle Tie Rods, East Saddle Anchor Rods, Tower Anchor Rods, Tower Saddle Tie Rods, and Pier E2 Bearing and Shear Key Anchor Rods. High-strength steel rods are commonly used throughout the bridge construction industry to make strong physical connections at high-load locations. High-loads are a function of a number of factors in design, including type of bridge, specified design loads, and site-specific requirements, like geology. On the SAS Bridge, high-load locations are inevitable given the higher-than-standard specified seismic design criteria and the challenging geology around the bridge. This, in turn, has required high pre-loads, or tensioning, to be applied for connecting restraining elements such as shear keys to provide slip resistance and minimum deformation.

**Figure 14 Geology Conditions at the SAS Bridge**



## San Francisco-Oakland Bay Bridge Designated as “Lifeline Bridge”

The San Francisco-Oakland Bay Bridge was designated by Caltrans as a “lifeline bridge.” Lifeline bridges are those whose economic consequences of failure are large, or that provide secondary life safety or are designated as important by local emergency officials. The San Francisco-Oakland Bay Bridge qualifies as a lifeline bridge, because following a major earthquake, it is expected to be restored to immediate service level — which means full access to normal traffic available almost immediately — and to be used as an emergency lifeline route.

Date	Event
May 1990	In its “Competing Against Time” report, Governor Deukmejian’s Board of Inquiry – which was tasked with investigating why the Cypress Viaduct and Bay Bridge failed during the 1989 Loma Prieta earthquake – recommended that the state “require that seismic safety be a paramount concern in the design and construction of all state-owned structures. Specific goals of this policy shall be that the state-owned structures be seismically safe and that important State-owned structures maintain their function after earthquakes.” <sup>2</sup>
June 1990	Governor Deukmejian issued Executive Order D-86-90 that states: “It is the policy of the State of California that seismic safety shall be given priority consideration in the allocation of resources for transportation construction projects, and in the design and construction of all state structures, including transportation structures and public buildings.”
September 1990	Caltrans appointed the Seismic Advisory Board, as directed by Governor Deukmejian in Executive Order D-86-90, to provide continued, focused evaluation of Caltrans seismic policy and technical procedures.
October 1994	In its “The Continuing Challenge” report to the Caltrans Director following the 1994 Northridge earthquake, the Seismic Advisory Board recommended that more emphasis must be given to starting toll bridge retrofit construction projects on as rapid a schedule as practical. The Bay Bridge was identified as a lifeline bridge. <sup>3</sup>

<sup>2</sup>Governor Deukmejian’s Board of Inquiry (May 1990). “*Competing Against Time*,” p. 9

<sup>3</sup>Seismic Advisory Board (October 1994). “*The Continuing Challenge: The Northridge Earthquake of January 17, 1994*,” p. 8.

## What High-Strength Steel Rod Options Were Available?

Table 3 shows high-strength fastener options that were available for consideration by bridge engineers for use on the SAS Bridge.

ASTM A354 is an American Society for Testing and Materials (ASTM) International standard that defines chemical and mechanical properties for a specific alloy for steel bolts, screws, studs and other externally threaded rods. A354 Specifications cover grade BC and grade BD anchor bolts, threaded rods and headed bolts for sizes 4-inch and under in diameter<sup>4</sup>. (ASTM A490 Specification covers only hexagon headed bolts up to 1 ½ -inch diameter.)

A354 grade BD rods feature minimum tensile strengths of 150 ksi for ¼-inch to 2½-inch diameter rods and 140 ksi for 2¾-inch to 4-inch diameters.

Other options for bolts and rods in excess of 1½-inch diameter include lower-strength A354 grade BC rods, with minimum tensile strength of 115 ksi, or F1554 grade 105 rods, with minimum tensile strength of 125 ksi. The lower tensile strengths of A354 grade BC or F1554 rods, however, mean more rods would be needed to do the same job a smaller number of A354 grade BD rods can do.

Equivalent-strength alternatives to the A354 grade BD rod are ASTM A722 and Macalloy rods. Williams Form Engineering Corporation and Dywidag Systems International both manufacture ASTM A722 fasteners but in 2001 neither produced rods that were as large as 3 inches in diameter<sup>5</sup>. In order to use A722 rods, bridge designers would have had to accommodate multiple potential vendor connections.

Each rod type has different material properties and associated pros and cons. Table 3 provides a comparison between various rod types.

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<sup>4</sup> For simplicity purposes, this report uses the term "rod."

<sup>5</sup> Letter from T.Y. Lin International/Moffatt & Nichol (September 2001) to Caltrans (Dr. Brian Maroney) regarding approval to use sole-source for Macalloy high-strength prestressing bars.

**Table 3 Comparison of High-Strength Steel Rod Types**

Rod Materials Type	Minimum Tensile Strength (Ksi)	Equivalent Diameter (in)	Pros	Cons
A354 Grade BD	140	3	<ul style="list-style-type: none"> <li>• High strength</li> <li>• Generally available</li> <li>• Has a minimum specified tensile strength between 140 and 150 ksi</li> </ul>	<ul style="list-style-type: none"> <li>• Susceptible to hydrogen embrittlement without due care when galvanizing</li> </ul>
A354 Grade BC	125	3.5	<ul style="list-style-type: none"> <li>• Generally available</li> <li>• Less susceptible to hydrogen embrittlement</li> <li>• Can be galvanized without cautions</li> <li>• Has a minimum specified tensile strength between 115 and 125 ksi</li> </ul>	<ul style="list-style-type: none"> <li>• Lower strength (than BD)</li> <li>• Would require more rods and larger connecting surfaces (than BD)</li> </ul>
F1554	125	3	<ul style="list-style-type: none"> <li>• Generally available</li> <li>• Less susceptible to hydrogen embrittlement</li> <li>• Can be galvanized without cautions</li> <li>• Has a minimum specified tensile strength between 125 and 150 ksi</li> </ul>	<ul style="list-style-type: none"> <li>• Lower strength (than BD)</li> <li>• Would require more rods and larger connecting surfaces (than BD)</li> </ul>
A722	150	3	<ul style="list-style-type: none"> <li>• High strength</li> <li>• Has a minimum specified tensile strength of 150 ksi</li> </ul>	<ul style="list-style-type: none"> <li>• Proprietary connectors might require waiver from sole-source restrictions</li> <li>• No domestic suppliers produced 3-inch A722 rods, proprietary or otherwise at the time specifications were prepared</li> <li>• Only available through certain suppliers</li> </ul>

## Why Were A354 Grade BD Steel Rods Selected for the SAS Bridge?

To make the strong connections, the designer selected A354 grade BD steel rods. The SAS Design Criteria, which were finalized on July 15, 2002, specify the use of ASTM A354 grade BD for a number of the structural steel connection locations. The criteria do not specifically discuss corrosion protection for any of the fasteners listed in the project-specific design criteria. Corrosion protection is typically covered in Caltrans construction contract specifications.

The highest-strength steel rods were required by the bridge design due to the low number of rod locations within the concrete pier cap at E2. At the east pier, if more rod locations were designed for, it would have required a larger upper and lower shear key and bearing base plate, which may have resulted in a larger pier cap and cross beam. These larger elements would have resulted in more mass, which would have affected the seismic forces that need to be accounted for in the design.

A354 grade BD steel is a high-strength material that is used in construction on very large bridges to make bonded connections when high loads are expected. Ungalvanized A354 grade BD rods, with high quality corrosion protection systems, have been used on comparable West Coast bridge projects including the retrofit of the Golden Gate Bridge and the construction of the new Tacoma-Narrows Suspension Bridge in the strait of Puget Sound in Pierce County, Washington.

Beyond the design requirements for a high-strength material, the decision to utilize A354 grade BD steel was due, in part, to sole-source restrictions that discouraged use of proprietary rods, unless it could be established that there was no alternative. A354 grade BD rods are generally available and could be competitively bid. To source alternative materials, bridge designers would have had to sole-source a vendor to complete the rod connector design, pass design responsibilities to the contractor to complete the connector design, or design to accommodate multiple vendor connections. Nonetheless, Caltrans did sole-source materials elsewhere on the project, including: 1) Macalloy bars for the western anchor connection of the SAS Bridge to Pier W2 and seismic Hinge K pipe beam anchors between Pier W2 and the deck of the Yerba Buena Island Transition Structure; 2) piston motor driven trolleys, the passive trolleys, and the brake trolleys for the SAS Maintenance Traveller; and 3) the components for epoxy asphalt binder and epoxy asphalt bond coat used on the roadway surface. In each instance sole-source waivers were requested and obtained, establishing the lack of any comparable item that could be competitively bid. However, non-proprietary materials are typically specified whenever possible.

An example of sole-source for the new East Span project relates to rods located at the base of Pier W2 tiedown. The jack size requirements and space limitations at this location required the use of 75mm high-strength steel conforming to ASTM A722. The Design Engineer contacted four major manufacturers and none manufactured rods that conformed to these specifications, except Macalloy. A sole-source approval was requested by the Engineer and subsequently granted by Caltrans and FHWA.

## Sole-Source Restrictions

Most public contract work in California is controlled by the provisions of the California Public Contract Code. This code represents the efforts of the California legislature to gather into one place all statutory enactments that deal with public contracts, such as laws that govern competitive bidding. Per Public Contract Code, Section 3400:

*3400. (b) No agency of the state, nor any political subdivision, municipal corporation, or district, nor any public officer or person charged with the letting of contracts for the construction, alteration, or repair of public works, shall draft or cause to be drafted specifications for bids, in connection with the construction, alteration, or repair of public works, (1) in a manner that limits the bidding, directly or indirectly, to any one specific concern, or (2) calling for a designated material, product, thing, or service by specific brand or trade name unless the specification is followed by the words "or equal" so that bidders may furnish any equal material, product, thing, or service."*

However, in some cases, the above code section is not applicable, such as when the awarding authority determines that a particular material or product is the only one that will fulfill the needs of the project (referred to as "sole-source"). Caltrans' Office of Structure Design requires that the Specifications Engineer obtain the necessary approvals from the Chief, Division of Structures and the FHWA. In addition, if a product is required for which there is only one known manufacturer, special firm price quotes must be obtained from the manufacturer for inclusion in the contract documents.

At the federal level, the Federal Highway Administration regulation in 23 CFR 635.411, "Material or product selection," prohibits the expenditure of Federal-aid funds on a Federal-aid highway project "for any premium or royalty on any patented or proprietary material, specification, or process" (referred to hereafter as "proprietary product"), unless specific conditions are met. This regulation is intended to ensure competition in the selection of materials, products, and processes while also allowing the opportunity for innovation where there is a reasonable potential for improved performance. Also, in accordance with 23 CFR 635.411, State Departments of Transportation (DOTs) may specify a higher standard of performance (i.e., above what would normally be set) on certain construction projects even though it would result in a single product being available.

An example of sole-source procurement for the new East Span project relates to rods located at the base of Pier W2 tiedown. The jack size requirements and space limitations at this location required the use of 75mm high-strength steel conforming to ASTM A722. The Design Engineer contacted four major manufacturers and none manufactured rods that conformed to these specifications, except Macalloy. A sole-source approval was requested by the Engineer and subsequently granted by Caltrans and FHWA.

## How Should the A354 Grade BD Steel Rods Be Protected From Corrosion?

When exposed to the atmosphere, all metals, except precious metals such as gold and silver, have a natural tendency to corrode. Steel is an excellent building material, but it is inevitable that steel will corrode. The most commonly used method to adequately protect exposed steel rods and bolts from corrosion is to galvanize them by applying a zinc coating. However, galvanizing is not the only method for providing corrosion protection. Other methods include, but are not limited to, sheathing the rods in grease or grout, paint, or other coatings like Geomet® or Dacromet®. Each option provides different levels of corrosion protection and challenges for application.

## What Are the Risks Associated With Galvanization?

The two most common galvanization methods for A354 steel rods are hot-dip galvanizing and mechanical galvanizing. Table 4 summarizes the differences between the two galvanization methods. In general, a hot-dip galvanization process requires the use of heat in which the fabricated steel is dipped into a bath of molten zinc at approximately 850 °F. High-strength steels over 150 ksi possess a metallurgical structure that can have an affinity for hydrogen, which is increased through the application of heat or when subjected to high levels of stress. A mechanical galvanization process does not require heat and is performed at room temperature by tumbling the fabricated steel in a barrel to cold-weld the zinc coating onto the surface.

While hot-dip galvanization may be more cost-effective and provide better coverage of the zinc coating, careful attention must be paid to the application of heat. Too much heat could cause the release of internal hydrogen and when encapsulated in the zinc coating increases the risk of hydrogen embrittlement. While the use of mechanical galvanization at room temperature may minimize the affinity for hydrogen, the process of tumbling end-over-end and rolling steel pieces that are long, heavy, or have large diameters may be difficult to do for most galvanizers. In addition, tumbling threaded rods can damage the threads.

**Table 4 Galvanizing Methods**

Method	Description	Process
<b>Hot-Dip Galvanizing</b>	A process of dipping fabricated steel into a kettle or bath of molten zinc at a temperature of around 850 °F. While the steel is in the kettle, the iron metallurgically reacts with the molten zinc to form a tightly bonded alloy coating that provides corrosion protection to the steel.	<ol style="list-style-type: none"> <li>1. Steel is cleaned using a caustic solution to remove oil/grease, dirt, and paint.</li> <li>2. The caustic cleaning solution is rinsed off.</li> <li>3. The steel is pickled in an acidic solution (typically for 20 minutes) to remove mill scale.**</li> <li>4. The pickling solution is rinsed off.**</li> <li>5. A flux, often zinc ammonium chloride, is applied to the steel to inhibit oxidation of the cleaned surface upon exposure to air. The flux aids the process of the liquid zinc wetting and adhering to the steel.</li> <li>6. The steel is dipped into the molten zinc bath and held there until the temperature of the steel equilibrates with that of the bath.</li> <li>7. The steel is cooled in a quench tank to reduce its temperature and inhibit undesirable reactions of the newly-formed coating with the atmosphere.</li> </ol> <p>**When there is a risk of hydrogen embrittlement, these operations are replaced by dry abrasive cleaning (grit blasting) and flash pickle (less than 30 seconds) wash/rinse.</p>
<b>Mechanical Galvanizing</b>	A room-temperature process in which steel pieces are tumbled in a barrel with a mixture of water, zinc powder, other chemicals, and glass impact beads. As the parts are tumbled in the slurry, the zinc is “cold welded” to the piece without the use of heat.	<ol style="list-style-type: none"> <li>1. The steel piece is cleaned either by an acid pickling process or by using a degreaser/descaler.</li> <li>2. The piece is rinsed.</li> <li>3. The piece is then tumbled in a mixture of various-sized glass beads and a predetermined amount of water, with small amounts of chemicals and powdered zinc added periodically. Collisions between the glass beads, zinc, and the piece cause a cold-welding process that applies the zinc coating.</li> <li>4. Powdered zinc is added until the specified thickness is attained.</li> </ol>

Regardless of the kind of galvanization method used, the steel is subjected to a cleaning process prior to galvanizing to remove surface impurities. There are two methods to prepare the steel for galvanizing, depending on the tensile strength of the steel.

For high-strength steels that are not susceptible to hydrogen embrittlement (e.g., A354 grade BC steels), the steel can go through a pickling process, followed by a water bath rinse. Pickling is a process in which a solution containing strong acids (usually a hydrochloric acid) is used to remove the surface impurities of the steel. The steel being pickled typically remains in the acid solution for 20 minutes depending upon the thickness of the oxide layer. When dry blast (abrasive) cleaning in-lieu of pickling, the steel is first dry blast cleaned then flash pickled for less than 30 seconds.

For high-strength steels that are susceptible to hydrogen embrittlement (e.g. A354 grade BD steel), the pickling process and water rinse can be replaced by abrasive blasting and flash pickling (less than 30-second dip) to avoid the potential absorption of hydrogen by the steel, which can occur through the lengthy initial pickling process.

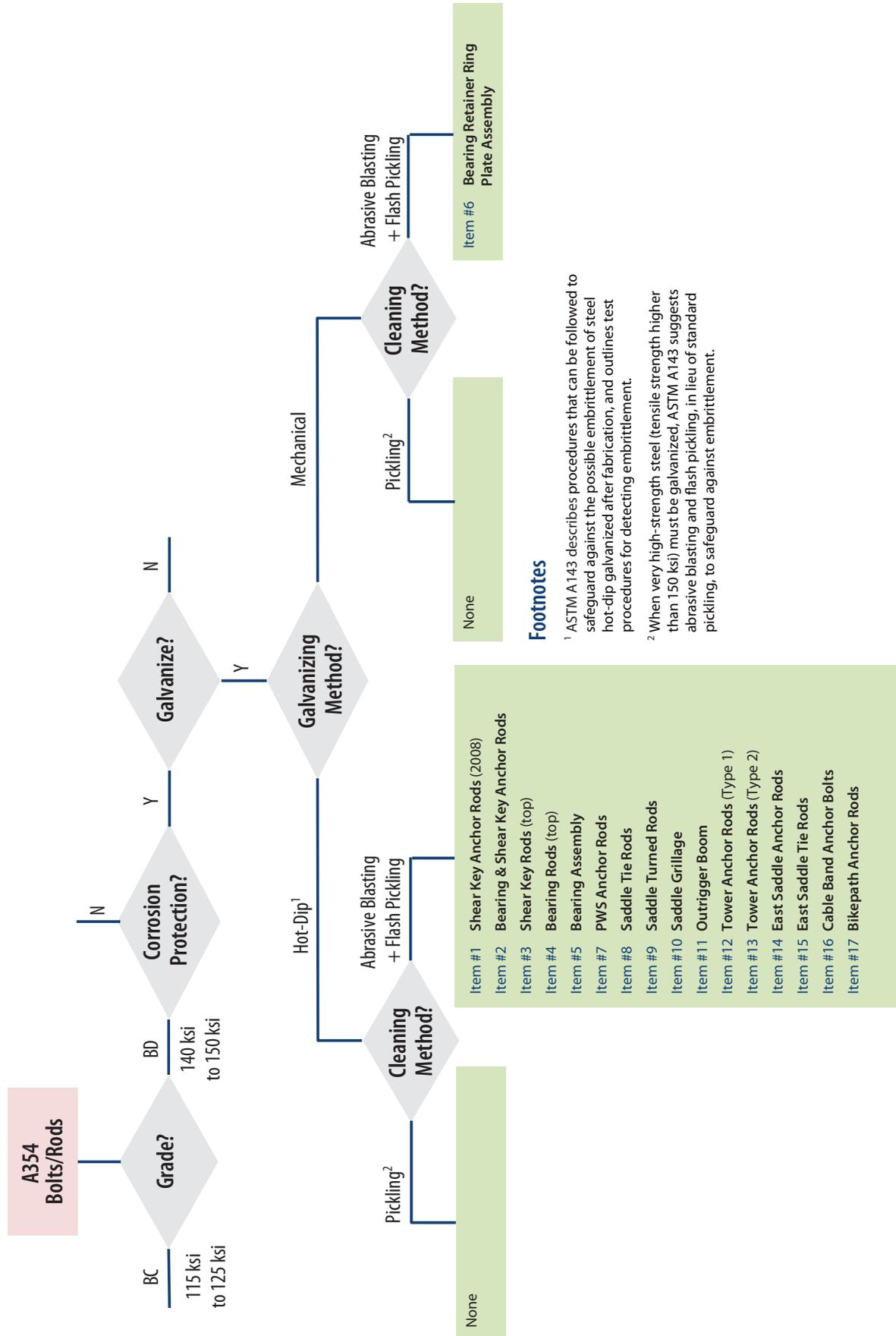
The galvanization process used for the A354 grade BD steel rods placed on the SAS Bridge is illustrated in Figure 15. Project documents indicate that all A354 grade BD steel rods were cleaned by the abrasive blast and flash pickling process and then hot-dip galvanized, except for Item #6 rods which were mechanically galvanized. The flash pickling process minimizes the potential for hydrogen absorption.

The steel fabrication industry has developed and published Standards and Codes of Practice, such as ASTM A143 (Safeguarding Against Embrittlement of Hot-Dip Galvanized Structural Steel Products & Procedure for Detecting Embrittlement), which provides guidance on how to reduce the risks associated with galvanizing high-strength steels. Excerpts from ASTM A143:

*7.1 Hydrogen can be absorbed during pickling and in some instances, noted in 4.2, may contribute to embrittlement of the galvanized product. The likelihood of this, or of surface cracking occurring, is increased by excessive pickling temperature, prolonged pickling time and poor inhibition of the hydrogen absorbed during pickling.*

*7.2 Abrasive blast cleaning followed by flash pickling may also be employed when over-pickling is of concern or when very high strength steel, ultimate tensile strength higher than 150 ksi, must be galvanized. The flash pickling after abrasive blast cleaning is used to remove any final traces of blast media before hot-dip galvanizing.*

**Figure 15 Galvanization Process Flowchart for the SAS Bridge**



**Footnotes**

<sup>1</sup> ASTM A143 describes procedures that can be followed to safeguard against the possible embrittlement of steel hot-dip galvanized after fabrication, and outlines test procedures for detecting embrittlement.

<sup>2</sup> When very high-strength steel (tensile strength higher than 150 ksi) must be galvanized, ASTM A143 suggests abrasive blasting and flash pickling, in lieu of standard pickling, to safeguard against embrittlement.

## What Corrosion Protection Method Was Selected for the Rods on the SAS Bridge?

For the East Span Replacement Project, Caltrans required the bridge to have a 150-year design life, making long-term corrosion protection an important consideration. The Caltrans Bridge Design Specifications call for all ferrous bridge materials on a reinforced concrete bridge within 1,000 feet of a marine environment to be protected by hot-dip galvanizing or an equivalent protective method. Further, Caltrans Standard Special Provisions direct that high-strength fastener assemblies and other bolts attached to structural steel with nuts and washers shall be zinc-coated. For the A354 grade BD steel rods on the SAS Bridge, the T.Y. Lin International/Moffatt & Nichol Design Joint Venture selected galvanization for long-term corrosion protection. This choice was supported by the Caltrans design oversight team. The specifics on how and why galvanization was selected compared to other methods were not documented.

Industry standards and practices cautioned about the risks associated with hot-dip galvanizing the A354 grade BD material because of susceptibility to hydrogen embrittlement, as follows:

1. The April 2000 update of the Caltrans Bridge Design Specifications Manual prohibits the galvanization of A354 grade BD rods due to hydrogen embrittlement problems.
2. ASTM A354 guidelines caution the use of hot-dip galvanizing on A354 grade BD materials, because the process could make the steel more susceptible to hydrogen embrittlement.
3. ASTM A143 provides guidance on the “Standard Practice for Safeguarding Against Hydrogen Embrittlement of Hot-Dip Galvanized Structural Steel Products and Procedure for Detecting Embrittlement.”
4. General industry concern over hot-dip galvanizing of A354 grade BD rods, including suppliers that will not galvanize this type of high-strength fastener<sup>6</sup>.

In regard to the April 2000 *Caltrans Bridge Design Specifications Manual's* restriction on galvanizing A354 grade BD rods, the design of the SAS Bridge began in early 1998 and is based on the 1995 *Caltrans Bridge Design Specifications Manual*, which was silent on the use of, and galvanizing of, A354 grade BD rods. To avoid potential design conflicts, releases of new design specifications typically are not applied mid-stream to projects already in design. As an example, the SAS Bridge contract specified the use of metric units. Newly-updated specifications required the use of English units. Updating the entire contract using English units would have been extremely costly and could have resulted in dimensional conflicts so Caltrans decided to continue design using metric units. Further, exceptions to standard bridge design specifications are allowed when necessary to meet project-specific needs. For these reasons, updates to the *Caltrans Bridge Design Specifications Manual*, released after design started, were not retroactively applied to the East Span Replacement Project.

While ASTM A354 cautioned that hot-dip galvanizing of A354 grade BD materials could make them more susceptible to hydrogen embrittlement, the guidelines did not preclude galvanizing.

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<sup>6</sup> See website notices and cautions from Portland Bolt & Manufacturing Company and American Galvanizers Association.

Further, ASTM A143 on “Standard Practice for Safeguarding Against Hydrogen Embrittlement of Hot-Dip Galvanized Structural Steel Products and Procedure for Detecting Embrittlement” suggests the elimination of pickling — a pre-galvanizing cleaning process — may reduce the risk of hydrogen embrittlement when galvanizing. For example, on the Golden Gate Bridge Seismic Retrofit, pickling was explicitly eliminated for A354 grade BD rods even though those rods were not to be galvanized.

Correspondence between Caltrans and the T.Y. Lin International/Moffatt & Nichol Design Joint Venture in 2003 indicates that both parties were aware of the challenges with hot-dip galvanizing the A354 grade BD rods and the potential for hydrogen embrittlement. To avoid the problem, the initial specifications for the SAS Bridge contracts required the rods to be mechanically galvanized — a method of galvanizing that would subject the rods to less heat and less potential for hydrogen embrittlement — versus hot-dip galvanizing. However, a bidder inquiry at the time of advertisement of the East Pier/Tower (E2/T1) Marine Foundation Contract noted an inability to mechanically galvanize the large 3-inch and 4-inch diameter tower anchor rods. After further investigation, the general conclusion among both T.Y. Lin International/Moffatt & Nichol Design Joint Venture and Caltrans design staff was that the tower rods were too long and too heavy for the mechanical process.

In March 2003, SAS design staff learned that the Richmond-San Rafael Bridge Seismic Retrofit Project also included A354 grade BD rods that were galvanized for corrosion protection. The Richmond-San Rafael Bridge Seismic Retrofit Project had changed its requirement for mechanical galvanizing of A354 grade BD rods to hot-dip galvanizing (because of the size of the rods), with an explicit instruction to use dry blast cleaning in lieu of cleaning in a pickling solution prior to galvanizing. The rods on the Richmond-San Rafael Bridge project were installed, in many locations underwater, to a low-tension snug-tight fit, without any apparent problems. Based on Caltrans’ experience on the Richmond-San Rafael Bridge, and by adding a requirement that certified test results be submitted for conformance to ASTM A143, the SAS Bridge design team and the Caltrans design oversight team appeared reassured that hot-dip galvanizing could be performed successfully while avoiding hydrogen embrittlement by requiring dry blast cleaning in lieu of pickling for the A354 grade BD high-strength rods. This led to the issuance of Addendum #3 to the E2/T1 Marine Foundation Contract in April 2003, which included these requirements.

There is little documented discussion regarding the variety of applications and far higher tension levels that would be placed on some of the high-strength rods on the SAS Bridge and potential alternative corrosion protection methods.

Table 5 presents the timeline of major design and contract milestones for the Bay Bridge East Span replacement project related to the use of A354 grade BD galvanized high-strength rods. These major milestones also are depicted in the timeline in Figure 16.

**Table 5 Major Design and Contract Decision Timeline**

Date	Event
<b>August 1994</b>	The Caltrans Bridge Design Specification Manual is updated. In this new 1995 Caltrans Bridge Design Specification Manual and in all previous releases, "Section 10 - Structural Steel," does not address the use of A354 grade BD high-strength rods.
<b>January 1998</b>	The T.Y. Lin International/Moffatt & Nichol Design Joint Venture, the Engineer of Record for the SAS Bridge, begins design of the bridge using design standards in effect at the time, including the 1995 Caltrans Bridge Design Specification Manual.
<b>August 1999</b>	Caltrans advertises the Richmond-San Rafael Bridge Seismic Retrofit Project with contract specifications that include A354 grade BD galvanized rods. Per this project's Special Provisions Section 10-1A.27 STEEL CASINGS, "High-strength threaded rods and rods for steel casings shall conform to ASTM Designation A354 grade BD and shall be installed snug-tight in 3/16-inch oversized holes. High-strength rod assemblies shall be galvanized using a mechanically-deposited zinc coating conforming to ASTM B695, Class 50." By mechanically galvanizing, the rods would be subjected to less heat and thereby reduce risks for hydrogen contamination and embrittlement.
<b>April 2000</b>	The Caltrans Bridge Design Specification Manual is updated. Additional structural fasteners are added to the specifications as a design choice, including A354 high-strength fasteners (Section 10.2.4 and Table 10.2C). Section 10.24.1.1 adds, "Galvanization of AASHTO M253 (ASTM A490) and A354 grade BD high-strength bolts is not permitted due to hydrogen embrittlement problems. These fasteners must be carefully evaluated before being utilized." <sup>7,8</sup>
<b>August 2001</b>	Caltrans issues Contract Change Order (CCO) #53 on the Richmond-San Rafael Bridge Seismic Retrofit Project that changes the galvanizing of A354 grade BD rods from mechanical galvanization to hot-dip galvanization, as the size of the rods specified were too large to be mechanically galvanized. The CCO also contained the following language: "In lieu of cleaning of the high-strength rod assemblies in pickling solution prior to galvanizing, all surfaces of the assemblies shall be dry blast cleaned in accordance with provisions of Surface Preparation Specification No. 10, "Near White Blast Cleaning," of Steel Structures Painting Council. The assemblies shall be coated with galvanizing within 4 hours of being dry blast cleaned." This contract change is consistent with ASTM A143 (Standard Practice for Safeguarding Against Embrittlement of Hot-Dip Galvanized Structural Steel Products and Procedure for Detecting Embrittlement) that specifies that abrasive blast cleaning, followed by flash pickling, may be employed when very high-strength steel (ultimate tensile strength higher than 150 ksi) is galvanized.

table continued

<sup>7</sup> Caltrans Bridge Design Specifications (February 2004). "Section 10-Structural Steel," p. 10-39.

<sup>8</sup> Memo from Steel Committee Chair (Lian Duan) to Caltrans (Ade Akinsanya) (January 2003). "Review Comments on SFOBB-East Span Seismic Safety Project Self-Anchored Suspension Bridge."

<sup>9</sup> Letter from T.Y. Lin International/Moffatt & Nichol Design Joint Venture in Response to Memo from Steel Committee Chair (January 2003).

**Table 5 Major Design and Contract Decision Timeline** (continued)

Date	Event
<b>September 2001</b>	The T.Y. Lin international/Moffatt & Nichol Design Joint Venture requests approval from Toll Program to seek limited sole-source approval for one particular location. Request communicates lack of availability of domestic 3-inch high-strength rods at the time and communicates intent to use A354 grade BD rods on other portions of the SAS bridge. There is no mention of corrosion protection. <sup>9</sup>
<b>July 2002</b>	The T.Y. Lin international/Moffatt & Nichol Design Joint Venture delivers the 100% submittal of the plans, specifications, estimate (PS&E) and the finalized project-specific Design Criteria for the SAS Bridge. The Design Criteria cite that the bridge shall be designed in accordance with “1995 Caltrans Bridge Design Specifications Manual” and modified and augmented as detailed in the design criteria. The criteria also cite a number of other standards and codes from American Association of State Highway and Transportation Officials (AASHTO), American Institute of Steel Construction (AISC), American Welding Society (AWS), and other technical reports. Section 6 of the Design Criteria covers structural steel and identifies ASTM A354 grade BD for use on a number of structural steel connections on the bridge, including the Pier E2 Bearing and Shear Key high-strength steel rods. The criteria do not address galvanizing or other corrosion protection for the connections.
<b>January 2003</b>	<p>Caltrans Structural Steel Technical Committee provides comments to Toll Program on 100% submittal of the PS&amp;E package for the SAS Bridge. Comments request “corrosion resistance specifications for A354 fasteners.”</p> <p>Caltrans advertises the Pier E2/Tower T1 (E2/T1) SAS Marine Foundation Contract that includes requirements for mechanically galvanized A354 grade BD high-strength steel rods similar to the A354 grade BD provisions specified in the original August 1999 advertisement of the Richmond-San Rafael Bridge Seismic Retrofit Project.</p>
<b>February 2003</b>	<p>Caltrans advertises the SAS Bridge Contract that includes requirements for A354 grade BD Parallel Wire Strand (PWS) anchor rods, and requirements that these rods be mechanically galvanized, similar to the provisions identified in the August 1999 advertisement of the Richmond-San Rafael Bridge Seismic Retrofit Project. Galvanization of other A354 grade BD rods on the SAS was not addressed.</p> <p>The T.Y. Lin international/Moffatt &amp; Nichol Design Joint Venture responds to January 10, 2003 comments from the Caltrans Structural Steel Technical Committee.<sup>10</sup> Response discusses concern about hydrogen embrittlement and continuing discussion about corrosion protection.</p>

<sup>10</sup> *Ibid.*

Date	Event
<b>March 2003</b>	Caltrans receives a bidder inquiry on the E2/T1 Marine Foundation Contract stating that high-strength steel rods cannot be mechanically galvanized due to their size; bidder inquires about how to coat the rods.
<b>March 2003 to April 2003</b>	In response to the bidder inquiry, staffs from Caltrans and the T.Y. Lin International/Moffatt & Nichol Design Joint Venture discussed the need to modify galvanizing specifications for A354 grade BD rods. They explored the Richmond-San Rafael Bridge Seismic Retrofit Project change order (CCO #53) for use of dry blast cleaning prior to hot-dip galvanizing as a response to the inquiry. Caltrans staff raised concerns about “strain age embrittlement” and suggested adding specification language for the contractor to follow ASTM A143. Both Caltrans and T.Y. Lin International/Moffatt & Nichol Design Joint Venture staffs conclude that adding the changes implemented on the Richmond-San Rafael Bridge Seismic Retrofit Project, including blasting and tensile testing, would allow for successful galvanizing while reducing the risk for hydrogen embrittlement.
<b>April 2003</b>	Caltrans issues Addendum #3 to the E2/T1 Marine Foundation Contract requiring that the A354 grade BD rods be dry blast cleaned 4 hours prior to hot-dip galvanizing and that the contractor submit certified test reports that the rods conform to ASTM A143.
<b>June 2003</b>	Caltrans issues Addendum #8 to the SAS Bridge Contract requiring that all A354 grade BD rods be dry blast cleaned 4 hours prior to hot-dip galvanizing, and that the Contractor submit certified test reports that the rods conform to ASTM A143.
<b>October 2003</b>	<p>Caltrans rejects the single bid received on the E2/T1 Marine Foundations Contract as unacceptably high. Note: when Caltrans awards the contract in April 2004, the new bid is \$50 million lower than the earlier bid.</p> <p>Caltrans re-advertises the E2/T1 Marine Foundations Contract with the special provisions to dry blast clean A354 grade BD rods 4 hours prior to hot-dip galvanizing and to conform to ASTM A143.</p>
<b>September 2004</b>	Caltrans receives a single bid for the SAS Bridge Contract in May 2004. The sole bid came in at \$1.8 billion (in contrast to the engineer’s estimate it would cost \$733 million). Caltrans did not accept the single bid. Instead, Caltrans stated it would further analyze whether to resubmit the original SAS design in an attempt to attract more bids, or possibly reopen the design process to find a less expensive design.
<b>August 2005</b>	Caltrans re-advertises the SAS Bridge Contract with special provisions included to dry blast clean A354 grade BD rods 4 hours prior to hot-dip galvanizing and to conform to ASTM A143.



## Fabrication

### What Are Standard Industry Practices for Fabricating A354 Grade BD Rods? Were They Followed?

The American Society for Testing and Materials (ASTM) is one of the largest organizations in the world for the development of voluntary consensus standards for test methods and material specifications. There are more than 12,000 ASTM standards today. Many users refer to them for guidance, as ASTM standards are voluntary. However, government regulators often give these voluntary standards the force of law by citing them in contract laws, regulations and codes. The ASTM standards relevant to the fabrication process for the type of high-strength steel rods that are the subject of this report are summarized in Table 6.

**Table 6 Summary of Relevant ASTM Standards**

ASTM	Title	Relevance to the SAS Bridge Project
A123	Standard Specification for Zinc (Hot-Dip Galvanized) Coatings on Iron and Steel Products	ASTM A123 describes hot-dip galvanizing specifications for steel.
A143	Standard Practice for Safeguarding Against Embrittlement of Hot-Dip Galvanized Structural Steel Products and Procedure for Detecting Embrittlement	<p>ASTM A143 describes procedures that can be followed to safeguard against, as well as to test for, possible embrittlement of steel that has undergone a hot-dip galvanization process, as was the case for the A354 grade BD high-strength steel rods for this project.</p> <p>ASTM A143 identifies an alternative test for hydrogen embrittlement, as described in ASTM F606.</p> <p>When very high-strength steel (tensile strength higher than 150 ksi) must be galvanized, ASTM A143 suggests abrasive blasting and flash pickling, in lieu of standard pickling, to safeguard against embrittlement.</p>
A354	Standard Specification for Quenched and Tempered Alloy Steel Bolts, Studs and Other Externally Threaded Fasteners	<p>Where high-strength steel is required for threaded rods of no more than 4 inches in diameter, as in the case of this project, ASTM A354 specifies the chemical composition required to qualify the steel as an alloy steel. ASTM A354 also specifies the required mechanical properties, in terms of hardness and tensile strength, for various diameter rod sizes.</p> <p>ASTM A354 identifies the type of test methods that shall be used to ensure the mechanical properties of the rods are met.</p> <p>ASTM A354 specifies requirements for the hot-dip galvanizing process, as described in ASTM F606, but cautions hot-dip galvanizing A354 steel by stating, "Research conducted on bolts of similar material and manufacture indicates that hydrogen-stress cracking or stress cracking corrosion may occur on hot-dip galvanized Grade BD bolts." The A354 rods that are the subject of this report were hot-dip galvanized.</p> <p>A354 grade BC steel rods: Minimum tensile strength = 125 ksi (¼-inch to 2½-inch diameter) and 115 ksi (2¾-inch to 4-inch diameter).</p> <p>A354 grade BD steel rods: Minimum tensile strength = 150 ksi (¼-inch to 2½-inch diameter) and 140 ksi (2¾-inch to 4-inch diameter).</p>

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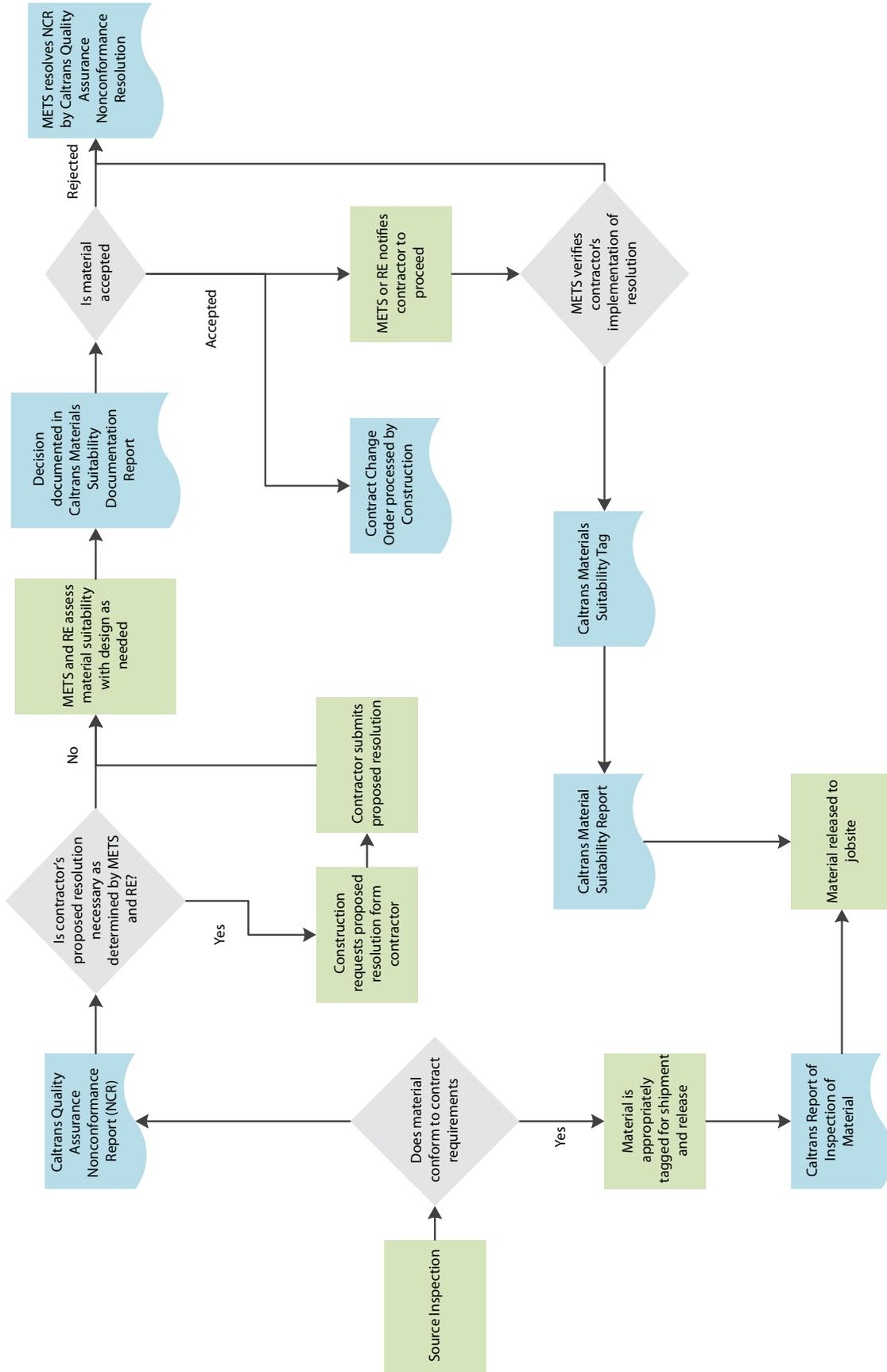
**Table 6 Summary of Relevant ASTM Standards** (continued)

ASTM	Title	Relevance to the SAS Bridge Project
A490	Standard Specification for Structural Bolts, Alloy Steel, Heat Treated, 150 ksi Minimum Tensile Strength	<p>For heavy hexagon headed structural bolts ½-inch to 1½-inch diameter, ASTM A490 specifies the chemical composition and mechanical properties of the steel.</p> <p>This specification also specifies acceptable metallic coatings for corrosion protection, and states that no other metallic coatings, such as hot-dip zinc coating, are permitted unless authorized by the ASTM Committee F16 on Fasteners.</p>
F606	Standard Test Methods for Determining the Mechanical Properties of Externally and Internally Threaded Fasteners, Washers, Direct Tension Indicators, and Rivets	<p>ASTM F606 establishes the procedures for conducting tests to determine whether the mechanical properties (i.e., hardness and tensile strength) of the A354 grade BD high-strength steel rods are within the values identified in the ASTM A354 specifications.</p> <p>ASTM F606 identifies a test method for detecting hydrogen embrittlement, whereby a rod is installed to a fixture, with a wedge of varying angles depending on the diameter size. The rod is tensioned to measure the tensile load. ASTM F606 indicates wedge tests for rods up to 1½ inches in diameter (e.g., for diameters over ¾ to 1½ inches, a zero-angle wedge test is to be conducted). There is no reference to diameters over 1½ inches, which would cover most of the A354 grade BD high-strength steel rods for this project.</p>
F1470	Standard Practice for Fastener Sampling for Specified Mechanical Properties and Performance Inspection	<p>ASTM F1470 describes the number of tests to be taken per sampling of total production lot.</p>

## Inspection and Quality Control/Quality Assurance (QC/QA) General Process

As per standard industry practice and adopted throughout the East Span Replacement Project, the contractor is responsible for performing quality control (QC) inspections, which include audits of its sub-suppliers, inspection of the materials being manufactured and final inspection prior to dispatch/acceptance at site. Caltrans is responsible for performing quality assurance (QA) audits of the contractor’s QC procedures and verification through sample inspection and testing of the materials and work being procured. The contractor’s QC is expected to comply with the inspection and testing requirements of the contract documents, while Caltrans’ QA meets the requirements of the Caltrans Office of Structural Materials (OSM) as deemed necessary by Caltrans Construction and the OSM Senior Materials Representative to provide assurance of the QC program. Figure 17 depicts Caltrans’ standard QA process.

**Figure 17 Caltrans QA Process — NCR and NPR Process**



The contractor for the rods on the SAS Superstructure (Items #1 through #11 and #14 through #17 in Table 1) was the American Bridge/Fluor Joint Venture. The contractor for the rods on the E2/T1 Marine Foundation (Items #12 and #13 in Table 1) was the Kiewit/FCI/Manson Joint Venture.

Issues that arise during QA inspection are incorporated into a formal issue resolution process including:

- Requests for Information (RFI) - RFIs are formal requests from the contractor for additional information or clarification regarding the design and construction of the project that may be initiated by anyone associated with the project. An RFI is not a request to change the design; it is only to clarify features or the intentions for the existing design. A response to an RFI that changes the design may require the issuance of a contract change order (CCO).
- Non-Conformance Reports (NCR) - A QA inspector identifies processes or materials that do not meet contract requirements, and the contractor's QC personnel accept the material as evidence of non-conformance in the contractor's QC process. As a result, the QA inspector will write an NCR for the material, if the contractor cannot correct the deficiency within a work shift. (The QA inspector will not typically write an NCR on a material that will be corrected within a work shift and the non-conformance is not repeated, and also on material that has not been inspected and accepted by the contractor's QC personnel).
- Notice of Potential Resolutions (NPR) - Once non-conformance issues are identified and reported, the contractor's disposition and corrective action to bring the condition back into conformance will be evaluated by the Caltrans Resident Engineer (RE). Potential resolutions to non-conformance issues include:
  - Rework to meet the originally specified requirements.
  - Repair to achieve fit-for-purpose.
  - Accept the conditions as-is (may require a CCO).
  - Reject the condition by removing it and replacing it with material meeting the specified requirements.
  - Alternative fit-for-purpose evaluation - This process will allow QA inspectors to release the material when the Caltrans RE determines that the material is suitable for its intended purpose on the project. The fit-for-purpose may be initiated by NCRs, RFIs, submittals, shop drawings, contractor requests, observations, meetings or other forms of revisions.
  - Addressed the non-conformance in the contractor's QC process.

## What Were the QC/QA Inspection Results for the Failed 2008 Rods?

All NCRs and NPRs issued for this project by QA were satisfactorily closed out and the 2008 rods (Item #1 in Table 1) were accepted by both the American Bridge/Fluor Joint Venture and Caltrans.

Testing on the 2008 rods was performed to contract specifications and ASTM A354 grade BD requirements. In general, the testing results during fabrication were within specification, except for low elongation results for two of the seven rod heats, which were reported in Non-Conformance Report (NCR 000199, DYSN 0005) and accepted as “fit-for-purpose” by Caltrans on October 16, 2008. Table 7 summarizes the average material test results for the 2008 rods.

**Table 7 QC/QA Inspection Results of 2008 Rods**

	Tensile (KSI)	Yield (KSI)	Elongation (%)	Reduction of Area (ROA)	Hardness (HRC)
<b>ASTM A354 BD Standard</b>	140 (Minimum)	115 (Minimum)	14 (Minimum)	40 (Minimum)	31-39 (Range)
<b>2008 Rod Average</b>	164	142	14.3	48.4	36.8
<b>2008 Rod Min/Max</b>	152/173	127/158	12.5/16.2	40/50	33/37

International and national standards, such as those issued by ASTM, provide advice to both the purchaser and the supplier. These standards are not project-specific and often require the purchaser’s designer and QA to include additional or supplementary parameters to ensure that project-specific performance requirements are achieved. The supplier may also apply additional or supplementary parameters to suit its means and methods, both of which should be captured in the respective Quality Plans.

QA also noted that the 2008 rods were subjected to a second heat treatment, as the documentation for the first treatment could not be produced by the fabricator. It is not uncommon to perform a second heat treatment. However, in this case, given what is now known about the poor quality of the 2008 rod material, the second heat treatment may have further hardened and strengthened the material and contributed to the rods’ susceptibility to hydrogen embrittlement.

The 2008 A354 grade BD rods used at Pier E2 were reported to have strength and hardness well above the minimum requirements of the specification. Also, when examined, the failed rods showed that the metallurgical structure was not uniform across the thickness of the rod and parts did not have the expected material properties. This indicates the steel production and heat treatment were not fully successful in achieving the desired uniform metallurgical structure and desired material properties.

## Purpose of Heat Treatment

The heat treatment process is a method by which material properties are altered to suit the intended service. For these particular high-strength rods, the steel produced to American Iron Steel Institute (AISI) 4140 is sufficiently malleable for the steel rolling process to form rods from the cast ingots. Once the rods have been formed, they are subjected to a three-stage heat treatment process: austenitizing, quenching and tempering.

- The austenitizing is achieved by elevating the temperature to about 1600 degrees F, which changes the metallurgical structure.
- The quenching operation is a rapid cooling of the steel from the austenitizing temperature, causing a further change to the metallurgical structure. This increases strength, but renders the material too hard and brittle for use in rod applications.
- The tempering operation is a further elevation of the temperature to above 800 degrees F, which reduces the hardness to yield a suitable ductile/tough material (less brittle).

AISI 4140 alloy steel is a chromium-molybdenum (41) low alloy steel with approximately 0.4% (40) carbon. When properly heat treated, it can achieve high tensile strength properties. It is a commonly used alloy in bridge applications in the manufacturing of high strength rods such as ASTM F1554, A320 Grade L7, A193 Grade B7, A490 and A354 Grade BD.

## Hardness

Hardness is a measure of a material's ability to resist abrasion and indentation. As a rule, an increase in the tensile strength of steel will correspondingly increase the hardness of the steel, and as explained elsewhere in this report it also increases the steel's susceptibility to hydrogen embrittlement and stress corrosion cracking.

ASTM A354 grade BD specifies an acceptance hardness range for high-strength rods of between 31 HRC and 39 HRC. In 2005, revisions to ASTM F2329 included the risk of embrittlement for high-strength rods 33 HRC and above, recommends measures to reduce such risks. The consensus of experts, including John W. Fisher and H.E. Townsend, is that the acceptable range of hardness for high-strength rods with appropriate fabrication control measures is 31 HRC to 35 HRC.

Hardness testing specified by ASTM on rods and bolts requires measurements to be taken at location mid-radius, or R/2, (mid-point between the rod center and the circumference/rod surface). QC and QA testing of high-strength rods per the SAS contract were performed in accordance with ASTM F606 with hardness measurements taken at R/2.

## Construction

### How Did Environmental Conditions at the Construction Site Affect A354 Grade BD Rods?

The rods installed at Pier E2 were manufactured by Dyson in Ohio in 2008, and were installed prior to the final concrete pour on December 5, 2008. These high-strength steel rods were embedded within the pier directly above the columns, and were sitting in ducts for five years before they were tensioned. During this five-year period, water was pumped out of the ducts a number of times at the request of Caltrans. Temporary drainage and sealing arrangements had not prevented the ingress and collection of rainwater, since it had not been anticipated that there would be such an extended period prior to completing the erection and grouting operation at Pier E2. The actual length of time during which water was present in these holes is unknown, but the presence of water may have been a contributing source of hydrogen contamination in the rods (see photo in Figure 18).

**Figure 18 Pier E2 Cap Construction Photograph** — Embedded rods are in holes



### Was the 2008 Rod Order Rushed? Did This Have an Effect on the Quality?

In the QA/QC records, it was noted that the 2008 rods were shipped from the fabricator to the project site prior to completion of laboratory QA testing. American Bridge/Fluor Joint Venture took the risk of shipment without full testing results in order to meet their construction schedule for completion of Pier E2. Nonetheless, the 2008 rods were not installed into place until all required tests were completed and passed. There is no evidence that this sequence of events led to the rod failures.

## 5. Question 2: What Retrofit Strategy Should Be Used to Replace the Lost Clamping Force of the 2008 Rods at Pier E2?

### Are the Failed 2008 A354 Grade BD Rods Replaceable?

The 2008 A354 grade BD rods installed in Pier E2 cannot be replaced. These rods were installed and embedded into the Pier E2 cap, and are in-line with the vertical columns of the pier. In addition, the OBGs have been placed over the shear keys, further limiting access to the rods. Therefore, replacing these 96 rods would require significant destruction of the pier cap to allow for the removal of the 2008 rods and installation of replacement rods. Thus, the lost clamping force from the failure of the 2008 rods must be replaced in another fashion.

### What Retrofit Strategies Were Considered?

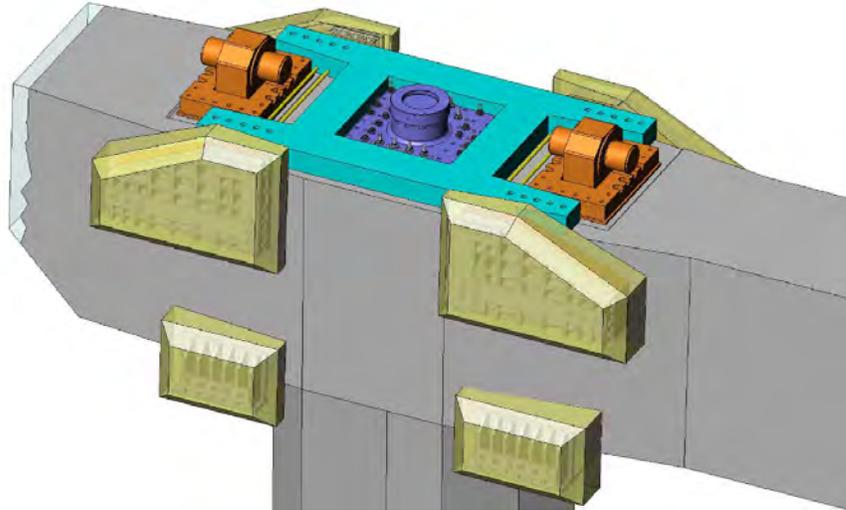
Upon discovering the fractured rods, Caltrans, the T.Y. Lin International/Moffatt & Nichol Design Joint Venture and the American Bridge/Fluor Joint Venture began to consider a retrofit strategy in lieu of replacement. Replacing the failed rods with a similar anchoring system was not a practical solution. The design constraints included:

- The rods could not simply be removed due to the original embedded design. The embedded end of the rod was permanently secured by deforming the thread to prevent the rod from unscrewing from the nut.
- The vertical clearance between the bottom of the roadway boxes and top of pier cap is far less than the 17 feet length of the rods. Also, with the bearings installed adjacent to the shear keys, the horizontal clearance is likewise limited.
- There is an extensive amount of steel reinforcement in the concrete pier cap, thus making modifications to accommodate a new anchor rod system in the pier cap challenging.
- High clamping force is still required to maintain the seismic design requirements.

Three potential alternatives were considered by the TBPOC for the retrofit of the lower housing of shear keys S1 and S2. These alternatives included: (1) a steel collar that captures the perimeter of the lower shear key housing and is anchored to the pier cap using through post-tensioning tendons; (2) a steel saddle that extends over the lower housing of the shear key with post-tensioning that extend over the sides of the cap; and (3) removal and replacement of the existing anchor rods with new coupled anchor rods that would have a bonded zone in the pier cap. These three options are described in more detail below.

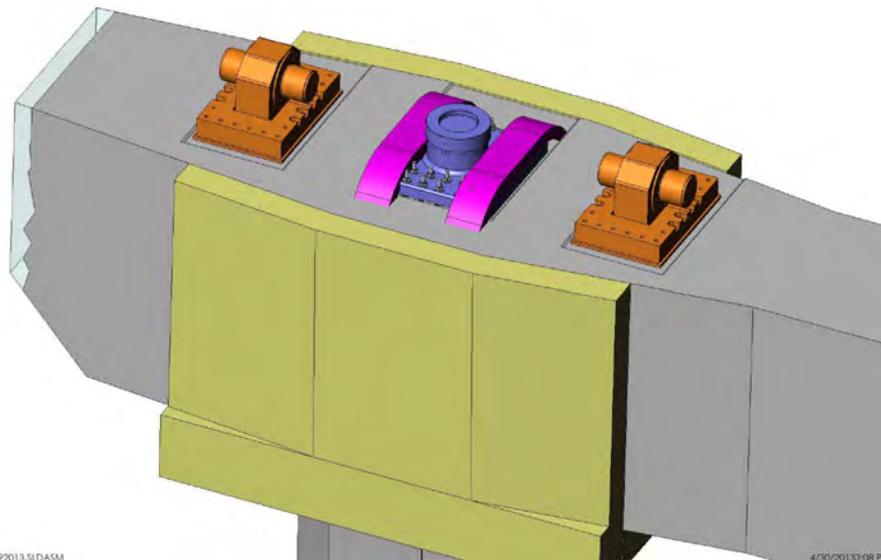
**Option 1: Steel Collar (Figure 19)** — This option secures the perimeter of the lower shear key housing with a steel brace that is anchored to the pier cap with anchor rods. The new anchor rods would be installed through cored holes in the pier cap on either side of the column.

**Figure 19** Early Rendering of Option 1 — Steel Collar



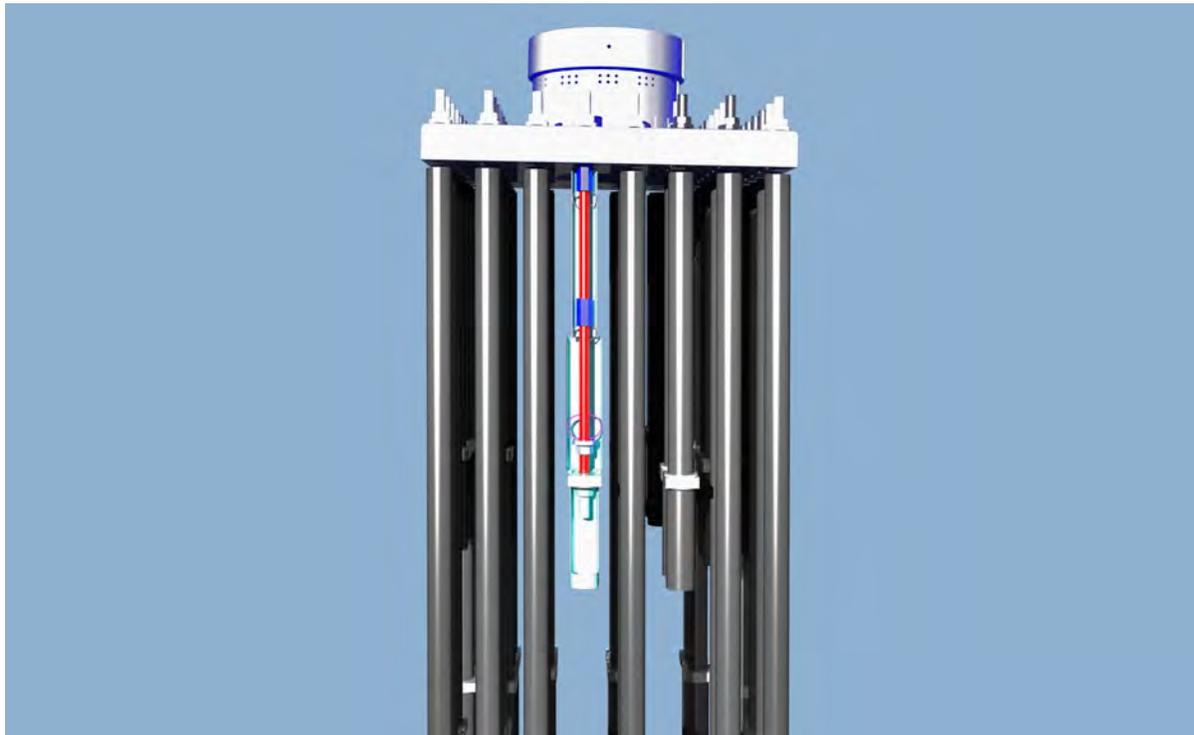
**Option 2: Steel Saddle (Figure 20)** — This option would secure the lower housing of the shear key with post-tensioning cables that extend over the sides of the cap. The cables would then be encased in a concrete blister outside of the pier cap, minimizing the impact to the cap.

**Figure 20** Early Rendering of Option 2 — Steel Saddle



**Option 3: Post-Tension Tie Down (Figure 21)** — This option explored the removal of the shear keys so that all the broken and unbroken rods could be replaced with post-tensioning cable anchors. Conceptually, this would require the removal of the unbroken rods, the broken rod remnants and grout with high-precision water jets from the anchorage ducts in Pier E2, and development of a post-tension anchor system for installation at the bottom of the 17-foot-deep ducts.

**Figure 21 Early Rendering of Option 3 – Post-Tension Tie Down**



In all cases, the 2008 A354 grade BD rods would be completely abandoned and replaced with equivalent clamping capacity.

## Which Retrofit Strategy Option Was Selected by the TBPOC and Why?

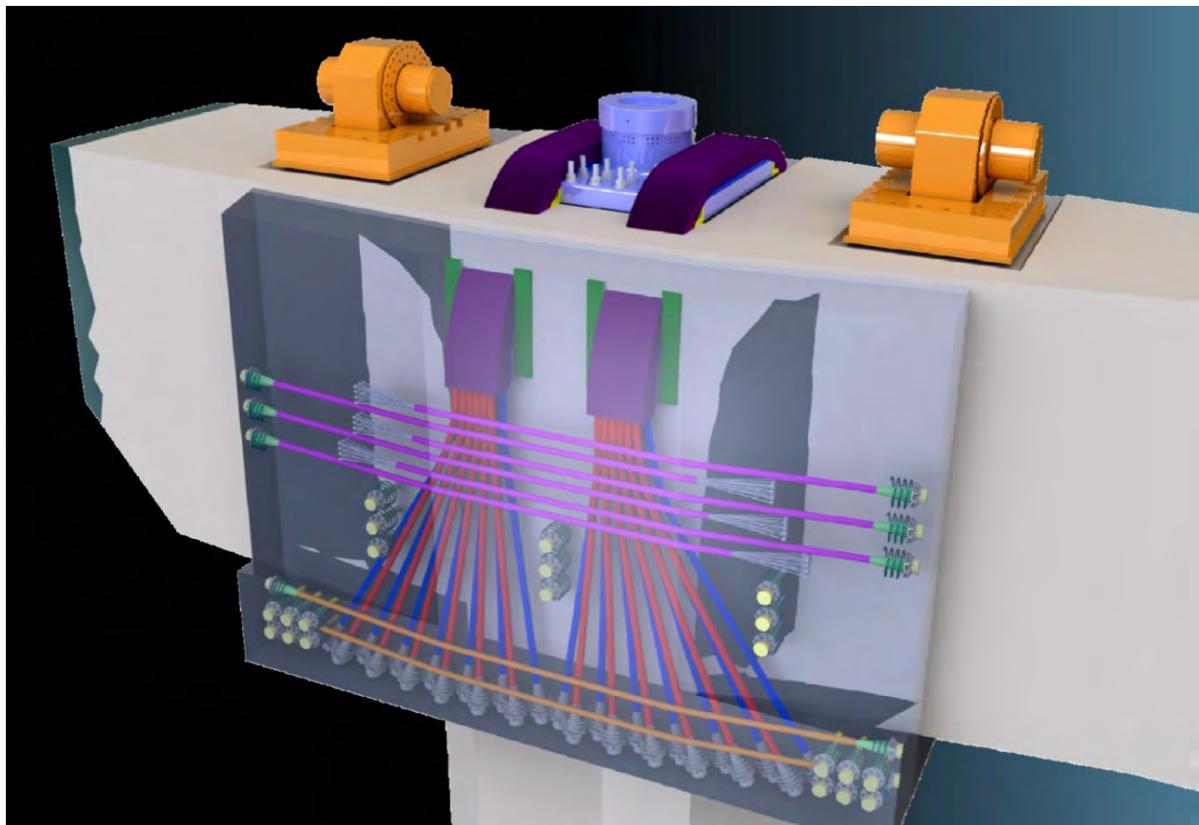
On May 8, 2013, the Toll Bridge Program Oversight Committee (TBPOC) evaluated three options that were presented by Caltrans bridge designers and then directed attention to two options: Option 1 (Steel Collar) and Option 2 (Steel Saddle). Both options and their pros and cons are shown in Table 8. Note that both options would provide equivalent clamping force as the original anchor rod design to secure the shear keys and resist the significant forces of a seismic event. Both options would completely abandon the 2008 A354 grade BD rods. Option 3 was eliminated from further consideration because the other options had fewer design and construction challenges, including no removal and reinstallation of the shear keys and no use of high-precision water jets within close proximity to the structural reinforcement tendons.

**Table 8 General Comparison of Retrofit Options for 2008 High-Strength Steel Rods**

Option 1: Steel Collar	Option 2: Steel Saddle
<b>Pros:</b> No need to remove shear keys S1 and S2 Potentially simpler to fabricate	<b>Pros:</b> No need to remove shear keys S1 and S2 Less coring of E2 required Potentially less difficult to install Less costly: \$10 million
<b>Cons:</b> Need to find sufficient materials and resources More coring of E2 required More costly: \$15 to \$20 million	<b>Cons:</b> Requires unique saddle system

Option 2 (Steel Saddle) was the selected retrofit strategy option because it was considered to be easier to construct and less expensive. As shown in Figure 22, it also applies a direct preload to the lower housing via the radial forces that are developed from the main vertical post-tensioning force being applied as intended in the original design. The project's Seismic Peer Review Panel also supported this option, and the American Bridge/Fluor Joint Venture indicated this option would be the easiest to construct and the fastest option to complete. On May 8, 2013, the TBPOC unanimously approved selection of the steel saddle retrofit (Option 2) after finding that it would meet all design requirements and objectives of the project.

**Figure 22 Recent Rendering of Selected Steel Saddle Option**



## **6. Question 3: What Should Be Done About Other A354 Grade BD Rods on the SAS Bridge?**

### **Where Are the A354 Grade BD Rods Located on the SAS Bridge?**

The A354 grade BD rods used on the SAS Bridge are at various locations and of varying diameters, lengths and applied tension levels. The A354 grade BD galvanized rods generally can be split into three groups: 1) tower anchor rods (Items #12 and #13) fabricated and installed under the SAS Bridge Marine Foundation contract; 2) the failed 2008 lower Pier E2 shear key anchor rods (Item #1) fabricated and installed under the SAS Bridge Superstructure contract; and 3) other rods (Items #2 to #11 and #14 to #17) fabricated and installed under the SAS Bridge contract. (Refer to Table 1 and Figure 2 for the locations of these rods.)

The contract work includes installation of a desiccant dehumidifier system in the Bottom of the Tower, at the Top of the Tower and in the Main Cable Anchorage. Rods at these locations (Items #7, 8, 9, 12 and 13) have required dehumidification systems controlling their environment. This equipment will remove moisture (hydrogen) from the air.

### **Who Fabricated the A354 Grade BD Rods? When Were the Rods Fabricated and Tensioned?**

The tower anchor rods (Items #12 and #13 in Table 1) were fabricated in early 2007 at Vulcan Threaded Products in Alabama under the SAS Bridge Marine Foundation contract. The remaining rods were fabricated by Dyson Corporation in Ohio under the SAS Bridge Superstructure contract. The Dyson rods were fabricated during two periods – in 2008 for the E2 shear keys S1 and S2 (Item #1 in Table 1) and between 2009 and 2012 for the remaining rods. Table 9 provides a summary of the fabricators and key fabrication and tensioning dates for all the high-strength steel rods.

**Table 9 Fabrication Dates and Status of A354 Grade BD High-Strength Steel Rods**

Item #	Fabricator	End of Fabrication	Tension or Loading Complete	# of Rods Installed	# of Fractured Rods After Tensioning	Days Under Tension Through July 1, 2013
1	Dyson	Sep 2008	Mar 2013	96	32*	Rods began failing after 3 days of tensioning
2	Dyson	Mar 2010	Apr 2013	192	0	91
3	Dyson	Mar 2010	Sep 2012	320	0	295
4	Dyson	Mar 2010	Sep 2012	224	0	292
5	Dyson	Aug 2009	Jun 2009**	96	0	1,429
6	Dyson	Dec 2009	Jan 2010	336	0	1,245
7	Dyson	Nov 2011	Sep 2012	274	0	278
8	Dyson	Jul 2010	Jul 2012	25	0	351
9	Dyson	Jan 2011	Jul 2012	108	0	351
10	Dyson	Jan 2011	Mar 2013	90	0	97
11	Dyson	Oct 2011	Jul 2012	4	0	334
12	Vulcan Threaded Products	Feb 2007	Mar 2011	388	0	821
13	Vulcan Threaded Products	Feb 2007	Mar 2011	36	0	821
14	Dyson	Jun 2010	May 2010**	32	0	1,125
15	Dyson	May 2010	Apr 2012	18	0	443
16	Dyson	Oct 2012	Feb 2013	24	0	142
17	Dyson	Jun 2009	TBD***	43	0	—

\* Caltrans reduced the tension on the remaining unfractured rods on March 15, 2013. Additional rods might have fractured if not detensioned.

\*\* Rods were tensioned in the fabrication shops, Item #5 in Japan and Item #14 in China, prior to the assembly being delivered to SAS Bridge site. Items #7, 8, 12 & 13 were adjusted as part of Load Transfer in October 2012.

\*\*\* Details for bike path support frame being redesigned to improve consistency with other design features of SAS.

## What Were the Differences in Fabrication?

While all the rods were fabricated by the same general processes, there were two notable differences in fabrication procedure for certain rods:

- Under the SAS Bridge Marine Foundation contract, the tower anchor rods (Items #12 and #13) were produced by a different fabricator (Vulcan Threaded Products in Alabama) than where the remaining SAS rods were fabricated. Contract specifications for this contract required A354 Grade BD rods to be galvanized with a dry blast cleaning. While complete Caltrans QA records have not been located, contractor QC documentation for these rods provided mechanical property information beyond that normally required by Caltrans. QC documentation included microstructural analysis and a full cross-sectional hardness survey. In addition, these rods were subjected to induction

heat treatment, similar to the 2010 rods but different from the 2008 rods both fabricated at Dyson. Recent documentation from the fabricator notes that the rods were dry blast cleaned and flash pickled as per specification.

- Under the SAS Bridge Superstructure Contract in October 2008, Caltrans directed American Bridge/Fluor Joint Venture, through Contract Change Order (CCO) #91, to perform additional Magnetic Particle Testing (MT) during fabrication — in accordance with ASTM specification A490 — on A354 grade BD high-strength steel rods tensioned in excess of 0.5 Fu. MT is a non-destructive method for detecting cracks and other discontinuities at or near the surface in ferromagnetic materials such metals as iron, nickel, cobalt and some of their alloys. This change was in addition to contract specifications to galvanize with a dry blast cleaning.

CCO #91 was further clarified by American Bridge/Fluor Joint Venture on May 22, 2009 in a request for information<sup>11</sup> to cover the rods listed below:

Item #2 - Bearing & Shear Key Anchor Rods

Item #3 - Shear Key Rods (top)

Item #4 - Bearing Rods (top)

Item #5 - Bearing Assembly

Item #7 - Parallel Wire Strand (PWS) Anchor Rods

Item #8 - Saddle Tie Rods (top of tower)

Item #15 - Saddle Tie Rods (East saddles)

Item #16 - Cable Band Anchor Rods

The 2008 rods were already galvanized and beyond the point when MT could be performed, therefore no MT was performed on the 2008 high-strength steel rods. The reason for directing the contractor to perform MT, through CCO #91, at this stage in the project is not documented.

According to American Bridge/Fluor Joint Venture and confirmed by project QC records, in response to this new requirement for MT, the American Iron and Steel Institute (AISI) 4140 steel alloy supplied by the steel manufacturer, Gerdau Long Steel, started coming from a steel mill that used the “vacuum de-gassing” process in their production. Vacuum degassing is a process where molten metal (commonly steel) is placed in a vacuum furnace in order to remove excess hydrogen or carbon. This additional process may have improved the material properties of the rods manufactured with degassing by reducing internal hydrogen trapped in the steel.

Table 10 shows the QA/QC inspection data for the high-strength rods used on the SAS with diameters greater than 2½ inches, and Table 11 shows the QA/QC inspection data for the high-strength rods used on the SAS with diameters between ¼ inch and 2½ inches.

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<sup>11</sup> Request for Information (RFI) (RFI No. ABF-RFI-00174R01) (May 2009) regarding CCO-91 Clarification

**Table 10 Post-Heat Treatment QC/QA Mechanical Test Results (> 2½-inch diameter)**

Item #	Component	Average Min/Max	Tensile (ksi)	Yield (ksi)	Elongation (%)	Reduction of Area (%)	Hardness (HRC)
ASTM D>2½"			140 (min)	115 (min)	14 (min)	40 (min)	31 - 39
1	E2 Shear Key Anchor Rods	Average	164	143	14	48	36.8
		Min/Max	152/173	127/158	13/16	40/50	33/37
2	E2 Bearing & Shear Key Anchor Rods	Average	159	139	16	51	34
		Min/Max	153/165	132/147	13/17	40/55	32/37
3	E2 Shear Key Rods (top)	Average	159	141	16	46	35
		Min/Max	153/163	133/148	14/17	40/53	32/37
7	PWS Anchor Rods	Average	157	137	16	53	35
		Min/Max	145/167	121/149	14/20	48/57	31/39
8	Tower Saddle Tie Rods	Average	161	133	15	44	35
		Min/Max	154/172	123/161	14/16	41/47	32/37
9	Tower Saddle Turned Rods	Average	148	125	19	57	37
		Min/Max	145/151	121/129	17/20	57/57	36/38
10	Tower Saddle Grillage	Average	150	124	16	53	34
		Min/Max	147/153	118/127	15/17	52/54	32/34
11	Tower Outrigger Boom Anchor Rods	Average	158	135	15	48	39
		Min/Max	156/161	132/140	14/16	48/48	39/39
12	Tower Anchor Rods (Type 1)	Average	160	144	18	51	34
		Min/Max	151/167	131/153	16/21	47/53	32/35
13	Tower Anchor Rods (Type 2)	Average	154	132	16	45	33
		Min/Max	152/158	129/136	15/17	40/50	33/34
15	East Saddle Tie Rods	Average	148	121	17	53	33
		Min/Max	146/152	118/127	16/18	52/54	32/34
16	Cable Bracket Anchor Rods	Average	156	134	16	53	36
		Min/Max	154/158	129/139	15/16	52/54	32/38

**Table 11 Post-Heat Treatment QC/QA Mechanical Test Results  
(1/4-inch to 2 1/2-inch diameter)**

Item #	Component	Average Min/Max	Tensile (ksi)	Yield (ksi)	Elongation (%)	Reduction of Area (%)	Hardness (HRC)
ASTM D = 1/4"–2 1/2"			140 (min)	115 (min)	14 (min)	40 (min)	31 - 39
4	E2 Bearing Rods (top)	Average	161	135	16	54	35
		Min/Max	156/164	126/151**	15/17	53/55	32/37
5	E2 Bearing Assembly Bolts	Average	166	154	18	56	36
		Min/Max	161/174	146/162	17/20	53/60	33/37
6	Retaining Ring Bearing Assembly Bolts	Average	166	148	15.8	50	35
		Min/Max	157/176	130/163	15/17	46/54	32/37
14	East Saddle Anchor Rods	Average	156	137	15	55	37
		Min/Max	154/160	132/142	14/16	55/55	37/37
17	Bikepath Anchor Rods	Average	167	160	15	52	36
		Min/Max	160/179	150/171	15/15	52/52	35/37

\*All mechanical property tests including elongation are the result of averaged data from two samples from each heat. If one sample is below specification and the second is above, with the average then being above specification, the test is passed. The minimums and maximums above reflect individual sample minimums and

\*\*Samples from Heat # NSH2 were rejected by CALTRANS TRANSLAB for low yield values. The lot was resampled & retested with satisfactory results.

## Is There Still a Risk of Hydrogen Embrittlement on the 192 Other Pier E2 A354 Grade BD Rods?

Following the 2008 rod failures, a test protocol was established for testing the 192 rods located on the east pier that were manufactured in 2010 (Item #2 in Table 1). Aside from the bike path anchor rods (Item #17), these A354 grade BD rods were the last remaining to be tensioned. The basis of the procedure was to perform a monitored, time-dependent, *in-situ* tensioning test on all remaining 192 rods to determine their susceptibility to hydrogen embrittlement. This tensioning test was conducted over a period of 30 days, which was considered sufficient time to ascertain whether ‘internal’ hydrogen was likely to embrittle the rods. Tensioning of the 192 rods was completed on April 9, 2013, at which time the 30-day *in-situ* test period began. The 30-day *in-situ* test period was completed on May 9, 2013, and resulted in no rod failures or evidence of hydrogen embrittlement. As of July 1, 2013, these rods continued to perform as designed.

A number of rods were extracted and subjected to an extended test program to determine if hydrogen embrittlement had occurred. These extracted rods were examined extensively and no evidence of hydrogen embrittlement was found. Further the 2010 material properties were substantially better than the 2008 material with homogenous microstructure and improved toughness. The 2008 material failed at a tension level substantially lower than the actual yield strength of the rods, which is evidence of hydrogen embrittlement and lack of toughness. Laboratory testing results for both the 2008 and 2010 rod results are shown in Table 12 and Figure 23.

The 2010 pier cap rods on Pier E2 exhibited substantially higher toughness values — at 40 degrees F — between 36.6 to 38.3 ft-lbs as compared to 13.5 to 17.5 ft-lbs for the 2008 rods. The hardness profiles for the 2010 rods also are somewhat more uniform through the rods as compared to the 2008 rods. Given these material differences in the 2010 rods and the elapsed time since they were tensioned, the near-term risk of further hydrogen embrittlement in the rods on Pier E2 is low.

**Table 12 Mechanical Test Comparison 2008 vs. 2010**

**1. Reduced Section Tensile Testing**

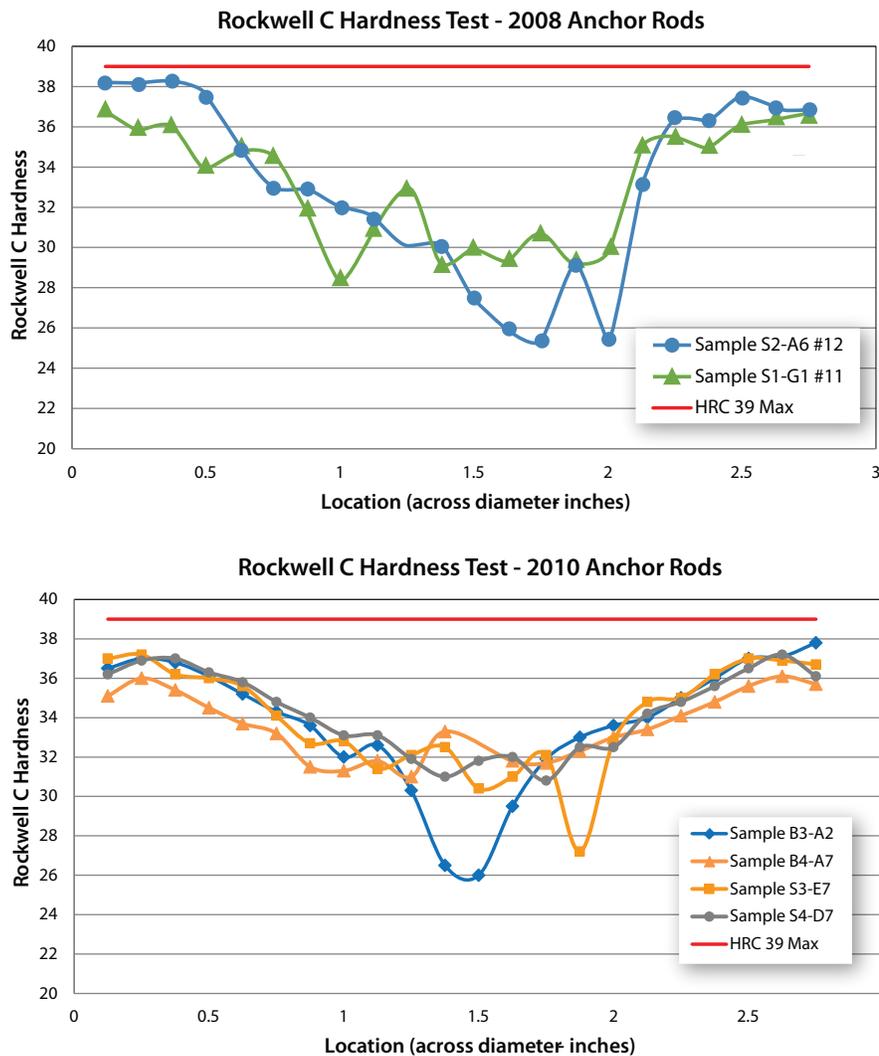
505 Sample Tensile Test Results								
	2010 Samples				2008 Samples			
Identification	B3-A2	S4-D7	S3-E7	B4-A7	S2-A6 #12	S2-A6 #2	S1-G1 #11	Requirement
Yield Strength (ksi)	143	138	139	143.1	149	146	136	115 min.
Tensile Strength (ksi)	160	157	157	160.2	170	168	159	140 min.
Elongation in 2" Gage %	17.0	19.0	17.5	16.8	15.5	14.0	15.0	14 min.
Reduction of Area (%)	53.5	53.4	54.0	42.7	46.0	48.0	48.4	40 min.

**2. Charpy V-Notch Impact**

Charpy V-Notch Impact Energy Test Results (ft-lb)											
	2010 Samples								2008 Samples		
Identification	B3-A2		S4-D7		S3-E7		B4-A7		S2-A6 #12	S2-A6 #2	S1-G1 #11
Temperature	40°F	70°F	40°F	70°F	40°F	70°F	40°F	70°F	70°F	70°F	70°F
Sample 1	35.5	37	37	37	38	38	39.5	39	18	15	13.5
Sample 2	38	38	37	38	37	37	36	39	18	14	13
Sample 3	37.0	38.0	37.5	37.0	37.0	37.0	37.5	37.0	17.0	15.0	14.0
Average	36.8	37.7	37.2	37.3	37.3	37.3	37.7	38.3	17.7	14.7	13.5

**Figure 23 Mechanical Test Comparison 2008 vs. 2010 (Pier E2)**

**Hardness Testing**



## Is There Still a Risk of Hydrogen Embrittlement on the Remaining A354 Grade BD Rods?

As noted earlier, hydrogen embrittlement is a phenomenon that is time-dependent — it typically occurs over days or weeks after high tensile stress is applied. Therefore, because the remaining SAS rods have not failed over the 91 to 1,429 days since being tensioned (as of July 1, 2013), these rods have low risk of hydrogen embrittlement. In contrast, approximately 30 percent of the 2008 A354 grade BD rods (Item #1) failed between 3 and 10 days after tensioning, and more might have failed if the tension level had been maintained.

Additional supplemental tests, similar to those done on the Pier E2 2008 and 2010 rods, have been performed on a large sample of the remaining rods to verify their hardness and toughness along with their chemical and mechanical properties, to provide additional confirmation that hydrogen embrittlement risk is low. (Refer to Tests I, II, and III results below.)

## What Is Stress Corrosion Cracking?

Stress Corrosion Cracking (SCC) is the growth of cracks in a corrosive environment, which can lead to unexpected sudden failure of normally ductile metals subjected to a tensile stress. Stress corrosion cracking is a phenomenon that exists when corrosion occurs in highly stressed high-strength steel, i.e., strength above 150 ksi. In the post-tensioning industry, highly stressed high-strength steel is usually placed inside of ducts, and capped and grouted after stressing to ensure the steel will not be subjected to a corrosive environment. If highly stressed steel is not protected, accelerated stress corrosion may occur, which could lead to stress corrosion cracking.

Stress corrosion cracking also occurs when hydrogen is generated from moisture, which then penetrates the steel that is susceptible to cracking and leads to embrittlement of the steel. Stress corrosion cracking can be considered a form of external hydrogen embrittlement and a longer-term phenomenon, as it is dependent upon corrosion taking place and, therefore, could take a long time to reach failure. In the situation where the steel is protected with galvanizing, a flaw or nick in the galvanizing will be a zone where the stress corrosion would be further accelerated.

SCC is a phenomenon that has existed since engineers have used ferrous materials; however, it has become a subject of greater importance with the introduction of higher-strength steels in higher-performance (highly stressed) applications and often in more aggressive corrosive environments, such as maritime applications including ships and offshore structures, where history has witnessed some catastrophic failures. Designers routinely consider the risks of stress corrosion and stress corrosion cracking in steel fabrications for boilers, pressure vessels, processing plants, high-pressure pipelines and marine structures. It is not a phenomenon restricted only to high-strength bolts, rods and tendons. In bridge construction, transient loads from traffic, wind or earthquakes do not result in stress corrosion cracking.

## Is There a Risk of Stress Corrosion Cracking?

Stress corrosion cracking has been shown to be a concern for high-strength steel having a tensile strength above 150 ksi. Similar to hydrogen embrittlement, stress corrosion cracking is also time-dependent — except that, unlike hydrogen embrittlement, it tends to occur over years or decades of sustained tension, and is based on the commencement and rate of corrosion. So the longer-term concern is whether the remaining A354 grade BD rods are susceptible to stress corrosion cracking and, if so, when cracking may occur.

Therefore, it has been necessary to establish which rods are at risk for stress corrosion cracking and to perform additional analytical testing — using as a guide the published research of John.W. Fisher<sup>12</sup> and H.E. Townsend<sup>13</sup>. Fisher published a book entitled, *Guide to Design Criteria for Bolted and Riveted Joints, 2nd Edition*, Kulak, G.L., Fisher, J.W., Struik, J.H.A. in 1987. His re-

<sup>12</sup> John W. Fisher is a retired Professor Emeritus of Civil Engineering at Lehigh University in Bethlehem, Pennsylvania. During his 45-year career, Fisher has won nearly every medal and distinction in his field, and has examined most of the major failures of steel structures in America throughout the last four decades. One of his most recent endeavors was serving on a panel of national experts that investigated the collapse of the World Trade Center following the September 11, 2001 terrorist attack.

<sup>13</sup> H.E. Townsend is a Research Supervisor of the Corrosion Prevention Group within Homer Research Laboratories at Bethlehem Steel Corp., Bethlehem, Pennsylvania.

search found that electroplated and hot-dipped zinc coatings decrease the resistance to stress corrosion cracking in direct levels of stress intensities. Stress intensity is a function of the diameter of the rod and the tension the rod is placed under. In general, the larger the rod diameter and the higher the stress, the rod will have higher stress intensity. The other key factor in stress corrosion cracking is the hardness of the steel, especially at the surface of the material.

## What Tests Are Being Conducted to Determine the Risk of Stress Corrosion Cracking and Why?

Protocols for determining stress corrosion cracking susceptibility have been established using five different tests, as described below.

**Test I** Test I is a test to conduct an *in-situ* hardness test on all accessible A354 grade BD rods and bolts on the SAS Bridge, thereby categorizing the susceptibility of each individual rod relative to hardness and applied stress intensity. The galvanized layer will be ground off the top surface of the rods using a grinder and sand paper. Once the surface is free from oil, grease, dust, rust, and surface coatings, measurements will be taken throughout the rod's diameter using a hardness tester that measures the depth of penetration of an indenter under a large load. A photo of such field testing on Pier E2 is shown below in Figure 24.

**Figure 24** Photograph of Inspector Performing *In-situ* Hardness Test on Bearing Rod



**Test II** Test II is to conduct laboratory tests on a selected number of specimens or spares in order to determine a rod's hardness (Rockwell hardness test), toughness (Charpy V-Notch test) and chemical composition (Figure 25). In some locations there are rods installed in the bridge that have an excess or protruding length which, if cut off, could serve as a test specimen in a controlled laboratory environment. There are also some spare rods from the original manufactured lots that are available for laboratory testing.

**Figure 25 Charpy Toughness Test in Process (left); Tested and Untested Specimens (right)**



**Test III** Test III is to conduct laboratory testing (full size tension test, coupon tension test, Rockwell C hardness, Charpy, chemistry, fracture analysis) of full-sized rods extracted from the Pier E2 shear key and bearings (Items #2, 3, and 4 in Table 1) and the Tower Anchorage (Item #12 in Table 1). All the other full-sized tests can be performed on spare rods and bolts from the original manufactured lots. Under Test III, rods will be loaded to failure, and the failed rods will be examined and tested to determine their hardness, toughness, mechanical properties, and chemical composition. Figure 26 shows such a full-sized laboratory test underway.

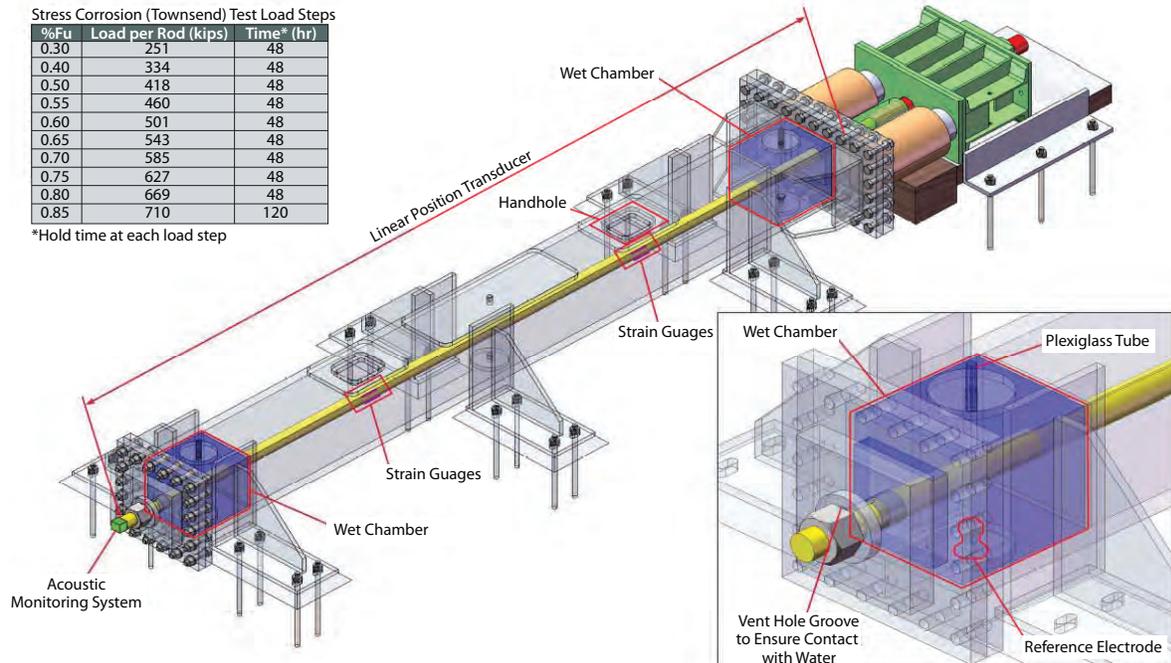
**Figure 26 Photograph of Full-Sized Rod Being Placed into Test Rig**



**Test IV** Test IV (Townsend Test) is to conduct an accelerated stress corrosion test which replicates the earlier Townsend research (Figure 27). The results of Test IV will provide the curve against which the results from Tests I, II and III will be plotted and assessed in an updated and completed in the figure in the following section. The sample selection focuses on the rods subjected to the higher stress intensities (i.e., 0.7 Fu) with one additional sample from each diameter size of 2, 3 and 4 inches. Also of interest is whether there is a difference in susceptibility between rolled threads

or cut threads and between galvanized and non-galvanized rods. Determining these differences will also require testing of rods with a diameter of 3½ inches under lower stress intensities.

**Figure 27 3D Rendering of Stress Corrosion Test Platform for Test IV**



**Test V** Test V (Raymond Test) involves laboratory tests of reduced size specimens from selected rods. These tests are of an accelerated type and measure the resistance of the material to stress corrosion cracking. The results of these tests are intended to supplement the data developed by the tests of full-sized rods in Test IV. These tests will include material from the 2008 rods to provide a basis of comparison. The test protocol is in the course of development.

A comprehensive plan has been prepared which takes sample rods from the bridge, utilizes spare rods as appropriate and tests additional rods exhibiting various diameters and finishes that have been placed on order with Dyson. When the test plan has been executed, all rod variations seen on the bridge will have been tested and assessed to determine the susceptibility of each individual rod to SCC. Rolled threads and cut threads are of interest since they exhibit different characteristics; rolled threads can offer a smoother thread profile due to the burnishing effect of the rolling operation, but this cold forming process can also increase the hardness at the thread end of the rods.

All tests, except for Tests IV and V, were completed by June 21, 2013. Tests II and III were conducted by independent laboratories in Texas and in Richmond, California. In consultation with Dr. Townsend, T.Y. Lin International prepared the shop drawings and the American Bridge/Fluor Joint Venture is constructing the equipment for Test IV, which is illustrated in Figure 27. The plans and protocols for Tests I through IV are shown in Figures 28 through 32.

Figure 28 Overall A354 Grade BD Rod Test Plan

SAS A354BD Bolt Tests											
ID	Priority	Structural Component	Number of Bolts	Nominal Bolt Diameter [in]	Sustained Bolt Tension % Fu (UTS)	Number of Heats	Testing Plan - Sampling (minimum)				
							I in-situ test	II Laboratory test (Note 1)	III Full Size Tension + Lab	IV Stress Corrosion test	V Incremental Step Load (ISL) Technique
1	TBD	Shear Key Anchor Bolts- Bottom (S1/S2)	96	3	0.70	7	90 +/-	10	1 (Note 9)	1 (Note 9)	10
2	A	Shear Key Anchor Bolts- Bottom (S3/S4)	96	3	0.70	1*	90	-	2**	2	2 (from test III)
	A	Pier E2 Bearing Bolts- Bottom Housing (B1, B2, B3, B4)	96	3	0.70	1*	74	-	2**	2	2 (from test III)
3	A	Shear Key Anchor Bolts-Top (S1/S2)	160	3	0.70	1*	160	6 (spares)	2 (spares)	-	2 (spares)
	A	Shear Key Anchor Bolts-Top (S3/S4)	160	3	0.70	1*	160	6 (spares)	2 (spares)	-	2 (spares)
4	A	Pier E2 Bearing Bolts- Top Housing (B1,B2,B3,B4)	224	2	0.70	1	224	7 (spares)	2 (spares)	1 (spare)	2 (spares)
5	-	Spherical Bearing Bushing Assembly Bolts	96	1	0.661	1	-	-	-	-	-
6	-	Bearing Retainer Ring Plate Assembly Bolts	336	1	0.40	2	-	-	-	-	-
7	B	PWS Strand Anchor Rods (Main Cable)	274	3-1/2	0.32	17	270 (Note 2)	43	1 (spare)	4***	4
8	C	Tower Saddle Tie Rods	25	4	0.68	1	19 (Note 3)	2 (spare)	1 (spare)	1 (spare)	1 (spare)
9	D	Tower Saddle Turned Rods (@ Splices)	100	3	0.45	2	20	2	-	-	-
	D	Tower Saddle Turned Rods (@ Splices)	8	3	0.10	1	-	-	-	-	-
10	-	Tower Saddle Grillage Bolts	90	3	0.10	1	-	-	-	-	-
11	D	Tower Outrigger	4	3	0.10	1	-	-	1 (spare)	-	-
12	C	Tower Anchorage Anchor Bolts (75 Dia. Anchor Bolts)	388	3	0.48	2	194	6 (Note 5)	1 (Note 7)	1 (Note 7)	2 (Note 5)
13	C	Tower Anchorage Anchor Bolts (100 Dia. Anchor Bolts)	36	4	0.37	1	36	3 (Note 5)	-	-	1 (Note 5)
14	D	East Saddle Anchor Rods	32	2	0.10	1	16	2 (Notes 4 & 7)	1 (spare)	-	-
15	D	East Saddle Tie Rods	18	3	0.20	1	9	1	-	-	-
16	D	Cable Bracket Anchor Rods	24	3	0.16	1	12	(Note 6)	-	-	-
17	E	Bikepath Anchor Bolts at Pier W2	43	1-1/4	0.10	1	9	1	-	-	-
18		E2 2013 Replacement Rods (CCO 312)	30	3	0.70	4	-	4	-	10 (Note 8)	4 (Note 8)

Notes: Notes:  
 1. Test at least one sample from each heat for Test II  
 2. Cut-off drill and tap hole @ end for testing. Sample lengths to be provided in separate attachment.  
 3. Test top surface of hex @ end of rod  
 4. No Charpy tests due to limited available rod stock-out.  
 5. Samples for lab test shall be taken after tests I, II and IV are completed.  
 6. Same heat as PWS, no sampling necessary.  
 7. Sample already removed.  
 8. Rods to be tested shall be as follows  
 + Galvanized: 3 Test IV  
 + Black: 3 Test IV  
 + Double Heat Treated, Galvanized: 2 Test IV  
 + Double Heat Treated, Black: 2 Test IV  
 + 1 rod Cut - half galv half black: Test V  
 + 1 rod Double Heat Treated, half galv, Half black: Test V  
 9. If sample of sufficient length is available  
 \*\*\* 2 rolled thread samples & 2 cut thread samples

Figure 29 Test I Protocol

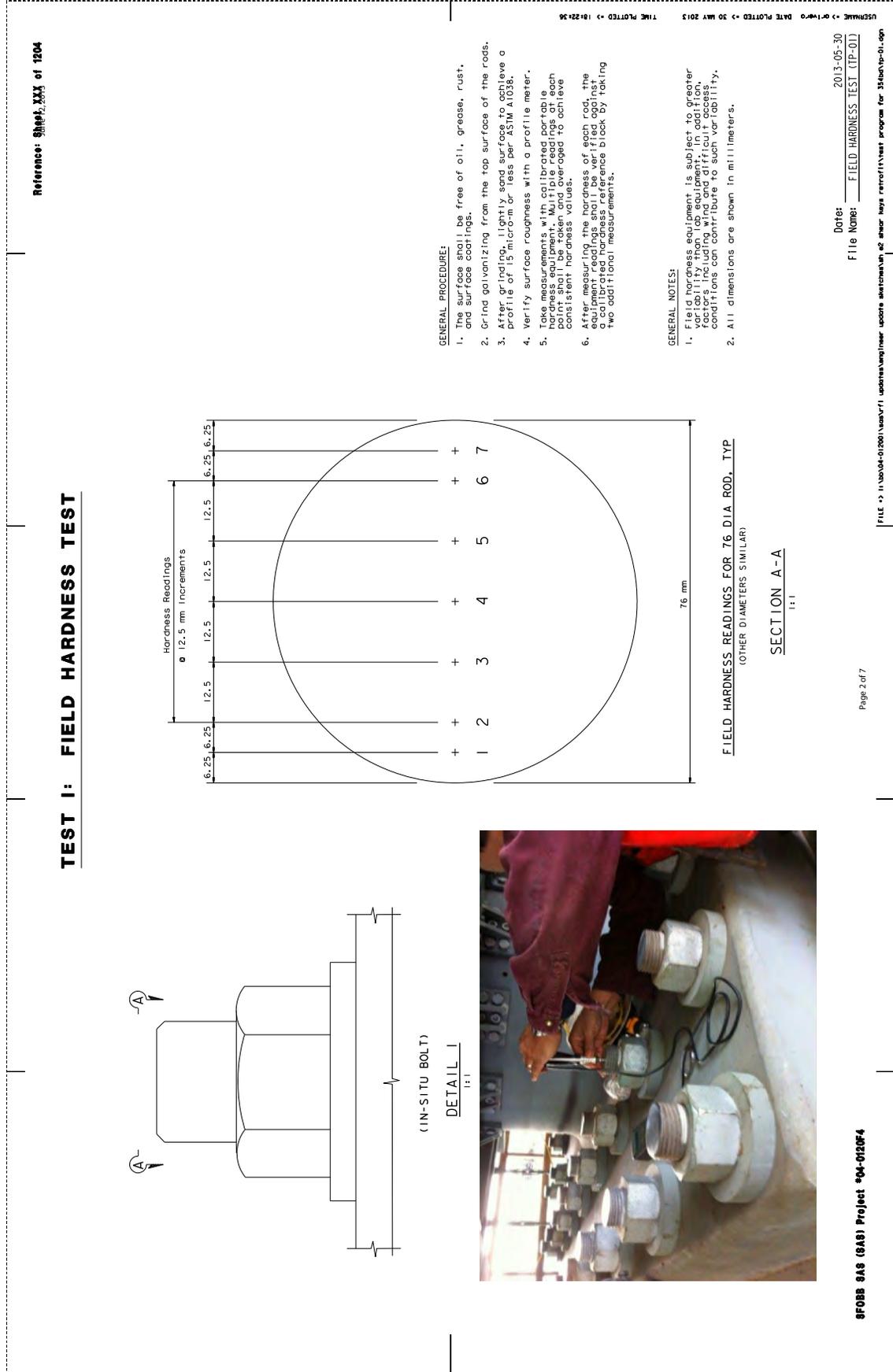
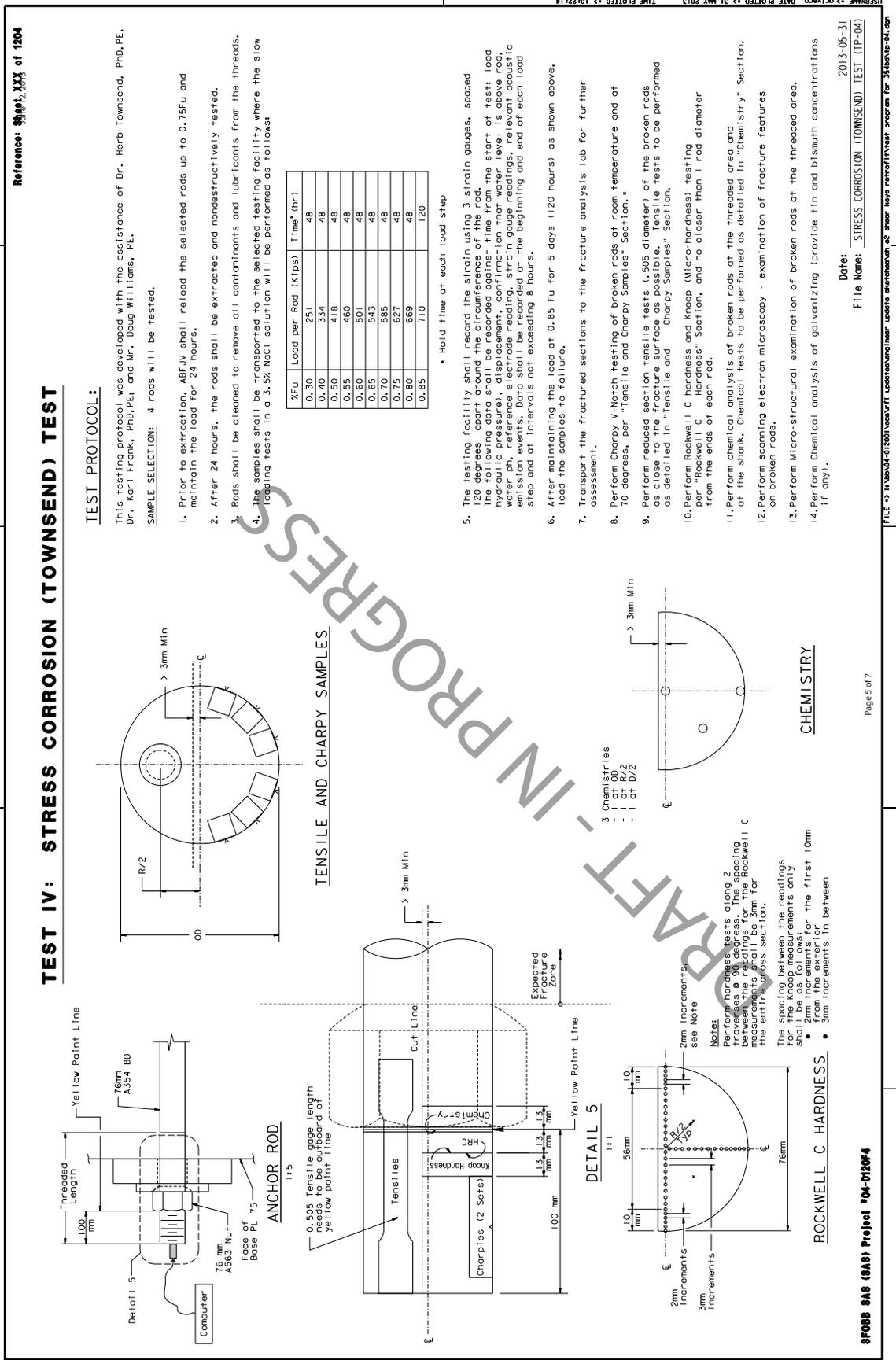






Figure 32 Test IV Protocol



**ANCHOR ROD**  
1:15

**TENSILE AND CHARPY SAMPLES**

**ROCKWELL C HARDNESS**

**CHEMISTRY**

**DETAIL 5**  
1:1

**ROCKWELL C HARDNESS**

- The spacing between the readings for the Knoop measurements only shall be as follows:
  - from the exterior
  - 3mm increments in between

**CHEMISTRY**

NOTE:  
Perform hardness tests along 2 planes at 90 degrees. The spacing between the readings for Rockwell C measurements shall be 3mm for the entire gross section.

3 Chemstrips  
- 1 at 0D  
- 1 at R/2  
- 1 at D/2

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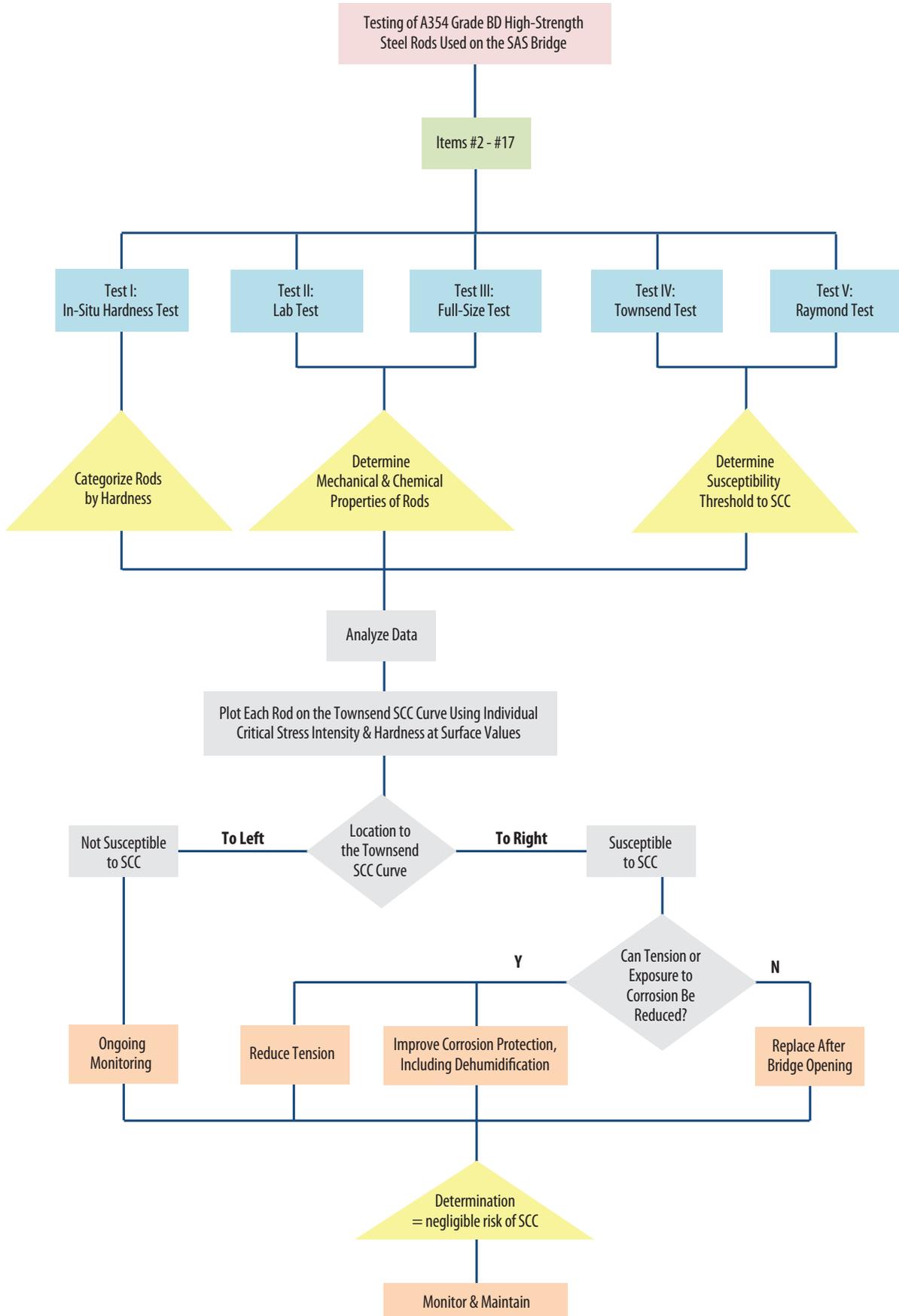
The flowchart displayed in Figure 33 shows the sequence of Tests I through V and at what point it can be determined that a group of rods has “passed the test” of longer-term stress corrosion cracking or, as deemed necessary, must be addressed by the implementation of mitigation measures. The tests are shown in the blue rectangular boxes; mitigating actions in the orange rectangular boxes; and the outcome/determination in the yellow triangle.

Tests I (*in-situ* hardness test), II (lab test) and III (full-sized test) were completed on June 21, 2013. The results from Tests I, II and III verified the mechanical properties of the rods and categorized each rod by hardness. The results from these tests are displayed in Table 13.

For Test IV (Townsend test), the construction of the test rig is underway and scheduled for completion in early July 2013 and used for the first group of Test IV samples. Additional test rigs are being constructed for Test IV to accommodate the range of ASTM A354 grade BD rod lengths and diameters. Results from Test IV will create the graphical curve per Townsend’s research based on the ASTM A354 grade BD rods in the SAS Bridge and superimposed onto Figure 34. Rods that are to the left of the Townsend curve would be deemed not susceptible to stress corrosion cracking, while rods that are to the right of the curve will be deemed susceptible to stress corrosion cracking.

Upon completion of all the testing and implementation of mitigating measures as depicted in Figure 33, the risk of hydrogen-associated damage to the metallurgical structure of the high-strength rods will have been addressed for the SAS Bridge. These test results also provide conclusive evidence that the cause of the high-strength rod failures observed in March 2013 from short-term hydrogen embrittlement is isolated to the shear key S1 and S2 anchor rods at the top of Pier E2 manufactured in 2008. This conclusion is further confirmed by the ongoing performance of the remaining rods under varying levels of tension as depicted earlier in Table 9.

**Figure 33 Determination for Susceptibility to Stress Corrosion Cracking**



As noted earlier, critical to the development of stress corrosion cracking is the tension the rod is placed under, its diameter, threads and the hardness of the material. Individual rods with higher tension levels and hardness levels at, or above, 35 HRC should be further evaluated for risk for stress corrosion cracking, per guidance from Dr. Fisher. Test I results for *in-situ* surface hardness continue to show varying hardness levels across all tested rods. These results are plotted in Figure 34.

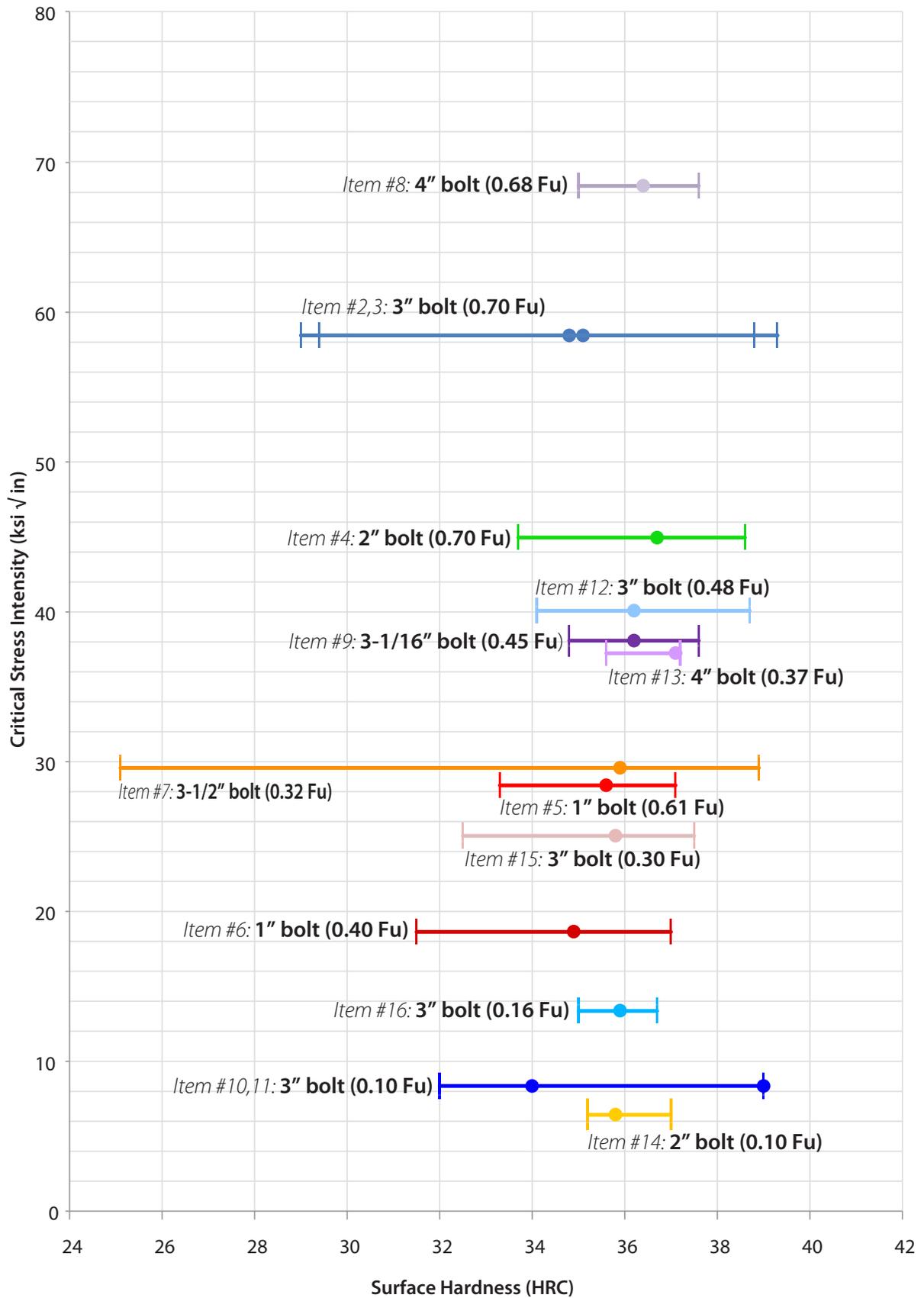
Upon completion of Test IV later this summer, a stress corrosion cracking susceptibility curve for the A354 Grade BD rods can be plotted on the critical stress intensity graph shown in Figure 34, displaying Critical Stress Intensity versus Surface Hardness. As described earlier, when the stress corrosion cracking susceptibility curve is included in Figure 34, in general, those rods plotted to the right of the curve will exhibit greater susceptibility to stress corrosion cracking over time. Conversely, rods plotted to the left of the curve are unlikely to be susceptible to stress corrosion cracking.

## What Do the Results of Tests I, II and III Say About the Material Properties of the Other A354 Grade BD Rods?

Tests I, II and III for the other rods verified QC/QA test results and confirmed that the rods have low risk for near-term hydrogen embrittlement failures because the rods exhibit better metallurgical uniformity and improved toughness as compared to the failed 2008 rods. As noted earlier, these rods have performed successfully under tension from a minimum of three months to a maximum of nearly four years.

In regards to longer-term stress corrosion cracking, there are a number of rods that exhibit surface hardness that is in excess of 35 HRC, a point at which there is increased risk for stress corrosion cracking under sustained high tension. However, based on the tests, these rods also exhibit better metallurgical uniformity and improved toughness. Further, many of the remaining rods are not subject to high sustained tension levels or are located in dehumidified or sealed areas that provided additional corrosion protection. Further, stress corrosion testing is underway as part of Tests IV and V that will provide important data for further analysis and remediation of the rods. A summary of findings based on the material properties obtained from Tests I through III (unless otherwise noted) is contained in Table 13.

**Figure 34 Critical Stress Intensity as Compared to Surface Hardness With *In-situ* Surface Hardness Test Data**



**Table 13 Summary Results of Testing for Susceptibility to SCC**

Item #		Microstructure	Surface Hardness of Tested Rods (HRC)	Ultimate Tensile Strength (ksi)
#1	Shear Key Anchor Rods (2008)	Incomplete martensitic transformation with alternate layers of ferrite and pearlite, and inclusions.	37.6 (avg) 36.9 – 38.2 (min – max) (Metallurgical Report)	165 (avg) 159 - 170 (min - max) (Metallurgical Report)
#2	Bearing & Shear Key Anchor Rods	Essentially Martensitic Structure	34.8 (avg) 29 – 39.3 (min – max)	158.6 (avg) 157 – 160.2 (min - max)
#3	Shear Key Rods (top)	Essentially Martensitic Structure	35.1 (avg) 29.4 – 38.8 (min – max)	157.3 (avg) 156.3 – 158.3 (min - max)
#4	Bearing Rods (top)	Essentially Martensitic Structure	36.7 (avg) 33.7 – 38.6 (min – max)	159.2 (avg) 158.4 – 159.9 (min - max)
#5	Bearing Assembly	Not tested	36 (avg) 33 – 37 (min – max) (QC/QA Data)	166 (avg) 161 - 174 (min - max) (QC/QA Data)
#6	Bearing Retainer Ring Plate Assembly	Not tested	35 (avg) 32 – 37 (min – max) (QC/QA Data)	166 (avg) 157 - 176 (min - max) (QC/QA Data)
#7	Parallel Wire Strands (PWS) Anchor Rods	Essentially Martensitic Structure	35.9 (avg) 25.1 – 38.9 (min – max)	158.5 (avg) 158.3 – 158.6 (min - max)
#8	Saddle Tie Rods	Essentially Martensitic Structure	36.4 (avg) 35 – 37.6 (min – max)	156.4 (avg) 151.3 – 161.5 (min - max)
#9	Saddle Turned Rods	Not tested	36.2 (avg) 34.8 – 37.6 (min – max)	148 (avg) 145 - 151 (min - max) (QC/QA Data)

For more information, see Appendix E.17 SAS A354BD Testing Program Results Tests I, II, and III; June 25, 2013

Mode of Fracture	Toughness CVN			
	at 40° F (ft-lb)	Sustained Applied Tension	Secondary corrosion protection	Any rods failed after being tensioned
Brittle	13.5 (avg) 13 - 14 (min - max)	0.7	N/A	Yes
Ductile	37.3 (avg) 35.5 – 39.5 (min - max)	0.7	N/A	No
Ductile	36.9 (avg) 35 – 39 (min - max)	0.7	N/A	No
Ductile	26.7 (avg) 22 – 31 (min - max)	0.7	N/A	No
Ductile (QC/QA Data)	Not tested	0.6	Installed in a sealed and lubricated assembly.	No
Ductile (QC/QA Data)	Not tested	0.4	Installed in a sealed and lubricated assembly.	No
Ductile	39 (avg) 28 – 52 (min - max)	0.3	Dehumidified	No
Ductile	16.8 (avg) 13 – 18.5 (min - max)	0.7	Dehumidified	No
Ductile (QC/QA Data)	32.7 (avg) 11.5 – 54 (min - max)	0.5	Dehumidified	No

table continued on next page

**Table 13 Summary Results of Testing for Susceptibility to SCC** (continued)

Item #		Microstructure	Surface Hardness of Tested Rods (HRC)	Ultimate Tensile Strength (ksi)
#10	Saddle Grillage	Not tested	34 (avg) 32 – 34 (min – max) (QC/QA Data)	150 (avg) 147 - 153 (min - max) (QC/QA Data)
#11	Outrigger Boom	Essentially Martensitic Structure	39 (avg) 39 – 39 (min – max) (QC/QA Data)	167 (single test)
#12	Tower Anchor Rods (Type 1)	Essentially Martensitic Structure	36.2 (avg) 34.1 – 38.7 (min – max)	160.3 (avg) 154.9 – 163.3 (min - max)
#13	Tower Anchor Rods (Type 2)	Not tested	37.1 (avg) 35.6 – 37.2 (min – max)	154 (single test)
#14	East Saddle Anchor Rods	Essentially Martensitic Structure	35.8 (avg) 35.2 – 37 (min – max)	150.4 (avg) 150.4 – 150.4 (min - max)
#15	East Saddle Tie Rods	Not tested	35.8 (avg) 32.5 – 37.5 (min – max)	148 (avg) 146 - 152 (min - max) (QC/QA Data)
#16	Cable Band Anchor Rods	Not tested	35.9 (avg) 35 – 36.7 (min – max)	156 (avg) 154 - 158 (min - max) (QC/QA Data)
#17	Bikepath Anchor Rods	Not tested	36 (avg) 35 – 37 (min – max) (QC/QA Data)	167 (avg) 160 - 179 (min - max) (QC/QA Data)

Mode of Fracture	Toughness CVN at 40° F (ft-lb)	Sustained Applied Tension	Secondary corrosion protection	Any rods failed after being tensioned
Ductile (QC/QA Data)	Not tested	0.1	N/A	No
Ductile	Insufficient sample length to perform test	0.1	N/A	No
Ductile	40.5 (avg) 32 – 56 (min - max)	0.5	Dehumidified	No
Ductile (QC/QA Data)	31.7 (avg) 23 – 46 (min - max)	0.4	Dehumidified	No
Ductile	27 (avg) 24 – 32 (min - max)	0.1	N/A	No
Ductile (QC/QA Data)	17.8 (avg) 17 – 18.5 (min - max)	0.2	N/A	No
Ductile (QC/QA Data)	Not tested	0.2	N/A	No
Ductile (QC/QA Data)	Not tested	TBD	N/A	No

## 7. Toll Bridge Program Oversight Committee (TBPOC) Findings

This Toll Bridge Program Oversight Committee (TBPOC) investigation entailed an exhaustive review of contract documents — including all the relevant and available QC, QA as supplied by Caltrans and other project records — and detailed discussions with key project staff to validate our technical reasoning. TBPOC also sought advice and informed opinions from both national and international experts to understand all the issues and to determine the industry’s current best practice approach. We present our findings below:

1. As noted in the joint Caltrans - American Bridge/Fluor Joint Venture metallurgical report dated May 7, 2013, “The [2008] anchor rods failed as a result of hydrogen embrittlement, resulting from the applied tensile load and from hydrogen that was already present and available in the rod material as they were tensioned. The root cause of the failures is attributed to higher than normal susceptibility of the steel to hydrogen embrittlement.” However, that same report concluded that “the steel rods comply with the basic mechanical and chemical requirements of ASTM A354 grade BD,” which was the basis of the rod specification selected by the designer and owner of the project.
2. The three factors contributing to the risk of failure due to hydrogen embrittlement are the presence of hydrogen, high tensile loads and the susceptibility of the material to hydrogen. The contract specifications for the East Span did not consider the unique requirements of the seven different rod locations on the SAS Bridge. One specification was inappropriately applied to all locations. In addition, it was inappropriate to adapt the fastener specification modified during the Richmond-San Rafael Bridge Retrofit Project, where the A354 grade BD galvanized rods were deployed underwater at low tension (snug tight), to the E2/T1 Marine Foundation and SAS Superstructure contracts for the new east span, where similar rods were deployed above water and at considerably higher tension levels.
3. There was inadequate consideration to allow for sole-source specifications, utilizing alternative or specific mechanical properties of steel. In fact, proprietary Macalloy high-strength rods were specified for the pre-stressing rods in the W2 cap beam in the SAS special provisions. Investigation into other types of high-strength steel rods, even if they might have required-sole sourcing, appears to have been warranted.
4. There was inadequate consideration given to the combined effect of high-strength rod material requirements and corrosion protection. The fastener selection process was completed during design, and the corrosion protection specification was modified during advertisement and construction. There was no subsequent return discussion to the fastener selection decision.

5. There was inadequate consideration of alternative corrosion protection treatments, given well-known concerns about the risk of hydrogen embrittlement from hot-dipped galvanizing of A354 grade BD rods. In particular, alternative treatments such as Geomet<sup>®</sup>, or greased and sheathed, or painted solutions should have been more fully considered depending on the various sizes and applications. A life cycle cost analysis should have been prepared for the various rod alternatives and the various methods of long-life corrosion protection.
6. The fastener specification for the E2/T1 Marine Foundations and SAS Superstructure contracts relied too heavily on generic ASTM standards and should have included special provisions reflecting a better understanding of the principles of the ASTM standards to guard against hydrogen embrittlement. In particular, the contracts should have more clearly addressed the following four requirements: 1) maximum steel hardness and through consistency, 2) minimum steel toughness, 3) magnetic particle testing, and 4) a time-dependent test of the rods under tension prior to their installation on the new bridge. As one peer review panelist noted: “National Standards are the minimum. You still need to do good engineering.”
7. The construction of Pier E2 should not have allowed for water to collect during the construction process. The collection of water in their support cylinders may have exacerbated the embrittlement of the 2008 high-strength steel rods. Because the rods were to be embedded in concrete, it was infeasible to remove and replace them. In the words of one engineer, “A good design should not be so sensitive to bad material.”
8. ASTM 143 required a hydrogen embrittlement test. The designer was aware of the potential of hydrogen embrittlement, but construction oversight technicians only tested rods with 1½-inch diameter or less. The large-diameter rods were not tested for hydrogen embrittlement and a Request for Information was not issued. Closer coordination was needed between design and construction staff.
9. It took a considerable amount of time, including significant manual effort, to assemble the QC/QA information for the SAS rods. In the case of the E2/T1 Marine Foundation contract, much of the information has not been located for a contract completed as recently as 2008. Such information is vital not only for an investigation of materials failure such as this, but for routine maintenance and major rehabilitation of the SAS over its 150-year design life.

## Responsible Parties

The design and construction of the Self-Anchored Suspension (SAS) Bridge of the new East Span involved several responsible parties:

- Caltrans is the owner and operator of the New East Span;
- T.Y. Lin International/Moffatt & Nichol Design Joint Venture is the Engineer of Record;
- American Bridge/Fluor Joint Venture is the contractor for the SAS Superstructure; and
- Kiewit/FCI/Manson Joint Venture is the contractor for the SAS E2/T1 Marine Foundation.

These parties are responsible for the actions that led to the following findings:

- T.Y. Lin International/Moffatt & Nichol Design Joint Venture, American Bridge/Fluor Joint Venture and Caltrans jointly share responsibility for Findings 1 and 7.
- T.Y. Lin International/Moffatt & Nichol Design Joint Venture and Caltrans jointly share responsibility for Findings 2, 3, 4, 5 and 6.
- American Bridge/Fluor Joint Venture and Caltrans jointly share responsibility for Finding 8.
- Caltrans is responsible for Finding 9.

## 8. Toll Bridge Program Oversight Committee (TBPOC) Decisions and Actions

On July 18, 2005, Governor Schwarzenegger signed Assembly Bill 144 (AB 144) into law and thereby authorized a \$1 increase in the seismic surcharge to be implemented no earlier than January 1, 2007. AB 144 also created the Toll Bridge Program Oversight Committee (TBPOC) to provide oversight and project control for the Toll Bridge Seismic Retrofit Program and the Benicia-Martinez Bridge Project in California.

The TBPOC is composed of the Director of the Department of Transportation (Caltrans), the Executive Director of the Bay Area Toll Authority (BATA), and the Executive Director of the California Transportation Commission (CTC). The TBPOC's project oversight and control activities include, but are not limited to, reviewing bid specifications and documents, providing field staff to review ongoing costs, reviewing and approving significant change orders and claims in excess of \$1 million (as defined by the Committee) and preparing project reports.

In April 2013, the TBPOC initiated an investigation into the failed A354 grade BD rods. As part of the investigative process, the TBPOC did the following:

- Conducted four workshops on April 17, May 1, May 15, and June 25, 2013;
- Met over 25 times in person or by phone;
- Consulted with industry experts, the Seismic Peer Review Panel, and the Federal Highway Administration Review Panel;
- Reviewed over 50 documents and over 5,000 pages of material;
- Briefed the Bay Area Toll Authority (BATA) and the BATA Oversight Committee on March 27, April 10, April 24, May 8, and May 29, 2013;
- Presented and responded to questions during the California Senate Transportation and Housing Committee hearing on May 14, 2013; and
- Briefed members of the Bay Area State Legislative Delegation on June 6, 2013.

On May 8, 2013, the TBPOC received a presentation from T.Y. Lin International/Moffatt & Nichol Design Joint Venture on the retrofit strategy options to address the failed 2008 rods. The TBPOC selected the steel saddle option because it would meet all the design requirements and objectives of the project. The Seismic Peer Review Panel agreed with this selection.

On the same day, the TBPOC sent a letter to the California Division Administrator of FHWA requesting its assistance to conduct an independent review of the findings and recommendations concerning the high-strength steel rods on the new East Span.

Based on the findings above and review of the 17 different types of A354 grade BD rods used on the East Span, there are four categories into which this report classifies the 2,306 high-strength steel rods on the SAS Bridge:

1. Rods whose clamping capacity is to be replaced before opening the bridge to traffic;
2. Rods that are to be replaced after opening the bridge, as a precautionary measure to address concerns of longer-term stress corrosion;
3. Rods that are subject to mitigating actions, such as reduced tension, dehumidification, or other corrosion protection systems; and
4. Rods that are acceptable for use, will meet performance expectations, and will undergo a regular inspection schedule.

## How Will Rods at Risk of Stress Corrosion Cracking Be Addressed?

Stress corrosion cracking is time-dependent — it occurs over years or decades of sustained tension and is based on the commencement and rate of corrosion. The longer-term concern is whether the remaining A354 grade BD rods are susceptible to stress corrosion cracking and, if so, when cracking may occur. Like hydrogen embrittlement, there are three factors that contribute to stress corrosion cracking — susceptible material, high tensile stress and hydrogen-related corrosion. Without any one of these three conditions, stress corrosion cracking will not occur. Assuming susceptible material, the mitigation strategy for avoiding stress corrosion cracking is to either reduce the tensile stress or reduce the potential for corrosion. The option of reducing the tension can only be considered by the designer after evaluating any excess redundancies in the completed structure versus the original design requirements. Mitigation of corrosion can be achieved in a number of ways, by the application of galvanizing, painting, greasing and sheathing, or dehumidifying to remove moisture (a source of hydrogen). If reducing applied tension or mitigating corrosion cannot be achieved in another fashion, then replacement of rods may be necessary.

As noted earlier, galvanizing is designed to protect the underlying steel from corrosion. However, galvanizing also can be detrimental to highly stressed, high-strength steels because a small penetration through the coating will establish a galvanic reaction that accelerates the generation of hydrogen at the point of the penetration, thereby introducing a strong source of environmental hydrogen.

## Rod-by-Rod Resolution

Based on the data available from Tests I through III and the design criteria and expected structural performance of the SAS, Table 14 depicts a provisional approach for remediating the stress corrosion cracking potential of the various A354 grade BD rods on the SAS Bridge. These recommendations are provisional pending completion of the final tests (referred to as the Townsend Test and Raymond Test). In no case, however, do we expect the remaining tests to indicate that any rods, other than the failed Item #1 anchor rods, will need to be replaced before opening the new East Span to traffic. The risk of near-term hydrogen embrittlement has passed. The potential for longer-term stress corrosion cracking can be managed safely and effectively after the SAS is placed into service.

The rod-by-rod resolution displayed in Table 14 is explained in the following section, which details the remediation strategy for each grouping of A354 grade BD rods. The “Replacement Before Opening” is self-explanatory. “Replace After Opening” and “Augment Dehumidification” are anticipated to occur before the end of 2014 to take advantage of the efficiencies offered by the existing contractor and the temporary work platforms that are still in place. Rods confirmed by T.Y. Lin International/Moffatt & Nichol Design Joint Venture, the Engineer of Record, as being appropriate for reduction in tension will be adjusted as soon as the load distribution ceases to change due to construction activities. The rods labeled “Accept and Monitor” do not require remediation and illustrate the fact that the original specification used for all 17 rod locations was only appropriate for fasteners installed under low tension. All high-strength rods will require routine and periodic maintenance.

**Table 14 Recommended Rod-by-Rod Resolution**

Location	Construction	Maintenance			
	Replace Before Opening	Replace After Opening	Reduce Tension	Augment Dehumidification	Accept and Monitor
E2	1. Shear Key Anchor Rods* (bottom)  *replaced by steel saddle retrofit	2. Bearing & Shear Key Anchor Rods (bottom) 3. Shear Key Rods (top) 4. Bearing Rods (top)			5. Bearing Assembly (bushings) 6. Bearing Retainer Ring Plate Assembly
Anchorage				7. PWS Anchor Rods	
Top of Tower		11. Outrigger Boom	8. Saddle Tie Rods 9. Saddle Turned Rods		10. Saddle Grillage
Bottom of Tower			12. Tower Anchor Rods (Type 1) 13. Tower Anchor Rods (Type 2)		
East Saddle					14. East Saddle Anchor Rods 15. East Saddle Tie Rods
East Cable					16. Cable Band Anchor Rod
W2					17. Bikepath Anchor Rods

Note: Dehumidification is already in place for the Top of Tower, Bottom of Tower and Main Cable Anchorage.

## Pier E2

### Items #1 - 3" diameter Pier E2 S1 and S2 Anchor Rods:

The clamping capacity of these failed rods will be replaced by the retrofit, and the retrofit must be completed prior to bridge opening.

### Items #2 - 3" diameter Pier E2 B1-B4, S3 and S4 through Anchor Rods:

These rods have a design stress of 0.7 Fu. At the tested range of surface hardness, these rods will be highly susceptible to stress corrosion cracking. Pending full evaluation of all test results, the designer may consider lowering the stress in these rods or full replacement to substantially eliminate the likelihood of stress corrosion cracking.

### Item #3 - 3" diameter Pier E2 S1 through S4 Upper Housing Rods:

These rods have a design stress of 0.7 Fu. At the tested range of surface hardness, these rods will be highly susceptible to stress corrosion cracking. Pending full evaluation of all test results, the designer may consider lowering the stress in these rods or full replacement to substantially eliminate the likelihood of stress corrosion cracking.

### Item #4 - 2" diameter Pier E2 Bearing Upper Housing Rods:

These rods have a design stress of 0.7 Fu. At the tested range of surface hardness, these rods will be highly susceptible to stress corrosion cracking. Pending full evaluation of all test results, the designer may consider lowering the stress in these rods or full replacement to substantially eliminate the likelihood of stress corrosion cracking.

### Item #5 - 1" diameter Pier E2 Bearing Assembly Rods:

These rods have a design stress of 0.61 Fu. These rods satisfactorily passed ASTM A143 embrittlement tests prior to installation. Although the rods themselves are inaccessible, the bearing assemblies that contain these rods will be monitored for performance. Further, these rods are sealed and lubricated inside the bearing, which should prove to be an effective deterrent to corrosion.

### Item #6 - 1" diameter Bearing Retainer Plate Assembly

These rods have a design stress of 0.4 Fu and satisfactorily passed ASTM A143 embrittlement tests prior to installation. Although the rods themselves are inaccessible, the bearing assemblies that contain these rods will be monitored for performance. Further, the rods are sealed and lubricated inside the bearing, which should prove to be an effective deterrent to corrosion.

## Anchorage

### Item #7 - 3.5" diameter Parallel Wire Strand (PWS) Anchor Rods:

These rods have a design stress of 0.32 Fu. The *in-situ* surface hardness of these rods varies widely from 25 to 39 HRC, with many rods at the upper end of that range, which indicates high susceptibility to stress corrosion cracking. PWS Anchor Rods are housed inside a water-tight, dehumidified chamber so moisture is not readily present, which will tend to mitigate stress corrosion cracking for the PWS rods. Since it is not possible to reduce the tension levels on these rods and replacement is not desirable, the near-term remediation strategy is to ensure adequate dehumidification to reduce the corrosion potential in the cable anchorage chamber. This may require augmenting the planned level of dehumidification in the chamber.

## Top of Tower

### Item #8 - 4" diameter Tower Saddle Tie Rods:

These rods had an installation design tension of 0.41 Fu, which increased to 0.68 Fu upon completion of load transfer. The *in-situ* surface hardness of these rods (from 35 to 38 HRC) indicates high susceptibility to stress corrosion cracking. However, the rods are housed inside a water-tight, dehumidified chamber so moisture is not readily present, which will tend to mitigate stress corrosion cracking for these rods. Pending full evaluation of all test results, the designer may consider lowering the stress in these rods or augmenting dehumidification to substantially eliminate the likelihood of stress corrosion cracking.

### Item #9 - 3" diameter Tower Saddle Turned Rods:

These rods have a design stress of 0.45 Fu, which was only necessary during the erection of the tower saddle segments at the top of the Tower. After erection of the cable and load transfer, these rods are no longer required due to the radial forces imposed by the main cable through the tower saddle. Further, these rods are housed inside a water-tight, dehumidified chamber so moisture is not readily present, which will tend to mitigate stress corrosion cracking. Although the surface hardness of these rods indicates high susceptibility to stress corrosion cracking, the combined possibility for lowering of tension and augmenting dehumidification should provide an effective means to substantially reduce the risk of stress corrosion cracking.

### Item #10 - 3" diameter Saddle Grillage:

These rods have a design stress of 0.1 Fu. The low tension of these rods indicates low susceptibility to stress corrosion cracking, but these rods have high surface hardness. Ongoing monitoring is recommended.

### Item #11 - 3" diameter Outrigger Boom:

These rods have a design stress of 0.1 Fu. The low tension of these rods indicates low susceptibility to stress corrosion cracking, even though these rods have high surface hardness. As the tower boom has not yet been installed, these rods should be replaced prior to boom installation.

## Bottom of Tower

### Item #12 - 3" diameter Tower Anchor Rods:

These tower anchor rods were installed under the E2/T1 contract. These rods have a design stress of 0.48 Fu. The *in-situ* surface hardness of the material is between 34 and 39 HRC. These rods are located on the exterior and interior face of the tower base. Replacing the interior rods will be difficult, if not impossible, due to the overall length of these rods and the limited amount of headroom available inside of the tower. However, these rods are housed inside a water-tight, dehumidified chamber so moisture is not readily present, which will mitigate stress corrosion cracking. Pending full evaluation of all test results, the designer may consider lowering the stress in these rods.

### Item #13 - 4" diameter Tower Anchor Rods:

These tower anchor rods were installed under the E2/T1 Marine Foundation Contract. These rods have a design stress of 0.37 Fu. The *in-situ* surface hardness of the material is between 35 and 37 HRC. These rods are housed inside a water-tight, dehumidified chamber so moisture is not readily present, which will mitigate stress corrosion cracking. Pending full evaluation of all test results, the designer may consider lowering the stress in these rods. Ongoing monitoring is recommended.

## East Saddles

### Item #14 - 2" diameter East Saddle Anchor Rods:

These rods have a design stress of 0.1 Fu. The low tension of these rods indicates low susceptibility to stress corrosion cracking. Ongoing monitoring is recommended.

### Item #15 - 3" diameter East Saddle Tie Rods:

These rods have a design stress of 0.2 Fu. The low tension of these rods indicates low susceptibility to stress corrosion cracking. Ongoing monitoring is recommended.

## East Cable

### Item #16 - 3" diameter Cable Band Anchor Rods:

These rods have a design stress of 0.16 Fu. The low tension of these rods indicates low susceptibility to stress corrosion cracking. Ongoing monitoring is recommended.

## Top of Pier W2

### Item #17 - 1.2" diameter Bikepath Anchor Rods:

The final design of the bikepath has not yet been completed since this is dependent upon the demolition of the existing East Span. These rods may be modified or replaced at a later time if necessary, but ongoing monitoring is recommended at a minimum.

## Revised Specifications for Replacement Rods

Additional high-strength steel rods are to be purchased to replace the 2010 rods on Pier E2 that have been selected for testing. The remediation strategy outlined in the previous section also will require procurement of additional high-strength steel rods. Caltrans has applied supplementary specifications for the rods identified for replacement, which limit the ultimate tensile strength, minimum toughness, maximum hardness and impose a tight tolerance on hardness, which will be measured at small intervals across the diameter, thereby ensuring homogeneous metallurgical structure. Caltrans also will be performing the time-dependent hydrogen embrittlement “pull test” required by ASTM F606 and the Townsend and Raymond Tests to determine the replacement rods’ susceptibility to stress corrosion cracking. Finally, alternative corrosion protection methods will be evaluated. The Toll Bridge Program Oversight Committee will review and approve all major actions regarding procurement of replacement rods.

## Maintenance Plan

One of the tasks of the design team is to prepare Bridge Maintenance and Inspection Manuals for each of the major components of the East Span shown in Figure 1, as each component is completed. Each set of manuals will provide documentation on the design, documentation on the construction, load ratings, detailed inspection procedures for each major element, an initial “baseline” inspection and inventory, sources and reference material, and post-seismic inspection and repair procedures. The manuals are to be used primarily by personnel engaged by Caltrans to perform routine inspections, in-depth or special inspections, and routine maintenance on the East Span structures. Regarding the A354 grade BD rods, the maintenance plan for these elements of the SAS Bridge will include existing baseline information (test data, etc.), required monitoring and testing, inspection and testing methods to be employed, required intervals, required routine and periodic maintenance, protocols for notification and action when required, and actions required after an extreme event (earthquake, vessel collision, etc.).

## Bridge Opening

The TBPOC concludes that it is safe to open the new East Span after replacing the capacity lost by the failed 2008 rods. It is unnecessary to replace any of the remaining rods (Items #2 through #17) before the bridge opening since the risk of near-term hydrogen embrittlement has passed, and especially in light of the safety imperative of moving traffic off the seismically deficient existing East Span Bridge. While some rods are highly susceptible to longer-term stress corrosion cracking, ample evidence exists that none are at high risk of near-term fracture. Replacement of rods on the east pier should begin prior to the contractor demobilizing, in order to take advantage of the current scaffolding and support structure in place.

## New Versus Old Bridge

As noted earlier, the San Francisco-Oakland Bay Bridge was designated by the California State Legislature as an important lifeline structure because of its location along transportation corridors crucial for emergency relief and economic revitalization following a major earthquake. Because of the Bay Bridge's designation as a lifeline structure, Caltrans required that the East Span Replacement Project incorporate design elements that exceed the requirements of standard seismic bridge design. The East Span Replacement Project was designed to withstand probable ground motions from largest earthquake to occur once every 1,500 years.

An excerpt from the Governor's Board of Inquiry<sup>20</sup> refers to the design of the old bridge that the new East Span will replace:

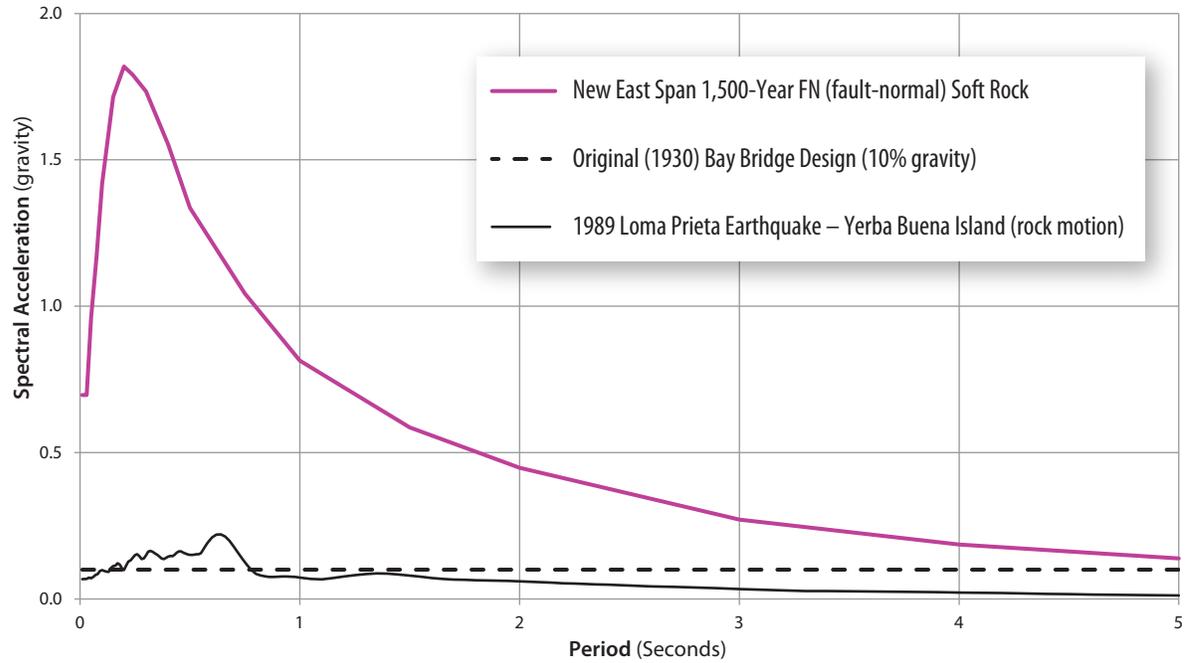
“The [1935] Bay Bridge was designed for 10% of g (the acceleration due to Earth's gravity) earthquake accelerations, comparable to the levels specified in the 1930 Uniform Building Code for buildings. It should be noted that knowledge of damaging earthquake motions was very limited at this time; the first few measurements of strong ground motions were not measured until the 1933 Long Beach earthquake.”

Ground accelerations have been plotted in Figure 35 comparing the design of the new East Span with the 1936 East Span and recorded Loma Prieta earthquake accelerations in 1989. The Loma Prieta earthquake was a 6.9-magnitude earthquake centered nearly 60 miles away from the Bay Bridge that still caused the partial collapse of a section of the existing cantilever structure. While the west spans of the Bay Bridge have been fully retrofitted, the east span of the bridge is still vulnerable until replaced.

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<sup>20</sup> Governor's Board of Inquiry on 1989 Loma Prieta Earthquake, page 26.

**Figure 35 Comparison of Ground Accelerations**



## 9. Review by the Seismic Peer Review Panel

The TBPOC has briefed the Seismic Peer Review Panel regarding its investigative report on the A354 grade BD high-strength steel rods on the SAS Bridge. The Seismic Peer Review Panel has provided comments on the report, and will provide its written review to the TBPOC under separate cover.

### Seismic Peer Review Panel

#### **Dr. Frieder Seible, Chair, Dean Emeritus, University of California at San Diego**

Dr. Seible is Chair of the Caltrans Seismic Advisory Board. He is also Dean and Professor Emeritus of the Jacobs School of Engineering, University of California at San Diego. He developed the Charles Lee Powell Structural Research Laboratories, which serve as a worldwide resource for full-scale testing and analysis of structures. He is a member of a federal Blue Ribbon Panel on Bridge and Tunnel Security. Seible received a Dpl. Ing. from the University of Stuttgart, a Masters of Science degree from the University of Calgary, and a Ph.D. from the University of California at Berkeley, all in civil engineering. Dr. Seible is a member of the National Academy of Engineering.

#### **Dr. John Fisher, Emeritus Professor of Civil Engineering, Lehigh University**

Dr. Fisher was Professor of Civil Engineering at Lehigh University from 1969 until 2002, when he became Professor Emeritus. He was Director of the Engineering Research Center on Advanced Technology for Large Structural Systems (ATLSS) since its establishment in May 1986 until September 1999. Dr. Fisher is a graduate of Washington University, St. Louis, Missouri, with M.S.CE and Ph.D. degrees from Lehigh University. A structural engineer, Dr. Fisher is a specialist in structural connections, the fatigue and fracture of riveted, bolted and welded structures, the behavior and design of composite steel-concrete members, and the performance of steel bridges. Dr. Fisher has published over 275 articles, reports and books in scientific and engineering journals. Dr. Fisher is a member of the National Academy of Engineering.

#### **Dr. I.M. Idriss, Emeritus Professor of Civil Engineering, University of California at Davis**

Dr. Idriss is a Professor in the Department of Civil Engineering and Environmental Engineering at the University of California at Davis. He completed his Ph.D. degree at the University of California at Berkeley. Dr. Idriss served as a member of Governor George Deukmejian's Board of Inquiry on the Loma Prieta Earthquake. Since 1998, Dr. Idriss has been a member of Caltrans' Seismic Peer Review Panel for the design and construction of the new East Span of the San Francisco-Oakland Bay Bridge. Dr. Idriss is a member of the National Academy of Engineering.

## Toll Bridge Program Oversight Committee

### **Steve Heminger, Chair**

Mr. Heminger is the Executive Director of the Metropolitan Transportation Commission (MTC), which is the regional transportation planning and finance agency for the nine-county San Francisco Bay Area. Since 1998, MTC has served as the Bay Area Toll Authority (BATA) responsible for administering all toll revenue from the seven state-owned bridges. Mr. Heminger is also Chair of the Toll Bridge Program Oversight Committee, overseeing construction of the new East Span Replacement Project. Mr. Heminger was appointed by House Democratic Leader Nancy Pelosi to serve on the National Surface Transportation Policy and Revenue Study Commission, which helped chart the future course for the federal transportation program. In addition, he is Chairman of the Board of Trustees for the Mineta Transportation Institute, a member of the Board of Directors for the Association of Metropolitan Planning Organizations, and a member of the Executive Committee for the Transportation Research Board. Mr. Heminger received his Master of Arts degree from the University of Chicago and a Bachelor of Arts degree from Georgetown University.

### **Malcolm Dougherty**

Mr. Dougherty is the Director of the California Department of Transportation (Caltrans), which builds, maintains and operates 50,000 lane-miles of the California transportation system. He is also a member of the Toll Bridge Program Oversight Committee. Before being appointed Director of Caltrans, Mr. Dougherty served as Chief Deputy Director, where he advised and assisted the Director with all aspects of Caltrans' policies and operations. He also served as District Director in the Fresno area. As District Director, he was responsible for Planning, Project Management, and Maintenance for the district's five counties, as well as the Capital Project Delivery Program for the Central Region, which spans from the Pacific coastline to Nevada, and from Amador County to Kern County. Dougherty's career also includes management positions in Design, Project Management, Maintenance, and Traffic Operations. Mr. Dougherty is a graduate of Rutgers University with a Bachelor of Science in Civil Engineering.

### **Andre Boutros**

Mr. Boutros is Executive Director of the California Transportation Commission (CTC), which is the agency responsible for programming and funding several billion dollars annually for transportation projects throughout California in partnership with regional transportation agencies and Caltrans. He is also a member of the Toll Bridge Program Oversight Committee. Mr. Boutros has more than 28 years of direct involvement in the development and preservation of California's transportation infrastructure. He has been a staff member to the CTC since 2007, and has served as the Chief Deputy Director since 2008, where he was responsible for the day-to-day operations of the CTC, acting as the chief of staff and the primary policy advisor to the Executive Director and the Commission. Prior to joining the CTC, Mr. Boutros held numerous technical, management and leadership positions with Caltrans.

## 10. Glossary of Terms

**A354 Grade BD Anchor Rod** An anchor rod conforming to the ASTM A354 specification, which covers the chemical and mechanical requirements of quenched and tempered alloy steel bolts, studs and other externally threaded rods 4 inches and under in diameter. Grade BD indicates level of strength, where the minimum tensile strengths are 140 ksi for 25/8-inch to 4-inch diameter rods and 150 ksi for ¼-inch to 2½-inch diameter rods.

**American Iron and Steel Institute (AISI)** An association of North American steel producers, developed in response to the need for a cooperative agency in the iron and steel industry for collecting and disseminating statistics and information, carrying on investigations, providing a forum for the discussion of problems and generally advancing the interests of the industry.

**American Society for Testing and Materials (ASTM)** Originally established in 1898 as the American Society for Testing and Materials, ASTM International is one of the largest organizations in the world to develop voluntary consensus standards for test methods and material specifications. One of its missions is to contribute to the reliability of materials, products, systems and services. The ASTM is made up of over 40 technical committees (e.g., Committee A-1 on Steel, Committee C-1 on Concrete). Its consensus approach to standards has resulted in the development of more than 12,000 ASTM standards today. For a description of the ASTM standards that are relevant to this project, refer to Table 6.

**Anchor Rod** A rod used to attach objects or structures to concrete. There are many types of anchor rod (also referred to as anchor bolts), consisting of designs that are mostly proprietary to the manufacturing companies. All consist of a threaded end, to which a nut and washer can be attached for the external load.

**Bearing** A device located between the bridge structure and a supporting pier or abutment.

**Caltrans Bridge Design Specifications Manual** All local bridges (in California) on and off the National Highway System shall be designed in accordance with the current edition of the *Caltrans Bridge Design Specifications Manual*. The 1995 version of the *Caltrans Bridge Design Specifications Manual* was in effect when the design of the new East Span Replacement Project began. The next update to the manual was released in April 2000.

**Caltrans Standard Specifications Manual** The *Caltrans Standard Specifications Manual* provides specifications that are standard to Caltrans construction projects.

**Charpy V-Notch Test** An impact test in which a rectangular specimen with a 'V' shaped notch cut into the midpoint of the length is struck by a pendulum mounted striker. The energy that is absorbed in fracture is calculated by comparing the height to which the striker would have risen had there been no specimen to the height to which it actually rises after fracture of the specimen.

**Compression** A force that pushes or presses toward the center of an object or from the ends toward the middle of a structural member.

**Corrosion** For steel, corrosion is an oxidation process where iron combines with oxygen to form iron oxide, which is commonly known as rust.

**Deck** The roadway portion of a bridge, including shoulders. Most bridge decks are constructed as reinforced concrete slabs.

**Ductility** The ability of a material to deform before it fractures.

**Elongation** Elongation is a measure of the ductility of a material (the percentage stretch in the length of a test specimen). It is the amount of strain (e.g., bending) a material can experience before failure in a tensile test. A ductile material will record a high elongation, while brittle materials, such as ceramics, tend to show very low elongation.

**Fatigue** A cyclic cracking mechanism that is progressive and localized, caused by repetitive loading over time and is more commonly transgranular.

**Ferrite** The metallurgical structure of iron alloys that forms if the material cools slowly from a high temperature

**Flash Pickling** A process of pickling where the steel product is dipped for less than 30 seconds to avoid a source of hydrogen that could be absorbed by the steel.

**Galvanic Corrosion** A phenomenon where the combination of different materials together with moisture establish an electric cell. Depending upon the material combination, one will become the anode and the other the cathode. The anode will display signs of corrosion while gas bubbles may be generated at the cathode.

**Galvanic Protection** An engineering solution to reduce or eliminate the corrosion of structural members where reliance on coatings may be impractical. Sacrificial anodes are attached to the structure so that a galvanic corrosion cell is established causing the anode to oxidize, thereby protecting the cathode (the structural member). The level of cathodic protection or rate of corrosion of the anode is dependent upon the ratio of the surface areas being exposed to the connecting moisture.

**Galvanizing** A means of applying a protective zinc coating that will corrode in preference to the steel substrate.

**Girder** A horizontal structural member supporting vertical loads by bending. Larger girders typically are made of multiple metal plates that are welded or riveted together.

**Grade BD** A level of strength specified in ASTM A354 that is higher than Grade BC and equal in strength to ASTM A490. The minimum tensile strengths of Grade BD rods are 140 ksi for 25/8-inch to 4-inch diameter rods and 150 ksi for 1/4-inch to 2 1/2-inch diameter rods. Unlike ASTM A490 however, the A354 grade BD specification is unrestricted in its configuration. Since A490 are heavy hexagon headed structural bolts and do not exceed 1 1/2-inches in diameter, specification A354 Grade BD should be considered for anchor bolts, threaded rods, other styles of headed bolts, and bolts larger than 1 1/2-inches in diameter where similar mechanical properties are desired. A354 Grade BD rods do not require a magnetic particle test, as is required by the A490 specification.

**Greased and Sheathed System** An alternative method for corrosion protection that does not require heat or chemical treatment that could potentially alter the chemical composition or mechanical properties of the steel. Steel rods are placed in a sheath (or sleeve/tube) and a corrosion-inhibiting wax or grease is injected into the sheath.

**Hardness** A measure of a material's ability to resist abrasion and indentation.

**Hardness Rockwell C Scale (HRC)** The Rockwell scale is a hardness scale based on indentation hardness of a material. There are several alternative scales, the most commonly used being the "B" and "C" scales. HRC is a gauge of the hardness of a material based on a test that measures the depth of penetration by an indenter under a large load compared to the penetration made by a preload. As specified in ASTM D785, the indenters for the Rockwell test include steel balls of several specific diameters and a diamond cone penetrator having an included angle of 120° with a spherical tip having a radius of 0.2 mm.

**High-Strength Steel (HSS) Bolts** A steel bolt or rod having a tensile strength greater than 125,000 pounds per square inch (125 ksi).

**Hot-Dip Galvanizing (HDG)** A process of dipping fabricated steel into a kettle or vat of molten zinc. While the steel is in the kettle, the iron metallurgically reacts with the molten zinc to form a tightly-bonded alloy coating that provides superior corrosion protection to the steel. It is the process of coating iron, steel or aluminum with a thin zinc layer, by passing the metal through a molten bath of zinc at a temperature of around 850° F (455° C). A typical hot-dip galvanizing process includes a cleaning operation that removes impurities, such as stains, inorganic contaminants, rust or scale, followed by a water rinse, application of flux and then submersion in the molten zinc.

**Hydrochloric Acid Dip** See "Pickling."

**Hydrogen Embrittlement (HE)** A phenomenon where atomic hydrogen migrates and accumulates in steel, causing weakness in the crystalline lattice and often observed by separation at the grain boundaries. This weakening of the steel is known as "hydrogen embrittlement."

**Kilopounds per square inch (ksi)** A unit of stress resulting from a force of one kilopound-force applied to an area of one square inch.

**Magnetic Particle Testing (MT)** A non-destructive method for detecting cracks and other discontinuities at or near the surface in ferromagnetic materials, such as iron, nickel, cobalt and some of their alloys. Magnetic particle testing may be applied to raw material, semi-finished material, finished material and welds, regardless of heat treatment or lack thereof.

**Martensite** The metallurgical structure of iron alloys that forms if the material cools quickly from a high temperature. Generally this material is hard and brittle until tempered.

**Mechanical Galvanizing** A room temperature process in which zinc coatings are applied to rods without electricity (which is used for electroplating) and without heat (which is used for hot-dip galvanizing). The process of mechanical galvanizing is similar to hot-dip galvanizing, in that a steel piece is cleaned and rinsed. The piece is then tumbled in a mixture of various-sized glass beads and a predetermined amount of water, with small amounts of chemicals and powdered zinc added periodically. Collisions between the glass beads, zinc and the piece cause a cold-welding process that applies the zinc coating. Powdered zinc is added until the specified thickness is attained. The room temperature process ensures no chance of re-tempering or softening high-strength pieces against hydrogen embrittlement, because the steel pieces are also never exposed to acid pickling in the process.

**Mechanical Grit (or Abrasive) Blasting** An operation of forcibly propelling a stream of abrasive material against a surface under high pressure to smooth a rough surface, roughen a smooth surface, shape a surface or remove surface contaminants.

**Mid-radius** A point at half the distance from the center of a circle to the perimeter (e.g., the radius divided by 2)

**Morphology** The characteristics of a fractured surface (e.g., intergranular, transgranular, cleavage)

**Non-Conformance Report (NCR)** A report outlining a deviation from product, process, procedure or compliance specifications.

**Non-Destructive Evaluation (NDE)** Also referred to as non-destructive testing or non-destructive inspection, this evaluation does not damage the test object. Technologies for non-destructive evaluation include MT, x-ray and ultrasound, which may be used to detect such defects as cracking and corrosion.

**Notice of Proposed Resolutions (NPR)** A report prepared in response to a non-conformance report (NCR) that outlines disposition and corrective action to bring the condition back into conformance.

**Orthotropic Box Girder (OBG)** A structural steel box that is stiffened either longitudinally or transversely, or in both directions, to allow the roadway to directly bear vehicular loads and to contribute to the bridge structure's overall load-bearing behavior.

**Pearlite** The metallurgical structure that forms together with ferrite when iron alloys are cooled slowly from a high temperatures.

**Pickling** A metal surface treatment used to remove surface impurities such as stains, inorganic contaminants, rust or scale from ferrous metals, copper and aluminum alloys. A solution called pickle liquor, which contains strong acids, is used to remove the surface impurities. It is commonly used to descale or clean steel in various steelmaking processes. The primary acid used is hydrochloric acid, thus pickling also is described as a hydrochloric acid dip.

**Pier** A vertical structure that supports the ends of a multi-span superstructure at a location between abutments.

**Pier E2** The first pier east of the main tower of the self-anchored suspension span, and where the twin steel orthotropic box girder roadways rest.

**Post-Tensioning** A method of stressing concrete using steel rods or cables that are stretched after the concrete has hardened. This stretching of the rods or cables puts the concrete in compression, with the compressive stresses designed to counteract the tensile (tension) forces on the concrete once it is under load.

**Self-Anchored Suspension (SAS)** The SAS portion of the new East Span of the Bay Bridge connects the Yerba Buena Island Transition Structures with the Skyway. A single continuous cable is anchored within the eastern end of the roadway, carried over the tower, wrapped around the two side-by-side decks at the western end carried back over the tower and re-anchored at the eastern end of the roadway. The 2,047-foot-long SAS has a single 525-foot-tall steel tower, and is designed to withstand a massive earthquake.

**Shear** A force that causes parts of a material to slide past one another in opposite directions to cause separation.

**Shear Key** A shaped joint between two prefabricated elements that can resist shear through the geometric configuration of the joint.

**Skyway** The Skyway portion of the new East Span of the Bay Bridge is a 1.2-mile-long, elevated viaduct between the SAS and the Oakland Touchdown, with two parallel roadways that will accommodate five lanes of traffic plus two 10-foot-wide shoulders in each direction.

**Specifications** A document that explains material and construction requirements of the bridge structure.

**SSPC-SP 10** A standard established by the Society for Protective Coatings that covers the requirements for near-white blast cleaning of unpainted or painted steel surfaces by the use of abrasives. A near-white metal blast-cleaned surface, when viewed without magnification, shall be free of all visible oil, grease, dust, dirt, mill scale, rust, coating, oxides, corrosion products and other foreign matter, except for staining as noted. Random staining shall be limited to no more than 5 percent of each unit area of surface as defined, and may consist of light shadows, slight streaks or minor discolorations caused by stains of rust, stains of mill scale or stains of previously applied coating.

**Strain-age Embrittlement** A phenomenon where steel becomes very brittle in areas of high stress when exposed to elevated temperatures. At room temperature, strain-aging happens very slowly, but at elevated temperatures, like those used in the galvanizing process, strain-aging can happen very quickly. When the steel has incurred enough stress due to strain-aging, it can become embrittled. The most common type of embrittlement encountered in the hot-dip galvanizing process is strain-age embrittlement.

**Stress Corrosion Cracking (SCC)** A phenomenon that can occur in any highly stressed high-strength steel component. In the context of this report, the SC phenomenon may occur on galvanized high-strength steel rods in cases where the zinc layer is incomplete and a relatively small area of high-strength, highly stressed steel is exposed. The ratio of surface areas between the anode (zinc) and the cathode (exposed steel) may be such that a strong galvanic reaction occurs, with the separation of oxygen and hydrogen molecules in water and the migration of oxygen to form zinc oxide and the release of atomic hydrogen free to be absorbed into the metallurgical structure of the exposed steel.

**Susceptibility to Hydrogen Embrittlement** High-strength steels over 150 ksi possess a metallurgical structure that has an affinity for hydrogen, which is increased through the application of heat or when subjected to high levels of stress.

**Tensile Load** A force that attempts to pull apart or stretch an object.

**Tension** A force that stretches or pulls on a material.

**Tension Member** Any member of a truss that is subjected to tensile (tension) forces.

**Townsend Test** An accelerated test to determine the longer-term susceptibility of a material to stress corrosion cracking. The material being tested is soaked in a controlled, concentrated salt solution while tensioned progressively over a number of days until failure.

**Ultimate Tensile Strength** The maximum stress that a material can withstand while being stretched or pulled before failing or breaking. Tensile strength is the opposite of compressive strength and the values can be quite different.

**Vacuum Degassing** A process where molten metal (commonly steel) is placed in a vacuum in order to remove excess hydrogen or carbon. During the production process, a product's metal parts or components can become infused with excess amounts of these gases. As a result, unwanted imperfections and side effects can impact the integrity or performance of the metal. Vacuum degassing to remove carbon not only reduces imperfections, but brings a larger added benefit. By removing the carbon, the metals become more ductile, or easily shaped and formed through cold metalworking.

**Yerba Buena Island Transition Structures** The Yerba Buena Island Transition Structures connect the SAS to the Yerba Buena Island tunnel and provide the transition from the East Span's side-by-side traffic to the upper and lower decks of the tunnel and the West Span. The new structures are made of cast-in-place reinforced concrete, with 13 supports (footings and columns).

**Zinc Electroplating** A process by which electricity is used to provide a protective zinc coating to metallic substances, such as nuts, bolts, fasteners, automotive parts and many other hardware items. Zinc electroplating is a common and cost-effective way to protect against the effects of corrosion. Using the electroplating process changes the chemical and physical properties of a metal.

# 11. List of Key Agencies and Organizations Involved

**California Department of Transportation (Caltrans)** Created in 1895, Caltrans is the owner and operator of more than 50,000 miles of California’s highway and freeway lanes, including the Bay Area’s seven state-owned toll bridges.

**California Transportation Commission (CTC)** Established in 1978 by Assembly Bill 402, the CTC replaced and assumed the responsibilities of four independent bodies the California Highway Commission, the State Transportation Board, the State Aeronautics Board and the California Toll Bridge Authority. The CTC is responsible for the programming and allocating of funds for the construction of highway, passenger rail and transit improvements throughout California.

**Bay Area Toll Authority (BATA)** Created by the California Legislature in 1997, BATA administers the base \$1 auto toll on the San Francisco Bay Area’s seven state-owned toll bridges. In January 1998, BATA began operations under the Metropolitan Transportation Commission. In August 2005, the California Legislature expanded BATA’s responsibilities to include administration of all toll revenue and joint oversight of the toll bridge construction program with Caltrans and the CTC.

**Toll Bridge Program Oversight Committee (TBPOC)** Assembly Bill 144 established the TBPOC to be accountable for delivering the Seismic Retrofit Program. Members of the TBPOC are:

- **Steve Heminger**, Executive Director, BATA (Chair)
- **Andre Boutros**, Executive Director, CTC
- **Malcolm Dougherty**, Director, Caltrans

**Project Management Team (PMT)** The PMT is responsible for reporting to the TBPOC. Members of the PMT are:

- **Tony Anziano**, Toll Bridge Program Manager, Caltrans
- **Andrew B. Fremier**, Deputy Executive Director, BATA
- **Stephen Maller**, Deputy Executive Director, CTC

**Caltrans Seismic Safety Peer Review Panel** This Panel provides guidance and technical expertise related to complex structure projects with major seismic design exceptions and issues. Members of this Panel are:

- **John Fisher**, Emeritus Professor of Civil Engineering, Lehigh University
- **I.M. Idriss**, Emeritus Professor of Civil Engineering, University of California at Davis
- **Frieder Seible**, Vice Chair of the Caltrans Seismic Advisory Board and Dean Emeritus, University of California at San Diego

**Federal Highway Administration (FHWA) Review Panel** This Review Panel was responsible for conducting an independent review of the findings and recommendations contained in this report. Members of the Review Panel were:

- **Joey Hartmann**, Team Leader, Office of Bridge Technology, FHWA Headquarters
- **Greg Kolle**, Structures Engineer, FHWA California Division Office
- **Myint Lwin**, Director, Office of Bridge Technology, FHWA Headquarters
- **Justin Ocel**, Research Structural Engineer, FHWA Highway Research Center
- **Waider Wong**, Senior Structural Engineer, Resource Center, FHWA Headquarters

**Metallurgical Investigative Team** In May 2013, a metallurgical investigative team was tasked with examining the cause of the failure of the A354 grade BD high-strength steel rods manufactured in 2008. Members of this team were:

- **Rosme Aguilar**, Chief of Structural Materials Testing Branch, Caltrans
- **Salim Brahim**, President, IBECA Technologies and Consultant to American Bridge/Fluor Joint Venture
- **Conrad Christensen**, Principal/Founder, Christensen Materials Engineering

## 12. List of Key Contractors and Consultants Involved

**Alta Vista Solutions** Provider of structural material source inspections and quality assurance services for the East Span Replacement Project.

**American Bridge/Fluor Joint Venture** Contractor for SAS Bridge contract of the East Span Replacement Project.

**Bay Area Management Consultants (BAMC)** Joint venture of Hatch Mott MacDonald and URS; contractor to BATA to augment staff as necessary to assist in performing their responsibilities and provide technical expertise.

**Dyson Corporation** Fabricator for the high-strength A354 grade BD rods under the SAS Bridge Contract of the East Span Replacement Project.

**Kiewit/FCI/Manson (KFM)** Joint venture and contractor for the E2/T1 Marine Foundation Contract of the East Span Replacement Project.

**MacTec Engineering and Consulting** Provider of structural material source inspections and quality assurance services for the East Span Replacement Project.

**Moffatt & Nichol** Designer of Record for the new East Span Replacement Project and part of the Design Joint Venture with T.Y. Lin International.

**T.Y. Lin International** Designer of Record for the new East Span Replacement Project and part of the Design Joint Venture with Moffatt & Nichol.

**Vulcan Threaded Products** Fabricator for the tower rods for E2/T1 Marine Foundation Contract of the East Span Replacement Project.

# 13. List of Technical Appendices

## Appendix A: BATA Meetings

- A.1** 3/27/2013 BATA meeting materials
- A.2** 4/10/2013 BATA Oversight Committee meeting materials
- A.3** 4/24/2013 BATA meeting materials
- A.4** 5/8/2013 BATA Oversight Committee meeting materials
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- B.1** 4/17/2013 TBPOC Workshop materials
- B.2** 5/01/2013 TBPOC Workshop materials
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## Appendix C: Other Meetings

- C.1** 4/14/2013 Senate Transportation and Housing Committee Informational Hearing materials
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- D.1** 3/29/2013 Caltrans Letter “Bay Bridge E2 Connector Rods”
- D.2** TBPOC and FHWA  
5/08/2013 Letter from TBPOC to FHWA

- D.3** Caltrans and Senate Committee on Transportation and Housing:
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- D.4** TBPOC and State Senate:
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- June 2013 Letter from TBPOC to Senate Committee on Transportation and Housing
- D.5** California Legislature and TBPOC:
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- E.1** Item 1- "E2 Shear Key Anchor Rods (2008) — 96 Rods Fabrication and Installation Processes"
- E.2** Item 2- "E2 Bearing & Shear Key Anchor Rods (2010) - 192 Rods Fabrication and Installation Processes"
- E.3** Item 3 and 4- "3&4 E2 Shear Key & Bearing Anchor Rods (Top) (2009-2010)-320 Shear Key Rods 224 Bearing Rods Fabrication Processes"
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- E.7** Item 8- “8 Tower Saddle Tie Rods (2010) - 25 Rods Fabrication Process”
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- E.13** Item 15- “15 East Saddle Tie Rods (2010) - 18 Rods Fabrication Process”
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## Appendix F: Design Criteria, Special Provisions, and Bridge Design Specifications

- F.1** Self-Anchored Suspension Bridge Design Criteria
- F.2** Contract 04-0120F4 Special Provisions and Addenda (SAS Superstructure First Advertisement)
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- G.4** A490 - 12 Structural Bolts, Alloy Steel, Heat Treated, 150 ksi Minimum Tensile Strength
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- G.6** F1470 - 12 Fastener Sampling for Specified Mechanical Properties and Performance Inspection

## Appendix H: Other Documents

- H.1** Caltrans (5/21/2013) - "Summary Timeline of Decision to Follow National Standards For Bolts Set by American Society for Testing and Materials"
- H.2** Caltrans (12/28/2007) - Construction Policy Bulletin "CPB 07-7 Release Procedures for Materials Requiring Fit-for-Purpose Decisions"
- H.3** Caltrans (5/30/2013) - "Comparison Graphs (Field Hardness vs. Lab Hardness)"
- H.4** TY Lin (5/17/2013) - "ASTM A354BD BOLTS Testing Program"
- H.5** ABF (3/11/2013) - "E2 Bearing and Shear Key Erection Plan: Anchor Rod Stressing Anchor Rod Failure Map"
- H.6** Dyson (April 2013) - "Customer Presentation"
- H.7** BAMC (5/10/2013) - SAS Project NCR List Related to Dyson
- H.8** CCO# 91 (10/07/2008 Contract Change Order Memorandum, 10/07/2008 Contract Change Order, 10/31/2008 Authority To Proceed- CCO #91 -Additional Magnetic Particle Testing of Anchor Rods/Bolts, 11/03/2008 Letter to Caltrans: Authority To Proceed CCO #91 Additional Magnetic Particle Testing of Anchor Rods/Bolts Confirmation of Scope Regarding Existing E2 Shear Key Rods, 05/22/2009 RFI 1741R01 CCO 91 Clarification)

- H.9** Caltrans (5/11/2013) - "Background on E2-T1 and SAS A354BD Anchor Rods"
- H.10** Tennessee Galvanizing (5/29/2013) - Letter on Galvanizing for Vulcan Threaded Products
- H.11** Page from Self-Anchored Suspension Bridge Design Criteria (specifying Caltrans Bridge Design Specifications Manual)
- H.12** Sole-source Documents (Various)
- H.13** "Metallurgical Analysis of Bay Bridge Broken Anchor Rods S1-G1 & S2-A6," May 7, 2013



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**BUY AMERICA UPDATE**

A VERBAL PRESENTATION ON THIS ITEM  
WILL BE MADE AT THE  
CALIFORNIA TRANSPORTATION COMMISSION MEETING

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5f.  
Information Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **INFORMATIONAL REPORTS – DELEGATED ALLOCATIONS**  
**EMERGENCY G-11, SHOPP G-03-10 SAFETY, AND MINOR G-05-05**

## **SUMMARY:**

Since the period reported at the last California Transportation Commission (Commission) meeting, the California Department of Transportation (Department) allocated or sub-allocated:

- \$3,200,000 for three emergency construction projects, pursuant to the authority granted under Resolution G-11 (2.5f.(1)).
- \$34,422,000 for eight safety projects, pursuant to the authority granted under Resolution G-03-10(2.5f.(3)).

As of July 2, 2013, the Department has allocated or sub-allocated the following for Fiscal Year (FY) 2012-13:

- \$70,365,000 for 57 emergency construction projects.
- \$60,490,000 for 35 safety delegated projects.
- \$26,413,000 for 38 SHOPP Minor A projects.

As of July 10, 2013, the Department has allocated or sub-allocated the following for Fiscal Year (FY) 2013-14:

- \$25,531,000 for three safety delegated projects.

## **BACKGROUND:**

The Commission, by Resolution G-11, as amended by Resolution G-00-11, delegated to the Department authority to allocate funds to correct certain situations caused by floods, slides, earthquakes, material failures, slip outs, unusual accidents or other similar events.

This authority is operative whenever such an event:

1. Places people or property in jeopardy.
2. Causes or threatens to cause closure of transportation access necessary for:
  - a. Emergency assistance efforts.
  - b. The effective functioning of an area's services, commerce, manufacture or

- agriculture.
- c. Persons in the area to reach their homes or employment.
3. Causes either an excessive increase in transportation congestion or delay, or an excessive increase in the necessary distances traveled.

Resolution G-11 authorizes the Department to allocate funds for follow-up restoration projects associated with, and that immediately follow an emergency condition response project. Resolution G-11 also requires the Department to notify the Commission, at their next meeting, whenever such an emergency allocation has been made.

On March 30, 1994, the Commission delegated to the Department authority to allocate funds under Resolution G-11, as amended by Resolution G-00-11, for seismic retrofit projects. This authority allows the Department to begin work without waiting for the next Commission meeting to receive an allocation.

On March 28, 2001, the Commission approved Resolution G-01-10, as amended by Resolution G-03-10, delegating to the Department authority to allocate funds for SHOPP safety and pavement rehabilitation projects. This authority allows the Department to begin work without waiting for the next Commission meeting to receive an allocation.

Resolution G-05-05 authorizes the Department to sub-allocate funds for Minor projects. At the June 2011 meeting, the funding and project listing for the FY 2011-12 Lump Sum Minor Construction Program was approved by the Commission under Resolution FM-10-05.

The SHOPP, as approved by the Commission, is a four-year program of projects with the total annual proposed expenditures limited to the biennial Commission-approved Fund Estimate. The Commission, subject to monthly reporting and briefings, has delegated to the Department the authority to amend programmed projects, the authority to allocate funds for safety projects, and the authority to allocate funds to emergency projects. The Department uses prudent business practices to manage the combination of individual project cost increases and savings to meet Commission policies.

In all cases, the delegated authority allows the Department to begin work without waiting for the next Commission meeting to receive an allocation.

The Department has complied with the National Environmental Policy Act and the California Environmental Quality Act requirements in preparing these projects.

Attachment

2.5 Highway Financial Matters

Project# Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv. Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5f. Informational Report – Emergency G-11 Allocations (2.5f.(1))</b>				
1 \$300,000 Yolo 03-Yol-50 2.6/3.2	In West Sacramento, at the Sacramento River Viaduct (Bridge No. 24-0004R). Inspection of the structure detected potential defects in the pin and hanger assemblies in two locations at Span 12 and Span 16. This project is necessary to install temporary shoring to stabilize the bridge at the two locations. A follow-up project will be requested to make permanent repairs.  Initial G-11 Allocation 06/11/13: \$300,000	03-8783 SHOPP/12-13 0313000296 4 3F9704  Emergency	2011-12 302-0042 SHA 20.20.201.130	\$300,000
2 \$400,000 Los Angeles 07-LA-1 8.3	In Long Beach near the Hobson Avenue Overhead. On May 5, 2013, a sinkhole was discovered in the Number 1 lane of the Pacific Coast Highway (PCH) prompting the closure of the lane for further investigation and repair. An opening in the asphalt concrete was found to be the cause of soil erosion and the massive void under the pavement. This project is to provide traffic control as necessary, and excavate, shore, refill, compact, and re-pave the sinkhole area.  Initial G-11 Allocation 05/10/13: \$400,000	07-4641 SHOPP/12-13 0713000420 4 4X0704  Emergency	2011-12 302-0042 SHA 20.20.201.130	\$400,000
3 \$2,500,000 Ventura 07-Ven-1 2.6/10.2	Near Camarillo, from Deer Creek Road to Las Posas Road. The Camarillo Springs wildfire started on May 2, 2013. The wildfire burned and damaged vegetation, roadway signs and highway fencing. This project is to place guardrail to protect the roadway from post-fire falling rocks and debris flows, protect drainage system, replace damaged roadway signs, replace damaged highway fencing, and repair wire mesh and cable anchored covered hillside.  Initial G-11 Allocation 05/21/13: \$2,500,000	07-4640 SHOPP/12-13 0713000419 4 4X0604  Emergency	2011-12 302-0042 SHA 20.20.201.130	\$2,500,000

Project # Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Prgm'd Amount Project ID Adv. Phase EA	Budget Year Item # Fund Type Program Codes	Amount by Fund Type
<b>Informational Report – SHOPP Safety-Resolution G-03-10 Delegated Allocations (2.5f.(3))</b>				
1 \$910,000 Humboldt 01-Hum-101 86.3/87.9	Near Arcata, from the 11 <sup>th</sup> Street Overcrossing to the Arcata Overhead. <u>Outcome/Output:</u> Install cable median barrier to reduce the frequency of cross median collisions within the project limits and improve safety.  Allocation date: 05/29/2013	01-2330 SHOPP/12-13 \$980,000 0112000009 4 0B1004	2011-12 302-0890 FTF 20.20.201.010	\$910,000
2 \$813,000 El Dorado 03-ED-193 18.6/18.8	Near Georgetown, at Chicken Flat Road. <u>Outcome/Output:</u> Realign compound curve, increase roadway super-elevation, construct paved shoulders and clear vegetation to reduce run-off the road collisions.  Allocation date: 06/12/2013	03-3626 SHOPP/12-13 \$1,625,000 0300001113 4 1F3304	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	\$16,000 \$797,000
3 \$6,145,000 Santa Clara 04-SCI-9 2.5/7.0	Near Saratoga, from 2.5 miles north of Route 35 to 6 <sup>th</sup> Street. <u>Outcome/Output:</u> Upgrade lanes and shoulders, improve superelevation to improve roadway geometrics, increase sight distance and increase clear recovery zone to reduce the number and severity of cross centerline collisions.  Allocation date: 07/10/2013	04-0385F SHOPP/12-13 \$8,746,000 0400000822 4 2A4304	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	\$123,000 \$6,022,000

2.5 Highway Financial Matters

Project # Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Prgm'd Amount Project ID Adv. Phase EA	Budget Year Item # Fund Type Program Codes	Amount by Fund Type
<b>Informational Report – SHOPP Safety-Resolution G-03-10 Delegated Allocations (2.5f.(3))</b>				
4 \$18,394,000 Santa Clara 04-SCI-152 0.0/5.2	In Santa Clara, from Hecker pass to Uvas Creek. <u>Outcome/Output:</u> Upgrade lanes and shoulders, overlay pavement, remove trees, construct retaining walls, and add left-turn lane to reduce the number and severity of cross centerline collisions.  Allocation date: 07/10/2013	04-0483J SHOPP/12-13 \$24,826,000 0400000813 4 2A2504	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	4 \$18,394,000 Santa Clara 04-SCI-152 0.0/5.2
5 \$3,584,000 Santa Barbara 05-SB-154 R7.8/R8.3	Near Santa Ynez, at the intersection with State Route 246. <u>Outcome/Output:</u> Construct rural roundabout to reduce the frequency and severity of traffic collisions.  Allocation date: 06/13/2013	05-2267 SHOPP/12-13 \$3,421,000 0500000471 4 0T0004	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	5 \$3,584,000 Santa Barbara 05-SB-154 R7.8/R8.3
6 \$992,000 Los Angeles 07-LA-105 R0.5	In the city of Los Angeles, near LAX and El Segundo, on the eastbound on-ramp from southbound Route 1. <u>Outcome/Output:</u> Safety improvements to address the number and severity of wet pavement collisions.  Allocation date: 07/10/2013	07-4508 SHOPP/12-13 \$985,000 0712000121 4 4T5704	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.010	6 \$992,000 Los Angeles 07-LA-105 R0.5
7 \$620,000 San Bernardino 08-SBd-38 5.2/5.5	Near Redlands and Mentone, from 0.2 mile east of Amethyst Street to Mountain View Lane. <u>Outcome/Output:</u> Improve safety by constructing left-turn lane to address the number and severity of collisions  Allocation date: 06/21/2013	08-0204V SHOPP/12-13 \$765,000 0800000481 4 0M4504	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	7 \$620,000 San Bernardino 08-SBd-38 5.2/5.5
8 \$2,964,000 San Joaquin 10-SJ-26 18.5/19.0	Near Linden, from west of Sandstone Creek Bridge to Shelly Road. <u>Outcome/Output:</u> Realign two horizontal roadway curves and replace the Sandstone Creek bridge to reduce the frequency and severity of traffic collisions.  Allocation date: 05/22/2013	10-0264 SHOPP/12-13 \$2,789,000 1000000271 4 0T1604	2011-12 302-0042 SHA 302-0890 FTF 20.20.201.010	8 \$2,964,000 San Joaquin 10-SJ-26 18.5/19.0

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 3.1  
Information Item

From: STEVEN KECK  
Acting, Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **MONTHLY REPORT ON PROJECTS AMENDED INTO THE SHOPP BY  
DEPARTMENT ACTION**

## **SUMMARY:**

Since the June 2013 report to the California Transportation Commission (Commission), the California Department of Transportation (Department) has amended 26 new capital projects into the 2012 State Highway Operation and Protection Program (SHOPP), as summarized in the attachment. The Department maintains annual reservations to fund anticipated safety, emergency, and other high priority projects that need to be amended into the 2012 SHOPP. The amendments noted below will be funded from the Major Damage Restoration, Safety Improvements and Bridge Reservations.

2012 SHOPP Summary of New Projects by Category	No.	FY 2012/13 (\$1,000)	FY 2013/14 (\$1,000)	FY 2014/15 (\$1,000)	FY 2015/16 (\$1,000)
Major Damage Restoration	11	\$4,660	\$98,308		\$1,016
Collision Reduction	13		485	\$10,467	\$14,489
Bridge Preservation	2			\$4,406	\$2,909
Total Amendments	26	\$4,660	\$98,793	\$14,873	\$18,414

## **BACKGROUND:**

In each even numbered year, the Department prepares a four-year SHOPP defining major capital improvements necessary to preserve and protect the State Highway System. Periodically, the Department amends the SHOPP to address newly identified needs prior to the next programming cycle. This report identifies 26 capital projects amended into the 2012 SHOPP.

The "List of New 2012 SHOPP Capital Project Amendments" provides specific project information.

Attachment

**List of New 2012 SHOPP Capital Project Amendments**

This list provides an overview of projects the Department has amended into the 2012 SHOPP since the June 2013 report. Copies of the actual amendments have been provided to Commission staff.

Amend # PPNO	Dist-Co-Rte PM EA Project ID	Project Location and Description of Work	R/W Cost Const. Cost (\$1,000)	FY	Support Costs (\$1,000)	Program Code Leg. /Congr. Dists. Perf. Meas.	
Major Damage Restoration							
12H-232  4641	7-LA-1 8.3  4X070 07 1300 0420	In Long Beach, near Hobson Avenue. Repair roadway pavement.	\$400 (C)	12/13	PA & ED PS & E R/W Sup Con Sup <u>Total</u>	\$0 \$0 \$0 \$80 \$80	201.130 Assembly: 55 Senate: 28 Congress: 37 1 Location
12H-233  4640	7-Ven-1 2.6/10.2  4X060 07 1300 0419	Near Camarillo, from Deer Creek Road to Las Posas Road. Repair fire damage.	\$2,500 (C)	12/13	PA & ED PS & E R/W Sup Con Sup <u>Total</u>	\$0 \$0 \$0 \$370 \$370	201.130 Assembly : 37 Senate: 19 Congress: 23 1 Location
12H-234  2381	1-Hum-254 4.4/11.2  47531 01 1300 0112	Near Garberville, from 1.2 miles north of State Park Road to 0.4 mile north of Bridge Creek. Required mitigation for EA 01-47502 to reconstruct and restore culverts.	\$50 (R/W) \$966 (C)	15/16	PA & ED PS & E R/W Sup Con Sup <u>Total</u>	\$208 \$238 \$89 \$150 \$685	201.131 Assembly: 2 Senate: 2 Congress: 1 4 Locations
12H-237  6665	6-Ker-var var  0Q580 06 1300 0305	In Kern County, on Routes 5, 58, 99, and 204 at various locations. Repair traffic operation systems.	\$60 (R/W) \$1,400 (C)	12/13	PA & ED PS & E R/W Sup Con Sup <u>Total</u>	\$100 \$500 \$40 \$800 \$1,440	201.131 Assembly: 30, 34 Senate: 16, 18 Congress: 22 605 Locations
12H-241  8783	3-Yol-50 2.6/3.2  3F970 03 1300 0296	West Sacramento, at the Sacramento River Viaduct Bridge No. 24-0004R. Install bridge shoring.	\$300 (C)	12/13	PA & ED PS & E R/W Sup Con Sup <u>Total</u>	\$10 \$10 \$10 \$50 \$80	201.130 Assembly: 8 Senate: 4 Congress: 3 1 Location
12H-245  4572	7-LA-5 Var  3X920 07 1300 0327	In various cities, on Routes 5, 47 and 101 at various locations. Copper wire theft damage restoration and upgrade with theft deterrent features.	\$25 (R/W) \$15,725 (C)	13/14	PA & ED PS & E R/W Sup Con Sup <u>Total</u>	\$10 \$3,911 \$10 \$3,931 \$7,862	201.131 Assembly: 38, 39, 45, 46, 50, 70 Senate: 18, 21, 24, 26, 27, 35 Congress: 25, 28, 29, 30, 33, 34, 44 23 Locations

Amend # PPNO	Dist-Co-Rte PM EA Project ID	Project Location and Description of Work	R/W Cost Const. Cost (\$1,000)	FY	Support Costs (\$1,000)	Program Code Leg. /Congr. Dist. Perf. Meas.
Major Damage Restoration (continued)						
12H-246  4574	7-LA-5 Var  3X940 07 1300 0331	In various cities, at various locations. Copper wire theft damage restoration and upgrade with theft deterrent features.	\$25 (R/W) \$4,650 (C)	13/14	PA & ED PS & E R/W Sup Con Sup Total \$10 \$1,143 \$10 \$1,162 \$2,325	201.131 Assembly: 36-39, 41, 43-46, 48-50, 52, 54, 55, 57-59, 62-64, 66, 70 Senate: 18-22, 24- 27, 29, 30, 32, 33, 35 Congress: 24-30, 32-35, 37-40, 43, 44, 47 40 Locations
12H-247  4571	7-LA-10 Var  3X910 07 1300 0324	In various cities, on Routes 10, 90 and 110 at various locations. Copper wire theft damage restoration and upgrade with theft deterrent features.	\$25 (R/W) \$20,736 (C)	13/14	PA & ED PS & E R/W Sup Con Sup Total \$10 \$5,164 \$10 \$5,184 \$10,368	201.131 Assembly: 50, 53, 54, 59, 62, 64 Senate: 22, 24, 25, 26, 30, 35 Congress: 27, 32, 34, 37, 43, 44 25 Locations
12H-248  4573	7-LA-118 Var  3X930 07 1300 0329	In various cities, on Routes 118, 170 and 405 at various locations. Copper wire theft damage restoration and upgrade with theft deterrent features.	\$25 (R/W) \$20,097 (C)	13/14	PA & ED PS & E R/W Sup Con Sup Total \$10 \$5,004 \$10 \$5,024 \$10,048	201.131 Assembly: 38, 39, 46, 50, 54, 62 Senate: 18, 26, 27, 30, 33, 35 Congress: 26, 29, 30, 33, 37, 43 25 Locations
12H-249  4575	7-LA-Var Var  3X950 07 1300 0333	On various routes at various locations. Repair, replace, and restore electrical equipment for highway facilities damaged by copper wire theft.	\$25,000 (C)	13/14	PA & ED PS & E R/W Sup Con Sup Total \$0 \$0 \$0 \$3,750 \$3,750	201.131 Assembly: 36, 38, 39, 41, 43, 45, 46, 48-50, 52, 54, 55, 57-59, 62-64, 66, 70 Senate: 18, 20-22, 24-27, 29, 30, 32, 33, 35 Congress: 25, 27- 30, 32-35, 37-40, 43, 44, 47 88 Locations
12H-250  4576	7-Ven-Var Var  3X960 07 1300 0335	On various routes at various locations. Repair, replace, and restore electrical equipment for highway facilities damaged by copper wire theft.	\$12,000 (C)	13/14	PA & ED PS & E R/W Sup Con Sup Total \$0 \$0 \$0 \$1,800 \$1,800	201.131 Assembly: 37, 44 Senate: 19, 27 Congress: 24, 26 27 Locations

Amend # PPNO	Dist-Co-Rte PM EA Project ID	Project Location and Description of Work	R/W Cost Const. Cost (\$1,000)	FY	Support Costs (\$1,000)	Program Code Leg./Congr. Dists. Perf. Meas.	
Collision Reduction							
12H-238  4620	7-LA-110 23.7/25.5  29770 07 1300 0224	In the city of Los Angeles, on northbound lanes from Route 101 to Route 5. Install safety lighting.	\$10 (R/W) \$1,420 (C)	15/16	PA & ED PS & E R/W Sup Con Sup <u>Total</u>	\$150 \$314 \$8 <u>\$405</u> \$877	201.010 Assembly: 51 Senate: 24 Congress: 34 122 Collisions reduced
12H-242  2380	5-Mon-1 72.9/74.8  1C300 05 1200 0113	Near Carmel, from Carmel Valley Road to north of Carpenter Street. Install rumble strips.	\$485 (C)	13/14	PA & ED PS & E R/W Sup Con Sup <u>Total</u>	\$110 \$321 \$3 <u>\$158</u> \$592	201.010 Assembly: 29 Senate: 17 Congress: 20 24 Collisions reduced
12H-243  6657	6-Fre-168 R6.9/R7.1  0P980 06 1200 0328	In Clovis, from Herndon Avenue to Clovis Avenue. Install concrete median barrier.	\$403 (C)	14/15	PA & ED PS & E R/W Sup Con Sup <u>Total</u>	\$103 \$370 \$5 <u>\$189</u> \$667	201.010 Assembly: 23 Senate: 8 Congress: 22 2 Collisions reduced
12H-244  6648	6-Ker-58 R94.5  0N560 06 0002 0488	In Tehachapi, at Tehachapi Summit Interchange. Widen intersection.	\$66 (R/W) \$714 (C)	14/15	PA & ED PS & E R/W Sup Con Sup <u>Total</u>	\$0 \$487 \$26 <u>\$256</u> \$769	201.010 Assembly: 34 Senate: 16 Congress: 23 20 Collisions reduced
12H-251  0226G	8-SBd-62 16.8/25.2  0R130 08 1200 0027	Near Joshua Tree, from west of Torres Avenue to east of Lee Drive. Provide continuous two-way left turn lane throughout corridor.	\$30 (R/W) \$4,300 (C)	15/16	PA & ED PS & E R/W Sup Con Sup <u>Total</u>	\$822 \$698 \$25 <u>\$748</u> \$2,293	201.010 Assembly: 34 Senate: 18 Congress: 41 40 Collisions reduced
12H-254  8141	3-Sut-20 16.5  3F790 03 1300 0189	In Yuba, at Plumas Street. Replace signal poles.	\$15 (R/W) \$682 (C)	14/15	PA & ED PS & E RW Sup Con Sup <u>Total</u>	\$125 \$160 \$10 <u>\$210</u> \$505	201.010 Assembly: 3 Senate: 4 Congress: 3 27 Collisions reduced
12H-255  0525A	4-SCI-82 14.8/18.1  2G540 04 0002 1004	In Sunnyvale, at Route 82 intersections with Henderson Avenue, South Wolfe Road, Maria Lane, Sunnyvale Saratoga Road/South Sunnyvale Avenue, South Mathilda Avenue, and South Bernardo Avenue. Upgrade drainage system, curb ramps and pedestrian passageways.	\$291 (R/W) \$6,133 (C)	14/15	PA & ED PS & E RW Sup Con Sup <u>Total</u>	\$0 \$900 \$54 <u>\$756</u> \$1,710	201.010 Assembly: 22 Senate: 13 Congress: 14 292 Collisions reduced

Amend # PPNO	Dist-Co-Rte PM EA Project ID	Project Location and Description of Work	R/W Cost Const. Cost (\$1,000)	FY	Support Costs (\$1,000)		Program Code Leg. /Congr. Dist. Perf. Meas.
12H-256  2205A	4-SCI-87 9.0/9.2  4G610 04 1200 0549	In San Jose, south of Route 101 and north of Airport Parkway undercrossing. Replace K-Rail barrier with metal beam guard rail and concrete barrier.	\$10 (R/W) \$515 (C)	14/15	PA & ED PS & E R/W Sup Con Sup Total	\$0 \$135 \$18 \$90 \$243	201.010 Assembly: 25 Senate: 10 Congress: 19 1 Collision reduced
12H-257  6636	6-Ker-155 1.5  0P290 06 1200 0106	Near Delano, at Browning Road. Construct a roundabout.	\$1,010 (R/W) \$2,540 (C)	15/16	PA & ED PS & E R/W Sup Con Sup Total	\$391 \$722 \$432 \$422 \$1,967	201.010 Assembly: 32 Senate: 14 Congress: 21 28 Collisions reduced
12H-258  3017	10-Ama-Var var  0X350 10 1200 0217	In Amador and Tuolumne counties, at various locations. Construct shoulder and centerline rumble strips.	\$603 (C)	14/15	PA & ED PS & E R/W Sup Con Sup Total	\$82 \$253 \$11 \$98 \$444	201.010 Assembly: 4, 10, 25 Senate: 1, 14 Congress: 3, 4, 19 10 Collisions reduced
12H-259  3019	10-Mer-165 28.1/32.4  0X320 10 1200 0212	Also in Stanislaus County on Route 108 (PM 33.1/36.14). Construct shoulder and centerline rumble strips.	\$1,035 (C)	14/15	PA & ED PS & E R/W Sup Con Sup Total	\$164 \$322 \$2 \$184 \$672	201.010 Assembly: 25, 26 Senate: 12, 14 Congress: 18 13 Collisions reduced
12H-260  3015	10-SJ-5 41.7/45.9  0X540 10 1200 0303	Near Thornton, from Turner Road to Beaver Slough Bridge. Construct double thrie beam median barrier.	\$2,992 (C)	15/16	PA & ED PS & E R/W Sup Con Sup Total	\$308 \$654 \$4 \$520 \$1,486	201.010 Assembly: 17 Senate: 5 Congress: 11 42 Collisions reduced
12H-261  3011	10-Sta-99 M18.7  0X560 10 1200 0313	In Modesto, at the southbound off-ramp at Carpenter/Briggsmore Avenue. Reconstruct off-ramp.	\$3 (R/W) \$2,184 (C)	15/16	PA & ED PS & E R/W Sup Con Sup Total	\$155 \$669 \$1 \$615 \$1,440	201.010 Assembly: 26 Senate: 12 Congress: 18 89 Collisions reduced
<b>Bridge Preservation</b>							
12H-235  4573	1-Men-101 89.2  0C370 01 1200 0283	Near Leggett, at Cedar Creek Arch Culvert. Repair arched culvert bridge.	\$37 (R/W) \$2,872 (C)	15/16	PA & ED PS & E R/W Sup Con Sup Total	\$492 \$544 \$86 \$1,126 \$2,248	201.119 Assembly: 2 Senate: 2 Congress: 1 1 Bridge
12H-236  3517	2-Sha-5 Var  4F610 02 1300 0013	In Shasta and Siskiyou counties at various locations. Place polyester concrete overlay and repair joint seal.	\$6 (R/W) \$4,400 (C)	14/15	PA & ED PS & E R/W Sup Con Sup Total	\$300 \$430 \$30 \$890 \$1,650	201.119 Assembly: 2 Senate: 4 Congress: 2 16 Bridges

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 3.2a.  
Information Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **STATUS OF CONSTRUCTION CONTRACT AWARD FOR STATE HIGHWAY PROJECTS**

## **SUMMARY:**

The California Department of Transportation (Department) is presenting this item to provide the status of construction contract award for projects on the State Highway System allocated in Fiscal Year (FY) 2011-12 and FY 2012-13.

In FY 2011-12, the California Transportation Commission (Commission) voted 354 state-administered State Transportation Improvement Program (STIP), State Highway Operation and Protection Program (SHOPP), and Proposition 1B projects on the State Highway System. As of July 11, 2013, 352 projects totaling \$4.9 billion have been awarded. Funding for two projects has lapsed. Given that all projects voted in FY 2011-12 have been either awarded or lapsed, this section will not be included in future reports.

In FY 2012-13, the Commission voted 282 state-administered STIP, SHOPP, and Proposition 1B projects on the State Highway System. As of July 11, 2013, 194 projects totaling \$734.6 million have been awarded.

## **BACKGROUND:**

Starting with July 2006 allocations, projects are subject to Resolution G-06-08 (adopted June 8, 2006), which formalizes the condition of allocation that requires projects to be ready to proceed to construction within six months of allocation. The policy also requires that projects that are not awarded within four months of allocation be reported to the Commission.

**FY 2011-12 Allocations**

Month Allocated	No. Projects Voted	Voted Projects \$ X 1000	No. Projects Awarded	No. Projects Funds Lapse	Awarded Projects \$ X 1000	No. Projects Pending Bid Opening/Award	No. Projects Awarded within 4 months	No. Projects Awarded within 6 months
August 2011	90	\$1,864,282	89	1	\$1,440,521	0	39	76
September 2011	18	\$76,605	18	0	\$68,050	0	7	12
October 2011	18	\$166,249	18	0	\$159,284	0	4	11
December 2011	21	\$264,582	21	0	\$170,911	0	15	15
January 2012	28	\$274,056	28	0	\$186,410	0	17	24
February 2012	9	\$155,085	8	1	\$108,819	0	3	4
March 2012	32	\$217,923	32	0	\$203,616	0	17	21
April 2012	27	\$659,617	27	0	\$563,354	0	12	21
May 2012	43	\$574,297	43	0	\$497,487	0	14	25
June 2012	68	\$685,977	68	0	\$650,530	0	26	52
<b>TOTAL</b>	<b>354</b>	<b>\$4,938,673</b>	<b>352</b>	<b>2</b>	<b>\$4,048,982</b>	<b>0</b>	<b>154</b>	<b>261</b>

- Note: 1. Total awarded amount reflects total project allotment, including G-12 and supplemental funds.  
2. Excludes non-construction Transportation Enhancement (TE) projects and combined locally-administered TE.  
3. FY 2011-12 table includes projects with financial contribution only, Department delegated safety, and emergency projects.

**FY 2012-13 Allocations**

Month Allocated	No. Projects Voted	Voted Projects \$ X 1000	No. Projects Awarded	No. Projects Funds Lapse	Awarded Projects \$ X 1000	No. Projects Pending Bid Opening/Award	No. Projects Awarded within 4 months	No. Projects Awarded within 6 months
August 2012	74	\$484,107	72	0	\$404,198	2	39	56
September 2012	15	\$88,281	14	0	\$70,659	1	7	13
October 2012	18	\$35,814	18	0	\$34,465	0	12	17
December 2012	26	\$133,477	25	0	\$94,967	1	18	24
January 2013	14	\$53,491	13	0	\$38,029	1	12	13
March 2013	40	\$120,390	36	0	\$69,532	4	33	36
May 2013	47	\$278,203	13	0	\$20,272	34	12	12
June 2013	48	\$632,722	3	0	\$2,565	45	2	2
<b>TOTAL</b>	<b>282</b>	<b>\$1,826,485</b>	<b>194</b>	<b>0</b>	<b>\$734,687</b>	<b>88</b>	<b>135</b>	<b>173</b>

- Note: 1. Total awarded amount reflects total project allotment, including G-12 and supplemental funds.  
2. Excludes non-construction Transportation Enhancement (TE) projects and combined locally-administered TE.  
3. FY 12-13 table includes projects with financial contribution only, Department delegated safety, and emergency projects.

Attachment

## FY 2012-13 Project Allocation Status

Dist	PPNO	EA	Co	Rte	Work Description	Allocation Date	Award Deadline	Allocation Amount	Project Status
04	0066	1G560	ALA	880	Upgrade median barrier.	22-Aug-12	30-Nov-13	\$1,174	Project will be re-advertised due to a conflict with a nearby project. A time extension for this project was approved on 3/5/13.
11	0840	29030	SD	5	Footing/Column Retrofit, Concrete Barrier, Planting and Irrigation.	22-Aug-12	31-Aug-13	\$2,641	Bids opened 6/13/13. Pending award.
07	0309S	1170U	LA	10	Construct soundwalls.	27-Sep-12	31-Dec-13	\$8,260	Project to be combined with EA 1170U. A time extension for this project was approved on 5/4/13.
07	0309N	1170U	LA	10	Construct HOV lanes, widen bridges, and realign ramps.	6-Dec-12	31-Dec-13	\$34,200	Project to be combined with EA 11172. A time extension for this project was approved on 6/11/13.
08	0176F	0G840	SBd	15	Upgrade Existing Roadside Rest Area.	8-Jan-13	31-Jul-13	\$11,273	Project will be re-scoped and re-advertised. Concurrent time extension is being requested.
02	3421	2E350	TRI	299	Curve Improvement with HMA over AB and Construct Retaining Walls.	5-Mar-13	30-Sep-13	\$2,330	Bids opened 4/30/13. Pending award.
06	6414	0E660	KER	99	Replace chain link railing, place slope paving and irrigation conduit.	5-Mar-13	30-Sep-13	\$1,065	Bids opened 6/26/13. Pending award.
10	7352	0G800	SJ	12	Pavement rehabilitation and bridge construction.	5-Mar-13	30-Sep-13	\$32,589	Project advertised on 4/29/13. Bid opening date 7/16/13.
10	0196	0S950	MER	99	Paint bridge and slope paving.	5-Mar-13	30-Sep-13	\$900	Bids opened 6/25/13. Pending award.
11	0836	29040	SD	94	Cold plane, place rumble strips, MBGR and curb ramps.	5-Mar-13	30-Sep-13	\$13,008	Bids opened 6/27/13. Pending award.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 3.2b.  
Information Item

From: STEVE KECK  
Acting Chief Financial Officer

Prepared by: Denix D. Anbiah  
Division Chief  
Local Assistance

Subject: **MONTHLY STATUS OF CONSTRUCTION CONTRACT AWARD FOR LOCAL ASSISTANCE STIP PROJECTS, PER RESOLUTION G-06-08**

## **SUMMARY:**

The California Department of Transportation (Department) is presenting this item for information purposes only. The item provides the status of locally-administered State Transportation Improvement Program (STIP) projects that received a construction allocation in Fiscal Year (FY) 2010-11, FY 2011-12 and FY 2012-13.

In FY 2010-11, the California Transportation Commission (Commission) allocated \$94,213,000 to construct 71 locally-administered STIP projects. As of July 1, 2013, 67 projects totaling \$93,205,000 have been awarded, and two projects have been approved for time extensions. Two projects (PPNO 01-4097P and PPNO 12-2135M) have lapsed.

In FY 2011-12, the Commission allocated \$105,182,000 to construct 77 locally-administered STIP projects. As of July 1, 2013, 71 projects totaling \$101,061,000 have been awarded, five projects have been approved for time extensions. One project (PPNO 06-B002P) has lapsed.

In FY 2012-13, the Commission allocated \$62,976,000 to construct 66 locally-administered STIP projects. As of July 1, 2013, 17 projects totaling \$15,174,000 have been awarded and four projects have been approved for time extensions.

## **BACKGROUND:**

Resolution G-06-08, adopted June 8, 2006, requires projects to be ready to proceed to construction within six months of allocation. The policy also requires Caltrans to report to the Commission on those projects that have not been awarded within four months of allocation.

**FY 2010-11 Allocations**

Month Allocated	No. Projects Voted	Voted Projects \$ X 1000	No. Projects Awarded	No. Projects Lapse	No. Projects Pending Award	No. Projects Awarded within 4 months	No. Projects Awarded within 6 months
July 2010	19	\$57,002	18	1	0	2	10
August 2010	0	\$0	0	0	0	0	0
September 2010	2	\$795	2	0	0	0	2
November 2010	3	\$3,284	3	0	0	0	2
January 2011	3	\$7,878	3	0	0	1	0
March 2011	11	\$4,960	10	1	0	1	7
May 2011	8	\$4,994	8	0	0	3	5
June 2011	25	\$15,300	23	0	2	1	16
<b>TOTAL</b>	<b>71</b>	<b>\$94,213</b>	<b>67</b>	<b>2</b>	<b>2</b>	<b>8</b>	<b>42</b>

**FY 2011-12 Allocations**

Month Allocated	No. Projects Voted	Voted Projects \$ X 1000	No. Projects Awarded	No. Projects Lapse	No. Projects Pending Award	No. Projects Awarded within 4 months	No. Projects Awarded within 6 months
August 2011	5	\$19,418	5	0	0	0	3
September 2011	2	\$1,007	2	0	0	0	2
October 2011	1	\$501	1	0	0	0	1
December 2011	7	\$4,666	6	1	0	0	5
January 2012	7	\$5,089	7	0	0	1	4
February 2012	7	\$13,614	7	0	0	2	4
March 2012	3	\$2,633	3	0	0	0	1
April 2012	8	\$4,644	7	0	1	2	1
May 2012	7	\$6,191	7	0	0	2	2
June 2012	30	\$47,419	26	0	4	4	11
<b>TOTAL</b>	<b>77</b>	<b>\$105,182</b>	<b>71</b>	<b>1</b>	<b>5</b>	<b>11</b>	<b>34</b>

**FY 2012-13 Allocations**

Month Allocated	No. Projects Voted	Voted Projects \$ X 1000	No. Projects Awarded	No. Projects Lapse	No. Projects Pending Award	No. Projects Awarded within 4 months	No. Projects Awarded within 6 months
August 2012	9	\$6,577	8	0	1	4	3
September 2012	3	\$3,198	3	0	0	0	2
October 2012	3	\$4,085	3	0	0	0	3
December 2012	4	\$878	2	0	2	2	0
January 2013	0	\$0	0	0	0	0	0
March 2013	6	\$4,654	1	0	5	0	0
May 2013	11	\$9,789	0	0	11	0	0
June 2013	30	\$33,795	0	0	30	0	0
<b>Total</b>	<b>66</b>	<b>\$62,976</b>	<b>17</b>	<b>0</b>	<b>49</b>	<b>6</b>	<b>8</b>

Note: Excludes STIP Planning, Programming, and Monitoring allocations and locally-administered STIP Regional Rideshare Program allocations, as no contract is awarded for these programs.

**Local STIP Projects, Beyond Four Months of Construction Allocation, Not Yet Awarded**

<b>Agency Name</b>	<b>Project Title</b>	<b>PPNO</b>	<b>Allocation Date</b>	<b>Award Deadline</b>		<b>Allocation Amount</b>	<b>Project Status</b>
City of Lindsay	Government Center Plaza	06-D022	23-Jun-11	31-Aug-13	(1)	\$199,000	The project will be awarded by the extended deadline.
City of Lindsay	Tulare Road Pedestrian Safety Bollards	06-6567	23-Jun-11	31-Aug-13	(1)	\$167,000	The project will be awarded by the extended deadline.
City of Mill Valley	Sycamore Avenue Improvement project	04-2127R	26-Apr-12	31-Oct-13	(2)	\$282,000	The project will be awarded by the extended deadline.
City of Hercules	Hercules Intercity Rail Station	04-2011F	28-Jun-12	30-Sep-13	(3)	\$1,097,000	The project will be awarded by the extended deadline.
City of Hercules	Sand Francisco Bay Trail - Bio-Rad Segment	04-2025E	28-Jun-12	30-Sep-13	(3)	\$862,000	The project will be awarded by the extended deadline.
City of Culver City	Pedestrian Improvement for Intersections with Bus.	07-4090	28-Jun-12	30-Sep-13	(3)	\$634,000	The project will be awarded by the extended deadline.
San Diego Association of Governments	Bayshore Bikeway Segments 4 & 5	11-7421S	28-Jun-12	31-Aug-14	(3)	\$995,000	The project will be awarded by the extended deadline.
City of Santa Ana	Flower Street Bike Trail Gap Closure	12-2136B	22-Aug-12	28-Aug-13	(4)	\$500,000	The project will be awarded by the extended deadline.
City of American Canyon	Napa Junction Elementary School Pedestrian Improvement.	04-2130G	6-Dec-12	30-Jun-13		\$197,000	A concurrent 3 month extension is being requested
City of Yountville	Bike Route, Sidewalk Extension, Enhancements	04-2130H	6-Dec-12	30-Jun-13		\$86,000	A concurrent 4 month extension is being requested
City of Concord	Monument Corridor Pedestrian and Bikeway Network Project	04-2119A	5-Mar-13	30-Sep-13		\$944,000	A concurrent 2 month extension is being requested
City of Rio Dell	Downtown Pedestrian and Streetscape Improvements	01-2260	5-Mar-13	30-Sep-13		\$538,000	The project will be awarded by the deadline
City of Yuba City	Butte House Road and Pease Road Class II Bicycle Lane Improvement project	03-8126	5-Mar-13	30-Sep-13		\$625,000	The project will be awarded by the deadline
City of San Luis Obispo	Bob Jones Bike/Pedestrian Pathway project	05-2372	5-Mar-13	30-Sep-13		\$600,000	The project will be awarded by the deadline
City of Lancaster	Downtown Lancaster Gateway & Roundabout	07-4318	5-Mar-13	30-Sep-13		\$728,000	The project will be awarded by the deadline
<b>Grand Total</b>						<b>\$8,454,000</b>	

(1) This extended deadline was approved in Jan 2012 (Waiver-12-02)

(2) This extended deadline was approved in Oct 2012 (Waiver-12-64)

(3) This extended deadline was approved in Dec 2012 (Waiver-12-68)

(4) This extended deadline was approved in Jan 2013 (Waiver-13-10)

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 3.4  
Information

From: ANDRE BOUTROS  
Executive Director

Subject: **REPORT ON LOCAL AGENCY NOTICES OF INTENT TO EXPEND FUNDS  
ON STIP PROJECTS PRIOR TO COMMISSION ALLOCATION, PER SB 184**

## **SUMMARY**

Senate Bill (SB) 184 (Chapter 462, Statutes of 2007) authorizes a regional or local agency, upon notifying the California Transportation Commission (Commission), to expend its own funds for a project in the State Transportation Improvement Program (STIP) to which the Commission has not yet made an allocation. This report includes a list of the STIP projects for which an SB 184 letter (and a copy of the allocation request) was submitted.

As reported at the June 2013 meeting, SB 184 notification letters had been received for 23 local STIP projects. Since then, eight additional notifications have been received, seven for planning, programming, and monitoring (PPM) projects and one for design on a local road project in Inyo County. The additional projects are highlighted in the attachment, indicating an effective date of July 1, 2013 for all eight new SB 184 notification letters.

## **BACKGROUND**

Government Code Section 14529.17, as amended by SB 184, permits an agency to expend its own funds for a STIP project, in advance of the Commission's approval of a project allocation, and to be reimbursed for the expenditures subsequent to the Commission's approval of the allocation.

Section 64A of the STIP guidelines directs the agency to submit a copy of the allocation request along with the SB 184 notification letter to the Executive Director of the Commission. The original allocation request should be submitted to Caltrans at the same time.

Expenditures are eligible from the date the Commission receives the notification letter.

Invoking SB 184 does not establish a priority for allocations made by the Commission nor does it establish a timeframe for when the allocations will be approved by the Commission.

The statute does not require that the Commission approve an allocation it would not otherwise approve. SB 184 advance expenditures must be eligible for reimbursement in accordance with state laws and procedures. In the event the advance expenditures are determined to be ineligible, the state has no obligation to reimburse those expenditures.

Attachment

## SB 184 Notifications for FY 2013-14 Local STIP Projects

Includes SB 184 Letters Received Through July 23, 2013.

County	Agency	Rte	PPNO	Project	Date Letter Effective	Meeting		FY 13-14	Project Totals by Component			
						Del.	Voted		R/W	Const	E & P	PS&E
Alameda	MTC		2100	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	118	0	118	0	0
Alameda	ACTC		2179	Planning, programming, and monitoring	01-Jul-13	Jun-13	Aug-13	750	0	750	0	0
Butte	BCAG		0L16	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	148	0	148	0	0
Contra Costa	MTC		2118	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	77	0	77	0	0
Contra Costa	CCTA		2011O	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	430	0	430	0	0
<b>Del Norte</b>	<b>Del Norte LTC</b>		<b>1032</b>	<b>Planning, programming, and monitoring</b>	<b>01-Jul-13</b>	<b>Aug-13</b>		<b>34</b>	<b>0</b>	<b>34</b>	<b>0</b>	<b>0</b>
El Dorado	EDCTC		0L14	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	140	0	140	0	0
<b>Humboldt</b>	<b>Humboldt CAOG</b>		<b>2002P</b>	<b>Planning, programming, and monitoring</b>	<b>01-Jul-13</b>	<b>Aug-13</b>	<b>Aug-13</b>	<b>150</b>	<b>0</b>	<b>150</b>	<b>0</b>	<b>0</b>
<b>Inyo</b>	<b>Inyo County</b>		<b>2599</b>	<b>West Bishop resurfacing</b>	<b>01-Jul-13</b>	<b>Aug-13</b>	<b>Aug-13</b>	<b>122</b>	<b>0</b>		<b>0</b>	<b>122</b>
<b>Lake</b>	<b>Lake CCAPC</b>		<b>3002P</b>	<b>Planning, programming, and monitoring</b>	<b>01-Jul-13</b>	<b>Aug-13</b>	<b>Aug-13</b>	<b>64</b>	<b>0</b>	<b>64</b>	<b>0</b>	<b>0</b>
Marin	MTC		2127	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	22	0	22	0	0
Marin	TA Marin		2127C	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	200	0	200	0	0
<b>Mendocino</b>	<b>MCOG</b>		<b>4002P</b>	<b>Planning, programming, and monitoring</b>	<b>01-Jul-13</b>	<b>Aug-13</b>	<b>Aug-13</b>	<b>140</b>	<b>0</b>	<b>140</b>	<b>0</b>	<b>0</b>
Napa	MTC		2130	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	13	0	13	0	0
<b>Nevada</b>	<b>Nevada CTC</b>		<b>0L83</b>	<b>Planning, programming, and monitoring</b>	<b>01-Jul-13</b>	<b>Aug-13</b>	<b>Aug-13</b>	<b>81</b>	<b>0</b>	<b>81</b>	<b>0</b>	<b>0</b>
Placer TPA	Placer TPA		0L11	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	142	0	142	0	0
Sacramento	SACOG		0L30	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	609	0	609	0	0
<b>San Bernardino</b>	<b>SANBAG</b>		<b>9811</b>	<b>Planning, programming, and monitoring</b>	<b>01-Jul-13</b>	<b>Aug-13</b>	<b>Aug-13</b>	<b>1,200</b>	<b>0</b>	<b>1,200</b>	<b>0</b>	<b>0</b>
<b>San Diego</b>	<b>SANDAG</b>		<b>7402</b>	<b>Planning, programming, and monitoring</b>	<b>01-Jul-13</b>	<b>Aug-13</b>	<b>Aug-13</b>	<b>854</b>	<b>0</b>	<b>854</b>	<b>0</b>	<b>0</b>
San Francisco	MTC		2131	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	60	0	60	0	0
San Francisco	SFCTA		2007	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	491	0	491	0	0
San Luis Obispo	SLOCOG		942	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	275	0	275	0	0
San Mateo	MTC		2140	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	62	0	62	0	0
Santa Clara	MTC		2144	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	138	0	138	0	0
Santa Clara	SCVTA		2255	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	696	0	696	0	0
Solano	MTC		2152	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	36	0	36	0	0
Sonoma	MTC		2156	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	43	0	43	0	0
Sonoma	SCTA		770E	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	373	0	373	0	0
Sutter	SACOG		1L53	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	56	0	56	0	0
Yolo	SACOG		0L37	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	119	0	119	0	0
Yuba	SACOG		0L41	Planning, programming, and monitoring	01-Jul-13	Jun-13	Jun-13	43	0	43	0	0
<b>Total (eligible on July 1, 2013, or if received later, from Effective Date of Letter)</b>								<b>7,686</b>	<b>0</b>	<b>7,564</b>	<b>0</b>	<b>122</b>

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 4.13  
Information

From: ANDRE BOUTROS  
Executive Director

Subject: **GARVEE BONDING CAPACITY UPDATE**

## **SUMMARY**

Government Code Section 14553(b) requires the Commission to prepare, in conjunction with the State Treasurer's Office (STO), an annual analysis of California's bonding capacity for issuing Federal Grant Anticipation Revenue (GARVEE) bonds. This year's analysis is being provided to the Commission at this meeting.

This update is being provided to discuss the relationship between GARVEE Bonding capacity and the adopted 2014 Fund Estimate which established the funding level for the quinquennium covered by the 2014 State Transportation Improvement Program (STIP) and updated the funding level for the remaining two-year period of the 2012 State Highway Operation and Protection Program (SHOPP).

## **DISCUSSION**

The 2013 STO analyses demonstrate that based on the 12-month period with the highest deposits (nearly \$4.226 billion), the 15% limitation on GARVEE debt is \$633,883. After taking into account the current maximum annual debt service of the Series 2004A Bonds and Series 2008A Bonds (\$84,295,000 in FY 2013-14), the remaining annual debt service capacity is \$549,588. The Base Case scenario, using a 12-year final maturity and 1.78% interest rate for the issuance, provides the highest bonding capacity of approximately \$5.9 billion.

The STO analysis of GARVEE bonding capacity is calculated as prescribed by statute, however, a review of the federal deposits into the SHA ignores the fact that not all federal funds are available to fund Department-administered projects. Additionally, by looking solely at reimbursements, the analysis assumes federal funding remains at a consistent level. Staff therefore recommends that the Commission take a more programmatic and forward-looking view, and develop a GARVEE capacity based on 15% of the federal funds estimated to be available annually for SHOPP projects in the 2014 Fund Estimate (\$2.3 billion). This level of federal funding, assuming a 12 year maturity and 1.78% interest rate, yields a \$3.698 billion SHOPP GARVEE bond capacity.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 4.13  
Information Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **ANNUAL ANALYSIS BY STATE TREASURER OF BONDING CAPACITY (GARVEE) OF  
FEDERAL TRANSPORTATION FUNDS**

## **SUMMARY:**

These analyses are provided to the California Transportation Commission (Commission) to assist in its compliance with provisions of Senate Bill (SB) 928 (Burton)(Chapter 862, Statutes of 1999) requiring the Commission to prepare, in conjunction with the State Treasurer's Office, an annual analysis of California's bonding capacity for issuing Grant Anticipation Revenue Vehicles, or GARVEE bonds and notes. GARVEE bonds are capital market borrowings repaid by federal transportation funds deposited in the State Highway Account.

These analyses demonstrate that a wide range of circumstances, including policy, revenues, and market factors, can affect the existing capacity for future State GARVEE financings. Therefore, the analyses should be used as a tool for understanding the implications of alternative project applications and the related potential GARVEE bond structures that the Commission may be asked to consider over the coming year.

## **BACKGROUND:**

The State's authority for issuance of GARVEE obligations derives both from federal legislation and from the passage of SB 928 in 1999, which established Government Code Sections 14550 through 14555.9. The bill was sponsored by the State's Treasurer's Office to ensure California had the necessary state legislative authority to make use of this new financing tool for accelerating high priority transportation projects. SB 928 became effective January 1, 2000, and was further amended by Assembly Bill (AB) 438 (Chapter 113, Statutes of 2001), AB 3026 (Chapter 438, Statutes of 2002), SB 1098 (Chapter 212 Statutes of 2004), and SB 1507 (Chapter 793, Statutes of 2004).

The California Department of Transportation issued the Series 2004A GARVEE bonds in the amount of \$614,850,000 and the Series 2008A GARVEE bonds in the amount of \$97,635,000. As of December 31, 2012, approximately 99 percent of the total bond proceeds for project construction had been expended. Approximately \$273 million of the total \$712.485 million bond principal was outstanding as of December 31, 2012.

Attachment

**Analyses of  
GARVEE  
Bonding Capacity  
2013**

Analyses for the  
California Transportation Commission  
April 2013

State Treasurer Bill Lockyer



# Table of Contents

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# Executive Summary

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These analyses are provided to the California Transportation Commission (“Commission”) to assist in its compliance with the provisions of Government Code Sections 14550 through 14555.9 requiring the Commission to prepare, in conjunction with the State Treasurer’s Office (“STO”), an annual analysis of California’s bonding capacity for issuing Grant Anticipation Revenue Vehicles (“GARVEE”) bonds and notes. GARVEE bonds are capital market borrowings which are repaid from federal transportation funds that are deposited into the State Highway Account. The bonding capacity takes into account the current maximum annual debt service of the Series 2004A Bonds and Series 2008A Bonds.

Legislation was enacted to ensure California had the necessary state legislative authority to make use of this financing tool for accelerating high priority transportation projects. The legislation became effective January 1, 2000, and was further amended by AB 438 (Chapter 113, Statutes of 2001), AB 3026 (Chapter 438, Statutes of 2002), SB 1098 (Chapter 212, Statutes of 2004), and SB 1507 (Chapter 793, Statutes of 2004).

The issuance of additional GARVEE bonds is subject to Government Code Section 14553.4, which states that the Treasurer may not authorize the issuance of additional bonds if the annual debt service on all outstanding GARVEE obligations would exceed 15 percent of the total amount of federal transportation funds deposited into the State Highway Account in the State Transportation Fund for any consecutive 12-month period within the preceding 24 months. Thus, current and future bonding capacity analyses must take place in the context of this “statutory cap.”

There are other factors which also affect bonding capacity, such as maturity structures, interest rates, and policy decisions. Accordingly, these analyses continue the approach of prior analyses by providing “sensitivity analyses” under different scenarios, with varying assumptions for maturity dates and interest rates. This method should continue to assist the Commission in examining and responding to future applications under the context of alternative scenarios.

As of April 1, 2013, there was \$135,670,000 principal amount of State of California (California Department of Transportation) Federal Highway Grant Anticipation Bonds (“GARVEE Bonds”) Series 2004A (“Series 2004A Bonds”) outstanding and \$66,135,000 principal amount of GARVEE Bonds, Series 2008A (“Series 2008A Bonds”) outstanding. **The maximum annual debt service of the outstanding Series 2004A and Series 2008A Bonds is \$84,294,950 in Fiscal Year 2013-14.** The Series 2004A and Series 2008A Bonds carry underlying ratings of ‘Aa3’ from Moody’s Investors Service, ‘AA’ from Standard & Poor’s, and ‘A+’ from Fitch Ratings, which are the ratings for the bonds without consideration of insurance.

The analyses for 2013 show a bonding capacity ranging from a low of approximately \$3.07 billion to a high of approximately \$5.89 billion under varying market conditions and amortization periods. The \$3.07 billion bonding capacity level results from a 6-year amortization with an assumed interest rate of 2.11 percent under a Market Sensitivity Case scenario, and the \$5.89 billion bonding capacity level results from a 12-year amortization with an assumed interest rate of 1.78 percent under a Base Case scenario.

The 2013 analyses show that the bonding capacity has increased by approximately 8.25 percent for a 6-year final maturity amortization period when compared to the same analyses of 2012, and has increased

by approximately 9.05 percent (Market Sensitivity Case) and 9.08 (Base Case) for a 12-year final maturity amortization period when compared to the same analyses of 2012. Factors contributing to the increase in bonding capacity from 2012 include the fact that the annual federal deposits in the State Highway Account were \$48 million higher than those for last year's analyses, while the municipal market interest rates used in this year's analyses continued to remain near historically low levels. For the 2013 analyses, the weighted average interest rates used in the 6-year amortization analyses saw a slight increase of 1 basis point over the prior year's rates, but due to a flatter yield curve in the 6- to 12-year range, the weighted average interest rates used in the 12-year amortization analyses decreased by 12 basis points compared to last year's analyses.

During Fiscal Year 2009 and Fiscal Year 2010, the California Department of Transportation ("Department") accelerated the construction of many federally eligible projects, which resulted in a significant increase of federal receipts beginning in calendar year 2011. Given that this increase in federal deposits is expected to last only a few years, secondary analyses of bonding capacity were conducted based on the assumption that federal receipts were \$2.70 billion, which is the approximate level of deposits prior to 2011 and the level that is expected after 2014. The analyses under these assumptions, of course, show much lower ranges of bonding capacity than is shown when using the actual 2012 federal aid receipts.

These analyses demonstrate that a wide range of circumstances, including policy, revenues, and market factors, can affect the existing capacity for future State GARVEE financings. Therefore, the analyses should be used as a tool for understanding the implications of alternative project applications and the related potential GARVEE bond structures that the Commission may be asked to consider over the coming year.

# **I. Purpose of Analyses**

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The following analyses are provided to assist the Commission in meeting the requirements of SB 928 (Chapter 862), sponsored by the STO to ensure that California has the necessary state legislative authority to make use of this financing tool for accelerating high priority transportation projects. The analyses relate specifically to the requirements in Section 14553(b) of the Government Code, pursuant to which the Commission and the STO shall annually prepare an analysis of the bonding capacity of federal transportation funds deposited in the State Highway Account in the State Transportation Fund. The analyses have been performed consistent with the GARVEE bonds bonding capacity guidelines provided in Government Code Section 14553.4, whereby the STO may not authorize the issuance of additional GARVEE bonds if the annual debt service on all outstanding GARVEE obligations in any fiscal year would exceed 15 percent of the total amount of federal transportation funds deposited into the State Highway Account in the State Transportation Fund for any consecutive 12-month period within the preceding 24 months.

The following analyses are intended to measure the capacity of the State Highway Account to support future issuance of GARVEE bonds, given:

1. the historical record of federal deposits to the State Highway Account (we examine only deposits of pledged funds);
2. requirements preceding any issuance of additional bonds under the Master Trust Indenture; and
3. the “statutory cap” on total outstanding GARVEE bonds.

# **II. The 2004 and 2008 GARVEE Financings**

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The Series 2004A Bonds are secured by a Master Trust Indenture dated February 1, 2004, as amended and supplemented by a First Supplemental Indenture dated February 1, 2004, by and among the Treasurer, the Commission and the Department. The Series 2008A Bonds are secured by the Master Trust Indenture dated February 1, 2004, as amended and supplemented by a Second Supplemental Indenture dated October 1, 2008, by and among the Treasurer, the Commission and the Department. The Series 2004A and Series 2008A Bonds and all future bonds and obligations issued under the Master Trust Indenture are secured solely by the Trust Estate, as defined in the Master Trust Indenture, which consists solely of federal transportation funds. The primary source of federal transportation funds is the amount appropriated to the State by the federal government pursuant to Federal Aid Authorization, pursuant to Title 23 of the U.S. Code authorizing federal funding of state transportation projects.

The Department entered into a Memorandum of Agreement with the Federal Highway Administration (“FHWA”) in anticipation of reimbursement by FHWA for debt service and other bond-related costs associated with the federal-aid projects approved by the FHWA.

The Master Trust Indenture provides for the issuance of additional bonds on parity with the Series 2004A Bonds. Any additional parity bonds or other bonds issued on a basis subordinate to the Series 2004A Bonds must comply with the “statutory cap.”

The \$657,713,000 of Series 2004A bond proceeds were generated to pay a portion of the costs of acquisition of right-of-way and/or construction costs for eight federal-aid State Transportation Improvement Program (“STIP”) projects approved by the Commission. As of December 31, 2012, 100 percent of these bond proceeds were expended for these projects.

The \$98,000,000 of Series 2008A bonds proceeds were generated to pay for the construction of two federal-aid State Highway Operation and Protection Program (“SHOPP”) projects approved by the Commission: Placer County – Interstate 80 Pavement Rehabilitation and Nevada and Sierra Counties – Interstate 80 Pavement Rehabilitation. Both projects have been completed.

Combined, approximately 99 percent of the proceeds of the Series 2004A Bonds and the Series 2008A Bonds were expended as of December 31, 2012. The Commission and the Department continue to monitor each project’s progress to ensure the expedient use of bond proceeds.

### **III. Need for Sensitivity Analyses**

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There are multiple factors that will influence the State’s future capacity to issue GARVEE bonds. These factors include the final maturity, interest rates and the available revenues for the additional bonds test. For this reason, no single bonding capacity analysis is sufficient for purposes of guiding the Commission’s evaluation of the potential for future use of GARVEE bonds. In order to facilitate an informed consideration of future applications with structures and terms not yet known to the Commission, we have performed a series of “sensitivity analyses” under alternative scenarios. The factors that have been varied in these different analyses are identified in the following table.

<b>Primary Factors Affecting Bonding Capacity Sensitivity Analyses</b>
Final Maturity
Assumed Interest Rates

## IV. Information Sources

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### **Pledged Revenues:**

In performing these bonding capacity analyses, the STO is using data obtained from the Department regarding deposits into the State Highway Account in the State Transportation Fund from federal transportation funds. The amounts provided by the Department represent federal funds that can be legally pledged under the Master Trust Indenture for payment of the Bonds. The federal transportation funds that are legally available for payment of debt service include those derived from Federal Aid Authorization under Title 23, including apportioned funds (i.e., National Highway System, bridges and the federal surface transportation programs, and amounts available under minimum guarantees) with corresponding Obligation Authority.

Starting with the 2009 bonding capacity report, to be consistent with Section 14553.4 of the Government Code, the total annual federal aid receipts, without exceptions, have been used to calculate the annual GARVEE bonding capacity. This information was provided on a monthly basis for the period of January 2011 through December 2012. See **Attachments A-1 and A-2** for the monthly deposits data and related calculations. The additional bonds test is based on the highest consecutive 12 months of pledged revenue deposits during the prior 24-month period. These historic annual deposits are a known quantity at any given point in time, but are clearly subject to change over time, and must be re-examined at the time of each potential GARVEE bond issuance.

### **Final Maturities:**

The analyses in the report assume that any additional GARVEE bonds issued in 2013 will have final maturities in 2019 and 2025.

### **Interest Rate Assumptions:**

Estimates of potential interest costs under various scenarios were developed by the STO based on the 'AA' index published by Municipal Market Data ("MMD"), a widely used industry benchmark. The interest rate assumptions used for the analyses are based on the weighted average coupon, using a level debt solution for each final maturity (or amortization period), which reflects the structure of the Series 2004A and Series 2008A Bonds.

## V. Summary of Alternative Assumptions

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For the 2013 bonding capacity analyses, we used the MMD 'AA' interest rate scale, which generally corresponds to the majority of the underlying ratings of the Series 2004A and Series 2008A Bonds. The two alternative scenarios for market conditions used in these analyses are as follows:

- 1. Base Case:** Based on the March 1, 2013 MMD 'AA' interest rate scale.
- 2. Market Sensitivity Case:** Base Case plus 100 basis points.

Many observers believe that over time interest rates could increase from the current levels. For this reason, and based on the expected short-term maturity structure of the State's current and future

GARVEE obligations, a 100 basis point increase in interest rates is used for the market sensitivity analyses.

Two alternatives for the final maturity of the bonds were analyzed for each case. The table below summarizes the range of assumptions for the sensitivity analyses. The different scenarios for each factor combine for a total of four different analyses.

Factors	Range of Assumptions
Final Maturity	Two scenarios: at 6 and 12 years from date of issuance
Assumed Interest Rates	Two scenarios: one at ‘AA’ MMD market rates on March 1, 2013 and one at 100 basis points above the March 1, 2013 ‘AA’ MMD market rates

See **Attachment B** for the detailed assumptions used in each sensitivity analysis.

It should also be noted that the current analyses, by necessity, require significant simplification as compared to the myriad of structuring nuances that would be involved in actual bond sales. As a result, certain ambiguities or alternative interpretations could lead to somewhat differing results in practice. One example of a simplification, common to all scenarios, is the assumption that all GARVEE bonds within the capacity of a given scenario would be issued in a single year and not staggered over multiple years, as typically would be expected in a bonding program of significant magnitude.

If, instead, such bonds were staggered and this financing structure was assumed to have a fixed “end date” represented by the assumed final maturity used in each scenario, each resulting measure of maximum bonding capacity would have to be adjusted downward. This would be necessary because the GARVEE bonds issued in subsequent years would have a shorter period during which to amortize principal before the fixed end date. This would increase the annual debt service necessary for a given par amount of bonds, causing a reduction in total bonding capacity, assuming a fixed amount of annual revenues for each scenario.

Alternatively, this simplification would not have this constraint on capacity if future financings were assumed to be structured on a “rolling maturity” basis; that is, with each GARVEE bond issued in subsequent years within each scenario having exactly the same underlying terms, such as total years to maturity and interest rate, regardless of the timing of any future bond issuance. This latter simplification would also assume a fixed amount of annual revenues for each scenario.

This discussion is offered as an example, which is by no means exhaustive, of the implications of the necessary simplifications involved in any analysis of bonding capacity given current uncertainty about the actual conditions that will exist at the time of any future issuance of GARVEE bonds or obligations. Therefore, care should be exercised in using these analyses to avoid erroneous interpretations or conclusions.

## VI. Summary of Results

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Due to continued historically low market interest rates and a \$48 million increase over the prior year in federal aid receipts deposited into the State Highway Account, the current analyses resulted in a higher bonding capacity than last year's results. As of March 1, 2013, the weighted average interest rate for 'AA' rated bonds with a 6-year final maturity was 1.11 percent (a slight increase of 0.01 percent compared to last year's level) and with a 12-year final maturity was 1.78 percent (a decrease of 0.12 percent compared to last year's level). The variation between maturities is attributable to a flatter 'AA' MMD yield curve in the 6- to 12-year range compared to last year. Furthermore, during Fiscal Year 2009 and Fiscal Year 2010, the Department accelerated the construction of many federally eligible projects, which resulted in a significant increase of federal receipts beginning in calendar year 2011. The Department projects that the annual federal receipts will decrease to about \$3.6 billion in the current year, to the \$3.2 billion level in 2014, and taper off toward the \$2.7 billion level thereafter, which was the approximate level of deposits prior to 2011.

The analyses show that a bond issuance with a 6-year maturity corresponds to a bonding capacity ranging from approximately \$3.07 billion (Market Sensitivity Case) to approximately \$3.17 billion (Base Case). These levels represent an increase of approximately \$233.71 million and \$241.77 million, respectively, compared to 2012, or an increase of approximately 8.25 percent for a 6-year maturity compared to last year's levels.

The Commission policy established 12 years as the maximum maturity for GARVEE bonds. If future bond issues are structured with a 12-year amortization period consistent with the current Commission policy and at current interest rate levels, the remaining capacity for issuance of GARVEE bonds would be from approximately \$5.54 billion (Market Sensitivity Case) to approximately \$5.89 billion (Base Case). These levels represent an increase of approximately \$460.22 million and \$490.24 million, respectively, compared to 2012, or an increase of approximately 9.05 percent (Market Sensitivity Case) and 9.08 (Base Case) percent for a 12-year maturity compared to last year's levels.

Given that the increase in federal deposits into the State Highway Account is expected to begin to trend back downward by the end of 2013, additional analyses of bonding capacity were conducted based on the assumption that federal receipts were \$2.70 billion, which is the approximate level of deposits prior to 2011 and currently expected after 2014. To provide specific ranges under this assumption, the 2013 capacity was calculated in two methods:

- 1) **Based on total receipts in 2012 being \$2.70 billion** and assuming that the receipts are allocated each month at the same percentages as the actual 2012 receipts in order to get a new highest consecutive 12-month total within the preceding 24 months. This assumption shows a bonding capacity ranging from \$2.77 billion (6-year amortization, Market Sensitivity Case) to \$5.32 billion (12-year amortization, Base Case).
- 2) **Based on the highest consecutive 12-month total within the preceding 24 months being \$2.70 billion** (under assumption #1, the highest consecutive 12-month total within the preceding 24 months is \$3.87 billion). This assumption shows a bonding capacity ranging from \$1.79 billion (6-year amortization, Market Sensitivity Case) to \$ 3.44 billion (12-year amortization, Base Case).

Under the current analyses, a longer amortization period would increase the additional bonding capacity. If the Commission policy changes to allow a longer maximum maturity, the bonding capacity would change accordingly.

The following table summarizes key results of our analyses based on the actual federal aid receipts deposited into the State Highway Account in 2012. Detailed worksheets supporting the results can be found in **Attachments C, D-1, and D-2**.

<b>Summary of Results for GARVEE Bonding Capacity Sensitivity Analyses</b>		
<b>Final Maturity Amortization Period</b>	<b>Base Case March 1, 2013 'AA' MMD Scale</b>	<b>Market Sensitivity Case Base Case plus 100 Basis Points</b>
6 years	\$3.17 billion	\$3.07 billion
12 years	\$5.89 billion	\$5.54 billion

## **VII. California Transportation Commission Policy**

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The Commission adopted a GARVEE policy in December 2003. This policy extends through the next Federal Transportation Reauthorization Act. The current transportation reauthorization act has been extended through September 30, 2013.

The policy, contained in Commission Resolution No. G-03-21, is as follows:

- **Debt Limit.** The Commission limits annual GARVEE debt service to 15 percent of qualifying federal revenues. This limit will be calculated on the basis described in Section 14553.4 of the Government Code (i.e., 15 percent of the total amount of federal transportation funds deposited in the State Highway Account for any consecutive 12-month period within the preceding 24 months). In 2004, SB 1507 amended the statutory cap from a 30 percent limit to a 15 percent limit, which aligned it with the Commission's policy.
- **Term.** Each bond is structured for debt service payments over a term of no more than 12 years.
- **Project Selection.** The Commission selects projects for accelerated construction through the use of GARVEE bonding. The selection will be made through the programming process for the STIP and the SHOPP. The Commission will select projects that are major improvements to corridors and gateways for interregional travel and goods movement. Major improvements include projects that increase capacity, reduce travel time, or provide long-life rehabilitation of key bridges or roadways.

## **VIII. Recent Events**

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On March 28, 2012, the Commission approved the 2012 SHOPP, a four-year program of projects for Fiscal Year 2012 through Fiscal Year 2015, which included no GARVEE projects. The Department does not anticipate any new GARVEE bond issuance in the near future.

## **IX. Conclusion**

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As the above analyses show, the ultimate capacity existing for the State's future GARVEE financings will depend on a wide range of circumstances over time, including market conditions, maturity structures, revenues, and other factors that may be considered by the Commission.

We are hopeful that these analyses will be useful in considering the structuring options that are available for GARVEE financings, in addition to meeting the immediate goal of assisting the Commission in preparing its annual report.

**ATTACHMENT A-1**

**FEDERAL DEPOSITS INTO THE  
STATE HIGHWAY ACCOUNT**

**Cumulative 12-Month  
Federal Deposits into the State Highway Account  
Over 24-Month Period, ending December 31, 2012**

<b>Period Covered</b>	<b>12-Month Total Revenues Deposited</b>	
Jan 11 - Dec 11	<b>\$3,873,134,414.60</b>	
Feb 11 - Jan 12	<b>\$3,773,219,460.44</b>	<b>Lowest 12-Month Total</b>
Mar 11 - Feb 12	<b>\$3,831,650,256.09</b>	
Apr 11 - Mar 12	<b>\$4,033,051,572.52</b>	
May 11 - Apr 12	<b>\$4,078,664,292.83</b>	
Jun 11 - May 12	<b>\$3,992,342,200.43</b>	
Jul 11 - Jun 12	<b>\$3,958,408,507.16</b>	
Aug 11 - Jul 12	<b>\$4,225,885,531.12</b>	<b>Highest 12-Month Total</b>
Sep 11 - Aug 12	<b>\$4,046,565,919.88</b>	
Oct 11 - Sep 12	<b>\$4,137,250,188.63</b>	
Nov 11 - Oct 12	<b>\$4,121,268,409.58</b>	
Dec 11 - Nov 12	<b>\$3,949,860,398.85</b>	
Jan 12 - Dec 12	<b>\$3,921,298,254.00</b>	
	<b>\$3,995,584,569.70</b>	<b>Average 12-Month Total</b>

Source: California Department of Transportation

**ATTACHMENT A - 2**

**FEDERAL DEPOSITS INTO THE  
STATE HIGHWAY ACCOUNT**

<b>Monthly Deposits of Legally Pledged Federal Transportation Fund</b>					
	<b>2008</b>	<b>2009</b>	<b>2010</b>	<b>2011</b>	<b>2012</b>
<b>Month</b>	<b>Deposit Amount</b>				
<b>January</b>	\$322,705,809.76	\$248,068,100.96	\$234,302,379.53	\$389,063,404.04	\$289,148,449.88
<b>February</b>	\$142,937,130.88	\$85,502,982.39	\$130,134,373.39	\$155,558,369.65	\$213,989,165.30
<b>March</b>	\$170,752,498.63	\$269,024,485.16	\$213,127,122.15	\$236,920,034.82	\$438,321,351.25
<b>April</b>	\$157,305,518.27	\$159,570,185.02	\$172,566,406.90	\$185,631,604.91	\$231,244,325.22
<b>May</b>	\$279,337,838.81	\$170,883,515.71	\$130,817,619.08	\$399,251,077.85	\$312,928,985.45
<b>June</b>	\$272,181,124.08	\$297,052,028.78	\$300,743,391.19	\$303,302,807.89	\$269,369,114.62
<b>July</b>	\$419,494,465.09	\$327,450,638.69	\$273,125,617.57	\$183,338,941.67	\$450,815,965.63
<b>August</b>	\$290,619,769.21	\$267,037,277.08	\$263,609,660.26	\$582,687,851.42	\$403,368,240.18
<b>September</b>	\$258,453,134.06	\$100,134,952.10	\$314,225,529.17	\$315,712,808.68	\$406,397,077.43
<b>October</b>	\$356,521,583.60	\$328,171,254.52	\$195,447,409.45	\$414,379,161.36	\$398,397,382.31
<b>November</b>	\$114,862,825.70	\$115,307,706.41	\$242,323,185.78	\$456,066,414.04	\$284,658,403.31
<b>December</b>	\$227,795,129.78	\$318,173,222.40	\$323,798,884.94	\$251,221,938.27	\$222,659,793.42
<b>TOTAL</b>	<b>\$3,012,966,827.87</b>	<b>\$2,686,376,349.22</b>	<b>\$2,794,221,579.41</b>	<b>\$3,873,134,414.60</b>	<b>\$3,921,298,254.00</b>
<b>Monthly Average</b>	<b>\$251,080,568.99</b>	<b>\$223,864,695.77</b>	<b>\$232,851,798.28</b>	<b>\$322,761,201.22</b>	<b>\$326,774,854.50</b>

Source: California Department of Transportation.

**ATTACHMENT B**

**DETAILED ASSUMPTIONS  
FOR SENSITIVITY ANALYSES**

Summary of Assumptions for GARVEE Bonding Capacity Sensitivity Analyses			
Base Case – Current Market Conditions			
	<b>Factors</b>	Assumptions	<b>Comments</b>
	Final Maturity	6 and 12 years	Analyses run at each final maturity listed at left.
	Interest Rates	1.11% and 1.78%	Rates indicated relate to each respective final maturity above; listed rates represent the weighted average coupon for a bond issue sizing with level annual debt service.
	Annual Revenues	\$4,225,885,531.12	The Treasurer may not authorize the issuance of the bonds if the annual debt service on all outstanding GARVEE obligations would exceed 15 percent of the State’s historical annual deposits in the State Highway Account from federal funding.

Market Sensitivity Case – Alternative Market Conditions			
	<b>Factors</b>	Assumptions	<b>Comments</b>
	Final Maturity	6 and 12 years	Analyses run at each final maturity listed at left.
	Interest Rates	2.11% and 2.78%	Rates indicated relate to each respective final maturity above; listed rates represent the weighted average coupon for a bond issue sizing with level annual debt service.
	Annual Revenues	\$4,225,885,531.12	The Treasurer may not authorize the issuance of the bonds if the annual debt service on all outstanding GARVEE obligations would exceed 15 percent of the State’s historical annual deposits in the State Highway Account from federal funding.

## ATTACHMENT C

### DETAILED WORKSHEET FOR SENSITIVITY ANALYSES

OVERVIEW OF GARVEE BONDING CAPACITY ANALYSES	
<p>The bond test requires that the annual payment obligations of all outstanding notes in any fiscal year do not exceed 15 percent of the total amount of Federal Transportation Funds deposited into the State Highway Account for the highest consecutive 12-month period within the preceding 24 months. The maximum Annual Debt Service on the outstanding Bonds has been subtracted from the highest 12 consecutive months of deposits during the preceding 24 months in order to calculate the remaining Additional Debt Capacity.</p>	
	<b>(Dollars in Thousands)</b>
<b>Base Case</b>	
Maximum Par Amount of Bonding Capacity	\$3,173,488
Interest rate	1.11%
Maximum Assumed Annual Debt Service *	-\$549,588
Term of Bond Issue	<b>6</b>
<b>Market Sensitivity</b>	
Maximum Par Amount of Bonding Capacity	\$3,067,435
Interest rate	2.11%
Maximum Assumed Annual Debt Service *	-\$549,588
Term of Bond Issue	<b>6</b>
<b>Base Case</b>	
Maximum Par Amount of Bonding Capacity	\$5,891,663
Interest rate	1.78%
Maximum Assumed Annual Debt Service *	-\$549,588
Term of Bond Issue	<b>12</b>
<b>Market Sensitivity</b>	
Maximum Par Amount of Bonding Capacity	\$5,543,396
Interest rate	2.78%
Maximum Assumed Annual Debt Service *	-\$549,588
Term of Bond Issue	<b>12</b>
<p>*15% of legally-pledged Federal Transportation Funds deposited into the State Highway Account less maximum annual debt service for the Series 2004A Bonds and Series 2008A Bonds.</p>	
(white / non-shaded)	= Base Case Scenarios based on March 1, 2013 'AA' MMD Scale
(yellow / shaded)	= Market Sensitivity Case Scenarios based on March 1, 2013 'AA' MMD Scale Plus 100 Basis Points

**ATTACHMENT D-1**

**DETAILED SUMMARY TABLES  
FOR SENSITIVITY ANALYSES**

**GARVEE BONDING CAPACITY**

**Base Case**

Highest 12-Month Revenue (\$ in 000's)	\$4,225,886
Debt Service Test (15% of Revenue)	\$633,883
Less: Existing Maximum Annual Series 2004A & 2008A D/S	-\$84,295
Remaining Maximum Annual Debt Service Capacity	\$549,588

(Dollars in Thousands)

	<b>6 Years</b>	<b>12 Years</b>
Assumed Date of Issuance	2013	2013
Assumed Final Maturity	2019	2025
Assumed Interest Rate <sup>(1)</sup>	1.11%	1.78%
<b>Par Capacity</b>	<b>\$3,173,488</b>	<b>\$5,891,663</b>
<b>Annual Debt Service Required</b>	<b>\$549,588</b>	<b>\$549,588</b>

<sup>(1)</sup> The assumed interest rates are based on the March 1, 2013 'AA' MMD bond scale. The rates used are the weighted average coupon for a level debt service bond sizing based upon the final maturity in each scenario.

**DETAILED SUMMARY TABLES  
FOR SENSITIVITY ANALYSES**

**GARVEE BONDING CAPACITY**

**Market Sensitivity Case**

Highest 12-Month Revenue (\$ in 000's)	\$4,225,886
Debt Service Test (15% of Revenue)	\$633,883
Less: Existing Maximum Annual Series 2004A & 2008A D/S	-\$84,295
Remaining Maximum Annual Debt Service Capacity	\$549,588

(Dollars in Thousands)

	<b>6 Years</b>	<b>12 Years</b>
Assumed Year of Issuance	2013	2013
Assumed Final Maturity	2019	2025
Assumed Interest Rate <sup>(1)</sup>	2.11%	2.78%
<b>Par Capacity</b>	<b>\$3,067,435</b>	<b>\$5,543,396</b>
<b>Annual Debt Service Required</b>	<b>\$549,588</b>	<b>\$549,588</b>

<sup>(1)</sup> The assumed interest rates are based on the March 1, 2013 'AA' MMD bond scale (increased by 100 basis points (1%) for market fluctuations). The rates used are the weighted average coupon for a level debt service bond sizing based upon the final maturity in each scenario.

<b>GARVEE Capacity (\$ in millions)</b>	Federal Deposits	SB 1507 Limit	Debt Service (1.78% Interest)	12-Year Bond Proceeds
Total GARVEE Capacity	\$4,226	15%	\$634	---
Existing Debt Service	---	---	(\$84)	---
Available Debt Service Capacity	---	---	<b>\$550</b>	<b>\$5,892</b>
<b>Proposed SHOPP Debt Service Level</b>				
Federal Funding for the SHOPP (per 2014 Fund Estimate)	\$2,300	15%	<b>\$345</b>	<b>\$3,698</b>

### **BACKGROUND**

Government Code Section 14553.4 states that the Treasurer may not authorize the issuance of additional bonds if annual debt service on all outstanding GARVEE obligations would exceed 15 percent of the total amount of federal transportation funds deposited into the State Highway Account for any consecutive 12-month period within the preceding 24 months.

The Commission has approved the issuing of GARVEE notes twice, once for STIP projects and once for SHOPP projects. On March 10, 2004, the State issued \$614,850,000 of GARVEE Bonds (Series 2004A Bonds) for STIP projects. The Series 2004A Bonds are structured with serial maturities from 2005 through 2015. On October 16, 2008, the State issued a second set of GARVEE Bonds (Series 2008A Bonds) \$97,635,000 for SHOPP projects. The Series 2008A Bonds are structured with serial maturities from 2009 through 2020.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.1c.(6)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Dennis Agar  
Division Chief  
Traffic Operations

Subject: **TRAFFIC LIGHT SYNCHRONIZATION PROGRAM BASELINE AMENDMENT  
RESOLUTION TLSP-PA-1314-01**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission) approve the baseline agreement amendment for the following Traffic Light Synchronization Program (TLSP) projects:

- City of Inglewood – La Brea Avenue

## **ISSUE:**

Amendments to the baseline agreements are needed for one TLSP project to reflect delays in construction.

## **BACKGROUND:**

The Commission has approved 22 traffic light synchronization projects totaling \$147,000,000 for the City of Los Angeles and 59 additional traffic light synchronization projects totaling \$96,484,029 for agencies other than the City of Los Angeles.

The project baseline agreement has been received and reviewed by the Department. The agreement for the project was signed by the Department on October 28, 2008. The baseline amendment is needed for the project below and specific changes to the baseline are reflected in the attached spreadsheet.

### **City of Inglewood – La Brea Avenue (Project 6758)**

The project schedule has been modified due to delays in design caused by a reduction in City of Inglewood staff; design is now complete. The changes are necessary to complete the project. The baseline agreement is being amended to show the new project milestone dates.

Attachment

**TRAFFIC LIGHT SYNCHRONIZATION PROGRAM  
PROJECT AMENDMENT LIST  
(other than City of LA)**

Reference No.: 2.1c.(6)  
August 6, 2013  
Attachment

County	Applicant Name	Corridor Name	Current Project Cost	Revised Project Cost	Current Match Amount	Revised Match Amount	Approved TLSP CONST Funding	Current CONST Start Date	Revised CONST Start Date	Current CONST End Date	Revised CONST End Date
Los Angeles	Inglewood	La Brea Avenue	\$ 606,000		\$ 180,000		\$ 426,000	February-13	August-13	June-13	January-14

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 2.2a. (1)  
Action

From: ANDRE BOUTROS  
Executive Director

Subject: **NOTICE OF PREPARATION – ENVIRONMENTAL IMPACT REPORT FOR THE INTERSTATE 680 (I-680) HIGH OCCUPANCY VEHICLE (HOV) DIRECT ACCESS RAMPS PROJECT**

## **ISSUE:**

Should the Commission, as a Responsible Agency, provide comments in response to the Notice of Preparation (NOP) that an Environmental Impact Report (EIR) will be prepared for the I-680 HOV Direct Access Ramps Project?

## **RECOMMENDATION:**

Staff recommends that the Commission make no comments regarding the environmental issues to be addressed in the EIR for this project; however Staff recommends that a letter be sent to the Department that states the following:

- The Commission has no comments with respect to the project's purpose and need, the alternatives to be studied, the impacts to be evaluated, and the evaluation methods used.
- The Commission recommends that the Department and its partners identify and secure the necessary funding to complete the project.
- If, in the future, funds or other actions under the purview of the Commission are anticipated, notification should be provided to the Commission as a Responsible Agency.

## **BACKGROUND:**

The Department is the designated CEQA Lead Agency responsible for the environmental review of these projects. For project summary information, please see the Department's memorandum included as agenda item 2.2a. (1).

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.2a.(1)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Katrina Pierce  
Division Chief  
Environmental Analysis

Subject: **NOTICE OF PREPARATION OF AN ENVIRONMENTAL IMPACT REPORT**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission), as a responsible agency, review and comment at the August 2013 Commission meeting on the following Notice of Preparation (NOP):

## **ISSUE:**

- 04-CC-680, PM R4.8/R7.3, Interstate 680 (I-680) in Contra Costa County. Construct High Occupancy Vehicle (HOV) on and off-ramps on I-680 in the city of San Ramon.

## **PROGRAMMING:**

The proposed project in Contra Costa County would construct High Occupancy Vehicle on- and off-ramps and auxiliary lanes on I-680 between Bollinger Canyon Road and Crow Canyon Road in the city of San Ramon. The project is not fully funded. The project is funded through the environmental phase with local funds. Funds for construction may be requested from the Commission in the future. The total estimated cost is \$102,000,000 for capital and support. Depending on the availability of funds, construction is estimated to begin in Fiscal Year 2017-18.

## **ALTERNATIVES BEING CONSIDERED:**

Alternatives for the project include:

- Alternative 1 – North Canyon Alternative. This alternative would construct direct on- and off-ramps from the I-680 median HOV lanes in both northbound and southbound directions, at a replaced Norris Canyon Road Overcrossing.
- Alternative 2 – Executive Parkway Alternative. This alternative would construct direct on- and off-ramps from the I-680 median HOV lanes in both northbound and southbound directions, at a new overcrossing.
- Alternative 3 – No-Build (No-Project).

**POTENTIAL SIGNIFICANT ENVIRONMENTAL EFFECTS:**

- Aesthetics
- Transportation/Traffic

Based on the potential for significant impacts to the areas listed above and anticipated public controversy, an Environmental Impact Report is being prepared for the project.

Attachments

Notice of Preparation

Notice of Preparation

To: State Clearinghouse  
1400 Tenth Street  
Sacramento, CA 95814 (Address)

From: California Department of Transportation  
PO Box 23660, MS 8B  
Oakland, CA 94623 (Address)

Subject: Notice of Preparation of a Draft Environmental Impact Report

The California Department of Transportation will be the Lead Agency and will prepare an environmental impact report for the project identified below. We need to know the views of your agency as to the scope and content of the environmental information which is germane to your agency's statutory responsibilities in connection with the proposed project. Your agency will need to use the EIR prepared by our agency when considering your permit or other approval for the project.

The project description, location, and the potential environmental effects are contained in the attached materials. A copy of the Initial Study (  is  is not ) attached.

Due to the time limits mandated by State law, your response must be sent at the earliest possible date but not later than 30 days after receipt of this notice.

Please send your response to Cristin Hallissy, Office of Environmental Analysis at the address shown above. We will need the name for a contact person in your agency.

Project Title: I-680 HOV Direct Access Ramps Project

Project Applicant, if any: \_\_\_\_\_

Date 2/26/13

Signature *Cristin Hallissy*

Title District Branch Chief, Office of Environmental Analysis

Telephone 510-622-8717

Reference: California Code of Regulations, Title 14, (CEQA Guidelines) Sections 15082(a), 15103, 15375

RECEIVED

FEB 27 2013

1:15 PM

STATE CLEARING HOUSE

# **Notice of Preparation of a Draft Environmental Impact Report for the I-680/ HOV Direct Access Ramps Project**

## **Project Location**

The project is located in the city of San Ramon in Contra Costa County, California. The project limits on Interstate 680 (I-680) extend between existing Bollinger Canyon and Crow Canyon. This is shown on the attached map.

## **Project Description**

The project would construct HOV (carpool, vanpool, bus, etc.) on- and off-ramps connecting directly to and from the northbound and southbound median HOV lanes of I-680 in the City of San Ramon between Bollinger Canyon Road and Crow Canyon Road. The proposed HOV on- and off-ramps and auxiliary lanes would be added in the median, and therefore the I-680 mainline lanes would need to be shifted to the outside. Both directions of the proposed I-680 mainline would have one 12' HOV lane, three 12' mixed-flow lanes and one 12' auxiliary lane, in compliance with current highway design standards for lane widths.

A 12' HOV auxiliary lane for a distance of up to 1,000' would be provided in the median upstream and downstream of each HOV ramp, in accordance with the current HOV Guidelines. Both the northbound and southbound HOV off-ramps would be single-lane off-ramps. The proposed ramps would comply with current highway design standards, and would include one 12' lane, 4' inside shoulder and 8' outside shoulder. The HOV on- and off-ramps would be constructed on an embankment retained by retaining walls, which would allow the ramp profiles to rise above the existing grade of I-680.

The freeway widening needed to accommodate the project would require construction of retaining walls along the proposed outside edge of shoulder to minimize right-of-way (ROW) impacts. Widening would also result in the need to reconstruct or remove existing overhead signs, retaining walls, concrete barriers, drainage structures, highway lighting, and highway planting. Temporary construction easements would need to be acquired from several properties abutting the freeway to construct the retaining walls.

## **Alternatives**

The EIR/EA will evaluate two build alternatives, as well as the No Build alternative.

### **Norris Canyon Alternative**

The Norris Canyon Alternative proposes to construct direct on- and off-ramps from the I-680 median HOV lanes in both northbound and southbound directions, at a replaced Norris Canyon Road overcrossing. The proposed Norris Canyon Road Overcrossing would be a two-span, precast concrete I-Girder bridge with spans of 113 feet and 115 feet.

As proposed, Norris Canyon Road would have three through lanes in the eastbound direction, two through lanes in the westbound direction, and one median turn lane between Bishop Drive and San Ramon Valley Boulevard. All six lanes would be 12' wide. Both directions of Norris Canyon Road would have 5' pedestrian sidewalks and 5' shoulders, which can accommodate a Class II bicycle facility (bicycle lane). The resulting roadway cross-section would be wider than the existing by approximately 14'. Due to the constraint with vertical clearance along the southern edge of the existing overcrossing, the additional widening would occur on the north side of Norris Canyon Road. This would require realigning the centerline of Norris Canyon Road to the east, and installing retaining walls along the north side of Norris Canyon Road. Right of Way (ROW) acquisition and temporary construction easements would be needed for construction of the retaining walls and the anticipated utility relocation work.

To accommodate the proposed HOV facility at Norris Canyon Road, the mainline lanes would undergo a transition that begins at Bollinger Canyon Road and ends at Fostoria Way Overcrossing. This transition would consist of restriping and pavement widening. The outside

shoulder would be widened to the standard 10'.. The inside shoulder would be 12' wide, except between Crow Canyon Road and Fostoria Way Overcrossings, where the proposed inside shoulder width varies from 2' to 10'. The transition striping at this location is constrained by the existing bridge columns supporting the Crow Canyon Road Overcrossing.

Several ramps at the Crow Canyon Road Interchange, including the northbound off-ramp, southbound diagonal on-ramp and loop on-ramp, would also need to be slightly realigned as a result of the mainline lane shift.

### **Executive Parkway Alternative**

The Executive Parkway Alternative also proposes to construct direct on- and off-ramps from the I-680 median HOV lanes in both northbound and southbound directions, at a new overcrossing that spans over the northbound freeway lanes and matches to the existing Executive Parkway east of the freeway. The proposed on- and off-ramps and the associated auxiliary lanes would be added in the median, and therefore the I-680 mainline lanes would need to be shifted to the outside between Crow Canyon Road and just south of Bollinger Canyon Road (Figure 2).

The proposed Executive Parkway Overcrossing is recommended to have two 12' lanes in the westbound direction and one 12' lane in the eastbound direction. The existing Executive Parkway would need to be raised and reconstructed as it approaches to the new overcrossing, which would provide standard vertical clearance over the northbound lanes on I-680 as well as Bishop Drive.

Bishop Drive, a frontage road east of I-680 between Norris Canyon Road and Bollinger Canyon Road, is proposed to be realigned to cross underneath the new overcrossing. The existing intersection of Bishop Drive and Executive Parkway would need to be relocated and reconfigured due to the significant difference in grades between the two roadways. Several options were evaluated for the configuration of the proposed HOV ramp terminus at Executive Parkway. Caltrans, City of San Ramon and key stakeholders have selected a configuration that resembles a "jug-handle", formed by the proposed HOV ramp, the realigned Executive Parkway and Bishop Drive (Figure 3). ROW acquisition and temporary construction easements would be needed for to establish the jug-handle configuration and the associated utility work.

The existing bicycle facility on Bishop Drive and Executive Parkway would be reconstructed along the proposed roadways. Since the HOV ramp terminus would be a new freeway entrance with access control, pedestrian and/or bicycle facility would not be provided along the HOV ramp or the new overcrossing.

It is proposed to construct a retaining wall in front of the bridge abutment of the existing Norris Canyon Road Overcrossing in both northbound and southbound directions to construct the necessary widening on I-680 while protecting the structure in place.

To accommodate the proposed HOV facility at Executive Parkway, the mainline lanes would undergo a transition that begins south of Bollinger Canyon Road Overcrossing and ends at Crow Canyon Road Overcrossing. This transition would consist of restriping and pavement widening. The outside shoulder would be 10' wide to meet standards. The inside shoulder would be 12' wide, except at Bollinger Canyon Road and Crow Canyon Road Overcrossings, where the proposed inside shoulder width varies from 2' to 10'. The transition striping at this location is constrained by the existing bridge columns supporting both overcrossings.

Several ramps at the Bollinger Canyon Road and Crow Canyon Road Interchanges would also need to be slightly realigned as a result of the mainline lane shift.

**No Build**

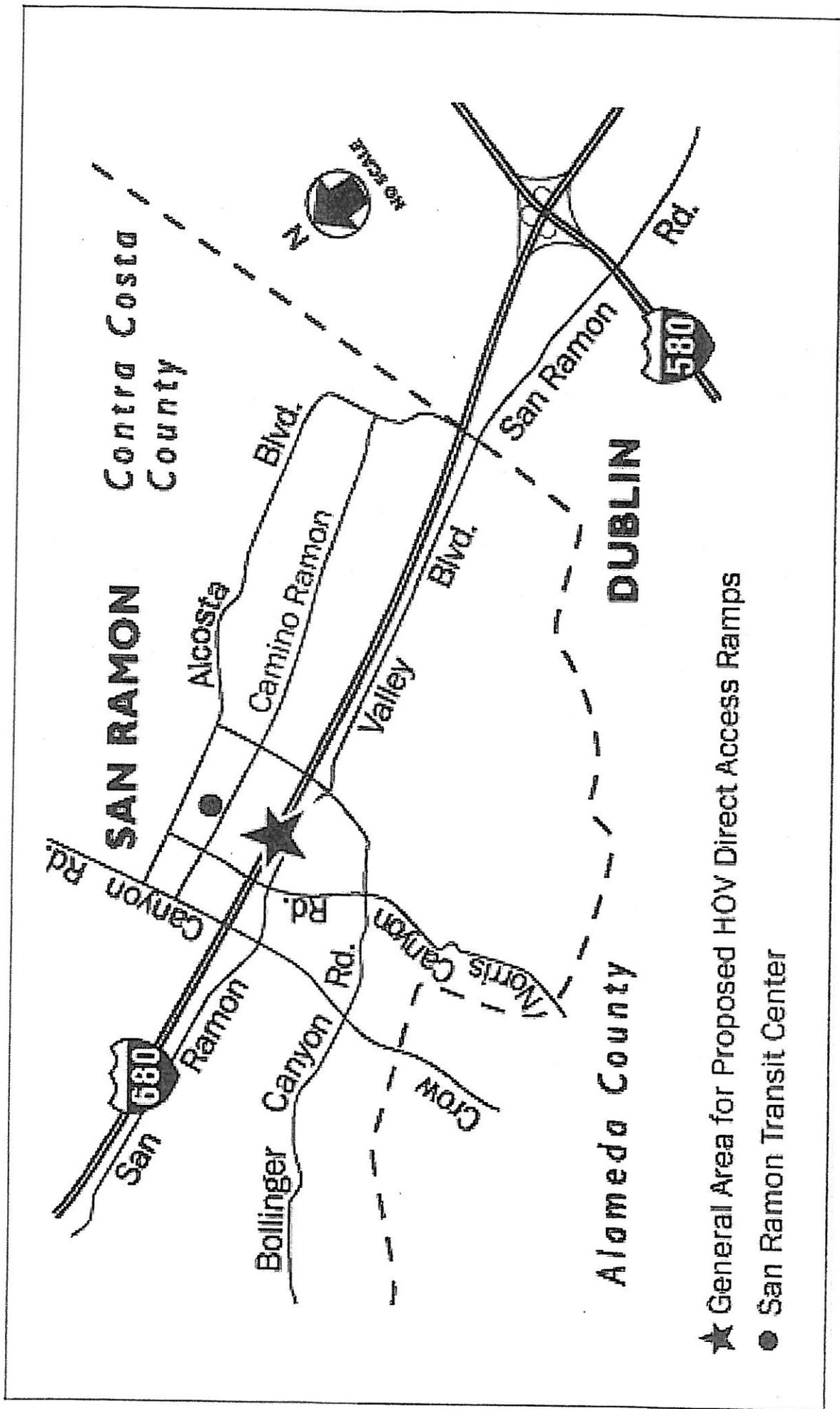
The No-Build alternative would leave the current HOV facility on I-680 unchanged. The express HOVs that utilize the HOV lanes are currently required to weave across all mixed-flow lanes from the median HOV lane to exit at either Bollinger Canyon Road or Crow Canyon Road, and vice versa in the other direction (entering the freeway via either Bollinger or Crow Canyon and weaving across all mixed-flow lanes to the median HOV lane).

**Environmental Effects**

In late November/early December of 2011 a comment period was held for the public to offer input on the scope of environmental studies to be conducted for the Initial Study/Environmental Assessment (IS/EA) that was being prepared for this project. A public meeting was held as a part of this scoping process on November 29, 2011. After taking all comments into consideration, the document level was elevated from an Initial Study to an Environmental Impact Report (EIR) in response to potential public controversy. In addition, a second build alternative was added, the Executive Parkway alternative, so two build alternatives and the no build alternative will be studied in the EIR.

**Project Scoping**

Public participation in the environmental scoping process is an important step in determining the full scope of issues to be addressed in the EIR. Comments on the scope and content of the EIR, as described in this NOP are requested. A formal scoping meeting, in an Open House format, has been scheduled for March 13, 2013 from 7-8:30 PM at the San Ramon Community Center, Terrace Room, 12501 Alcosta Blvd, San Ramon, CA. Notices will be published in the local newspaper, and mailed to landowners, local agencies, resource and permitting agencies, and other stakeholders. The Project Development Team will be present to answer questions.



- ★ General Area for Proposed HOV Direct Access Ramps
- San Ramon Transit Center

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 2.2a. (2)  
Action

From: ANDRE BOUTROS  
Executive Director

Subject: **NOTICE OF PREPARATION – ENVIRONMENTAL IMPACT REPORT FOR THE STATE ROUTE 67 (SR 67) MEDIAN BARRIER FEASIBILITY SAFETY PROJECT**

## **ISSUE:**

Should the Commission, as a Responsible Agency, provide comments in response to the Notice of Preparation (NOP) that an Environmental Impact Report (EIR) will be prepared for the SR 67 Median Barrier Feasibility Safety Project?

## **RECOMMENDATION:**

Staff recommends that the Commission make no comments regarding the environmental issues to be addressed in the EIR for this project; however Staff recommends that a letter be sent to the Department that states the following:

- The Commission has no comments with respect to the project's purpose and need, the alternatives to be studied, the impacts to be evaluated, and the evaluation methods used.
- As the project is programmed in the State Highway Operation and Protection Program and actions under the purview of the Commission are anticipated, notification should be provided to the Commission as a Responsible Agency.

## **BACKGROUND:**

The Department is the designated CEQA Lead Agency responsible for the environmental review of these projects. For project summary information, please see the Department's memorandum included as agenda item 2.2a. (2).

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.2a.(2)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Katrina Pierce  
Division Chief  
Environmental Analysis

Subject: **NOTICE OF PREPARATION OF AN ENVIRONMENTAL IMPACT REPORT**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission), as a responsible agency, review and comment at the August 2013 Commission meeting on the following Notice of Preparation (NOP):

## **ISSUE:**

- 11-SD-67, PM 6.7/19.0. State Route 67 (SR 67) in San Diego County. Construct roadway improvements along a portion SR 67 in the cities of Lakeside, Poway, and Ramona.

## **PROGRAMMING:**

The proposed project in San Diego County will construct safety improvements on State Route 67 near the city of Poway. The project is programmed in the 2012 State Highway Operation and Protection Program. The total estimated cost is \$49,183,000 for capital and support. Construction is estimated to begin in Fiscal Year 2015-16. The scope, as described for the preferred alternative, is consistent with the project scope programmed by the Commission in the 2012 State Highway Operation and Protection Program.

## **ALTERNATIVES BEING CONSIDERED:**

Alternatives for the project include:

- Alternative 1 – This alternative would remove the existing inside lane to install a median barrier in the three and four- lane segments of SR 67.
- Alternative 2 – This alternative would install a median barrier on the existing centerline of SR 67 without removing a lane.
- Alternative 3 – This alternative would remove a lane and install a 12-foot painted buffer in the median of SR 67 in the three and four-lane segments of the highway.
- Alternative 4 – No-build

**POTENTIAL SIGNIFICANT ENVIRONMENTAL EFFECTS:**

- Community
- Wildlife Crossings
- Visual
- Traffic

Based on the potential for significant impacts to the areas listed above, an Environmental Impact Report is being prepared for the project.

Attachments

# ATTACHMENT 1

SCH NO. \_\_\_\_\_

## NOTICE OF PREPARATION

To: \_\_\_\_\_  
(leave blank—will be filled in with Responsible/Trustee Agency)  
\_\_\_\_\_

From: Caltrans, District 11  
4050 Taylor Street MS: 242  
San Diego CA 92110

Subject: **Notice of Preparation of a Draft Environmental Impact Report**  
*Reference:* California Code of Regulations, Title 14, (CEQA Guidelines) Sections 15082(a), 15103, 15375.

Project Title: State Route 67 Median Barrier Feasibility Safety Project

Project Location: 11-SD-67 Postmiles: 6.7-19.0

Project Description: Install a median barrier on State Route 67 (SR-67) from postmile 6.7 to 19.0. to reduce the number and severity of cross centerline accidents.

This is to inform you that the California Department of Transportation will be the lead agency and will prepare an environmental impact report for the SR-67 Median Barrier Feasibility Safety Project. Caltrans is proposing three build and one no-build alternative to be studied for this project. The proposed project is approximately 13 miles in length and proposes to install a median barrier to reduce the number and severity of cross centerline accidents. Alternatives 1 and 2 each have three types of barrier as options: either a concrete barrier, high tension cable barrier, or metal barrier rail. Alternative 1 proposes removing the existing inside lane to install the median barrier. Alternative 2 proposes to install a median barrier on the existing centerline. Alternative 3 proposes to remove a lane and install a 12' median buffer. Alternative 4 is the no-build alternative. Your participation as a responsible agency is requested in the preparation and review of this document.

We need to know the views of your agency as to the scope and content of the environmental information that is germane to your agency's statutory responsibilities in connection with the proposed project. Your agency will need to use the EIR prepared by our agency when considering your permit or other approval for the project.

A more detailed project description, location map, and the potential environmental effects are contained in the attached materials.

A copy of the Initial Study is not attached.

Due to the time limits mandated by State law, your response must be sent at the earliest possible date but not later than 30 days after receipt of this notice.

Please direct your response to David Nagy Telephone 619-688-0224 at the address shown above. Please supply us with the name for a contact person in your agency.

Date 5/15/13

Signature   
Title Senior Environmental Planner



State Route 67 Median Barrier Feasibility Safety Project Location

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 2.2b. (1)  
Action

From: ANDRE BOUTROS  
Executive Director

Subject: **SUPPLEMENTAL DRAFT ENVIRONMENTAL IMPACT REPORT/ENVIRONMENTAL IMPACT STATEMENT (SDEIR/EIS) FOR THE SAN DIEGO FREEWAY (I-405) IMPROVEMENT PROJECT**

## **ISSUE:**

Should the Commission, as a Responsible Agency, provide comments in response to the SDEIR/EIS for the San Diego Freeway (I-405) Improvement Project in Orange and Los Angeles Counties?

## **RECOMMENDATION:**

Staff recommends that the Commission make no comments relative to the alternatives or environmental impacts addressed in the SDEIR/EIS. However, staff recommends that a letter be sent to the Department that states the following:

- The Commission has no comments with respect to the alternatives or environmental impacts addressed in the SDEIR/EIS.
- The Commission recommends that the Department and its partners identify and secure the necessary funding to complete the project.
- If funds or other actions under the purview of the Commission are anticipated, upon completion of the final environmental document, notification should be provided to the Commission as a Responsible Agency.
- Since design-build procurement and tolling are under consideration, the Commission encourages Caltrans and its partners to ensure early communication and coordination with the Commission in the event it is anticipated that the Commission will be requested to approve the project for delivery through either a design-build or public private partnership procurement consistent with the provisions of Senate Bill 4 (SBX2 4, Statutes of 2009), or for construction approval to allow for financing approval by the California Transportation Financing Authority as provided for in Assembly Bill 798 (AB 798, Statutes of 2009).

## **BACKGROUND:**

The Department is the designated CEQA/NEPA Lead Agency responsible for the environmental review of the project. For project summary information, please see the Department's memorandum included as agenda item 2.2b (1).

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.2b.  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Katrina Pierce  
Division Chief  
Environmental Analysis

Subject: **COMMENTS ON DRAFT ENVIRONMENTAL IMPACT REPORTS**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission) review and comment at the August 2013 Commission meeting on the following Supplemental Draft Environmental Impact Report (SDEIR):

## **ISSUE:**

**12-Ora-405, PM 9.3/24.2, 07-LA-405, PM 0.0/1.2, 12-Ora-22, PM R0.7/R3.8, 12-Ora-22, PM R0.5/R0.7, 12-Ora-73, PM R27.2/R27.8, 12-Ora-605, PM 3.5/R1.6 07-LA-605, PM R0.0/R1.2**

This project in Los Angeles and Orange Counties proposes to construct roadway improvements on Interstate 405 (I-405) within the project limits, including 15 local street interchanges and three freeway-to-freeway interchanges. The project is not fully funded. Depending on the alternative selected, the total estimated project cost ranges between \$1.3 billion and \$1.7 billion. The project will seek funding through the State Transportation Improvement Program, federal and local fund sources, and possibly toll revenue. Depending on the availability of funding, construction is estimated to begin in Fiscal Year 2015-16.

Alternatives considered for the proposed project include:

- No Build Alternative.
- Alternative 1 - This alternative would add one general purpose lane in each direction of I-405 from Euclid Street to the Interstate 605 (I-605) interchange.
- Alternative 2 - This alternative would add one general purpose lane in each direction of I-405 from Euclid Street to the I-605 interchange and add a second general purpose lane in the northbound direction from Brookhurst Street to the State Route 22 (SR-22)/ 7<sup>th</sup> Street interchange.

- Alternative 3 - This alternative would add one general purpose lane in each direction of I-405 from Euclid Street to the I-605 interchange and add a tolled Express Lane in each direction from State Route 73 (SR-73) to SR-22 East.

The decision to prepare a Supplemental Draft EIR was made after information from a supplemental traffic study indicated additional project related impacts were possible. The supplemental traffic study was prepared in response to comments on the original Draft Environmental Impact Report for the project. Impacts include:

- Traffic and Circulation
- Pedestrian/Bicycle Facilities
- Cumulative Impacts

The following measures would be incorporated to minimize impacts of the project:

- A payment shall be made by the Orange County Transportation Authority (OCTA) to the City of Long Beach to cover the project's fair share cost of the improvements at intersections owned by the City of Long Beach.
- A payment shall be made by OCTA to the California Department of Transportation based on a Traffic Mitigation Agreement Fair Share Deferment.

Attachments

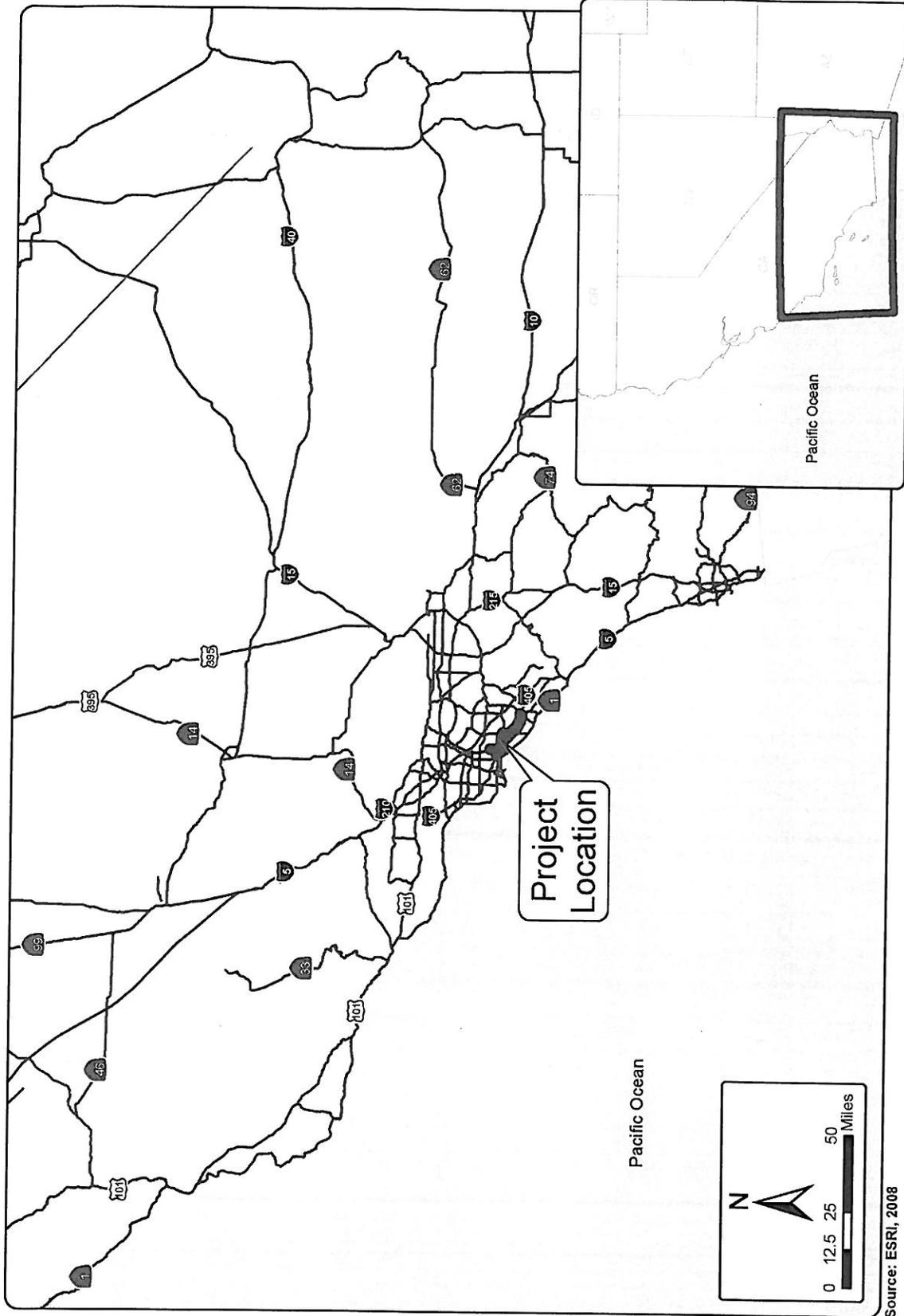


Figure 1-1: Regional Vicinity Map

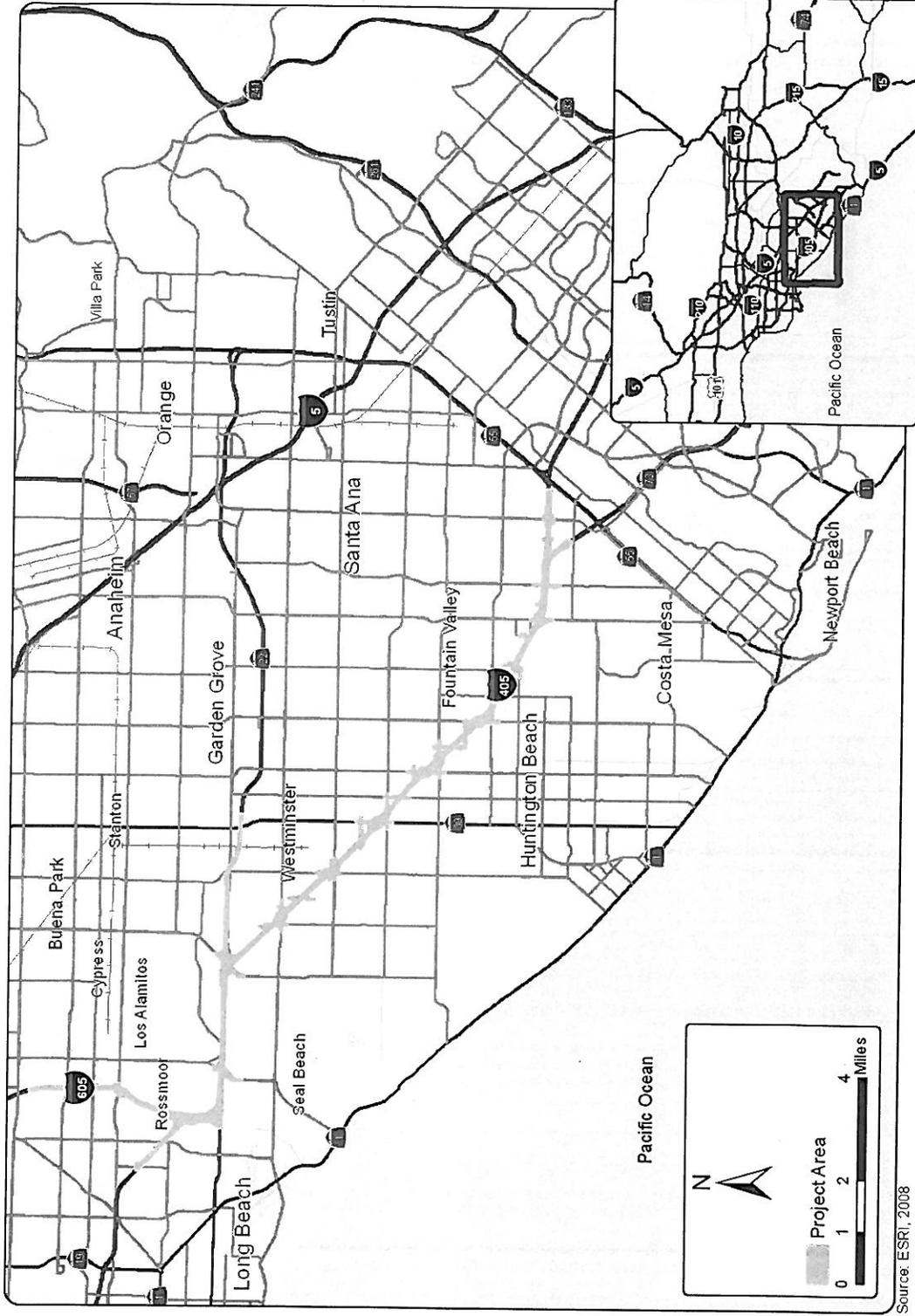


Figure 1-2: Project Location Map

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.2c.(1)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Katrina Pierce  
Division Chief  
Environmental Analysis

Subject: **APPROVAL OF PROJECTS FOR FUTURE CONSIDERATION OF FUNDING**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission), as a responsible agency, approve the attached Resolutions E-13-58, E-13-59, E-13-60, E-13-61, E-13-62, and E-13-63.

## **ISSUE:**

### **01-Men-271, PM 7.12/7.2** **RESOLUTION E-13-60**

The attached resolution proposes to approve for future consideration of funding the following project for which a Negative Declaration (ND) has been completed:

- State Route 271 (SR 271) in Mendocino County. Installation of groundwater remediation equipment at an existing Caltrans maintenance station on SR 271 near the town of Leggett. (PPNO 4510)

This project in Mendocino County will install groundwater remediation equipment at the Leggett Maintenance Station near the town of Leggett. The project is programmed in the 2012 State Highway Operation and Protection Program. The total estimated cost is \$1,057,000 for capital and support. Construction is estimated to begin in Fiscal Year 2013-14. The scope, as described for the preferred alternative, is consistent with the project scope programmed by the Commission in the 2012 State Highway Operation and Protection Program.

A copy of the ND has been provided to Commission staff. The project will result in no significant impacts to the environment. As a result, a ND was completed for this project.

Attachment 1

**ISSUE:**

**02-Tri-299, PM 12.2/12.9**

**RESOLUTION E-13-59**

The attached resolution proposes to approve for future consideration of funding the following project for which a Mitigated Negative Declaration (MND) has been completed:

- State Route 299 (SR 299) in Trinity County. Roadway improvements on SR 299 near the town of Burnt Ranch. (PPNO 3438)

This project in Trinity County will pave shoulders and improve curves on a portion of State Route 299 near the town of Burnt Ranch. The project is programmed in the 2012 State Highway Operation and Protection Program. The total estimated cost is \$5,713,000 for capital and support. Construction is estimated to begin in Fiscal Year 2014-15. The scope, as described for the preferred alternative, is consistent with the project scope programmed by the Commission in the 2012 State Highway Operation and Protection Program.

A copy of the MND has been provided to Commission staff. The project will result in no significant impacts to the environment. The following resource areas may be impacted by the project: biological resources. Avoidance and minimization measures would reduce any potential effects on the environment. These measures include, but are not limited to, the creation of habitat for the Trinity bristle snail within the project area. As a result, a MND was completed for this project.

Attachment 2

**ISSUE:**

**03-Pla-193, PM 4.4/5.5**

**RESOLUTION E-13-63**

The attached resolution proposes to approve for future consideration of funding and a route adoption for the following project for which a Mitigated Negative Declaration (MND) has been completed:

- State Route 193 (SR 193) in Placer County. Roadway improvements on SR 193 near the city of Lincoln. (PPNO 5580)

This project in Placer County will realign the roadway and widen the shoulders on State Route 193 near the city of Lincoln. The project is programmed in the 2012 State Highway Operation and Protection Program. The total estimated cost is \$1,601,000 for capital and support. Construction is estimated to begin in Fiscal Year 2014-15. The scope, as described for the preferred alternative, is consistent with the project scope programmed by the Commission in the 2012 State Highway Operation and Protection Program.

A copy of the MND has been provided to Commission staff. The project will result in no significant impacts to the environment. The following resource areas may be impacted by the project: visual resources, biological resources, and jurisdictional waters of the United States.

Avoidance and minimization measures would reduce any potential effects on the environment. These measures include, but are not limited to, the re-planting of vegetation in areas disturbed by construction activities, compensatory mitigation for loss of valley oak woodlands, in-lieu fee payments for impacts to wetlands, compensatory mitigation for riparian communities, and the purchase of credits at a Valley Elderberry Longhorn Beetle mitigation bank. As a result, a MND was completed for this project.

Attachment 3

**ISSUE:**

**06-Tul-190, PM 0.0/8.0**  
**RESOLUTION E-13-61**

The attached resolution proposes to approve for future consideration of funding the following project for which a Mitigated Negative Declaration (MND) has been completed:

- State Route 190 (SR 190) in Tulare County. Roadway improvements on SR 190 in and near the city of Tipton. (PPNO 6508)

This project in Tulare County will rehabilitate the pavement and widen existing shoulders on State Route 190, add left-turn channelization to northbound State Route 99 (SR 99) from State Route 190, and relocate utility poles. The project is programmed in the 2012 State Highway Operation and Protection Program. The total estimated cost is \$30,200,000 for capital and support. Construction is estimated to begin in Fiscal Year 2014-15. The scope, as described for the preferred alternative, is consistent with the project scope programmed by the Commission in the 2012 State Highway Operation and Protection Program.

A copy of the MND has been provided to Commission staff. The project will result in no significant impacts to the environment. The following resource areas may be impacted by the project: biological resources. Avoidance and minimization measures would reduce any potential effects on the environment. These measures include, but are not limited to, the purchase of credits at a mitigation bank for impacts to the foraging habitat of the San Joaquin kit fox. As a result, a MND was completed for this project.

Attachment 4

**ISSUE:**

**08-SBd-18, PM 53.12/53.87**  
**RESOLUTION E-13-58**

The attached resolution proposes to approve for future consideration of funding the following project for which a Negative Declaration (ND) has been completed:

- State Route 18 (SR 18) in San Bernardino County. Roadway improvements on SR 18 in the city of Big Bear. (PPNO 0188C)

This project in San Bernardino County will widen the roadway, provide eight-foot paved shoulders, and re-stripe the intersection to provide a left-turn lane on a portion of State Route 18 in the city of Big Bear. The project is programmed in the State Highway Operation and Protection Program. The total estimated cost is \$2,778,000 for capital and support. Construction is estimated to begin in Fiscal Year 2013-14. The scope, as described for the preferred alternative, is consistent with the project scope programmed by the Commission in the 2012 State Highway Operation and Protection Program.

A copy of the ND has been provided to Commission staff. The project will result in no significant impacts to the environment. As a result, a ND was completed for this project.

Attachment 5

**ISSUE:**

**10-Sta-4, PM 6.3/6.5**  
**RESOLUTION E-13-62**

The attached resolution proposes to approve for future consideration of funding the following project for which a Mitigated Negative Declaration (MND) has been completed:

- State Route 4 (SR 4) in Stanislaus County. Roadway improvements on SR 4 near the community of Farmington. (PPNO 0287)

This project in Stanislaus County will widen the eastbound shoulder and install rumble strips on a portion of State Route 4 near the community of Farmington. The project is programmed in the 2012 State Highway Operation and Protection Program. The total estimated cost is \$1,601,000 for capital and support. Construction is estimated to begin in Fiscal Year 2014-15. The scope, as described for the preferred alternative, is consistent with the project scope programmed by the Commission in the 2012 State Highway Operation and Protection Program.

A copy of the MND has been provided to Commission staff. The project will result in no significant impacts to the environment. The following resource areas may be impacted by the project: biological resources. Avoidance and minimization measures would reduce any potential effects on the environment. These measures include, but are not limited to, the purchase of credits at a mitigation bank for impacts to the habitat of the California tiger salamander. As a result, a MND was completed for this project.

Attachment 6

**CALIFORNIA TRANSPORTATION COMMISSION**

**Resolution for Future Consideration of Funding**

**01-Men-271, PM 7.12/7.2**

**Resolution E-13-60**

- 1.1** **WHEREAS**, the California Department of Transportation (Department) has completed a Negative Declaration pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- State Route 271 (SR 271) in Mendocino County. Installation of groundwater remediation equipment at an existing Caltrans maintenance station on SR 271 near the town of Leggett. (PPNO 4510)
- 1.2** **WHEREAS**, the Department has certified that the Negative Declaration has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3** **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Negative Declaration; and
- 1.4** **WHEREAS**, the project will not have a significant effect on the environment.
- 2.1** **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve the above referenced project to allow for future consideration of funding.

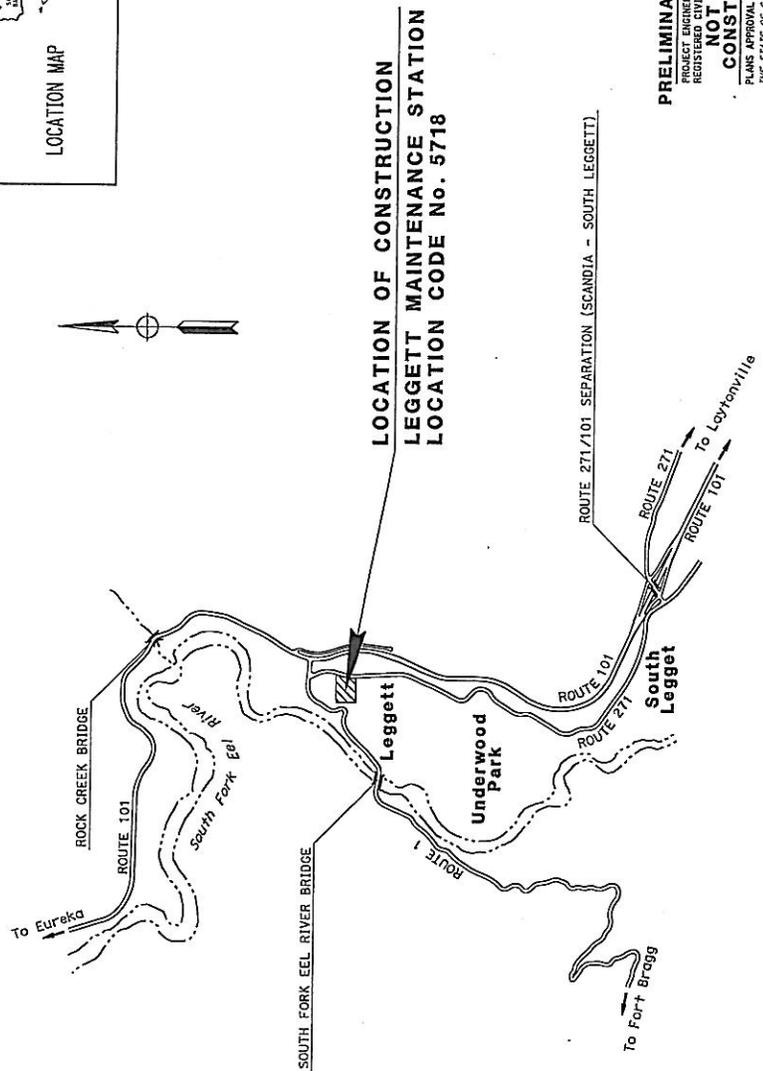
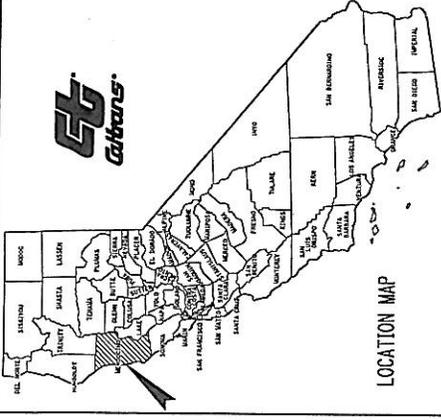
# ATTACHMENT 1

INDEX OF PLANS

## STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION PROJECT PLANS FOR CONSTRUCTION IN MENDOCINO COUNTY AT LEGGETT AT THE LEGGETT MAINTENANCE STATION

TO BE SUPPLEMENTED BY STANDARD PLANS DATED 2010

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET TOTAL SHEETS
01	Mend	5718		



**LOCATION OF CONSTRUCTION**  
**LEGGETT MAINTENANCE STATION**  
**LOCATION CODE No. 5718**

NO SCALE

**PRELIMINARY PLANS**  
PROJECT ENGINEER DATE  
REGISTERED CIVIL ENGINEER  
**NOT FOR CONSTRUCTION**  
PLANS APPROVAL DATE  
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



CONTRACT No.	01-497104
PROJECT ID	0100000684
UNIT	0337 PROJECT NUMBER & PHASE 01000006840

THE CONTRACTOR SHALL POSSESS THE CLASS (OR CLASSES) OF LICENSE AS SPECIFIED IN THE "NOTICE TO BIDDERS."

BORDER LAST REVISED 7/22/2010 CALTRANS WEB SITE IS: [HTTP://WWW.DOT.CA.GOV/](http://www.dot.ca.gov/)

RELATIVE BORDER SCALE 0 1 2 3  
15 IN INCHES  
USERNAME => s109681  
DGN FILE => 0100000684e0001.dgn

PROJECT MANAGER	STEVEN BLAIR
DESIGN ENGINEER	MASTRI ALVANDI

DATE PLOTTED => 07-FEB-2013  
12-2-11

**CALIFORNIA TRANSPORTATION COMMISSION**

**Resolution for Future Consideration of Funding**

**02-Tri-299, PM 12.2/12.9**

**Resolution E-13-59**

- 1.1** **WHEREAS**, the California Department of Transportation (Department) has completed a Mitigated Negative Declaration pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- State Route 299 (SR 299) in Trinity County. Roadway improvements on SR 299 near the town of Burnt Ranch. (PPNO 3438)
- 1.2** **WHEREAS**, the Department has certified that the Mitigated Negative Declaration has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3** **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Mitigated Negative Declaration; and
- 1.4** **WHEREAS**, the project will not have a significant effect on the environment.
- 2.1** **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve the above referenced project to allow for future consideration of funding.

Dist	02	County	Trinity	Route	299	Post Miles	12.3/12.9	Total Project	12.3/12.9	SHEET TOTAL	12.3/12.9
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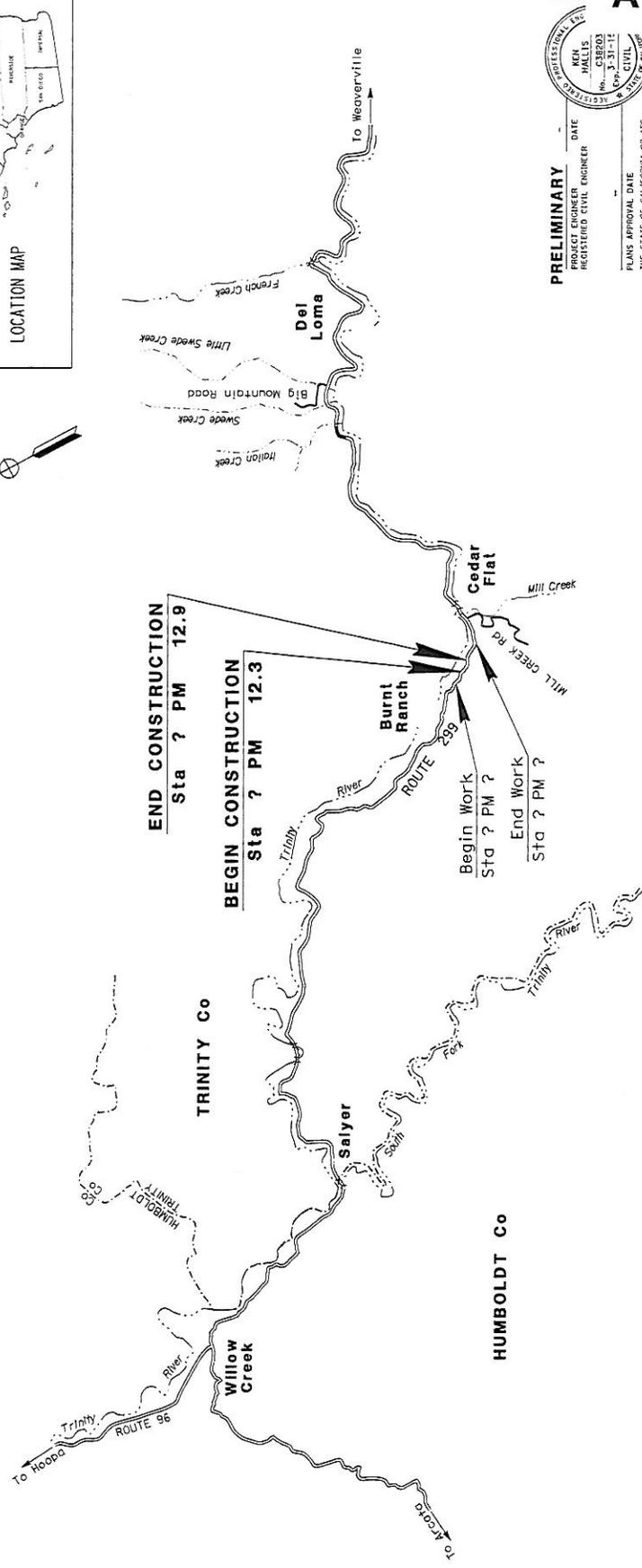
# STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION PROJECT PLANS FOR CONSTRUCTION ON STATE HIGHWAY

TO BE SUPPLEMENTED BY STANDARD PLANS DATED MAY 2006

INDEX OF PLANS

SHEET No.	DESCRIPTION	TITLE
1		

THE STANDARD PLANS LIST APPLICABLE TO THIS CONTRACT ARE LISTED IN THE STANDARD PLANS BOOK, TO BIDDERS AND SPECIAL PROVISIONS BOOK.



CONTRACT No.	02-3E79C
PROJECT ID	02000201
PROJECT NUMBER & PHASE	UNIT 0000

**PRELIMINARY**  
PROJECT ENGINEER  
REGISTERED CIVIL ENGINEER  
DATE  
KEN HALLIS  
No. C-88003  
Exp. 3-31-11  
CIVIL  
STATE OF CALIFORNIA  
PLANS APPROVAL DATE  
THE STATE OF CALIFORNIA OR ITS  
AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR  
COMPLETENESS OF SCALED COPIES OF THIS PLAN SHEET



NO SCALE

THE CONTRACTOR SHALL POSSESS THE CLASS (OR CLASSES) OF LICENSE AS SPECIFIED IN THE "NOTICE TO BIDDERS."

BORDER LAST REVISED 7/2/2010 CALTRANS WEB SITE IS: [HTTP://WWW.DOT.CA.GOV/](http://www.dot.ca.gov/)

DESIGN ENGINEER	KEN HALLIS
PROJECT MANAGER	MICHAEL WEBB

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**CALIFORNIA TRANSPORTATION COMMISSION**

**Resolution for Future Consideration of Funding and Route Adoption  
03-Pla-193, PM 4.4/5.5  
Resolution E-13-63**

- 1.1** **WHEREAS**, the California Department of Transportation (Department) has completed a Mitigated Negative Declaration pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- State Route 193 (SR 193) in Placer County. Roadway improvements on SR 193 near the city of Lincoln. (PPNO 5580)
- 1.2** **WHEREAS**, the Department has certified that the Mitigated Negative Declaration has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3** **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Mitigated Negative Declaration; and
- 1.4** **WHEREAS**, the project will not have a significant effect on the environment.
- 2.1** **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve the above referenced project to allow for future consideration of funding.

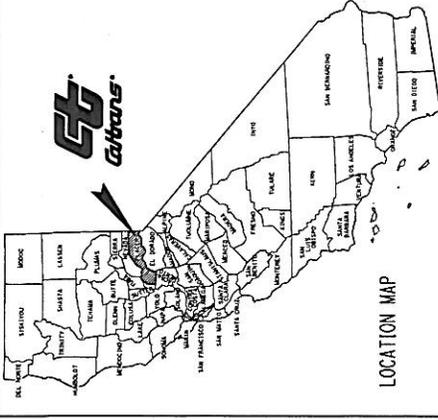
INDEX OF PLANS

# STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION PROJECT PLANS FOR CONSTRUCTION ON STATE HIGHWAY

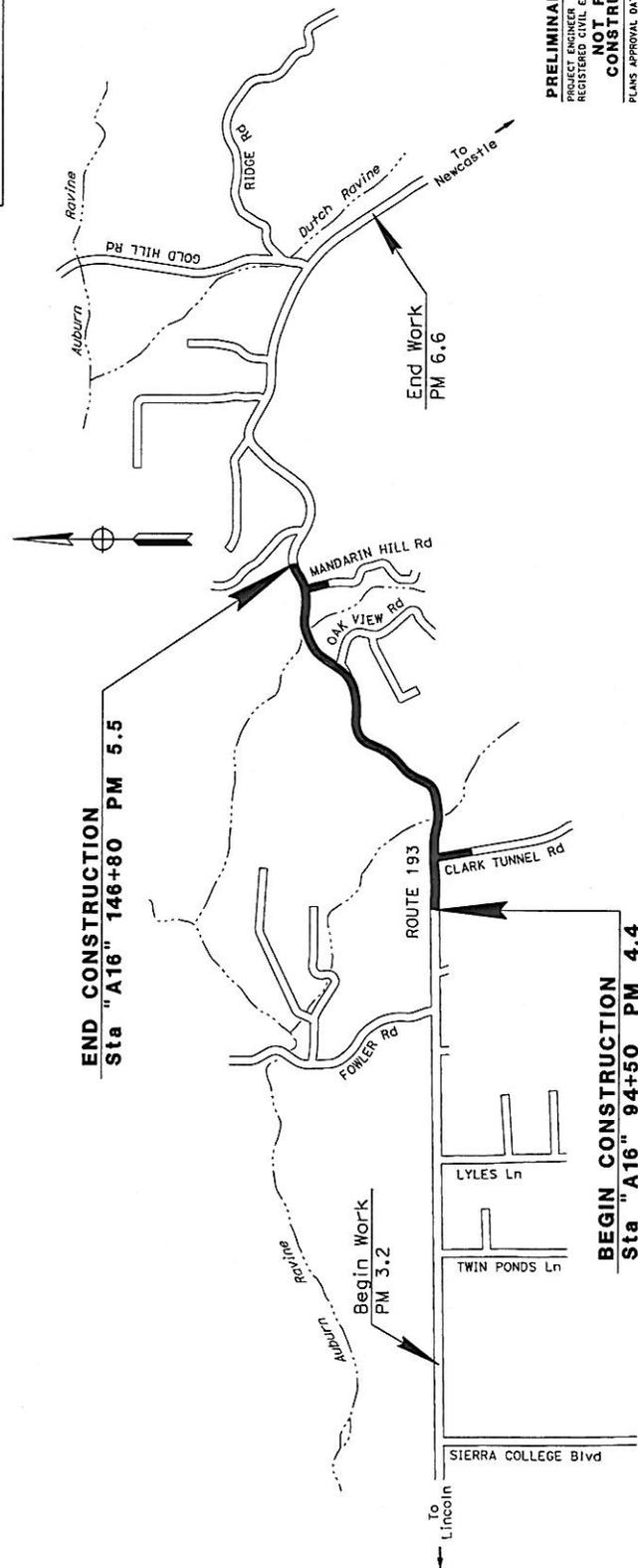
## IN PLACER COUNTY NEAR LINCOLN FROM 0.1 MILE WEST TO 1.0 MILE EAST OF CLARK TUNNEL ROAD

TO BE SUPPLEMENTED BY STANDARD PLANS DATED 2010

Dist#	County	Route	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
03	Plac	193	4.4/5.5		



# ATTACHMENT 3



NO SCALE

THE CONTRACTOR SHALL POSSESS THE CLASS (OR CLASSES) OF LICENSE AS SPECIFIED IN THE "NOTICE TO BIDDERS."

BORDER LAST REVISED 7/1/2010 CALTRANS WEB SITE IS: [HTTP://WWW.DOT.CA.GOV/](http://www.dot.ca.gov/)

RELATIVE BORDER SCALE 0 1 2 3 USERNAME # 8109881 DDM FILE # 03\_4E8600\_1TitleSheet\_Nov2012.dgn

UNIT 0304 PROJECT NUMBER & PHASE 03000007250

**PRELIMINARY PLANS**  
PROJECT ENGINEER REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_  
**NOT FOR CONSTRUCTION**  
PLANS APPROVAL DATE \_\_\_\_\_  
REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_  
CIVIL ENGINEER DATE \_\_\_\_\_

THE STATE OF CALIFORNIA ON ITS BEHALF HEREBY CERTIFIES THAT THE ABOVE IS A TRUE AND CORRECT COPY OF THE ORIGINAL AS SUBMITTED TO THE DIVISION OF HIGHWAYS AND THAT THE SAME IS COMPLETELY ACCURATE AND CORRECT IN ALL RESPECTS.

CONTRACT No.	03-4E8604
PROJECT ID	0300000725

DATE PLOTTED # 29-NOV-2012  
LAST REVISION 11-26-12

DESIGN ENGINEER	MASTRI ALYANDI
PROJECT MANAGER	JOHN HOLDER

**CALIFORNIA TRANSPORTATION COMMISSION**

**Resolution for Future Consideration of Funding**

**06-Tul-190, PM 0.0/8.0**

**Resolution E-13-61**

- 1.1** **WHEREAS**, the California Department of Transportation (Department) has completed a Mitigated Negative Declaration pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- State Route 190 (SR 190) in Tulare County. Roadway improvements on SR 190 in and near the city of Tipton. (PPNO 6508)
- 1.2** **WHEREAS**, the Department has certified that the Mitigated Negative Declaration has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3** **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Mitigated Negative Declaration; and
- 1.4** **WHEREAS**, the project will not have a significant effect on the environment.
- 2.1** **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve the above referenced project to allow for future consideration of funding.

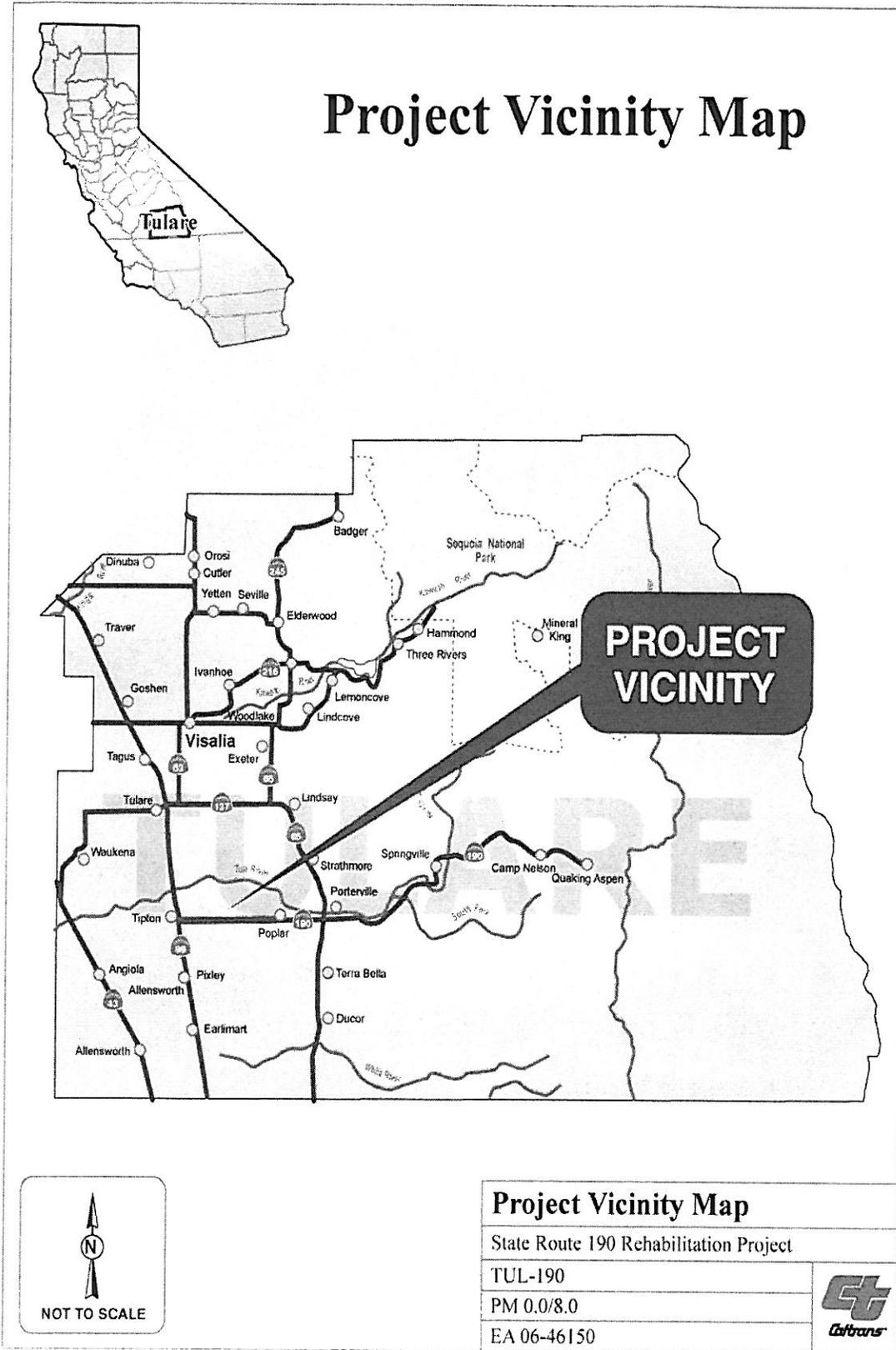


Figure 1-1 Project Vicinity Map

**CALIFORNIA TRANSPORTATION COMMISSION**

**Resolution for Future Consideration of Funding**

**08-SBd-18, PM 53.12/53.87**

**Resolution E-13-58**

- 1.1** **WHEREAS**, the California Department of Transportation (Department) has completed a Negative Declaration pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- State Route 18 (SR 18) in San Bernardino County. Roadway improvements on SR 18 in the city of Big Bear. (PPNO 0188C)
- 1.2** **WHEREAS**, the Department has certified that the Negative Declaration has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3** **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Negative Declaration; and
- 1.4** **WHEREAS**, the project will not have a significant effect on the environment.
- 2.1** **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve the above referenced project to allow for future consideration of funding.



**CALIFORNIA TRANSPORTATION COMMISSION**

**Resolution for Future Consideration of Funding**

**10-Sta-4, PM 6.3/6.5**

**Resolution E-13-62**

- 1.1** **WHEREAS**, the California Department of Transportation (Department) has completed a Mitigated Negative Declaration pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- State Route 4 (SR 4) in Stanislaus County. Roadway improvements on SR 4 near the community of Farmington. (PPNO 0287)
- 1.2** **WHEREAS**, the Department has certified that the Mitigated Negative Declaration has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3** **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Mitigated Negative Declaration; and
- 1.4** **WHEREAS**, the project will not have a significant effect on the environment.
- 2.1** **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve the above referenced project to allow for future consideration of funding.

# Rockaway Shoulder Widening Stanislaus, CA SR4, PM 6.3/6.5 EA 10-0S870

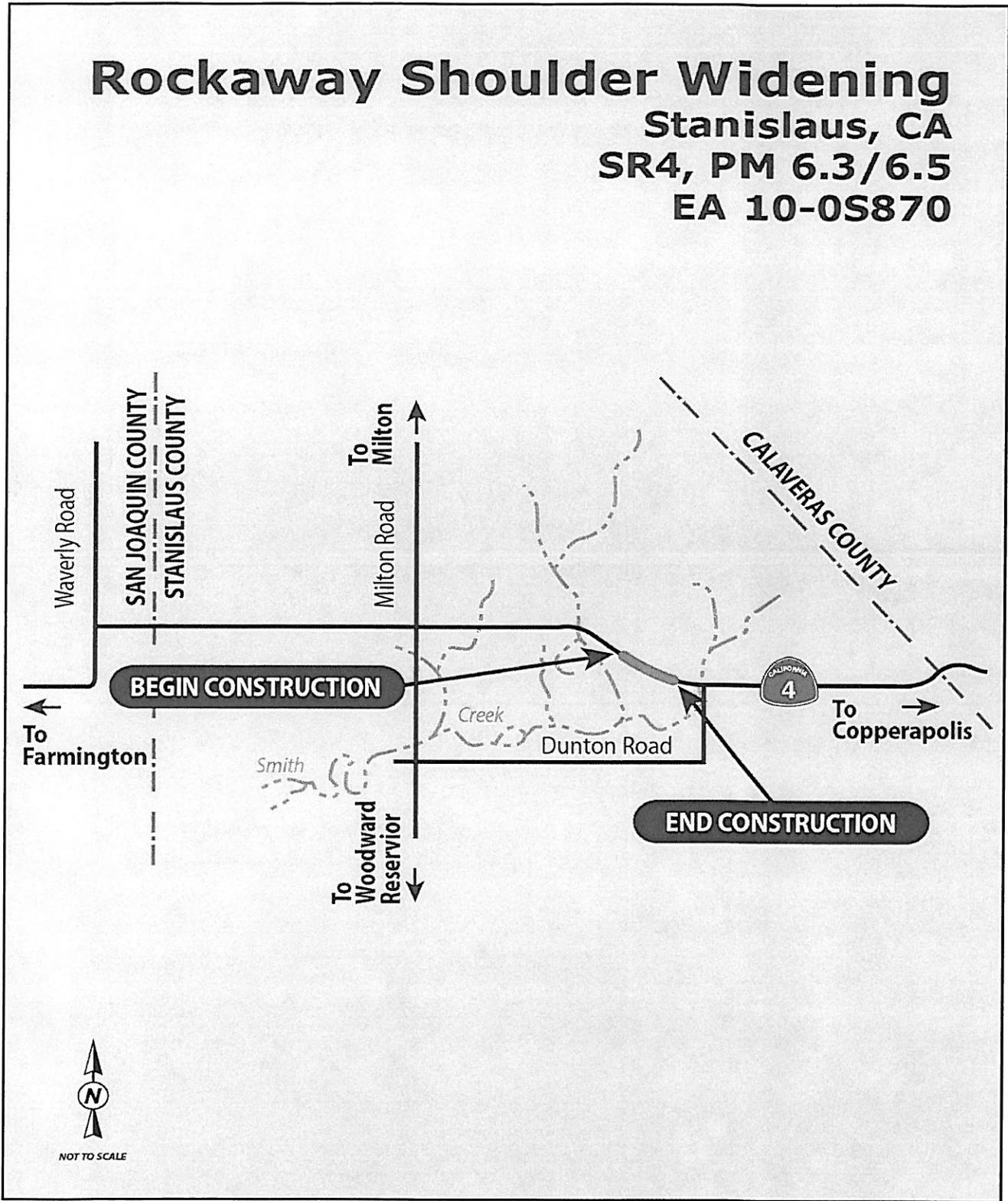


Figure 2 Project Vicinity Map



## **BACKGROUND**

SR 193 is a traversable highway without an adopted alignment by the Commission. West of SR 80, SR 193 connects the city of Lincoln and the unincorporated community of Newcastle in Placer County. East of SR 80, SR 193 connects the cities of Auburn and Placerville. The purpose of this route adoption is to request the Commission's approval of this route-alignment as a conventional highway to allow the Department to construct a curve correction project on a 1.1 mile segment of SR 193 west of SR 80.

The segment of the route that pertains to this route adoption was brought into the State Highway System in 1935 as former Route 91 and, in 1963 was designated as SR 193. This portion of SR 193 is not part of the Freeway and Expressway System. This route began at the intersection of old SR 65 in the city of Lincoln. In January of 2011, the portion of the route that is located within city limits was relinquished to the City of Lincoln (from the intersection with Old Route 65 to the intersection with McBean Park Drive).

This route is a two lane highway and intersects with various public and private roads within the project limit. It has many operational deficiencies including tight curves in its alignment, limited sight distance, short distance tangent between curves, no paved shoulders, and non-standard super-elevation transitions. There are some paved shoulder flaring for driveways and intersections that were constructed under encroachment permit. Corner sight distance at road and driveway intersections is limited due to both roadway geometry and roadside vegetation. The existing run off road recoverability for errant vehicles is limited due to adjacent drainage ditches, embankments and vegetation.

Due to the geometric and operational deficiencies of the roadway, drivers are not able to adjust their speed in a timely manner causing collisions through most of the stretch of roadway proposed for realignment and reconstruction. Currently, to warn drivers to the changes in alignment, warning signs have been installed and more delineation along the horizontal curves has been added.

This project was initiated by the Department's District 3 Traffic Safety Branch in April 2008 after it was determined that the highway segment from PM 4.6 to PM 5.3 had a high concentration of run off roads collisions. The preferred alternative proposes to modify the horizontal alignment using a design speed of 50 mph. This alternative represents a balance of improved safety while minimizing impacts to adjacent property owners and the environment. The new alignment will follow fewer horizontal curves with increased radii and improved sight distance associated with the vertical alignment.

The Department conducted a public open house on August 31, 2011 at the Newcastle Elementary School regarding the project. The meeting was primarily attended by local residents and

individuals active in local community organizations. Many attendees acknowledged the need for the project. No opposition to the project proposal was identified.

The proposed route adoption and subsequent construction for this segment for SR 193 will improve traffic safety. The total cost is estimated at \$12.44 million as of October 26, 2012. This project is proposed to be funded from State Highway Operation and Protection Program as a Safety Improvement Project in the Fiscal Year 2014-2015.

A project report was prepared by the Department and approved on April 23, 2013. An Initial Study with Mitigated Negative Declaration and a Categorical Exclusion were prepared pursuant to the California Environmental Quality Act and the National Environmental Protection Act. A concurrent action to approve the environmental document for route adoption is on this month's Commission agenda.

Attachments:

Resolution HRA 13-02  
Location Map  
Route Adoption Map

**CALIFORNIA TRANSPORTATION COMMISSION**  
**Highway Route Adoption Resolution**  
**03-Pla-193 PM 4.4/5.5**

**Resolution HRA 13-02**

**WHEREAS**, the California Department of Transportation (Department) and Placer County jointly request approval of this Route Adoption as State Highway; and

**WHEREAS**, an Initial Study with Mitigated Negative Declaration prepared pursuant to the California Environmental Quality Act was signed on April 2, 2013; and

**WHEREAS**, a Categorical Exclusion prepared pursuant to the National Environmental Protection Act was signed on April 3, 2013; and

**WHEREAS**, the Project Report recommending the route adoption was approved on April 26, 2013.

**NOW, THEREFORE, BE IT RESOLVED** by the Commission that pursuant to the authority vested in it by law, this Commission does hereby select, adopt, and determine the location of that segment of State Highway Route 193, in the county of Placer, and officially designate it as 03-PLA-193, a State Highway, as said location is shown on the Route Adoption map submitted by Timothy Craggs, Chief Design Engineer; and

**BE IT FURTHER RESOLVED** that this Commission has found and determined and hereby declares that such location of said State highway is for the best interest of the State.



I hereby certify that this map was prepared pursuant to and under my direction and is a true and correct map of a section of proposed State Highway Route 193

Date Submitted: \_\_\_\_\_

Chief Design Engineer  
 Civil Engineer License No. \_\_\_\_\_

I hereby certify that by resolution of the California Transportation Commission on \_\_\_\_\_ the alignment indicated on this map was selected, adopted and determined as the location for a section of State Highway Route 193 and declared a State Highway.

Attest: \_\_\_\_\_  
 Executive Director,  
 California Transportation Commission

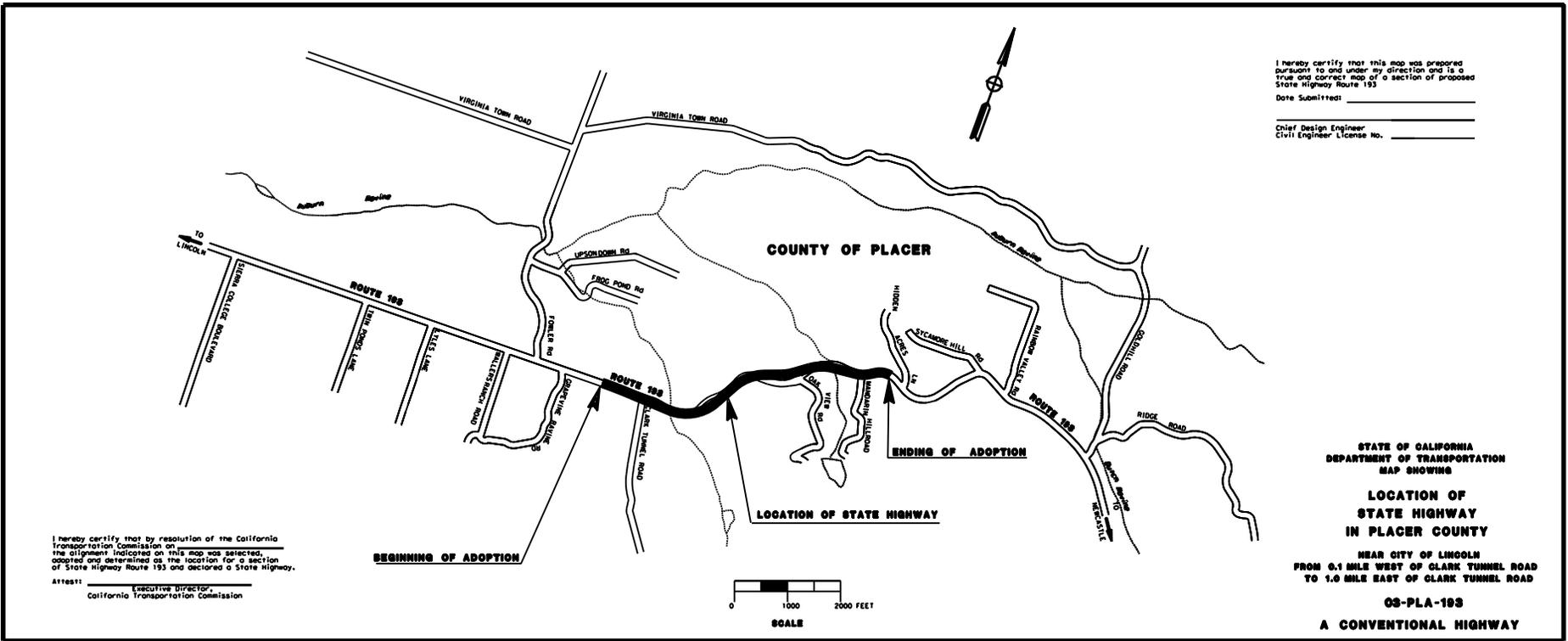
STATE OF CALIFORNIA  
 DEPARTMENT OF TRANSPORTATION  
 MAP SHOWING

**LOCATION OF  
 STATE HIGHWAY  
 IN PLACER COUNTY**

NEAR CITY OF LINCOLN  
 FROM 0.1 MILE WEST OF CLARK TUNNEL ROAD  
 TO 1.0 MILE EAST OF CLARK TUNNEL ROAD

**OS-PLA-193**  
**A CONVENTIONAL HIGHWAY**

IN PLACER COUNTY PM 4.4 TO 5.5



# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.3c.  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Timothy Craggs  
Chief  
Division of Design

Subject: **RELINQUISHMENT RESOLUTIONS**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) approve the relinquishment resolutions, summarized below, that will transfer highway facilities no longer needed for the State Highway System to the local agency identified in the summary.

## **ISSUE:**

It has been determined that each facility in the specific relinquishment resolutions summarized below is not essential to the proper functioning of the State Highway System and may be disposed of by relinquishment. Upon the recording of the approved relinquishment resolutions in the county where the facilities are located, all rights, title and interest of the State in and to the facilities to be relinquished will be transferred to the local agencies identified in the summary. The facilities are safe and drivable. The local authorities have been advised of the pending relinquishments a minimum of 90 days prior to the Commission meeting pursuant to Section 73 of the Streets and Highways Code. Any exceptions or unusual circumstances are described in the individual summaries.

## **RESOLUTIONS:**

Resolution R-3878 – 04-SM-1-PM 38.4/40.0  
(Request No. 56101) – 1 Segment

Relinquishes right of way in the county of San Mateo on Route 1 in the Devil's Slide area, consisting of superseded highway right of way. The County, by resolution dated July 9, 2013, waived the 90-day notice requirement and agreed to accept title upon relinquishment by the State.

Resolution R-3879 – 05-Mon-101-PM 85.5/85.7  
(Request No. 11586) – 5 Segments

Relinquishes right of way in the city of Salinas along Route 101 on Airport Boulevard, De La Torre Street, and Moffett Street, consisting of collateral facilities. The City, by freeway agreement dated October 6, 2009, agreed to accept title upon relinquishment by the State. The 90-day notice period expired June 23, 2013, without exception.

Resolution R-3880 – 05-Mon-101-PM 85.3/85.5  
(Request No. 11587) – 1 Segment

Relinquishes right of way in the county of Monterey adjacent to Route 101 on De La Torre Street, consisting of collateral facilities. The County, by freeway agreement dated December 15, 2009, agreed to accept title upon relinquishment by the State. The 90-day notice period expired June 23, 2013, without exception.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No: 2.4b.  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Brent L. Green  
Chief  
Division of Right of Way and  
Land Surveys

Subject: **RESOLUTIONS OF NECESSITY**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) adopt Resolutions of Necessity (Resolution) C-21077 through C-21081, C-21083 through C-21095, and C-21097 through C-21101, summarized on the following pages.

## **ISSUE:**

Prior to initiating Eminent Domain proceedings to acquire needed Right of Way for a programmed project, the Commission must first adopt a Resolution stipulating specific findings identified under Section 1245.230 of the Code of Civil Procedure.

Moreover, for each of the proposed Resolutions, the property owners are not contesting the following findings contained in Section 1245.230 of the Code of Civil Procedure:

1. The public interest and necessity require the proposed project.
2. The proposed project is planned and located in a manner that will be most compatible with the greatest public good and the least private injury.
3. The property is necessary for the proposed project.
4. An offer to purchase the property in compliance with Government Code Section 7267.2 has been made to the owner of record.

The only remaining issues with the property owners are related to compensation.

## **BACKGROUND:**

Discussions have taken place with the owners, each of whom has been offered the full amount of the Department's appraisal and, where applicable, advised of any relocation assistance benefits to which the owners may subsequently be entitled. Adoption of the Resolutions will not interrupt our efforts to secure equitable settlement. In accordance with statutory requirements, each owner has been advised that the Department is requesting the Resolution at this time. Adoption will assist the Department in the continuation of the orderly sequence of events required to meet construction schedules.

C-21077 - Gary U. Eickerman and Wonetta L. Eickerman

06-Fre-180-PM R119.90 - Parcel 84562-1, 2 - EA 342539.

Right of Way Certification (RWC) Date: 11/01/14; Ready to List (RTL) Date: 12/01/14.

Expressway - Convert a two-lane conventional highway to a four-lane expressway. Authorizes condemnation of land in fee for a State highway, extinguishment of abutter's rights of access, all of those certain improvements which straddle the right of way line, and a temporary easement for removing existing improvement. Located near the city of Sanger at 15901 East Kings Canyon Road. Assessor's Parcel Number (APN) 333-021-07.

C-21078 - Tash Investments, LLC, a Nevada limited liability company

07-LA-5-PM 1.2 - Parcel 77803-1, 2, 3, 4, 01-01 - EA 215929.

RWC Date: 03/14/14; RTL Date: 03/28/14. Freeway - widen Interstate 5 (I-5) to add high occupancy vehicle (HOV) and mixed-flow lanes. Authorizes condemnation of land in fee for a State highway, extinguishment of abutter's rights of access, permanent easements for maintenance purposes, a temporary easement for construction purposes, and excess land in fee to which the owner has consented. Located in the city of La Mirada at 14320 Firestone Boulevard.

APN 7003-006-023.

C-21079 - Clear Channel Outdoor, Inc.

07-LA-5-PM 0.70 - Parcel 80383-1 - EA 215929.

RWC Date: 03/14/14; RTL Date: 03/28/14. Freeway - widen I-5 to add HOV and mixed-flow lanes. Authorizes condemnation of leasehold interest of outdoor advertising company. Located in the city of La Mirada at 14400 Firestone Boulevard. APN 7003-007-004.

C-21080 - Kevin L. Cook and Lisa H. Cook

07-LA-405-PM 33.2 - Parcel 79960-1, 2 - EA 120309.

RWC Date: 01/11/13; RTL Date: Design-Build. Freeway - widening for HOV lane. Authorizes condemnation of a permanent tie-back easement and a temporary easement for construction purposes. Located in the city of Los Angeles at 251 South Thurston Avenue. APN 4366-014-019.

C-21081 - James S. Tomlinson and Natalie C. Khoury

07-LA-405-PM 33.2 - Parcel 79973-1, 2 - EA 120309.

RWC Date: 01/11/13; RTL Date: Design Build. Freeway - widening for HOV lane. Authorizes condemnation of a permanent tie-back easement and a temporary easement for construction purposes. Located in the city of Los Angeles at 349 South Thurston Avenue. APN 4366-015-026.

C-21083 - Brian V. Gaines, Trustee, etc., et al.

08-SBd-15-PM 44.3 - Parcel 20943-1 - EA 355569.

RWC Date: 01/03/14; RTL Date: 02/03/14. Freeway - add northbound mixed flow lane with auxiliary lane; reconstruct D and E Streets and the Stoddard Wells interchange. Authorizes condemnation of land in fee for a State highway and extinguishment of abutter's rights of access. Located near the city of Victorville, approximately 580 feet south of the southeast quadrant of the Stoddard Wells Road interchange. APNs 0473-163-07; 0473-172-05, -07.

C-21084 - Mojave Vistas, LLC, a California Limited Liability Company

08-SBd-15-PM 43.3/43.4 - Parcels 20957-1; 20972-1 - EA 355569.

RWC Date: 01/03/14; RTL Date: 02/03/14. Freeway - add northbound mixed flow lane with auxiliary lane; reconstruct D and E Streets and the Stoddard Wells interchange. Authorizes condemnation of land in fee for a State highway and extinguishment of abutter's rights of access. Located in the city of Victorville, on the southwest side of Interstate 15 (I-15), at the corner of D Street and Tawney Ridge Lane. APN 0478-021-22.

C-21085 - Sung Jo Kim

08-SBd-138-PM 2.85 - Parcel 21560-1 - EA 3401U9.

RWC Date: 12/05/13; RTL Date: 01/06/14. Conventional highway - widen to four lanes. Authorizes condemnation of land in fee for a State highway and underlying fee in adjoining public roadway. Located in the community of Pinon Hills at the northeast corner of State Route (SR) 138 and Evergreen Road. APN 3067-351-20.

C-21086 - Harry J. Marinelli, et al.

08-SBd-138-PM 3.00 - Parcel 21562-1 - EA 3401U9.

RWC Date: 12/05/13; RTL Date: 01/06/14. Conventional highway - widen to four lanes. Authorizes condemnation of land in fee for a State highway and underlying fee in adjoining public roadway. Located in the community of Pinon Hills on the southeast corner of SR 138 and Phelan Road. APN 3067-481-05.

C-21087 - Hai Y. Cheong, et al.

08-SBd-138-PM 3.20 - Parcel 21563-1, 2 - EA 3401U9.

RWC Date: 12/05/13; RTL Date: 01/06/14. Conventional highway - widen to four lanes. Authorizes condemnation of land in fee for a State highway and underlying fee in adjoining public roadway. Located in the community of Pinon Hills on the north side of SR 138, south of Phelan Road, east of Silver Rock Road, and west of Buckwheat Road. APN 3067-521-01

C-21088 - Jose J. Saldana

08-SBd-138-PM 3.71 - Parcel 21565-1 - EA 3401U9.

RWC Date: 12/05/13; RTL Date: 01/06/14. Conventional highway - widen to four lanes. Authorizes condemnation of land in fee for a State highway and underlying fee in adjoining public roadway. Located in the community of Pinon Hills at the northeast corner of Buckwheat Road and SR 138. APN 3067-561-18.

C-21089 - Junior A. Rastall, Trustee of the R & R Trust

08-SBd-138-PM 9.55 - Parcel 21578-1, 2 - EA 3401U9.

RWC Date: 12/05/13; RTL Date: 01/06/14. Conventional highway - widen to four lanes. Authorizes condemnation of land in fee for a State highway, underlying fee in adjoining public roadway, and a permanent easement for drainage purposes. Located in the community of Pinon Hills at 5652 SR 138. APN 0351-053-10.

C-21090 - Chung Family Trust

08-SBd-138-PM 11.65 - Parcel 21592-1 - EA 3401U9.

RWC Date: 12/05/13; RTL Date: 01/06/14. Conventional highway - widen to four lanes.

Authorizes condemnation of land in fee for a State highway and underlying fee in adjoining public roadway. Located in the city of Phelan at 4747 SR 138. APN 0351-081-39.

C-21091 - George A. Okamoto

08-SBd-138-PM 2.44 - Parcel 22776-1 - EA 3401U9.

RWC Date: 12/05/13; RTL Date: 01/06/14. Conventional highway - widen to four lanes.

Authorizes condemnation of a permanent easement for drainage purposes. Located in the community of Pinon Hills, on the north side of SR 138, west of Evergreen Road and east of Oasis Road. APNs 3067-161-05, -06.

C-21092 - Southern California Edison Company, a California Corporation

08-SBd-215-PM 0.71 - Parcel 21983-1 - EA 0M94U9.

RWC Date: 06/13/12; RTL Date: 06/20/12 (under construction). Freeway - construct an HOV lane in each direction. Authorizes condemnation of a temporary railroad easement for construction purposes. Located in the city of Grand Terrace east of Interstate 215 (I-215), west of Taylor Street, and north of the northbound Iowa Avenue/La Cadena Drive off ramp. APN 1167-151-18.

C-21093 - Alfred Troy Hodges, Trustee, etc. et al.

08-SBd-215-PM 16.90 - Parcel 22501-1, 2 - EA 0K7109.

RWC Date: 07/31/14; RTL: Design-Build. Freeway - add additional through lane in each direction on I-15 from Glen Helen Parkway to the I-15/I-215 interchange and reconfigure connectors to the I-215 truck bypass lanes and auxiliary lanes. Authorizes condemnation of land in fee for a State highway, extinguishment of abutter's rights of access, and a temporary easement for construction purposes. Located in the unincorporated area of San Bernardino County at 18620 Cajon Boulevard. APN 0348-132-06.

C-21094 - Carlos Coronado and Reina Coronado.

08-SBd-215-PM 17.39 - Parcel 22653-01-01 - EA 0K7109.

RWC Date: 07/31/14; RTL: Design-Build. Freeway - add additional through lane in each direction on I-15 from Glen Helen Parkway to the I-15/ I-215 interchange and reconfigure connectors to the I-215 truck bypass lanes and auxiliary lanes. Authorizes condemnation of land in fee for a State highway. Located in the unincorporated area of San Bernardino County on the north side of Arrowhead Boulevard, approximately 215 feet west of Devore Boulevard. APN 0349-152-06.

C-21095 - William L. Potter and Alene Potter, Co-Trustees

11-Imp-8-PM 39.0 - Parcel 34975-1, 2, 3 - EA 263309.

RWC Date: 08/19/13; RTL Date: 08/23/13. Freeway - reconstruct interchange. Authorizes condemnation of permanent easements for access purposes, a permanent easement for slope purposes, and permanent easements for access and slope purposes to be conveyed to the city of El Centro. Located in the city of El Centro at 2002 South Dogwood Road. APN 054-041-020-000.

C-21097 - Alaskan Copper Companies, Inc., a Washington Corporation

11-SD-11-PM 0.97 - Parcel 34803-1, 2, 3, 4, 5, 6, 7, 8 - EA 056329.

RWC Date: 03/15/13; RTL Date: 03/25/13. Freeway - construct freeway to freeway connectors. Authorizes condemnation of land in fee for a State highway, underlying fee, extinguishment of abutter's rights of access, a temporary easement for construction purposes, a permanent easement for access, construction, and maintenance of drainage facilities, permanent easements for sewer purposes, a permanent easement for water line purposes, permanent easements for access and maintenance of retaining walls, abutments, and drainage facilities, and permanent easements to be conveyed to the city of San Diego and the San Diego County Sanitation District for utility purposes. Located in the city of San Diego at 1840 Dornoch Court. APNs 646-131-04; -05.

C-21098 - SANYO E & E CORPORATION

11-SD-11-PM 1.03 - Parcel 34804-1, 2, 3, 4, 5, 6, 7 - EA 056329.

RWC Date: 3/15/13; RTL Date: 3/25/13. Freeway - construct freeway to freeway connectors. Authorizes condemnation of land in fee for a State highway, underlying fee, extinguishment of abutter's rights of access, permanent easements for access and maintenance of retaining walls, abutments, footings, and drainage culverts, a permanent easement for access, construction, and maintenance of drainage facilities, a temporary easement for construction purposes, permanent easements for drainage, and permanent easements to be conveyed to the city of San Diego for access and drainage purposes. Located in the city of San Diego near Airway Road. APN 646-131-14.

C-21099 - LBA Realty Fund III-Company I, LLC, a Delaware Limited Liability Company

11-SD-11-PM 1.11 - Parcel 34805-1, 2, 3, 4, 5, 6 - EA 056329.

RWC Date: 03/15/13; RTL Date: 03/25/13. Freeway - construct freeway to freeway connectors. Authorizes condemnation of land in fee for a State highway, extinguishment of abutter's rights of access, a temporary easement for construction purposes, a permanent easement for sewer purposes, a permanent easement for water line purposes, permanent easements for access, construction, and maintenance of retaining walls, footings, and drainage culverts, and permanent easements to be conveyed to the San Diego County Sanitation District and Otay Water District for utility purposes. Located in the city of San Diego at 1855 Dornoch Court. APN 646-131-09.

C-21100 - Airway Diego, LLC, a California limited liability company

11-SD-11-PM 1.2 - Parcel 34807-1, 2, 3, 4, 5 - EA 056329.

RWC Date: 03/15/13; RTL Date: 03/25/13. Freeway - construct freeway to freeway connectors. Authorizes condemnation of land in fee for a State highway, underlying fee, extinguishment of abutter's rights of access, permanent easements for access, utility, and sewer purposes, and a permanent easement for sewer purposes to be conveyed to the San Diego County Sanitation District. Located in the County of San Diego at 10132 Airway Road. APN 646-130-60-00.

C-21101 - Sean M. Murphy

11-SD-76-PM 13.5 - Parcel 34779-1, 2, 3 - EA 257159.

RWC Date: 8/15/13; RTL Date: 8/15/13. Conventional highway - widening and realignment. Authorizes condemnation of land in fee for a State highway, underlying fee, a permanent easement for drainage purposes, and a temporary easement for construction purposes. Located in the city of Fallbrook at 2818 Pala Road. APN 123-310-60.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.4d.  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Brent L. Green  
Chief  
Division of Right of Way  
and Land Surveys

Subject: **DIRECTOR'S DEEDS**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) authorize the execution of the Director's Deeds summarized below. The conveyance of excess State owned real property, including exchanges, is pursuant to Section 118 of the Streets and Highways Code.

The Director's Deeds, included in this item, involve an estimated current value of \$6,065,700. The State will receive a return of \$6,817,900 from the sale of these properties. A recapitulation of the items presented and corresponding maps are attached.

## **ISSUE:**

<b>01-</b> 01-Hum-101 PM 79.14	Eureka
Disposal Unit #DE 7910-2	144 square feet
DE 7910-3	606 square feet
DE 11185-2	614 square feet
Convey to: City of Eureka	\$0 (Appraisal N/A)

Direct conveyance for no monetary consideration. Department acquired fee property from the city for the realignment of northbound US Route 101 at the intersection of 5<sup>th</sup> and R streets in the city of Eureka. The acquired property contained public street and city utility facilities. The Department's project is complete and easements can be conveyed to city for operation and maintenance of its facilities.

<b>02-</b> 03-Sac-80 PM 2.8	Sacramento
Disposal Unit #DD 17601-01-01	0.22 acre
Convey to: Lukas & Georgia Konandreas	\$6,500 (\$6,500 Appraisal)

Direct sale. Direct sale at appraised value to adjoining owner as a result of a decertification. The property is narrow, linear and encumbered with multiple easements. The property is incapable of independent development and the highest and best use is as plottage to adjoining property.

**03-04-Ala-580 PM 28.78** Castro Valley  
Disposal Unit #DD 030163-01-01 1.68 acres  
Convey to: Castro Valley Sanitary District \$1,500,000  
(\$1,500,000 Appraisal)  
Direct sale at the appraised value to a local public agency for a government office building.

**04-04-Ala-238 PM 12.9** Hayward  
Disposal Unit #DD 032702-01-01 0.27 acre  
Convey to: Xiaoqing Ma \$359,000  
A Married Woman as her sole (\$310,000 Public sale estimate)  
and separate property  
Public sale. Selling price represents the highest bid received at the public sale

**05-04-Ala-238 PM 12.6** Hayward  
Disposal Unit #DD 033888-01-01 0.29 acre  
Convey to: Maryam Omer \$408,000  
(\$325,000 Public sale estimate)  
Public sale. Selling price represents the highest bid received at the first public sale. There were four active bidders.

**06-04-Ala-238 PM 10.2** Hayward  
Disposal Unit #DD 039105-01-01 0.39 acre  
Convey to: Mehdavi Automotive, Inc. \$321,800  
(\$321,800 Appraisal)  
Direct Sale to commercial tenant per Section 118.1 of the Streets and Highway Code. Selling price represents the appraised value for the subject property.

**07-04-Ala-238 PM 13.5** Hayward  
Disposal Unit #DD 039384-01-01 0.20 acre  
Convey to: VVT Builders LLC \$150,000  
(\$100,000 Public sale estimate)  
Public sale. Selling price represents the highest oral bid received at the first public sale.

**08-04-Ala-238 PM 13.5** Hayward  
Disposal Unit #DD 039396-01-01 0.19 acre  
Convey to: Jaswant S. Nahal & Sharnjit K. Nahal \$110,000  
(\$60,000 Public sale estimate)  
Public sale. Selling price represents the highest oral bid received at the first public sale.

**09-04-CC-680 PM 17.3** Pleasant Hill  
Disposal Unit #DD 048706-01-01 0.08 acre  
Convey to: Fjeld Family Limited Partnership \$50,000  
(\$50,000 Appraisal)

Direct sale. Selling price represents appraised value received from the only adjoining owner. The property is irregularly shaped, encumbered with a pole line easement, and the highest and best use is as plottage to adjoining owner.

**10-04-Mrn-101 PM 11.5** San Rafael  
Disposal Unit #DD 054617-01-01 0.23 acre  
Convey to: Sonora Group, LLC \$1,856,200  
(\$1,500,000 Public sale estimate)

Public sale. Sale price represents the highest sealed bid received at public sale.

**11-04-Mrn-101 PM 11.9** San Rafael  
Disposal Unit #DD 054635-01-01 0.24 acre  
Convey to: Martin Neville \$1,625,000  
(\$1,600,000 Public sale estimate)

Public sale. Sale price represents the highest bid received at public sale.

**12-04-SCI-880 PM 3.5** San Jose  
Disposal Unit #DD 052511-01-01 0.11 acre  
Convey to: Community Partnership LLC \$399,000  
(\$250,000 Public sale estimate)

Public sale. Selling price represents the highest bid received at the public sale.

**13-05-SLO-41 PM R16.4** Atascadero  
Disposal Unit #DD 005463-01-01 0.608 acre  
Convey to: Kenneth A. Isola, et al \$30,000  
(\$40,000 Public sale estimate)

Public Sale. Selling price is the highest bid received from the second competitive sale. There was one registered bidder.

**14-07-LA-5 PM 9.6** Commerce  
Disposal Unit #DE 980573-1 0.04 acre (permanent sewer easement)  
#DE 980573-2 0.49 acre (temporary easement)  
Convey to: County Sanitation District #2 \$2,400  
of Los Angeles County (\$2,400 Appraisal)

Direct Sale. Subject property consists of 0.04 acre (1,757 SF) permanent sewer line easement and a 0.49 acre (21,596 SF) temporary construction easement (TCE) to expire on December 31, 2013.

**15-11-SD-52** PM 15.9 & 16.9

Disposal Unit #DK 033292-5

#DK 031397-2

Convey to: Padre Dam Municipal Water District

Santee

0.03 acre

0.18 acre

\$0

(Appraisal N/A)

Direct Conveyance for no monetary consideration is 100 percent State's obligation pursuant to Utility Agreement Nos. 31919 and 31920 dated 10/15/08.

Attachments

**SUMMARY OF DIRECTOR'S DEEDS - 2.4d.**

**PRESENTED TO CALIFORNIA TRANSPORTATION COMMISSION - August 6, 2013**

Table I - Volume by Districts

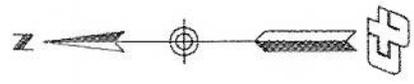
District	Direct Sales	Public Sales	Non-Inventory Conveyances	Other Funded Sales	Total Items	Current Estimated Value	Return From Sales	Recovery %
								% Return From Sales Current Value
01	1				1	\$0.00	\$0.00	n/a
02								
03	1				1	\$6,500.00	\$6,500.00	100%
04	3	7			10	\$6,016,800.00	\$6,779,000.00	113%
05		1			1	\$40,000.00	\$30,000.00	75%
06								
07	1				1	\$2,400.00	\$2,400.00	100%
08								
09								
10								
11	1				1	\$0.00	\$0.00	n/a
12								
<b>Total</b>	<b>7</b>	<b>8</b>			<b>15</b>	<b>\$6,065,700.00</b>	<b>\$6,817,900.00</b>	<b>112%</b>

Table II - Analysis by Type of Sale

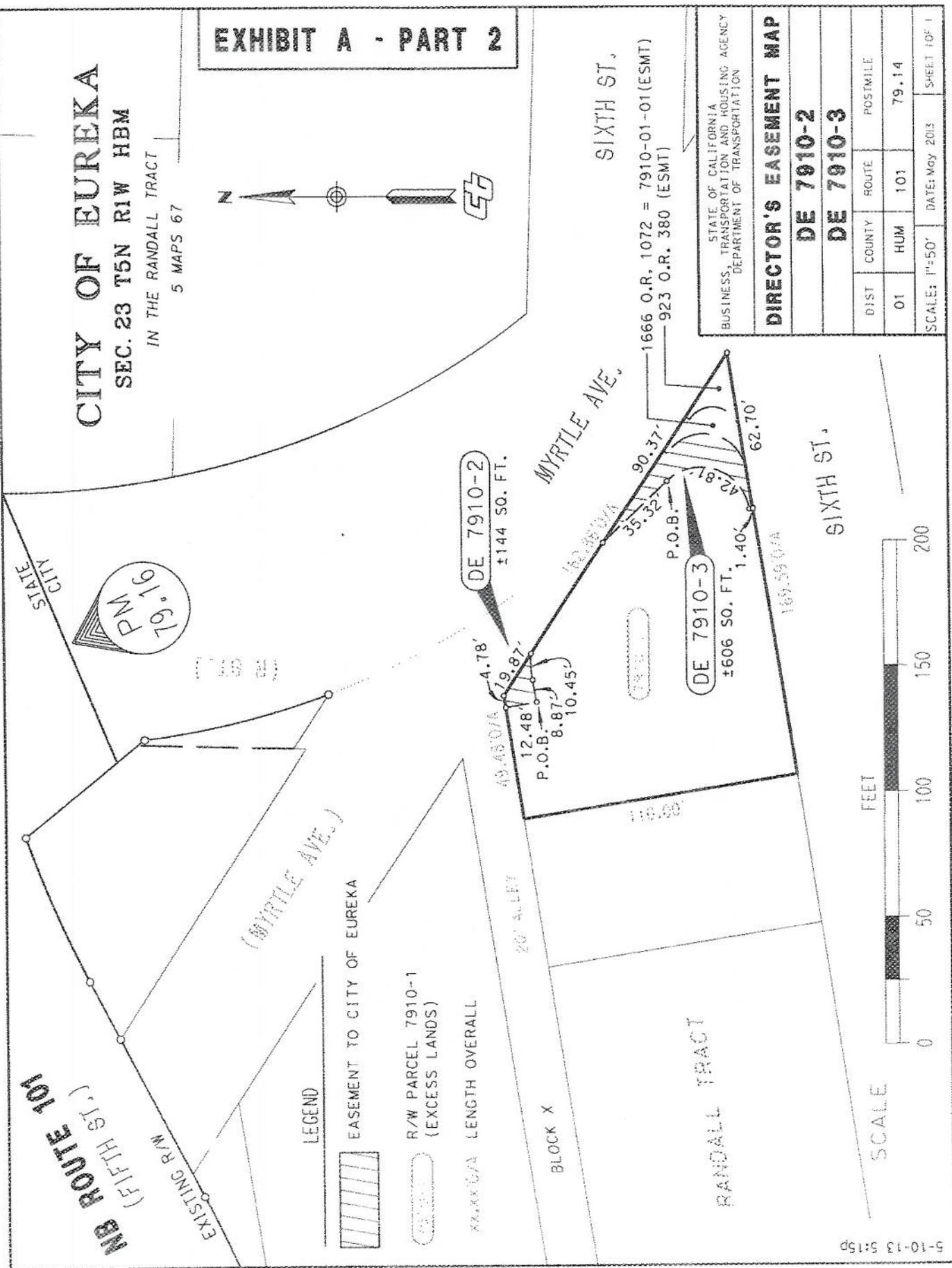
Type of Sale	# of Items	Current Estimated Value	Return From Sales	Recovery %
				% Return From Sales Current Value
Direct Sales	7	\$1,880,700.00	\$1,880,700.00	100%
Public Sales	8	\$4,185,000.00	\$4,937,200.00	118%
Non-Inventory Conveyances				
<b>Sub-Total</b>	<b>15</b>	<b>\$6,065,700.00</b>	<b>\$6,817,900.00</b>	<b>112%</b>
Other Funded Sales	0			
<b>Total</b>	<b>15</b>	<b>\$6,065,700.00</b>	<b>\$6,817,900.00</b>	<b>112%</b>

**EXHIBIT A - PART 2**

**CITY OF EUREKA**  
**SEC. 23 T5N R1W HBM**  
 IN THE RANDALL TRACT  
 5 MAPS 67



STATE OF CALIFORNIA BUSINESS, TRANSPORTATION AND HOUSING AGENCY DEPARTMENT OF TRANSPORTATION			
<b>DIRECTOR'S EASEMENT MAP</b>			
<b>DE 7910-2</b>			
<b>DE 7910-3</b>			
DIST	COUNTY	ROUTE	POSTMILE
01	HUM	101	79.14
SCALE: 1"=50'			DATE: May 2013
			SHEET 1 OF 1



5-10-13 5:15p

**EXHIBIT 8 - PART 2**

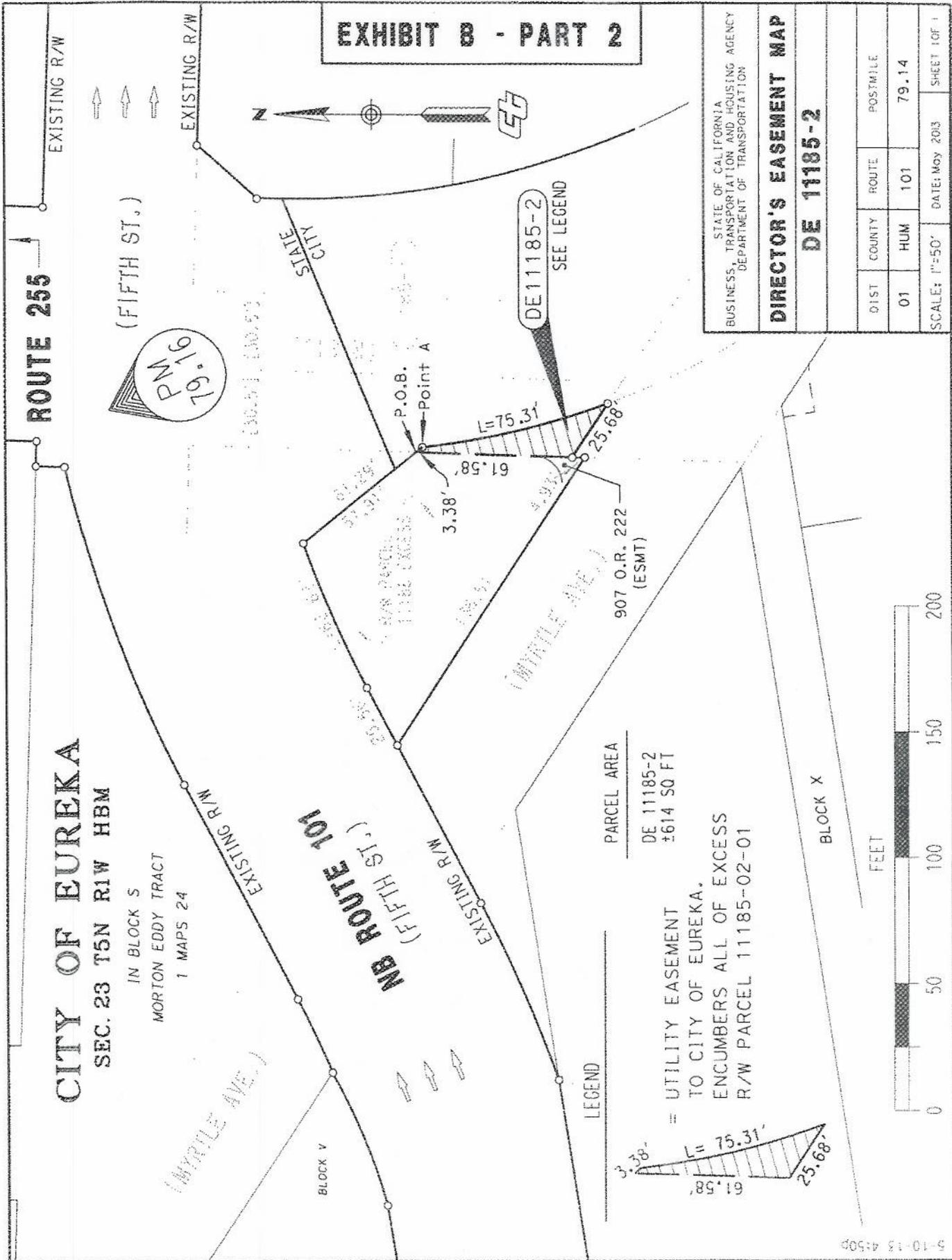
STATE OF CALIFORNIA  
BUSINESS, TRANSPORTATION AND HOUSING AGENCY  
DEPARTMENT OF TRANSPORTATION

**DIRECTOR'S EASEMENT MAP**

**DE 11185-2**

DIST	COUNTY	ROUTE	POSTMILE
01	HUM	101	79.14

SCALE: 1"=50' DATE: Mgy 2013 SHEET 1 OF 1



**CITY OF EUREKA**

SEC. 23 T5N R1W HBM

IN BLOCK S  
MORTON EDDY TRACT  
1 MAPS 24

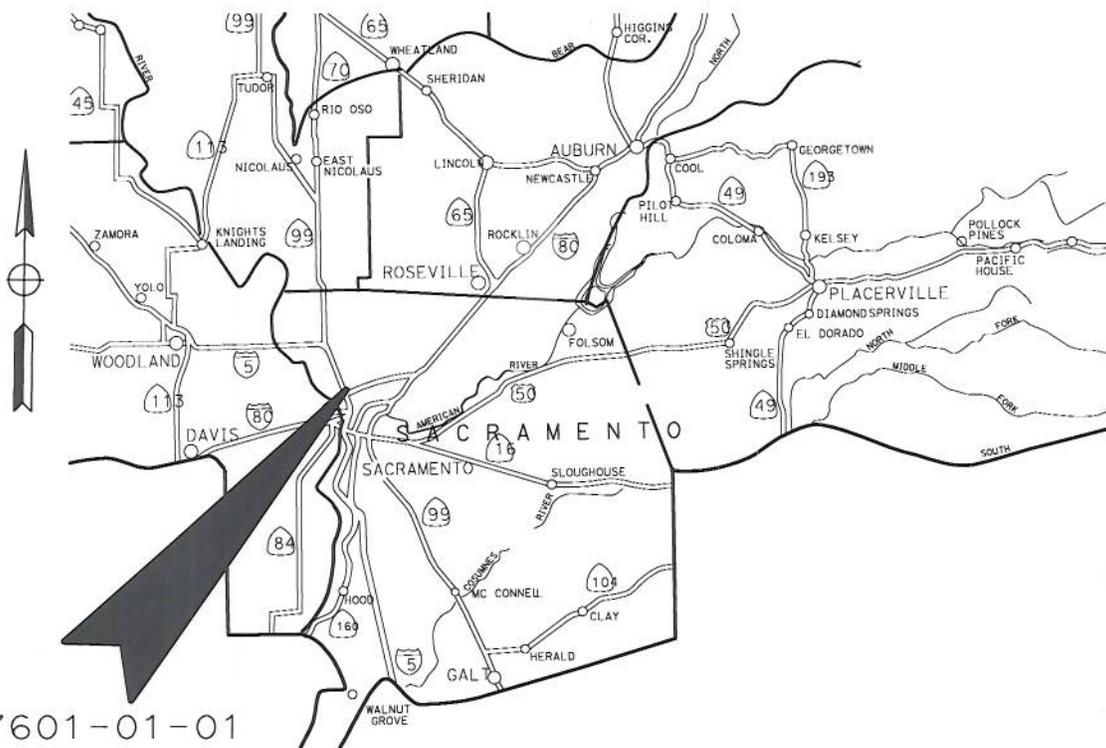
**NB ROUTE 101**  
(FIFTH ST.)

LEGEND  
= UTILITY EASEMENT  
TO CITY OF EUREKA,  
ENCUMBERS ALL OF EXCESS  
R/W PARCEL 11185-02-01

PARCEL AREA  
DE 11185-2  
±614 SQ FT

5-10-13 4:50p

CITY OF SACRAMENTO  
 SACRAMENTO COUNTY  
 SAC 80 NEAR SAC 5  
 AT SAN JUAN RD.  
 & AIRPORT RD.



DD-17601-01-01

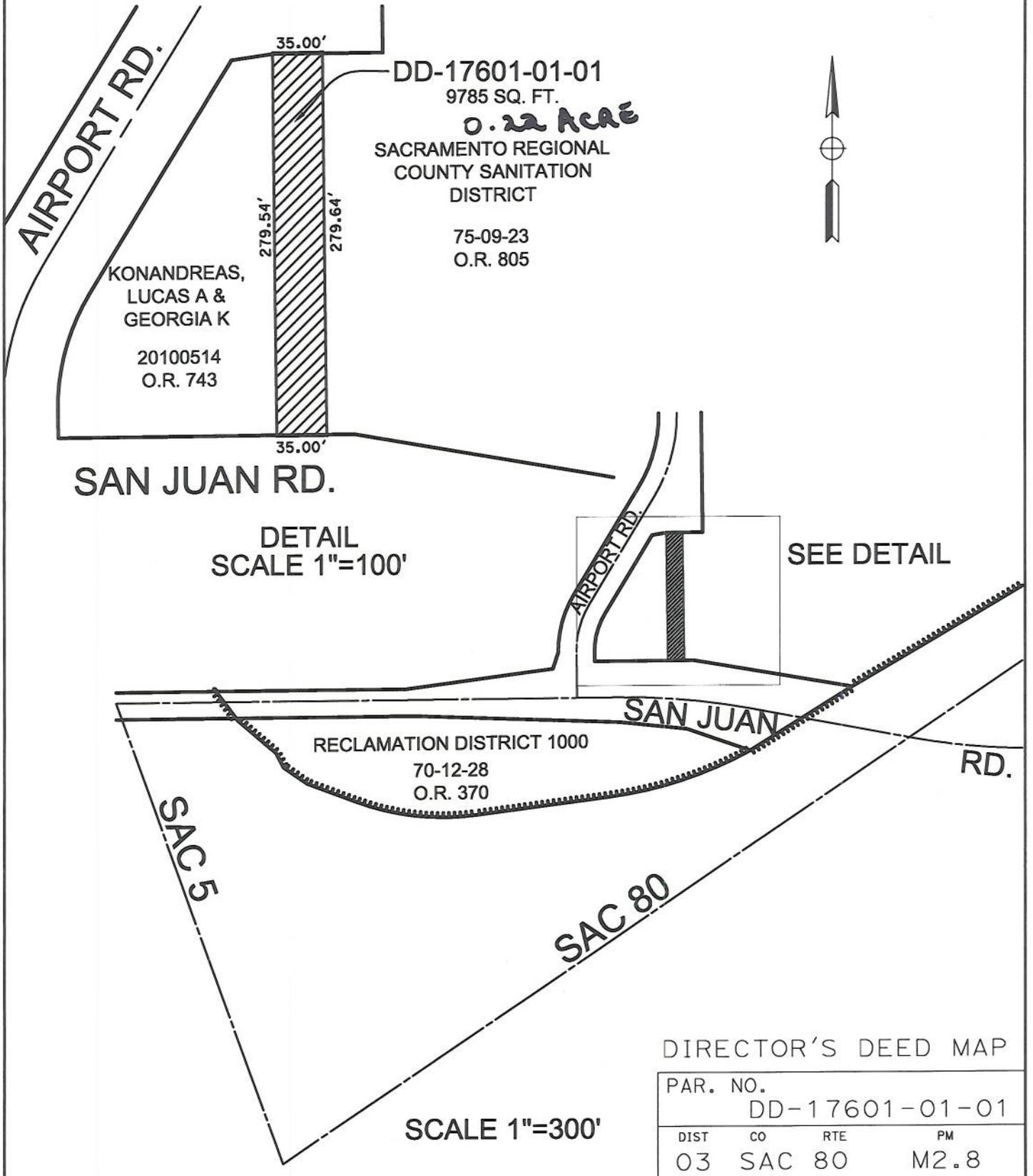
LOCATION MAP

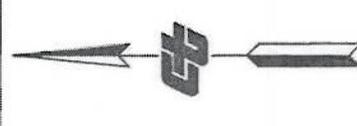
NOT TO SCALE

DIRECTOR'S DEED MAP

PAR. NO.			
DD-17601-01-01			
DIST	CO	RTE	PM
03	SAC	80	M2.8

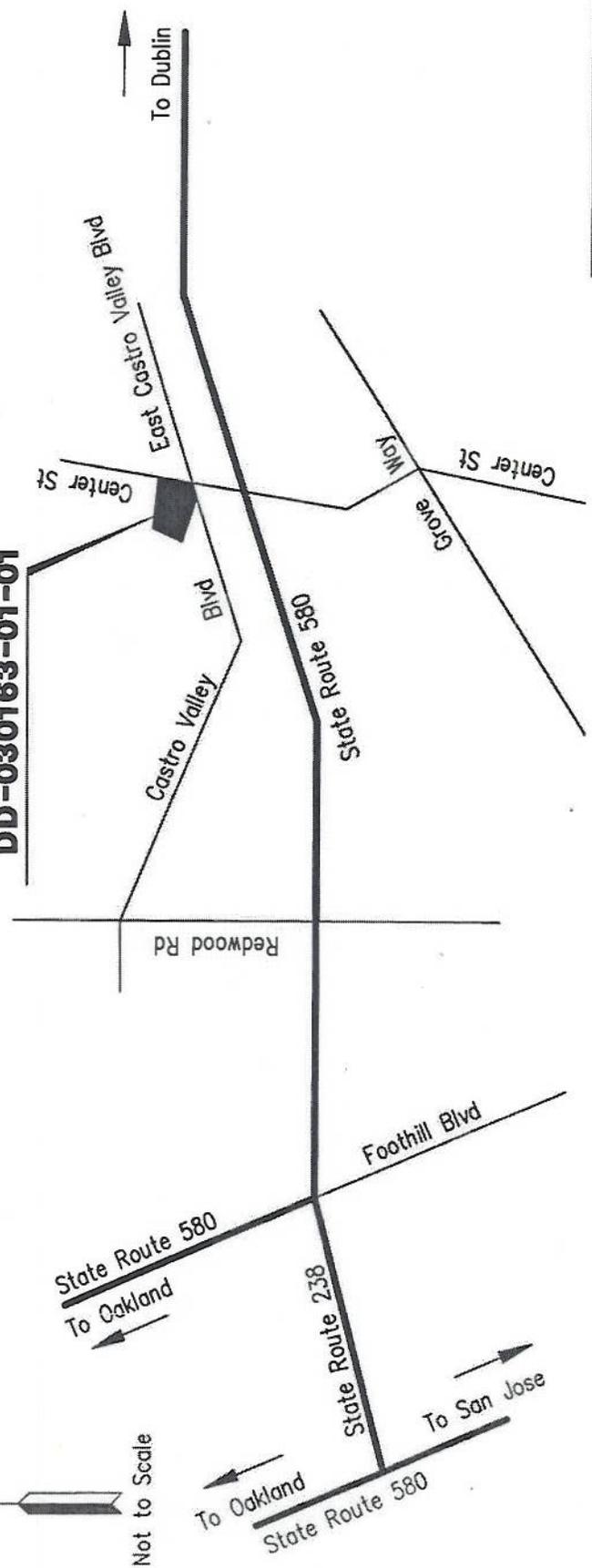
CITY OF SACRAMENTO  
T9N, R4E, MDM  
NATOMAS CENTRAL SUBDIVISION





Not to Scale

**DD-030163-01-01**



# Alameda County

STATE OF CALIFORNIA  
 BUSINESS, TRANSPORTATION  
 AND HOUSING AGENCY  
 DEPARTMENT OF TRANSPORTATION  
 DISTRICT 4

**DIRECTOR'S DEED**  
**DD-030163-01-01**

DRAWN BY: MAL	DATE: 2-19-13	
CHECK BY: PD	SCALE: None	
COUNTY ROUTE	P.M.	DR.NO.
ALA 580	28.78	1 of 2

- A-892.6a
- R-93.2
- R-93x.13
- R-51.10
- H-93.4

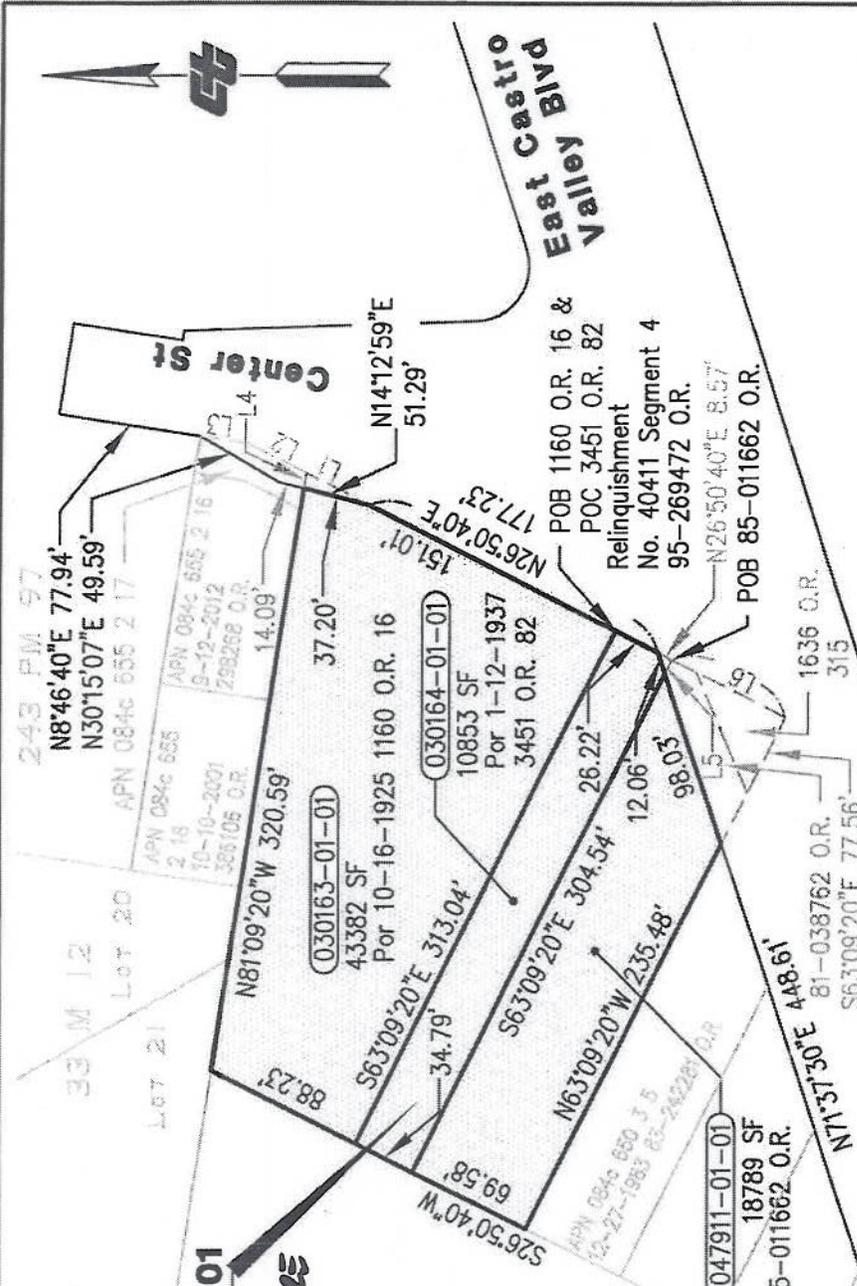
DISTANCES & BEARINGS SHOWN ARE ON THE CALIFORNIA COORDINATE SYSTEM OF 1927, ZONE 3. MULTIPLY DISTANCES SHOWN BY 1.0000759 TO OBTAIN GROUND LEVEL DISTANCES.

**DD-030163-01-01**

73,024 SF Total

**1.68 ACRES**

Line Table		
Line #	Direction	Length
L1	S26°50'40"W	38.94'
L2	S26°50'40"W	35.27'
L3	N8°46'40"E	26.65'
L4	S81°09'20"E	8.55'
L5	S63°09'20"E	8.50'
L6	N26°50'40"E	69.58'



STATE OF CALIFORNIA  
 BUSINESS, TRANSPORTATION  
 AND HOUSING AGENCY  
 DEPARTMENT OF TRANSPORTATION  
 DISTRICT 4

**DIRECTOR'S DEED**  
**DD-030163-01-01**

DRAWN BY: MAL DATE: 2-19-13  
 CHECK BY: PD SCALE: 1"=100'  
 COUNTY ROUTE P.M. DR.NO.  
 ALA 580 28.78 2 of 2

**LEGEND**

- APN Assessor's Parcel #
- O.R. Official Records
- SF Square Feet
- POB Point of Beginning
- POC Point of Commencement
- Por Portion of

SCALE IN FEET



**Alameda County**

**Castro Valley Blvd**

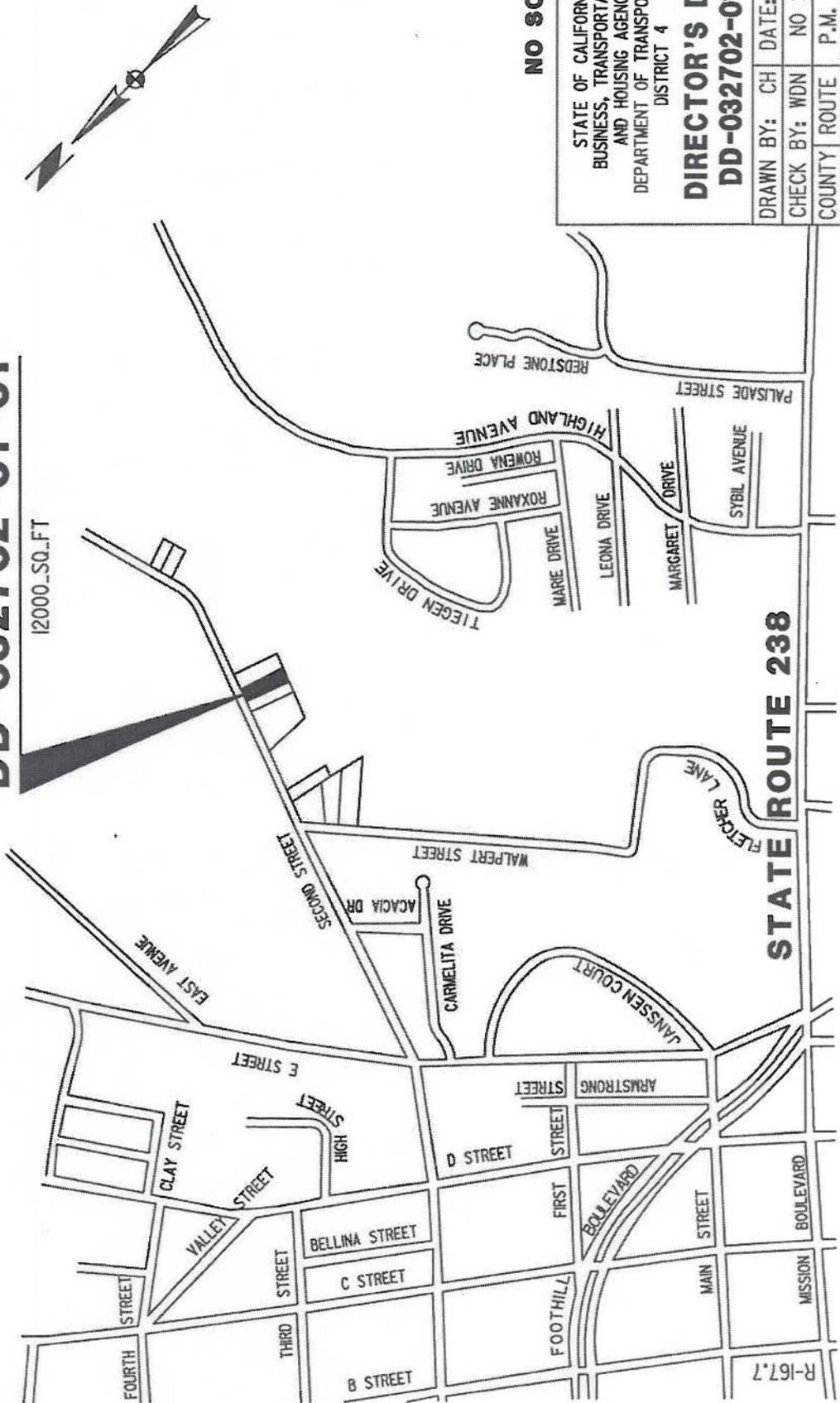
$R=748.00' \Delta=40^{\circ}52'44'' L=533.68'$

A-892.60  
 R-93.2  
 R-93X.13  
 R-51.10  
 H-93.4

# ALAMEDA COUNTY CITY OF HAYWARD

**DD-032702-01-01**

12000\_SQ\_FT



**NO SCALE**

STATE OF CALIFORNIA  
BUSINESS, TRANSPORTATION  
AND HOUSING AGENCY  
DEPARTMENT OF TRANSPORTATION  
DISTRICT 4

**DIRECTOR'S DEED**  
**DD-032702-01-01**

DRAWN BY:	CH	DATE:	6/19/12
CHECK BY:	WDN	NO SCALE	
COUNTY ROUTE	P.M.	DR.NO.	
ALA	238	12.9	1 OF 2

**STATE ROUTE 238**

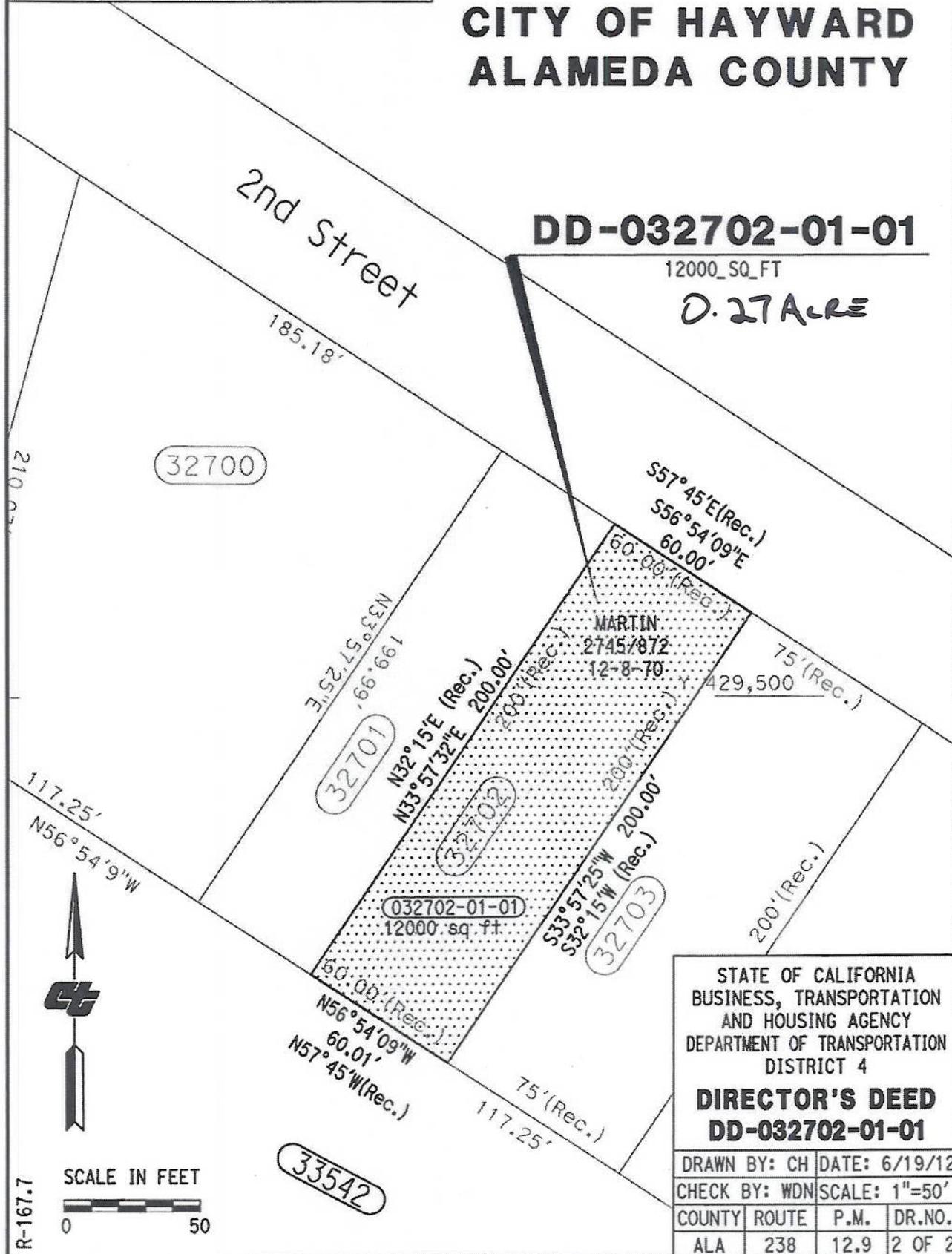
R-167.7

DISTANCES SHOWN ARE ON THE CALIFORNIA COORDINATE SYSTEM OF 1927, ZONE 3. MULTIPLY DISTANCES SHOWN BY 1.0000787 TO OBTAIN GROUND LEVEL DISTANCES.

# CITY OF HAYWARD ALAMEDA COUNTY

**DD-032702-01-01**

12000\_SQ\_FT  
**0.27 Acre**



STATE OF CALIFORNIA  
BUSINESS, TRANSPORTATION  
AND HOUSING AGENCY  
DEPARTMENT OF TRANSPORTATION  
DISTRICT 4

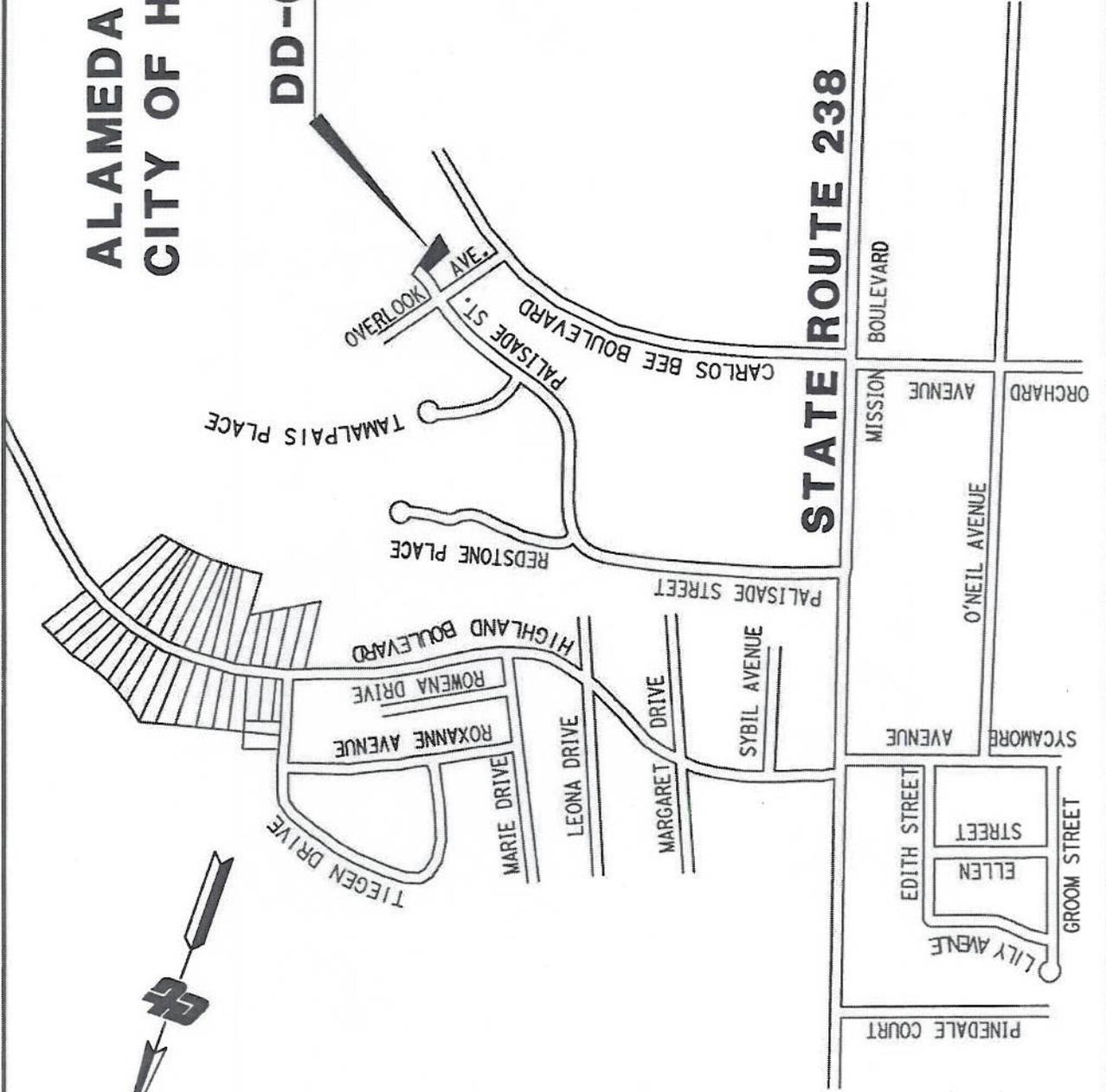
**DIRECTOR'S DEED  
DD-032702-01-01**

DRAWN BY: CH		DATE: 6/19/12	
CHECK BY: WDN		SCALE: 1"=50'	
COUNTY	ROUTE	P.M.	DR.NO.
ALA	238	12.9	2 OF 2

**ALAMEDA COUNTY  
CITY OF HAYWARD**

**DD-033888-01-01**

12829\_SQ\_FT



STATE OF CALIFORNIA BUSINESS, TRANSPORTATION AND HOUSING AGENCY DEPARTMENT OF TRANSPORTATION DISTRICT 4			
<b>DIRECTOR'S DEED</b>		DRAWN BY: CH	DATE: 12/30/11
<b>DD-033888-01-01</b>		CHECK BY: WDN	SCALE: NTS
COUNTY	ROUTE	P.M.	DR.NO.
ALA	238	12.6	1 OF 2

R-97A.00

**ALAMEDA COUNTY  
CITY OF HAYWARD**



DISTANCES SHOWN ARE ON THE CALIFORNIA  
COORDINATE SYSTEM OF 1927, ZONE 3.  
MULTIPLY DISTANCES SHOWN BY 1.0000787  
TO OBTAIN GROUND LEVEL DISTANCES.

TRACT 1801  
38 MAPS 43-44

**PALISADE ST.**

**OVERLOOK AVE.**

1  
(AF33529)

BLOCK  
D

(AF33876)

4

(33887)

BLOCK  
A

5

(033888-01-01)  
12829 50 FT

(33888)

2809 OR 142  
3-18-71

3  
(33886)

2

1  
(33885)

[N2°42'11"W 221.29']

L=68'+/-

133'+/-

165'+/-

111'+/-

**DD-033888-01-01**

12829\_50\_FT

*D. 29 Acres*

STATE OF CALIFORNIA  
BUSINESS, TRANSPORTATION  
AND HOUSING AGENCY  
DEPARTMENT OF TRANSPORTATION  
DISTRICT 4

**DIRECTOR'S DEED  
DD-033888-01-01**

DRAWN BY: CH DATE: 12/30/11

CHECK BY: WDN SCALE: 1"=50'

COUNTY ROUTE P.M. DR.NO.

ALA 238 12.6 2 OF 2

N68°39'59"W 20.22'  
[N68°33'33"W 20.00']

LEGEND

[ ] PER RW RECORD MAP  
R-97A.21

SCALE IN FEET



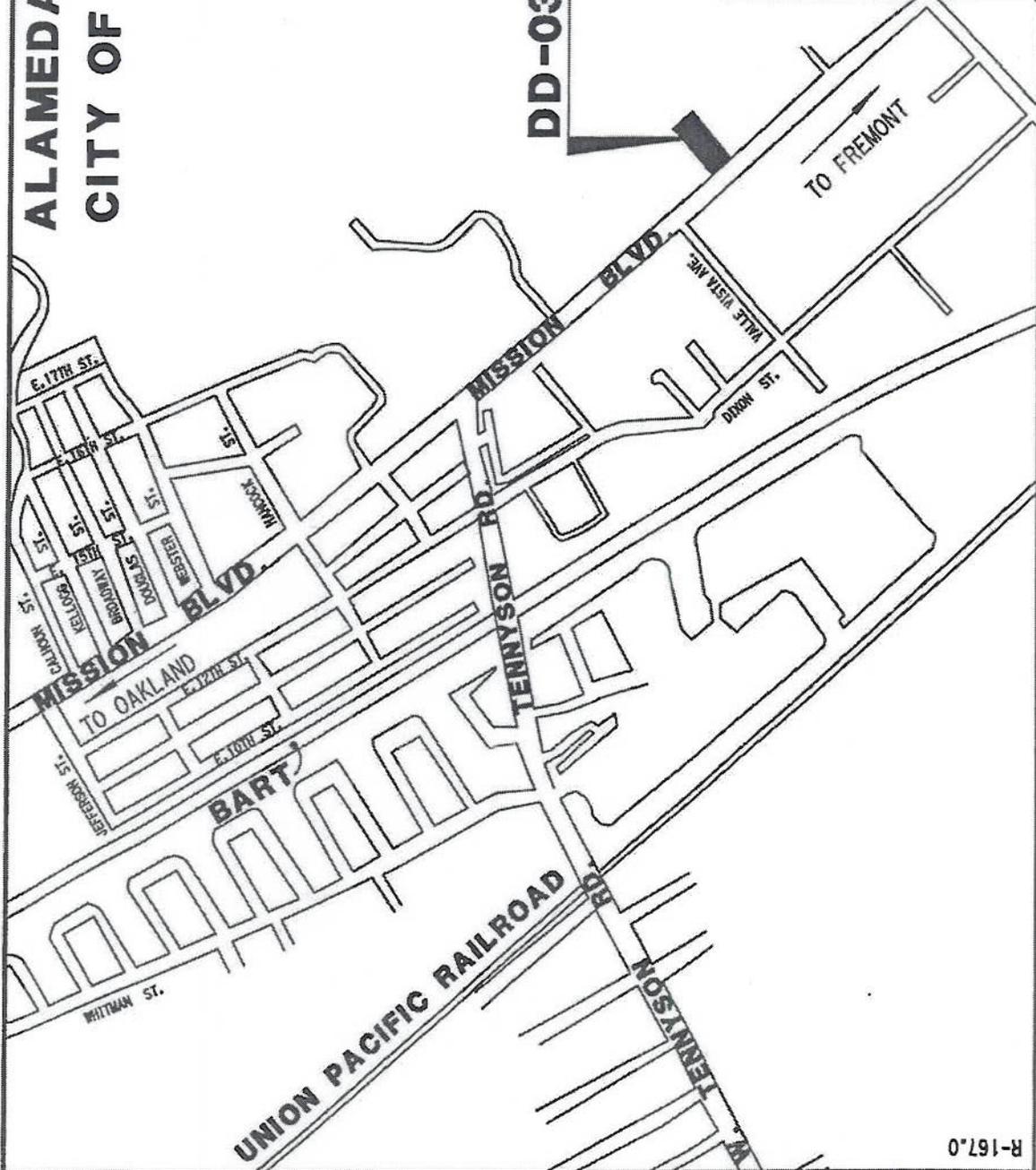
R-97A.21

**ALAMEDA COUNTY  
CITY OF HAYWARD**



Not to Scale

**DD-039105-01-01**



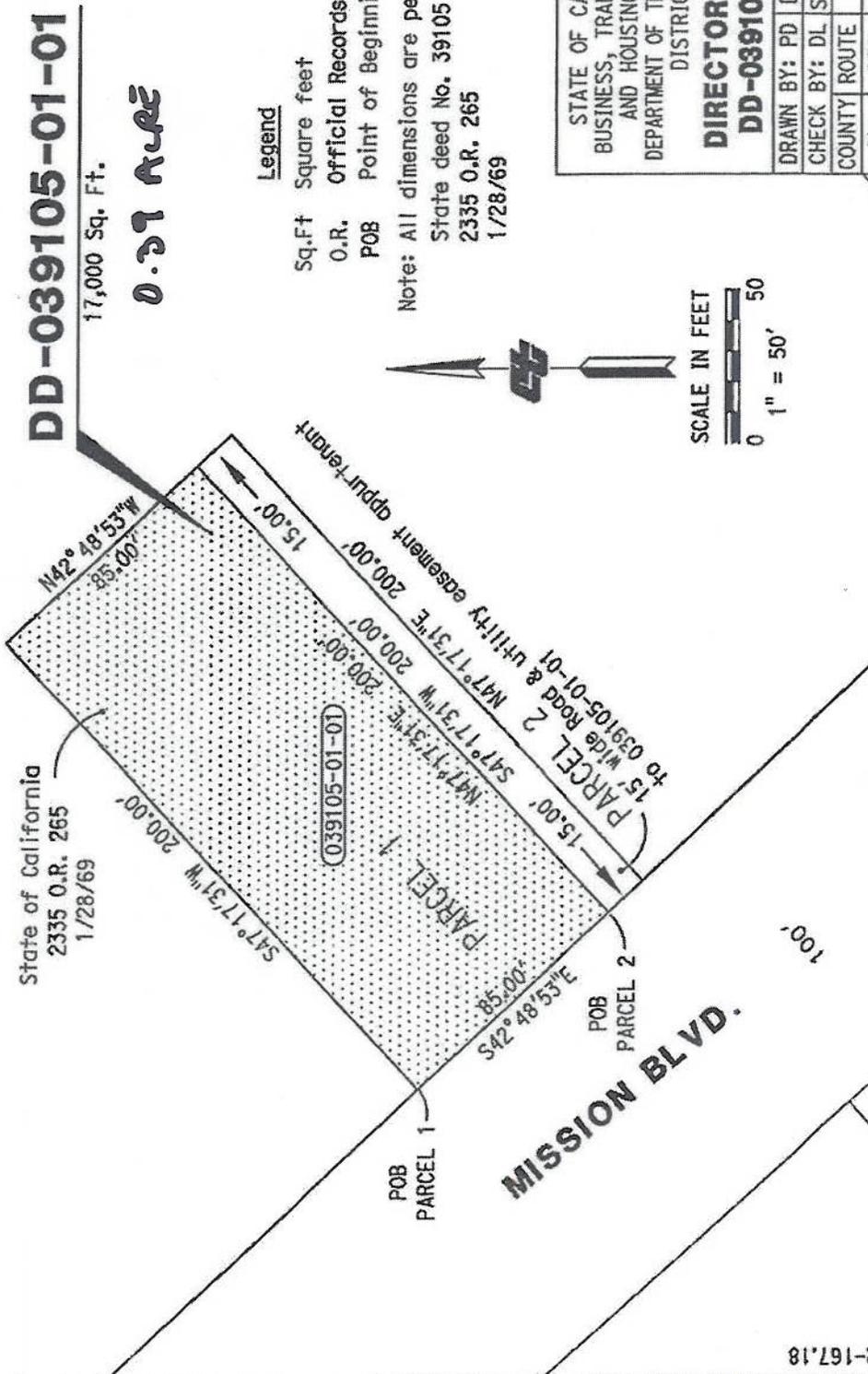
STATE OF CALIFORNIA  
BUSINESS, TRANSPORTATION  
AND HOUSING AGENCY  
DEPARTMENT OF TRANSPORTATION  
DISTRICT 4

**DIRECTOR'S DEED  
DD-039105-01-01**

DRAWN BY: PD	DATE: 9/7/12
CHECK BY: DL	SCALE: NTS
COUNTY ROUTE	P.M. DR.NO.
A10 238	10.2 1 of 2

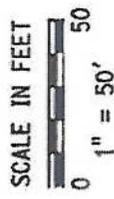
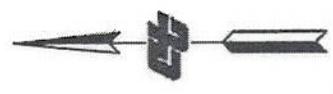
R-167.0

# ALAMEDA COUNTY CITY OF HAYWARD



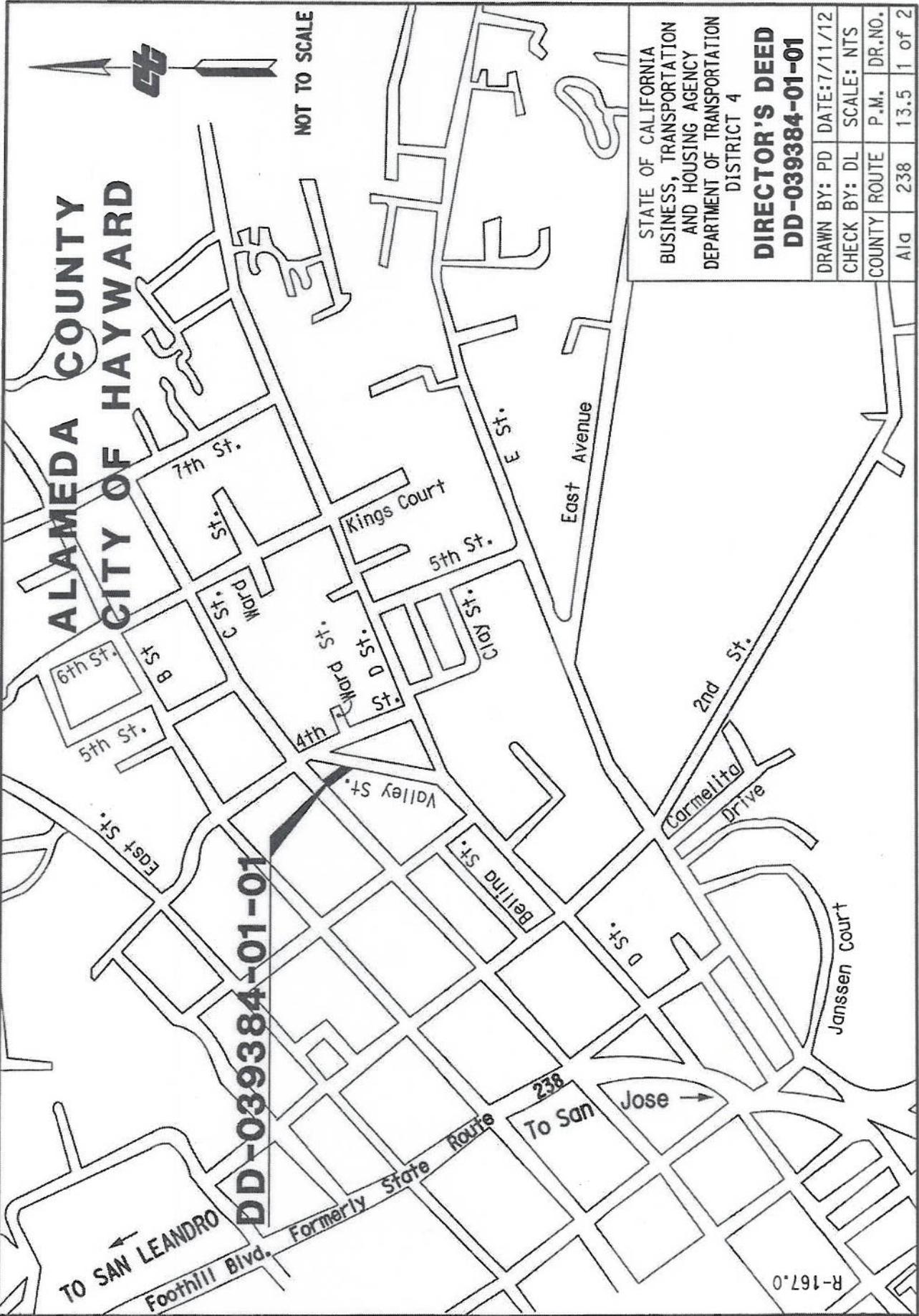
**Legend**  
 Sq.Ft Square feet  
 O.R. Official Records  
 POB Point of Beginning

Note: All dimensions are per  
 State deed No. 39105  
 2335 O.R. 265  
 1/28/69



STATE OF CALIFORNIA BUSINESS, TRANSPORTATION AND HOUSING AGENCY DEPARTMENT OF TRANSPORTATION DISTRICT 4			
<b>DIRECTOR'S DEED</b> <b>DD-039105-01-01</b>			
DRAWN BY: PD	DATE: 9/7/12		
CHECK BY: DL	SCALE: 1"=50'		
COUNTY ROUTE	P.M.	DR.NO.	
Ala	238	10.2	2 of 2

R-167.18



**ALAMEDA COUNTY  
CITY OF HAYWARD**

NOT TO SCALE

STATE OF CALIFORNIA  
BUSINESS, TRANSPORTATION  
AND HOUSING AGENCY  
DEPARTMENT OF TRANSPORTATION  
DISTRICT 4

**DIRECTOR'S DEED  
DD-039384-01-01**

DRAWN BY: PD	DATE: 7/11/12
CHECK BY: DL	SCALE: NTS
COUNTY ROUTE	P.M. DR.NO.
Alc 238	13.5 1 of 2

**DD-039384-01-01**

TO SAN LEANDRO

Formerly State Route 238  
To San Jose

R-167.0

**ALAMEDA COUNTY  
CITY OF HAYWARD**

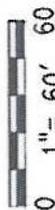
**DD-039384-01-01**

8,592 Sq. Ft.

*0.20 Acs*



SCALE IN FEET



LEGEND

- Sq. Ft. square feet
- O.R. Official Records
- POB point of beginning
- R1 Record data per 2445 O.R. 295

R-167.5

4TH ST.

C ST.

60'

150' R1

STATE OF CALIFORNIA  
POB 2445 O.R. 295  
7/23/69

40'

4TH ST.

VALLEY ST.

60'

56.67' R1

154.025' R1

193.45' R1

STATE OF CALIFORNIA BUSINESS, TRANSPORTATION AND HOUSING AGENCY DEPARTMENT OF TRANSPORTATION DISTRICT 4			
<b>DIRECTOR'S DEED</b>		<b>DD-039384-01-01</b>	
DRAWN BY: PD	DATE: 7/11/12	CHECK BY: DL	SCALE: 1"=60'
COUNTY ROUTE	P.M.	DR.NO.	
A10	238	13.5	2 of 2



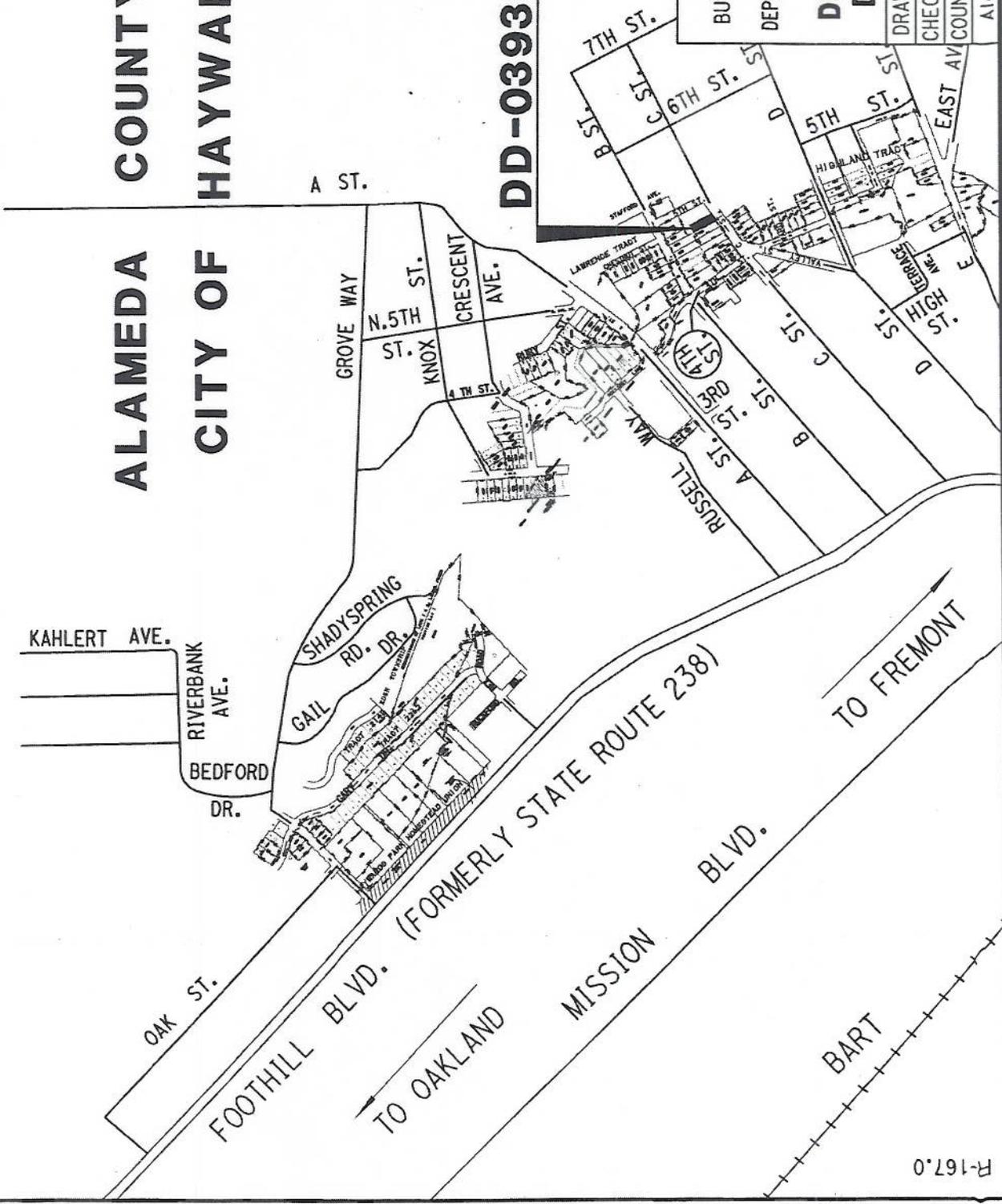
**ALAMEDA COUNTY**  
**CITY OF HAYWARD**

**DD-039396-01-01**

STATE OF CALIFORNIA  
BUSINESS, TRANSPORTATION  
AND HOUSING AGENCY  
DEPARTMENT OF TRANSPORTATION  
DISTRICT 4

**DIRECTOR'S DEED**  
**DD-039396-01-01**

DRAWN BY: PD	DATE: 10/4/11
CHECK BY: WDN	SCALE: None
COUNTY ROUTE	P.M. DR.NO.
Ala 238	13.5 1 of 2



R-167.0

**ALAMEDA COUNTY  
CITY OF HAYWARD**

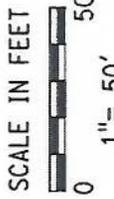
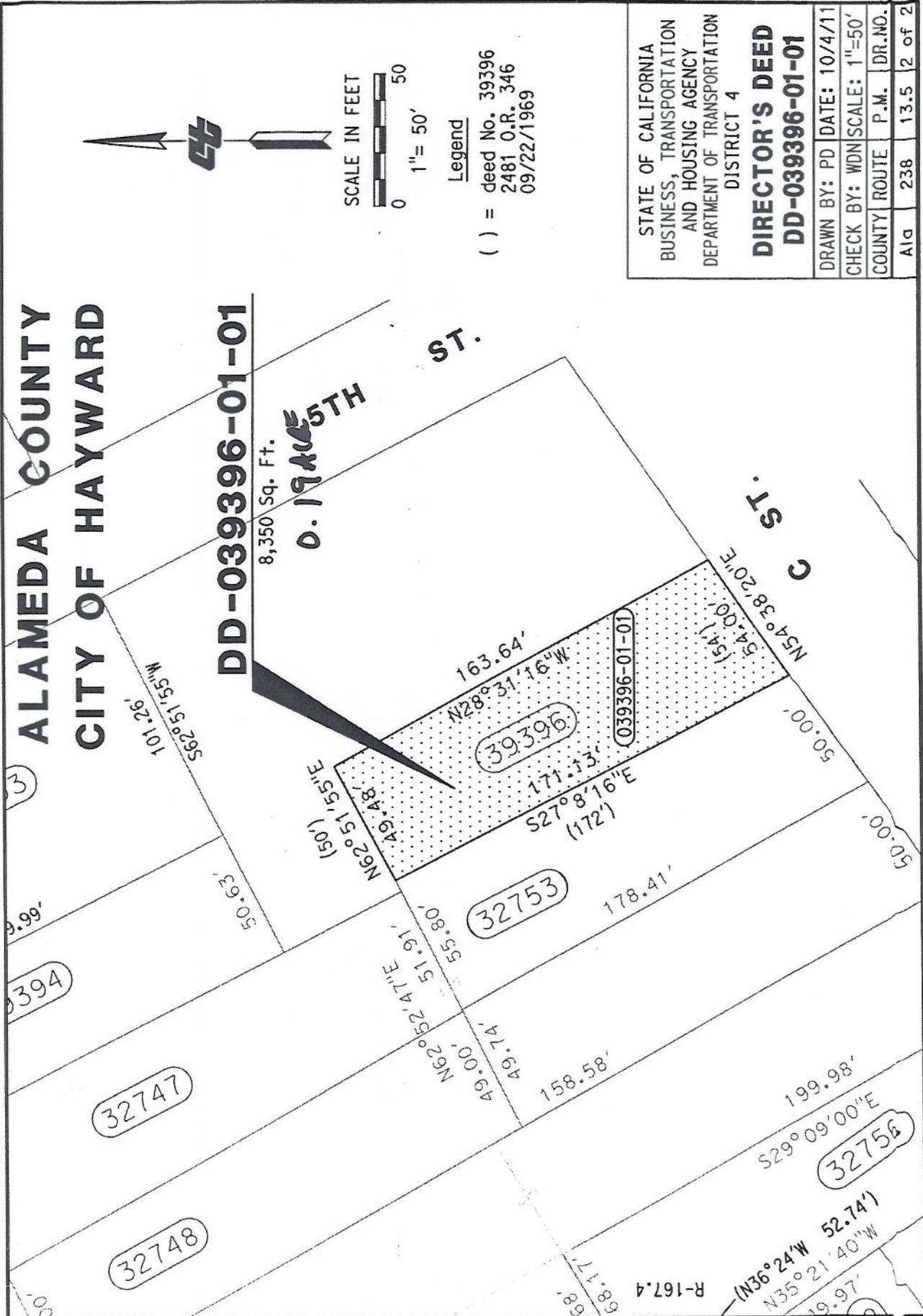
**DD-039396-01-01**

8,350 Sq. Ft.

**0.19 ACRES**

**ST.**

**C ST.**



Legend

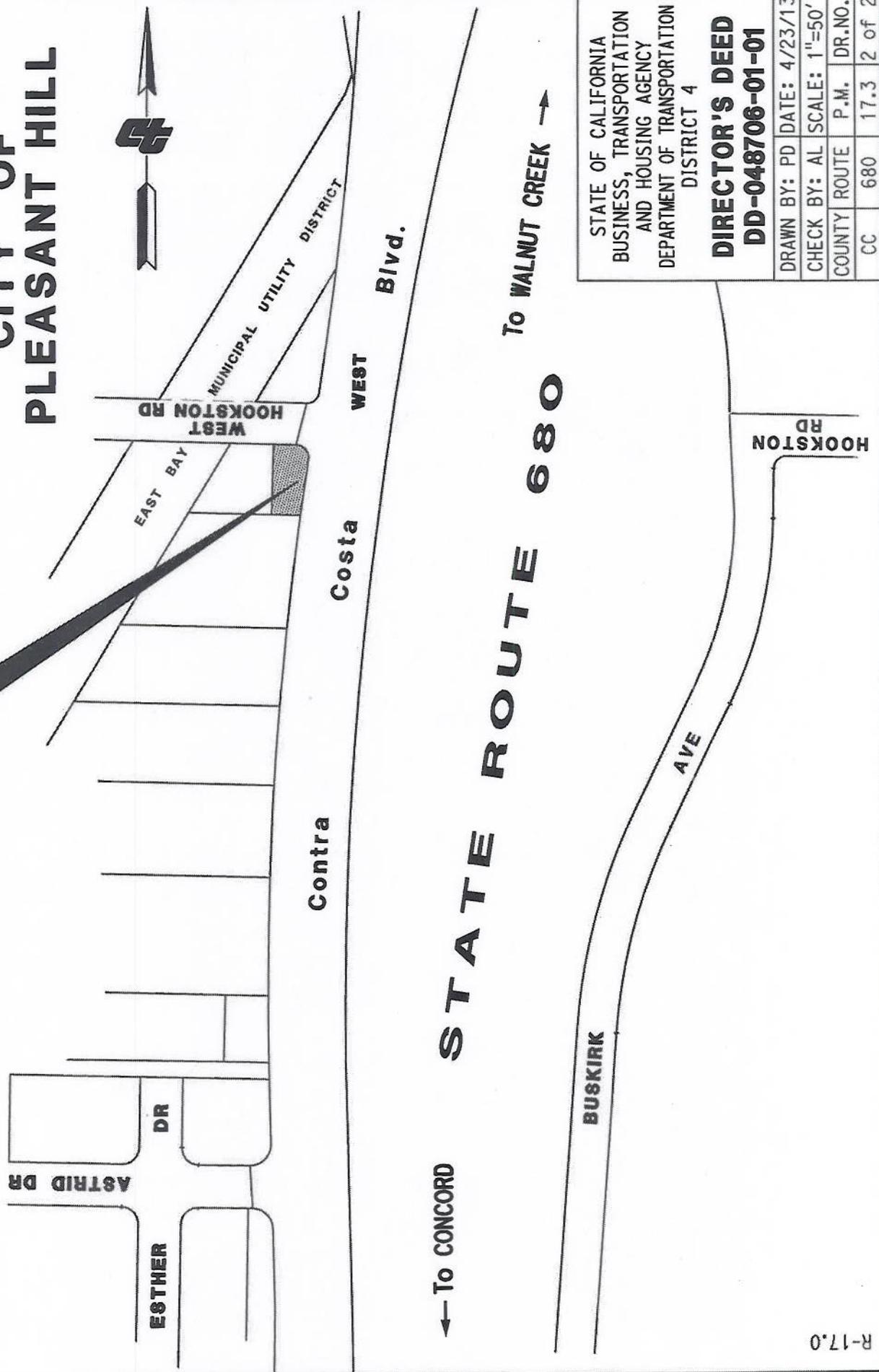
( ) = deed No. 39396  
2481 O.R. 346  
09/22/1969

STATE OF CALIFORNIA  
BUSINESS, TRANSPORTATION  
AND HOUSING AGENCY  
DEPARTMENT OF TRANSPORTATION  
DISTRICT 4

**DIRECTOR'S DEED  
DD-039396-01-01**

DRAWN BY: PD	DATE: 10/4/11
CHECK BY: WDN	SCALE: 1"=50'
COUNTY ROUTE	P.M. DR.NO.
Alc 238	13.5 2 of 2

**DD-048706-01-01 CONTRA COSTA COUNTY  
CITY OF PLEASANT HILL**



STATE OF CALIFORNIA  
BUSINESS, TRANSPORTATION  
AND HOUSING AGENCY  
DEPARTMENT OF TRANSPORTATION  
DISTRICT 4

**DIRECTOR'S DEED  
DD-048706-01-01**

DRAWN BY: PD	DATE: 4/23/13
CHECK BY: AL	SCALE: 1"=50'
COUNTY ROUTE	P.M. DR.NO.
CC 680	17.3 2 of 2

R-17.0

# CONTRA COSTA COUNTY CITY OF PLEASANT HILL

BEARINGS & DISTANCES SHOWN ARE ON THE CALIFORNIA COORDINATE SYSTEM OF 1927, ZONE 3. MULTIPLY DISTANCES SHOWN BY 1.0000746 TO OBTAIN GROUND LEVEL DISTANCES.

WALNUT CREEK  
ACRES  
UNIT No. 1  
25 MAPS 848  
9-22-1941

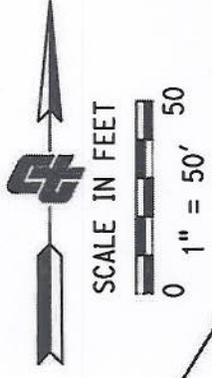
Saint Vincent DePaul  
Society  
APN 149-122-024-8  
23

Fjeld Family LP  
DOC. 2009-0272575-00  
11-17-2009  
22

3,274 Sq. Ft.  
**WEST 0.08 ACRES**  
HOOKSTON DD-048707-01-01  
Doc. No. 2001-0033699-00  
State to the Pleasant Hill  
Redevelopment Agency  
2-13-2001

**DD-048706-01-01**

EAST BAY  
MUNICIPAL UTILITY DISTRICT  
(EBMUD)



S88°19'00"E  
34.81'

POB 50°02'00"E  
83.93'

R=491.00'  
Δ=0°55'20"  
L=67.46'

Relinquishment Request No. 56027  
segment 3  
Contra Costa Blvd. Doc. No. 2008-0063585-00  
3-25-2008

R=15.00'  
Δ=97°20'42"  
L=25.48'

N88°19'00"W  
30.53'

**LEGEND**

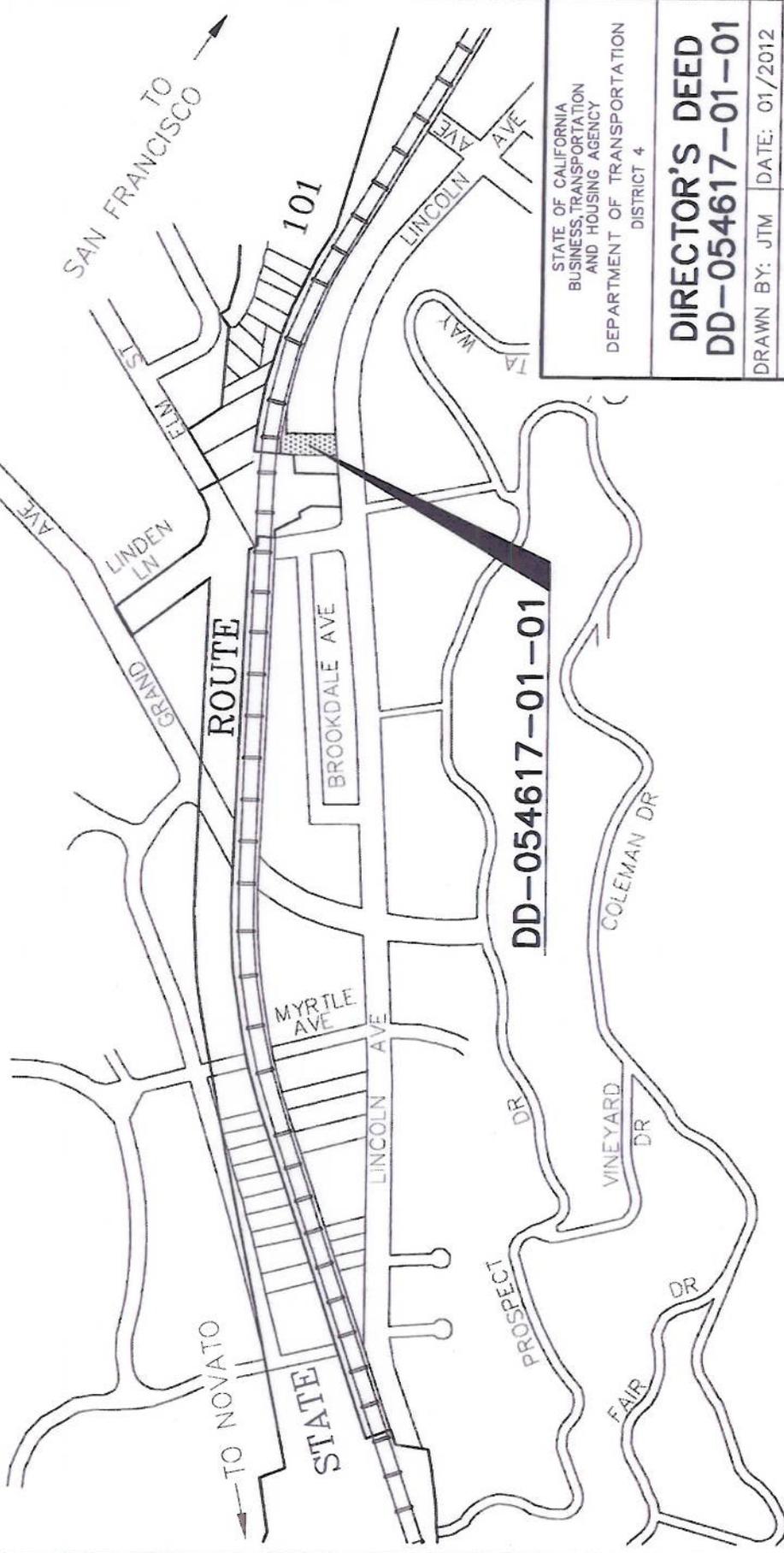
- POB point of beginning
- Doc. No. Document number
- PRC Point of reverse curve.
- R radial

Contra Costa Blvd.  
continues here in a  
northerly direction

STATE OF CALIFORNIA BUSINESS, TRANSPORTATION AND HOUSING AGENCY DEPARTMENT OF TRANSPORTATION DISTRICT 4	
<b>DIRECTOR'S DEED</b> <b>DD-048706-01-01</b>	
DRAWN BY: PD	DATE: 4/23/13
CHECK BY: AL	SCALE: 1"=50'
COUNTY ROUTE	P.M. DR.NO.
CC 680	17.3 2 of 2

R-17X.18

CITY OF SAN RAFAEL  
COUNTY OF MARIN



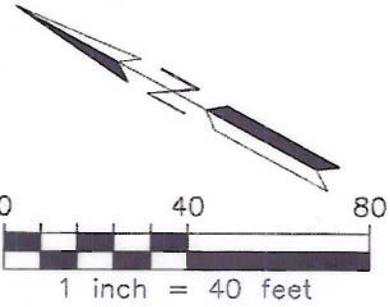
STATE OF CALIFORNIA BUSINESS, TRANSPORTATION AND HOUSING AGENCY	
DEPARTMENT OF TRANSPORTATION DISTRICT 4	
<b>DIRECTOR'S DEED</b> <b>DD-054617-01-01</b>	
DRAWN BY: JTM	DATE: 01/2012
CHECKED BY: WN	SCALE: NONE
CO. RTE.	P.M.
DR.NO.	
MRN 101	11.5 1 OF 2

DISTANCES SHOWN ARE ON THE CALIFORNIA COORDINATE SYSTEM OF 1983, ZONE 3. MULTIPLY DISTANCES BY 1.0000625 TO OBTAIN GROUND-LEVEL DISTANCES.

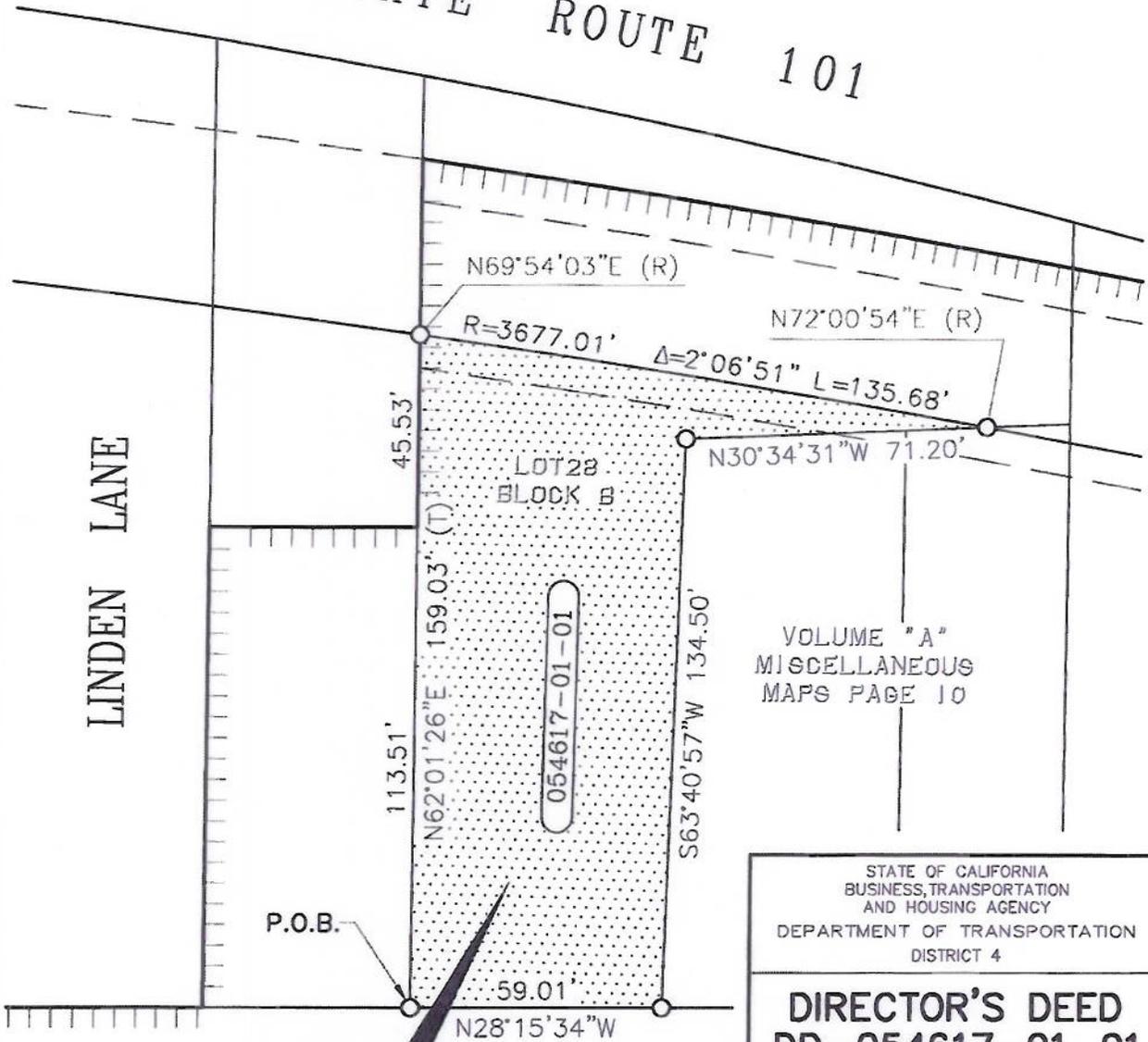
# CITY OF SAN RAFAEL COUNTY OF MARIN

## LEGEND

-  DIMENSION POINT
-  TOTAL DISTANCE
-  RADIAL BEARING
-  ACCESS PROHIBITED
-  POINT OF BEGINNING



STATE ROUTE 101



VOLUME "A"  
MISCELLANEOUS  
MAPS PAGE 10

STATE OF CALIFORNIA  
BUSINESS, TRANSPORTATION  
AND HOUSING AGENCY  
DEPARTMENT OF TRANSPORTATION  
DISTRICT 4

**DIRECTOR'S DEED**  
**DD-054617-01-01**

DRAWN BY: JTM      DATE: 01/2012

CHECKED BY: WN      SCALE: 1"=40'

CO.	RTE.	P.M.	DR.NO.
MRN	101	11.5	2 OF 2

**DD-054617-01-01**

9,990 Sq. Ft.

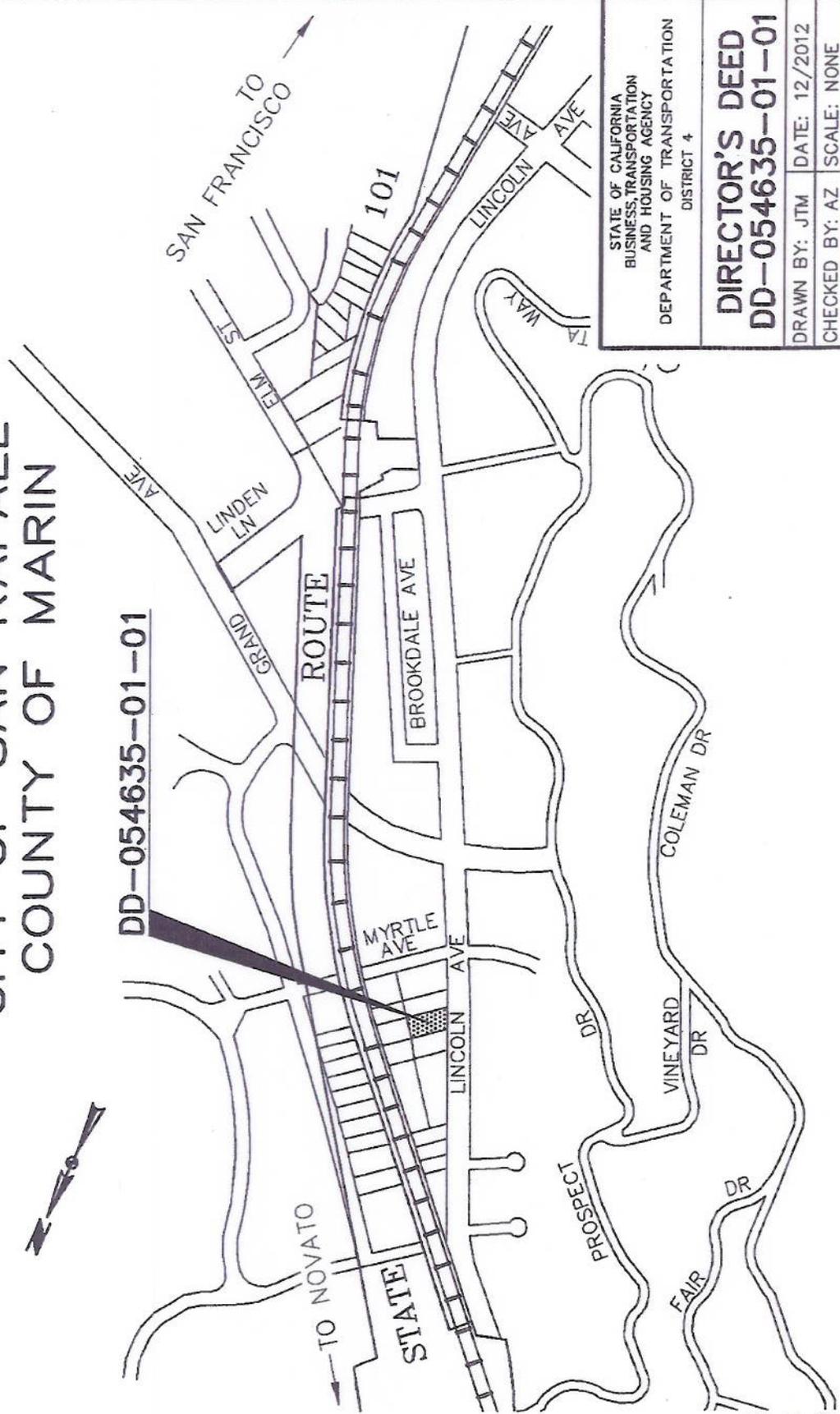
A-1311.3

**0.23 Acre**

**LINCOLN AVENUE**

CITY OF SAN RAFAEL  
COUNTY OF MARIN

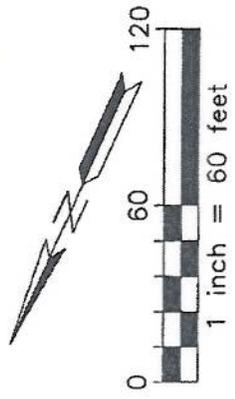
DD-054635-01-01



STATE OF CALIFORNIA BUSINESS, TRANSPORTATION AND HOUSING AGENCY	
DEPARTMENT OF TRANSPORTATION DISTRICT 4	
<b>DIRECTOR'S DEED</b>	
<b>DD-054635-01-01</b>	
DRAWN BY: JTM	DATE: 12/2012
CHECKED BY: AZ	SCALE: NONE
CO. RTE.	P.M. DR.NO.
MRN 101	11.9 1 OF 2

A-1311 & R-22

DISTANCES SHOWN ARE ON THE CALIFORNIA COORDINATE SYSTEM OF 1983, ZONE 3. MULTIPLY DISTANCES BY 1.0000625 TO OBTAIN GROUND-LEVEL DISTANCES.



CITY OF SAN RAFAEL  
COUNTY OF MARIN

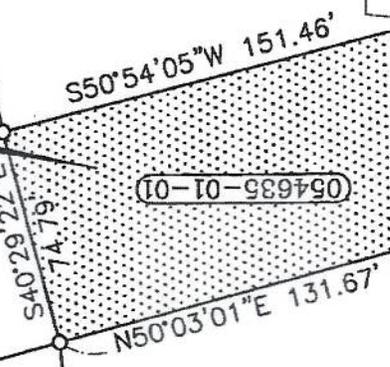
STATE ROUTE 101

DD-054635-01-01  
10,438 Sq. Ft.

*D. N. A. G. G.*

MYRTLE AVE

(54635)  
2003-0092307  
08/01/2003



P.O.B.  
N25°13'10"W  
75.00'

LINCOLN AVENUE

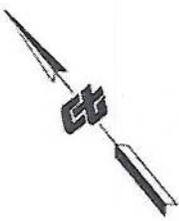
**LEGEND**  
 O DIMENSION POINT  
 L L L L L ACCESS PROHIBITED  
 P.O.B. POINT OF BEGINNING

STATE OF CALIFORNIA BUSINESS, TRANSPORTATION AND HOUSING AGENCY	
DEPARTMENT OF TRANSPORTATION DISTRICT 4	
<b>DIRECTOR'S DEED</b> <b>DD-054635-01-01</b>	
DRAWN BY: JTM	DATE: 12/2012
CHECKED BY: AZ	SCALE: 1"=60'
CO. RTE.	P.M.
MRN 101	11.9
	2 OF 2

A-1311.1 & R-22.2



Coordinates and bearings are on CC37, Zone 3. Distances and bearings are in feet unless otherwise noted. All distances are in feet unless otherwise noted.



COUNTY OF SANTA CLARA  
CITY OF SAN JOSE

**DD-052511-01-01**

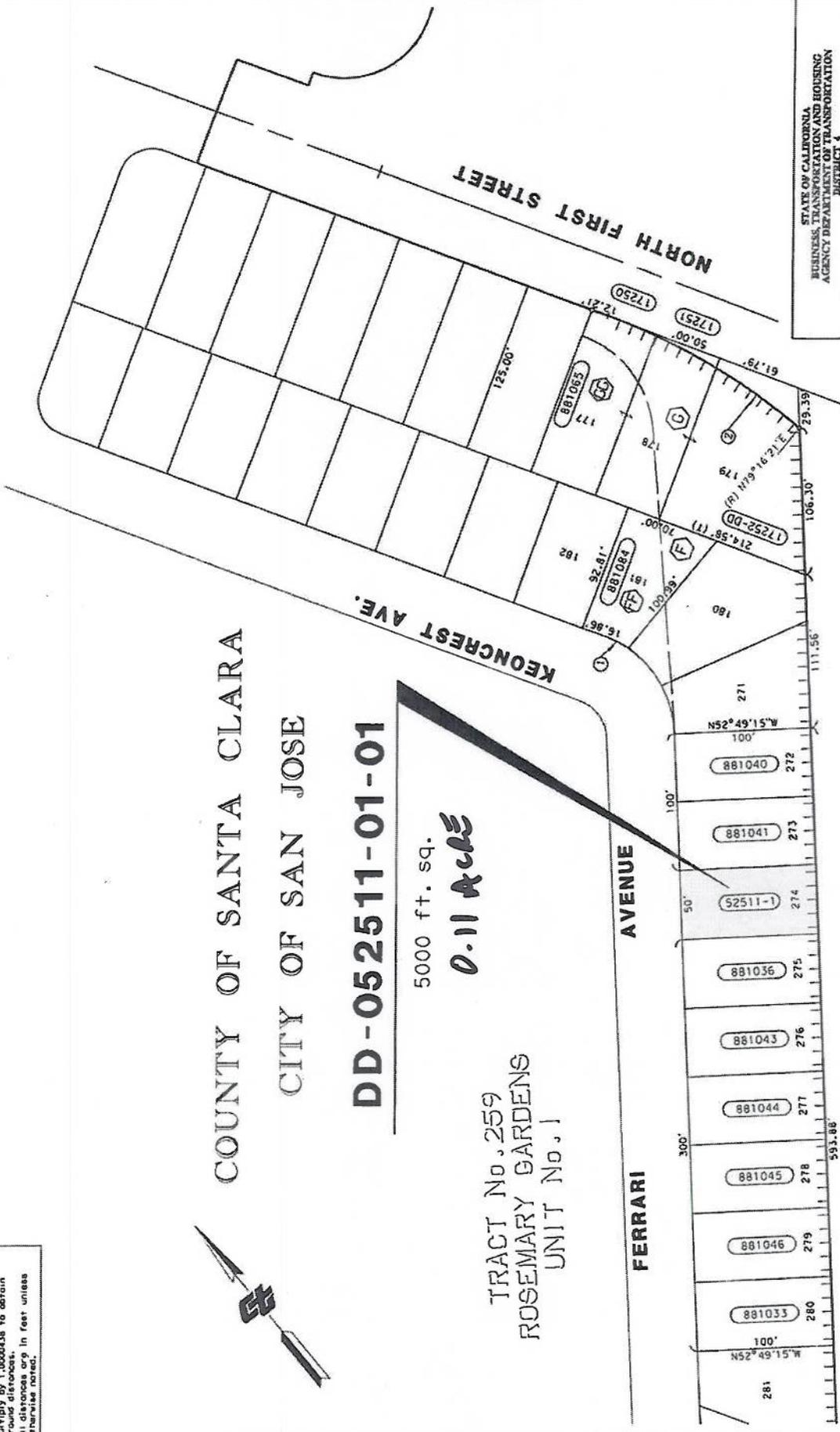
5000 ft. sq.  
**0.11 Acre**

TRACT No. 259  
ROSEMARY GARDENS  
UNIT No. 1

**FERRARI AVENUE**

**KEONCREST AVE.**

**NORTH FIRST STREET**



**LEGEND**

Access Prohibited  
Access Superseaded  
Access Restricted  
Access Restricted (Private)  
Indicates Right-of-Way  
Indicates Found Monument  
as noted  
Indicates calculated monument set  
Title to State  
Required for Others

**CURVE DATA**

No.	R	Δ	T	L
(1)	80.00	14° 6' 7"	9.89	19.69
(2)	487.00	19° 17' 51"	82.80	164.04

State Route 880

R-63.57  
R-1077.35

STATE OF CALIFORNIA  
BUSINESS TRANSACTIONS DIVISION  
AGENCY DEPARTMENT OF TRANSPORTATION  
DISTRICT 4

**DIRECTOR'S DEED**  
**DD-052511-01-01**

DRW: C. S. DATE: 05/2008  
CHK: P. D. SCALE: 1" = 100'

CO.	RTE	PM	SHEET No.
SCI	880	3.5	2 OF 2

City of Atascadero

LIFE RESIDENCE PARK

HIGHWAY

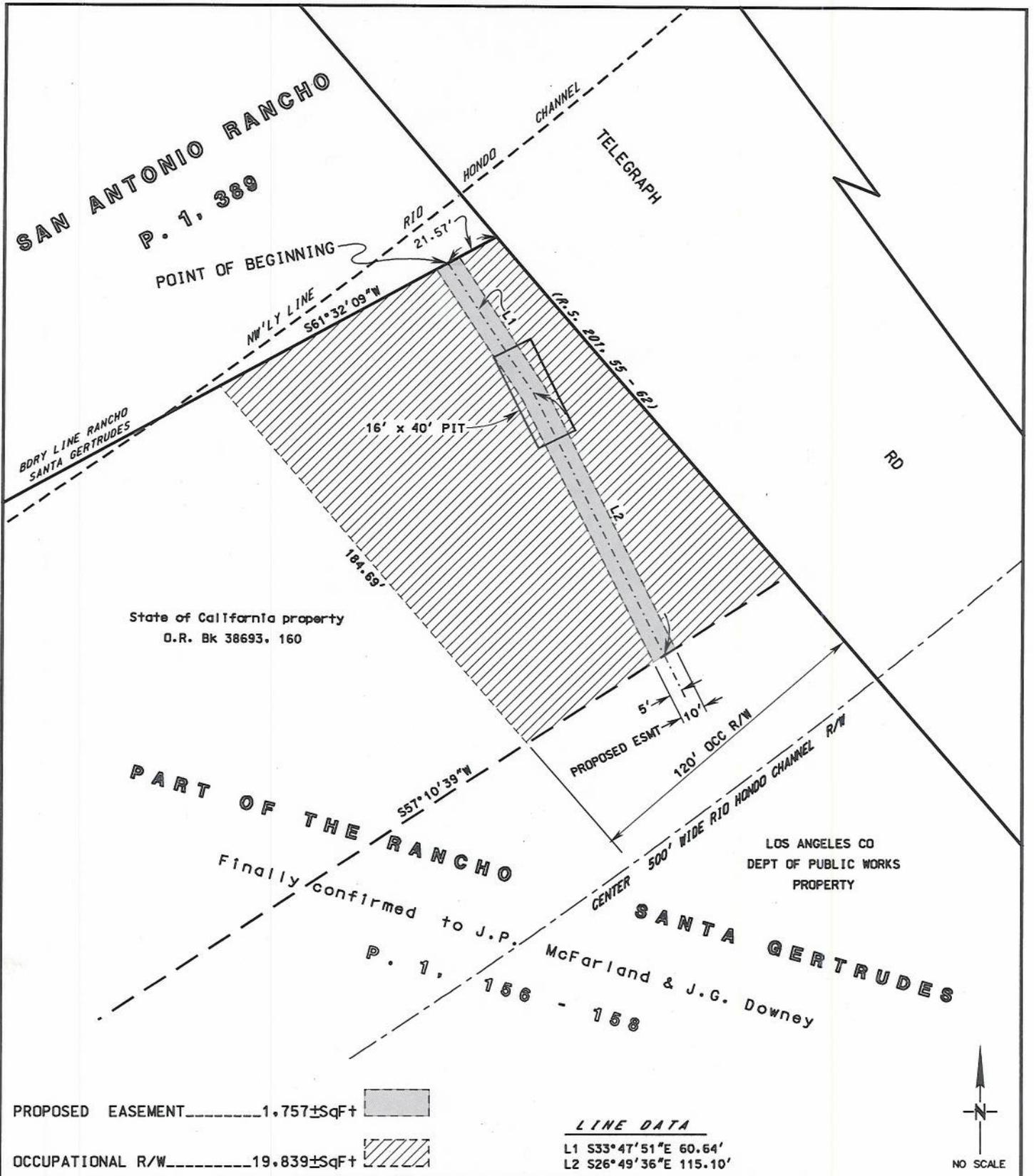
SERENA COURT



Access Denial



STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION		DIRECTORS DEED MAP	
DD 5463-01-01		ROUTE	POST MILE
COUNTY	SL0	41	16.4



**COUNTY SANITATION DISTRICT NO. 2  
OF LOS ANGELES COUNTY, CALIFORNIA**

TRUNK SEWER: UNION STREET TRUNK SEWER SECTION 1	
GRANTOR: STATE OF CALIFORNIA	DWG. NO.: 2-p-154
GRANTEE: County Sanitation District No. 2	EASEMENT NO.:
THOMAS GUIDE: 676, C7	ASSESSOR'S PARCEL NO. 6367 001 907

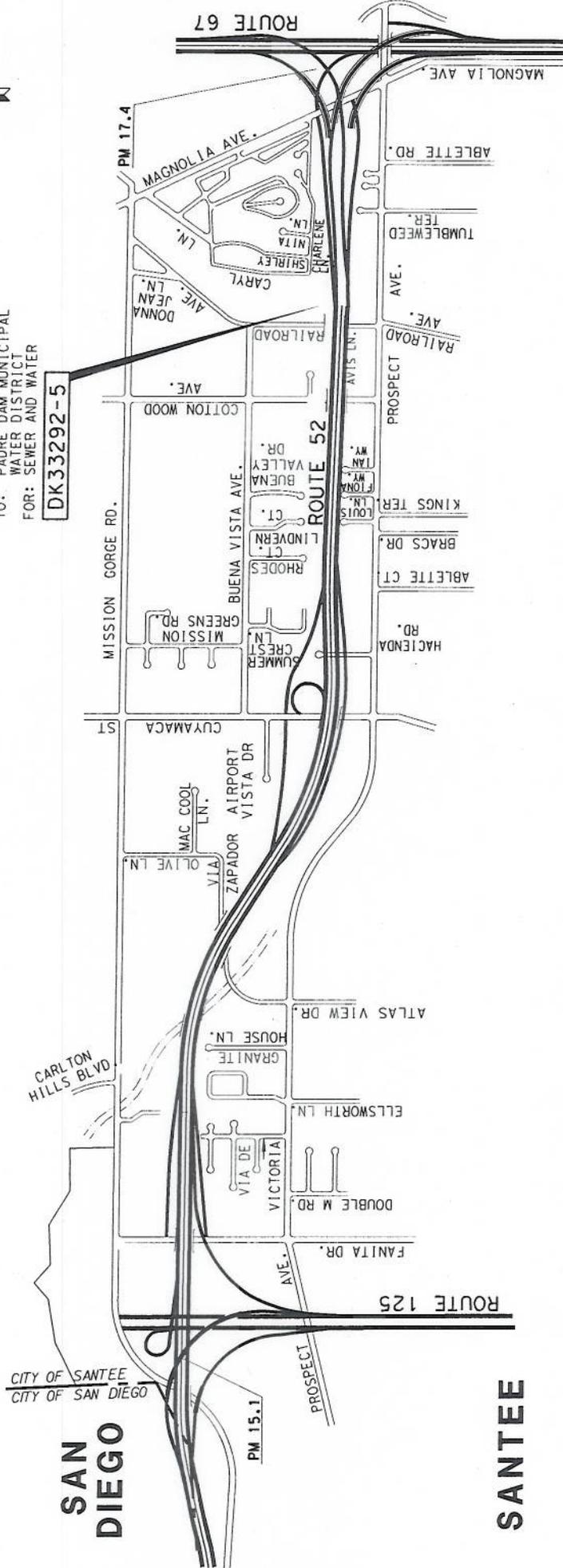
EXHIBIT "B"

# CITY OF SANTEE



TO: PADRE DAM MUNICIPAL  
WATER DISTRICT  
FOR: SEWER AND WATER

DK33292-5



KEY MAP

STATE OF CALIFORNIA  
DEPARTMENT OF TRANSPORTATION

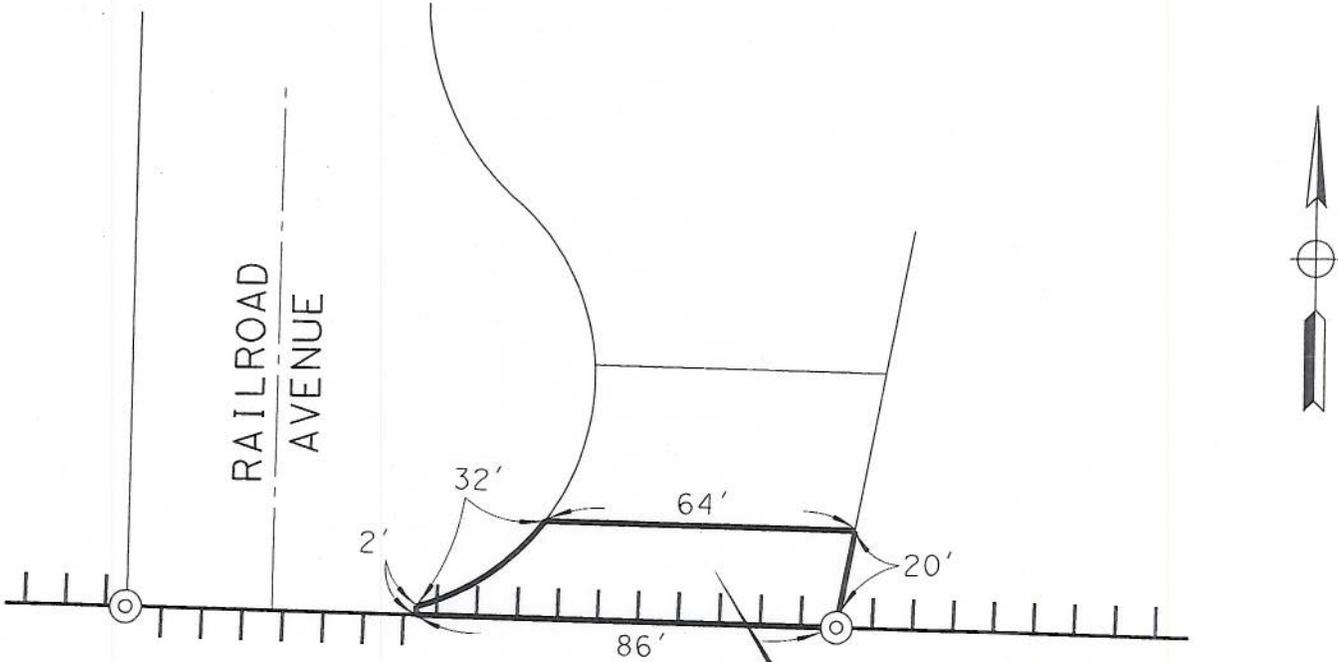
DISTRICT -II

DIRECTOR'S DEED MAP  
DK33292-5

RIGHT OF WAY MAP NO.	Por. 47526-1km		SCALE
COUNTY	ROUTE	KP/PM	NONE
SD	52	27.2/16.9	NONE
			SHEET 1 OF 2

E.A. 010622

# CITY OF SANTEE

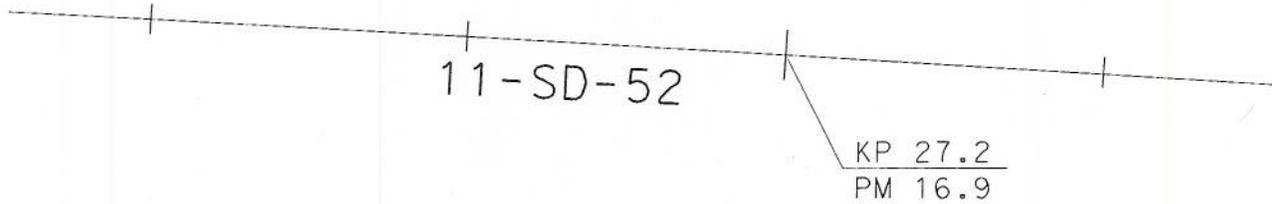


**0.03 ACRE**

AREA=1448 SQ. FT.

**DK33292-5**

TO: PADRE DAM MUNICIPAL  
WATER DISTRICT  
FOR: SEWER AND WATER



STATE OF CALIFORNIA  
DEPARTMENT OF TRANSPORTATION  
DISTRICT II

DIRECTOR'S DEED MAP  
DK33292-5

RIGHT OF WAY MAP NO. Por. 47554m

COUNTY	ROUTE	KP/PM	SCALE
SD	52	27.2/16.9	NONE

E.A. 010622



# CITY OF SANTEE

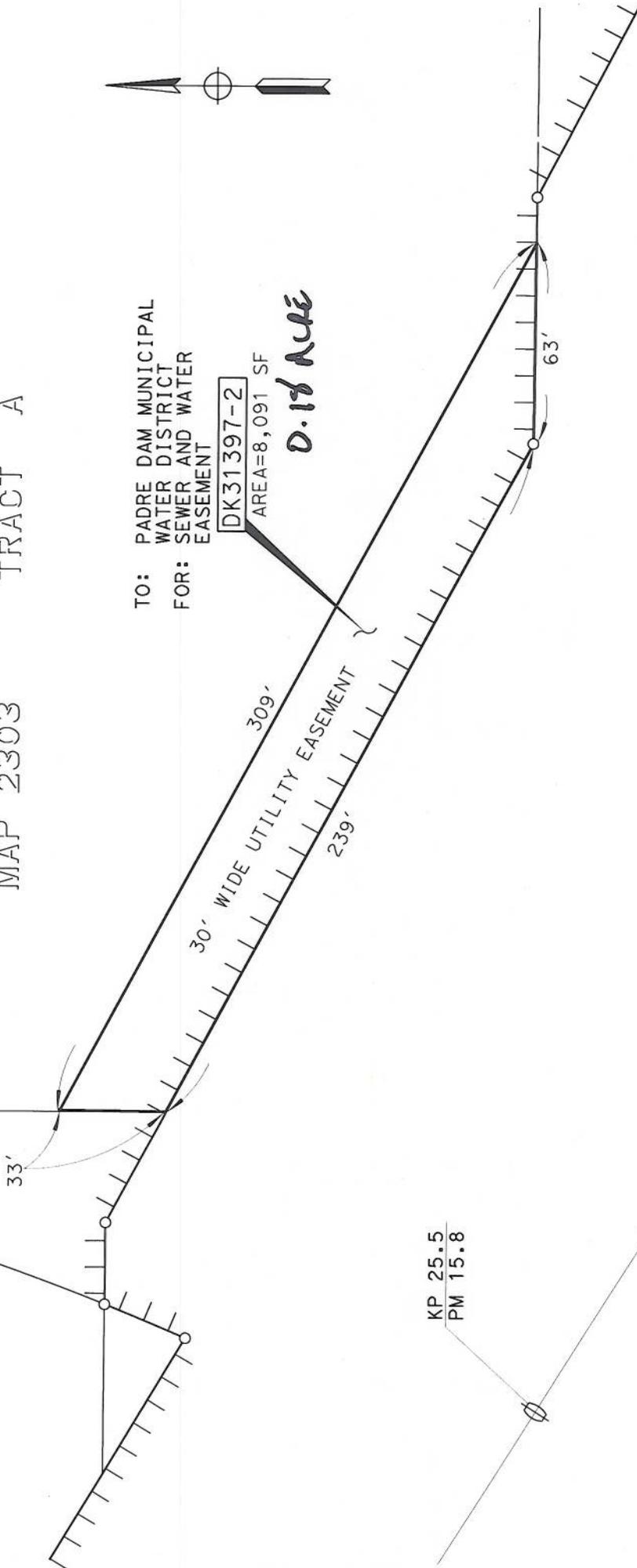
MAP 2303 TRACT A

TO: PADRE DAM MUNICIPAL  
WATER DISTRICT  
FOR: SEWER AND WATER  
EASEMENT

DK31397-2

AREA=8,091 SF

*0.18 ACRES*



STATE OF CALIFORNIA  
DEPARTMENT OF TRANSPORTATION  
DISTRICT II

DIRECTOR'S DEED MAP  
DK31397-2

RIGHT OF WAY  
MAP NO.      POR. 47547.1m

COUNTY	ROUTE	KP/PM	SCALE
SD	52	25.6/15.9	NONE

SHEET 2 OF 2

E.A. 010652

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5b.(5a)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **FINANCIAL ALLOCATION AMENDMENT FOR MULTI-FUNDED PROJECTS WITH SHOPP AND PROPOSITION 1B TCIF FUNDS RESOLUTION FP-13-14, AMENDING RESOLUTION FP-10-33 RESOLUTION TCIF-AA-1314-02**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) approve an adjustment to the State Highway Operation and Improvement Project (SHOPP)/Proposition 1B Trade Corridor Improvement Fund (TCIF) allocation amount for Project 5 – I-80 Eastbound Cordelia Truck Scales Relocations project (PPNO 5301R) in Solano County.

## **ISSUE:**

At its June 2011 meeting, the Commission approved Resolution FP-10-33 allocating \$42,300,000 in SHOPP/TCIF funds for Project – I-80 Eastbound Cordelia Truck Scales Relocations project. At this time, there are overall project cost savings in the amount of \$9,508,000, reducing the original allocation amount from \$42,300,000 to \$32,792,000.

## **BACKGROUND:**

The Highway Safety, Traffic Reduction, Air Quality, and Port Security Bond Act of 2006, passed by the voters in November 2006, included \$2,000,000,000 for the Proposition 1B TCIF Program. The Commission recognized that the needs far exceeded the amount authorized in the Proposition 1B TCIF program and increased the TCIF funding by approximately \$500,000,000 from the State Highway Account.

The Department is requesting an adjustment of \$9,508,000 in order to fund additional SHOPP/TCIF projects to fully utilize the \$500,000,000 directed in the SHOPP for SHOPP-eligible TCIF projects.

## **FINANCIAL RESOLUTION:**

Resolved, that the \$42,300,000 in State Highway Operation and Improvement Project (SHOPP)/Proposition 1B Trade Corridor Improvement Fund (TCIF) funds originally allocated under Resolutions FP-10-33, for Project 5 – I-80 Eastbound Cordelia Truck Scales Relocations project (PPNO 5301R) in Solano County, is hereby amended by \$9,508,000, reducing the SHOPP/TCIF amount to \$32,792,000, in accordance with the attached vote list.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(5a) Allocation Amendment – Multi-Funded Projects with SHOPP and Proposition 1B TCIF</b>				
			<b>Resolution FP-13-14, Amending Resolution FP-10-33 Resolution TCIF-AA-1314-02</b>	
1 \$42,300,000 <del>\$32,792,000</del>	<b>I-80 Eastbound Cordelia Truck Scales Relocation.</b> Near Fairfield, at the EB Cordelia Truck Scale. <u>Outcome/Output:</u> Relocate and expand truck scale facility and relocate and realign ramps to improve CHP truck inspection operation and improve freeway efficiency and safety for vehicular traffic. (TCIF Project 12)	04-5301R SHOPP/11-12 <del>\$49,800,000</del> <b>\$32,792,000</b> 0400000153 4 0A5354	2009-10 302-0890 FTF 20.20.201.321	<del>\$42,300,000</del> <b>\$32,792,000</b>
Solano 04N-Sol-80 13.3/15.7	(SHOPP funded Trade Corridor Improvement Fund (TCIF) project.)  (Contributions by others \$14,900,000)			
<b><u>Amend Resolution FP-10-33 to de-allocate \$9,508,000 in SHOPP/TCIF CONST.</u></b>				

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5b.(5b)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **FINANCIAL ALLOCATION AMENDMENT FOR MULTI-FUNDED PROJECTS WITH SHOPP AND PROPOSITION 1B TCIF FUNDS**  
**RESOLUTION FP-13-15, AMENDING RESOLUTION FP-10-33**  
**RESOLUTION TCIF-AA-1314-02**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) approve an adjustment to the State Highway Operation and Improvement (SHOPP)/Proposition 1B Trade Corridor Improvement Fund (TCIF) allocation amount for Project 12 – I-580 Eastbound Truck Climbing Lane project (PPNO 0104) in Alameda County.

## **ISSUE:**

At its June 2011 meeting, the Commission approved Resolution FP-10-33, which allocated \$48,959,000 in SHOPP/TCIF funds for Project 12 – I-580 Eastbound Truck Climbing Lane project. The construction contract was awarded on June 18, 2012, with a savings of \$7,209,000, reducing the original allocation from \$48,959,000 to \$41,750,000.

## **BACKGROUND:**

The Highway Safety, Traffic Reduction, Air Quality, and Port Security Bond Act of 2006, passed by the voters in November 2006, included \$2,000,000,000 for the Proposition 1B TCIF Program. The Commission recognized that the needs far exceeded the amount authorized in the Proposition 1B TCIF program and increased the TCIF funding by approximately \$500,000,000 from the State Highway Account.

The Department is requesting an adjustment of \$7,209,000 in order to fund additional SHOPP/TCIF projects to fully utilize the \$500,000,000 directed in the SHOPP for SHOPP-eligible TCIF projects.

## **FINANCIAL RESOLUTION:**

Resolved, that the \$48,959,000 in State Highway Operation and Improvement (SHOPP)/Proposition 1B Trade Corridor Improvement Fund (TCIF) funds originally allocated under Resolutions FP-10-33, for Project 12 – I-580 Eastbound Truck Climbing Lane project (PPNO 0104) in Solano County, is hereby amended by \$7,209,000 reducing the SHOPP/TCIF amount to \$41,750,000, in accordance with the attached vote list.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(5b) Allocation Amendment – Multi-Funded Projects funded with SHOPP and Proposition 1B TCIF</b>		<b>Resolution FP-13-15, Amending Resolution FP-10-33 Resolution TCIF-AA-1314-03</b>		
1 \$48,959,000 <b>\$41,750,000</b>	<b>I-580 Eastbound Truck Climbing Lane.</b> In Livermore, from North Flynn road to Greenville Road. <u>Outcome/Output:</u> Construct truck climbing lane and rehabilitate pavement to enhance the movement of goods, improve freeway safety and operations, and to relieve traffic congestion. (TCIF Project 5)	04-0104 SHOPP/10-11 <del>\$63,000,000</del> <b>\$41,750,000</b>	2009-10 302-0042 SHA 302-0890 FTF 20.20.201.310	<del>\$979,000</del> <b>\$835,000</b> <del>\$47,980,000</del> <b>\$40,915,000</b>
Alameda 04N-Ala-580 4.7/8.2	(SHOPP funded Trade Corridor Improvement Fund (TCIF) project.)  <b><u>Amend Resolution FP-10-33 to de-allocate \$7,209,000 in SHOPP/TCIF CONST.</u></b>	<del>0400004103</del> <b>0400020643</b> 4 4A0704 <b>4A07U4</b>		

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.1c.(2)/2.5g.(2)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **FINANCIAL ALLOCATION AMENDMENT FOR PROPOSITION 1B LOCALLY  
ADMINISTERED STATE ROUTE 99 CORRIDOR BOND PROGRAM PROJECT  
ON THE STATE HIGHWAY SYSTEM  
RESOLUTION R99-AA-1314-01, AMENDING RESOLUTION R99-A-1213-05  
RESOLUTION R99-PA-1314-01, AMENDING RESOLUTION R99-P-1213-06**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) amend Resolution R99-A-1213-05 to de-allocate \$5,479,000 in Proposition 1B State Route 99 (SR99) bond funds from the Atwater-Merced Expressway Phase 1A project (PPNO 5264A) in Merced County, thereby reducing the original SR99 construction capital and construction support allocation of \$52,000,000 to \$46,521,000, to reflect contract award savings, and revise the project funding plan accordingly.

## **BACKGROUND:**

At its October 2012 meeting, the Commission approved Resolution R99-A-1213-05 allocating \$52,000,000 in Proposition 1B SR99 bond funds for construction capital and construction support for the Atwater-Merced Expressway Phase 1A project. The construction contract was awarded on June 20, 2013 with savings of \$5,479,000.

The necessary changes are reflected in strikethrough and bold on the vote box on the following page.

## **RESOLUTION R99-AA-1314-01:**

Be it Resolved, that \$52,000,000 in Proposition 1B State Route 99 (SR99) bond funds (Budget Act Item 2660-304-6072) originally allocated under Resolution R99-A-1213-05 for the Atwater-Merced Expressway Phase 1A project (PPNO 5264A) in Merced County is hereby amended by \$5,479,000, reducing the original SR99 construction capital and construction support allocation to \$46,521,000 in accordance with the vote box on the following page.

Project #	Allocation Amount	Recipient	Project Title	PPNO	Program/Year	Phase	Prgm'd Amount	Item #	Amount by
RTPA/CTC	County	Dist-Co-Rte	Location	Project ID	Adv Phase	EA	Fund Type	Fund Type	Fund Type
Postmile	Project Description			EA	Fund Type	Fund Type	Fund Type	Fund Type	Fund Type
<b>2.5g.(2)</b>	<b>Allocation Amendment - Proposition 1B –Locally Administered Route 99 Projects on the State Highway System</b>			<b>Resolution R99-AA-1314-01</b>					
				<b>Amending Resolution R99-A-1213-05</b>					
1	\$52,000,000 <b>\$46,521,000</b>	Merced County MCAG Merced 10-Mer-99 19.5/20.7	<b>Atwater-Merced Expressway Phase 1A.</b> Near Atwater, from 1.0 mile south of Buhach Road to 0.1 mile north of Buhach Road. Widen to 6-lane freeway, including demolition of Buhach Road interchange, and construct new interchange.  (Concurrent SR 99 project baseline amendment under Resolution R99-PA-1314-01; August 2013.)  (SR 99 project funding broken down as <del>\$45,000,000</del> <b>\$39,521,000</b> for CONST and \$7,000,000 for CON ENG.)  (Future Consideration of Funding under Resolution E-11-59; August 2011.)  <u>Outcome/Output:</u> Daily travel time savings: 5,022 hours. Peak period time savings: 301,320 minutes.  <b><u>Amend Resolution R99-A-1213-05 to de-allocate \$5,479,000 SR 99 CONST to reflect contract award savings.</u></b>	10-5264A SR-99/12-13 CONST <del>\$52,000,000</del> <b>\$46,521,000</b> 1000000045 4CONL 0G4404	2011-12 304-6072 SR-99 20.20.722.000			\$52,000,000 <b>\$46,521,000</b>	

**REVISE: Atwater-Merced Expressway Phase 1A (PPNO 5264A)**

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Merced	10	5264A	0G440	CO	2012-13	19.5	20.7	99					
<b>Implementing Agency: (by component)</b>		<b>PA&amp;ED</b>	Merced County Association of Governments			<b>PS&amp;E</b>	Merced County Association of Governments						
		<b>R/W</b>	Merced County Association of Governments			<b>CON</b>	Merced County						
<b>RTPA/CTC:</b>		Merced County Association of Governments											
<b>Project Title:</b>		Atwater-Merced Expressway Phase 1A											
<b>Location:</b>		Near Atwater, from 1.0 mile south of Buhach Road to 0.1 mile north of Buhach Road.											
<b>Description:</b>		Widen to 6-lane freeway, including demolition of Buhach Road interchange, and construct new interchange.											
<b>(DOLLARS IN THOUSANDS)</b>													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	12/13	13/14	14/15	15/16	16/17	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
<b>RIP</b>													
Existing	11,882	11,882					11,882						
Change	0	0					0						
Proposed	11,882	11,882					11,882						
<b>Local Funds</b>													
Existing	7,477	7,477					500		3,277	3,700			
Change	0	0					0		0	0			
Proposed	7,477	7,477					500		3,277	3,700			
<b>State Bond (SR 99)</b>													
Existing	52,000		52,000					52,000					
Change	(5,479)		(5,479)					(5,479)					
Proposed	46,521		46,521					46,521					
<b>Total</b>													
Existing	71,359	19,359	52,000				12,382	52,000	3,277	3,700			
Change	(5,479)	0	(5,479)				0	(5,479)	0	0			
Proposed	65,880	19,359	46,521				12,382	46,521	3,277	3,700			

**RESOLUTION R99-PA-1314-01:**

Be it Resolved, that the California Transportation Commission does hereby amend the State Route 99 Corridor Bond Program baseline agreement for the Atwater-Merced Expressway Phase 1A project (PPNO 5264A) in Merced County with the information described above.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5g.(5e)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Denix D. Anbiah  
Division Chief  
Local Assistance

Subject: **FINANCIAL ALLOCATION AMENDMENT FOR PROPOSITION 1B LOCALLY  
ADMINISTERED TRADE CORRIDOR IMPROVEMENT FUNDS PROJECT OFF  
THE STATE HIGHWAY SYSTEM  
RESOLUTION TCIF-AA-1314-01, AMENDING RESOLUTION TCIF-A-1112-12**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) amend Resolution TCIF-A-1112-12 de-allocate \$4,079,000 in Proposition 1B Trade Corridor Improvement Funds (TCIF) for Project 88, Baldwin Avenue Grade Separation project (PPNO TC88) in Los Angeles County, reducing the original TCIF allocation of \$37,638,000 to \$33,559,000, to reflect contract savings.

## **BACKGROUND:**

On May 23, 2012, the Commission allocated \$37,638,000 in TCIF funds under Resolution TCIF-A-1112-12 to the Baldwin Avenue Grade Separation project. The contract has been awarded with a savings of \$4,079,000 in TCIF funds. The necessary changes are reflected in strikethrough and bold on the attached revised vote list.

## **RESOLUTION:**

Be it Resolved, that the \$37,638,000 for the Proposition 1B Trade Corridor Improvement Fund (104-6056) originally allocated under Resolution TCIF-A-1112-12 for the Baldwin Avenue Grade Separation project (PPNO TC88) in Los Angeles County, is hereby amended by \$4,079,000, reducing the Proposition 1B TCIF financial allocation from \$37,638,000 to \$33,559,000, in accordance with the attached revised vote list.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(5e) Allocation Amendment - Proposition 1B – Locally Administered TCIF Projects Off the State Highway System</b>		<b>Resolution TCIF-AA-1314-01, Amending Resolution TCIF-A-1112-12</b>		
1 <del>\$37,638,000</del> <b>\$33,559,000</b>	<b>Baldwin Avenue Grade Separation.</b> In El Monte, at Baldwin Avenue. Construct double-track railroad bridge over a four-lane depressed roadway. (TCIF Project 88)	07-TC88 TCIF/11-12 CONST <del>\$37,638,000</del> <b>\$33,559,000</b> 0712000280 <b>4U2714</b>	2010-11 104-6056 TCIF 20.30.210.300	<del>\$37,638,000</del> <b>\$33,559,000</b>
Alameda Corridor East – Construction Authority LACMTA 07-Los Angeles	(Future consideration of Funding – Resolution E-11-08; January 2011.)  (TCIF baseline agreement approved under Resolution TCIF-P-1112-029B; April 2012.)  (Contributions from other sources: \$43,832,000.)  (The programmed TCIF funds are to be split: <del>\$8,024,389</del> <b>\$8,401,000</b> for construction engineering and <del>\$29,613,614</del> <b>\$25,158,000</b> for construction capital.)  <u>Outcome/Output:</u> This project will eliminate the railroad crossing at Baldwin Avenue, which carries 25,336 vehicles per day and where passing trains blocked for 19.8 vehicle-hours per day, projected to increase to 61 vehicle-hours of delay by 2020; increased truck freight velocity by eliminating a bottleneck at a railroad crossing provides bridge abutments for future track expansion; eliminates the dangers of collisions between trains and vehicles; reduces pollution caused by idling cars and trucks; and eliminates train horns and crossing alarms with removal of the grade crossing.  <b><u>Amend Resolution TCIF-A-1112-12 to de-allocate \$4,079,000 in TCIF Bond Program CONST to reflect contract savings.</u></b>			

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(5e) Allocation Amendment - Proposition 1B – Locally Administered TCIF Projects Off the State Highway System</b>			<b>Resolution TCIF-AA-1314-01, Amending Resolution TCIF-A-1112-12</b>	
1 <del>\$37,638,000</del> <b>\$33,559,000</b>	Baldwin Avenue Grade Separation. In El Monte, at Baldwin Avenue. Construct double-track railroad bridge over a four-lane depressed roadway. (TCIF #88)	07-TC88 TCIF/11-12 CONST <del>\$37,638,000</del> <b>\$33,559,000</b> 0712000280 <b>4U2714</b>	2010-11 104-6056 TCIF 20.30.210.300	<del>\$37,638,000</del> <b>\$33,559,000</b>
Alameda Corridor East – Construction Authority LACMTA 07-Los Angeles	(Future consideration of Funding – Resolution E-11-08; January 2011.)  (TCIF baseline agreement approved under Resolution TCIF-P-1112-029B; April 2012.)  (Contributions from other sources: \$43,832,000.)  (The programmed TCIF funds are to be split: <del>\$8,024,389</del> <b>\$8,401,000</b> for construction engineering and <del>\$29,613,611</del> <b>\$25,158,000</b> for construction capital.)  <u>Outcome/Output:</u> This project will eliminate the railroad crossing at Baldwin Avenue, which carries 25,336 vehicles per day and where passing trains blocked for 19.8 vehicle-hours per day, projected to increase to 61 vehicle-hours of delay by 2020; increased truck freight velocity by eliminating a bottleneck at a railroad crossing provides bridge abutments for future track expansion; eliminates the dangers of collisions between trains and vehicles; reduces pollution caused by idling cars and trucks; and eliminates train horns and crossing alarms with removal of the grade crossing.  <b>Amend Resolution TCIF-A-1112-12 to de-allocate \$4,079,000 in TCIF Bond Program CONST to reflect contract savings.</b>			

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5g.(7)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR PROPOSITION 1B PROJECTS IN THE  
TRAFFIC LIGHT SYNCHRONIZATION PROGRAM  
RESOLUTION TLS1B-A-1314-01**

## **RECOMMENDATION:**

The Department of Transportation (Department) recommends the California Transportation Commission (Commission) allocate \$426,000 for the City Inglewood – La Brea Avenue Traffic Light Synchronization Program (TLSP) project to be funded from Proposition 1B TLSP Program.

## **ISSUE:**

The attached vote list describes one Proposition 1B TLSP project for the total of \$426,000. This traffic light synchronization project and other technology-based improvements will improve safety, operations, and the effective capacity of local streets and roads. The allocation request for this project is consistent with the signed baseline agreement. The agency is ready to proceed with this project and is requesting an allocation at this time.

## **FINANCIAL RESOLUTION:**

Resolved, that \$426,000 be allocated from the Budget Act of 2012, Budget Act Item 2660-104-6064, for the Proposition 1B Traffic Light Synchronization Program project, as described on the attached vote list; and

Be it further resolved that as a condition of allocation of these funds and to perform its administrative role established by Senate Bill 88, the Commission requests that the Department perform the functions necessary to ensure proper accountability measures are employed and reporting requirements are met for the Proposition 1B Traffic Light Synchronization Program.

Attachment

2.5 Highway Financial Matters

Project #	Allocation Amount	Recipient Agency	Dst-County	RTPA/MPO	Corridor Name / Project Location	Program Prgm'd Amt	Project ID	Adv Phase	Budget Year Item #	Fund Type	Amount by Fund Type
<b>2.5g.(7) Proposition 1B - Traffic Light Synchronization Program (TLSP)</b>						<b>Resolution TLS1B-A-1314-01</b>					
1	\$426,000	City of Inglewood	LACMTA	07-Los Angeles	City of Inglewood – La Brea Avenue. <u>Outcome/Output:</u> This TLSP project is expected to provide traffic congestion relief by improving travel times over 17 percent for the corridor of 19 signals from 64 <sup>th</sup> Street to 104 <sup>th</sup> Street.	TLSP	\$426,000	0712000233	2012-13	104-6064	\$426,000
							4			TLSP	

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5.g.(9)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: William D. Bronte  
Division Chief  
Rail

Subject: **FINANCIAL ALLOCATION AMENDMENT FOR PROPOSITION 1B HIGHWAY-RAILROAD CROSSING SAFETY ACCOUNT PROJECT**  
**RESOLUTION GS1B-AA-1314-01, AMENDING RESOLUTION GS1B-AA-1112-005**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) amend Resolution GS1B-AA-1112-005 to deallocate \$1,534,213 in Proposition 1B Highway-Railroad Crossing Safety Account (HRCSA) funds from the Hageman Road Grade Separation project, reducing the allocation of \$15,293,000 to \$13,758,787 due to construction cost savings.

## **ISSUE:**

The County of Kern (County) was initially awarded \$17,650,000 in Proposition 1B HRCSA funds by the Commission in June 2010. In May 2012, the Commission approved under Resolution GS1B-AA-1112-005, a request from the agency to de-allocate \$2,357,000 reducing the project allocation to \$15,293,000. The project has been completed and there are additional construction cost savings. The County now requests that the Commission reduce the current Proposition 1B HRCSA funds for the project by an additional \$1,534,213, from \$15,293,000 to \$13,758,787.

The proposed changes are reflected in strikethrough and bold underline on the attached revised vote list.

## **RESOLUTION GS1B-AA-1314-01:**

Be it Resolved, that the \$15,293,000 in Proposition 1B Highway-Railroad Crossing Safety Account (HRCSA) funds amended under Resolution GS1B-AA-1112-005 for the Hageman Road Grade Separation project, is hereby amended by \$1,534,213, reducing the overall HRCSA amount allocated for the project to \$13,758,787 in accordance with the attached revised vote box.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Project Description	EA Program / Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(9) Allocation Amendment - Proposition 1B – Locally Administered HRCSA Projects</b>			<b>Resolution GS1B-AA-1314-01, Amending Resolution GS1B-AA-1112-005</b>	
1 \$15,293,000 <del>\$13,758,787</del> County of Kern KCOG 06-Kern	<b>Hageman Road/BNSF Grade Separation.</b> In Kern County, on Hageman Road and Allen Road. Construct grade separation to re-establish a direct connection of Allen Road, eliminating circuitous routing to Hageman Road and continue to the north providing continuity to the traveling public. A railroad underpass on Hageman Road and Allen Road are both required to replace the current crossing. Replace an at-grade crossing.  (CEQA – SE, July 2009)  <u>Outcome/Output:</u> The project will eliminate public safety hazards; alleviate traffic congestion and degradation of air quality.  <b><u>Amend Resolution GS1B-AA-1112-005 to de-allocate an additional \$1,534,213 in HRCSA CONST due to overall project cost savings.</u></b>	H013BA HRCSA/09-10 CONST <del>\$15,293,000</del> <b>\$13,758,787</b> 0000020467	2007-08 104-6063 HRCSA 20.30.010.400	<del>\$15,293,000</del> <b>\$13,758,787</b>

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.9a.  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Denix D. Anbiah  
Division Chief  
Local Assistance

Subject: **TECHNICAL CORRECTION TO PREVIOUSLY APPROVED RESOLUTION-  
RESOLUTION SLP1B-A-1213-18**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) approve a technical correction to Resolution SLP1B-A-1213-18 originally approved on March 5, 2013.

## **ISSUE:**

At its March 2013 meeting, the Commission approved Resolution SLP1B-A-1213-18 to allocate \$30,827,000 for 47 locally administered Proposition 1B State-Local Partnership Program (SLPP) projects. The attached revised vote list described the 47 SLPP projects. A technical correction is needed for Project 22 Broadway and McFadden Avenue Rehabilitation project, to revise the Project ID from "1200000559 to 1213000190" in the vote box on the Book Item Attachment.

There is no change to the Book Item Memorandum.

The required changes are reflected in strikethrough and bold on the attached revised vote list.

Attachment

2.5 Highway Financial Matters

Technically Corrected August 6, 2013  
(Project 22)

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(10a) Proposition 1B – Locally Administered State-Local Partnership Program (SLPP)</b>			<b>Resolution SLP1B-A-1213-18</b>	
<b>Projects off the State Highway</b>				
1 \$71,000 Town of Truckee Nevada CTC 03-Nevada	<b>Annual Slurry Seal.</b> In Truckee on various roadways.  (CEQA – CE, 11/26/2012.)  (Contributions from other sources: \$399,000.)  <u>Outcome/Output:</u> Slurry seal to provide a cost-effective means of significantly extending the life of the Town's roadways consistent with an adopted Pavement Maintenance Program.	SLPP/12-13 CONST \$71,000 0313000162	2012-13 104-6060 SLPP 20.30.210.200	\$71,000
2 \$613,000 City of San Mateo MTC 04-San Mateo	<b>Road Rehabilitation – Various Locations.</b> In the city of San Mateo at various locations. Remove and replace pavement section and sub-section as necessary, install storm system, replace depressed or broken curb, gutter and sidewalk, reinstall traffic striping and adjust manhole rim to grade.  (CEQA – CE, 11/27/2012.)  (Contributions from other sources: \$613,000.)  <u>Outcome/Output:</u> This project will increase the service life of these streets for more than 15 years and the PCI for these segments will be increased from 30 to 100.	SLPP/12-13 CONST \$613,000 0413000260	2012-13 104-6060 SLPP 20.30.210.200	\$613,000
3 \$457,000 Town of Hillsborough MTC 04-San Mateo	<b>Street Resurfacing – Various Locations.</b> In the town of Hillsborough at various locations. AC overlay treatments on approximately 6.5 miles of roadways.  (CEQA – CE, 12/19/2012.)  (Contributions from other sources: \$957,106.)  <u>Outcome/Output:</u> This project will improve traffic operations, reduce traffic accidents, and provide safe pedestrian access.	SLPP/12-13 CONST \$457,000 0413000261	2012-13 104-6060 SLPP 20.30.210.200	\$457,000
4 \$49,000 Town of Colma MTC 04-San Mateo	<b>Hillside Boulevard Pavement.</b> On Hillside Boulevard, between Serramonte Boulevard and the Town's southern limits with the City of San Francisco. Asphalt pavement rehabilitation by grinding existing pavement, overlay of asphalt concrete sections, and striping work.  (CEQA – CE, 12/13/2012.)  (Contributions from other sources: \$799,000.)  <u>Outcome/Output:</u> This project will improve Hillside Boulevard and extend the useful life by more than 15 years. Travel time and fuel savings benefits are anticipated from these improvements.	SLPP/12-13 CONST \$49,000 0413000262	2012-13 104-6060 SLPP 20.30.210.200	\$49,000
5 \$375,000 San Luis Obispo County SLOCOG 05-San Luis Obispo	<b>Willow Road Extension Mitigation.</b> In Nipomo in south San Luis Obispo County. The project consists of approximately 4,000 acorns planting, irrigation system and a 7- year plant establishment.  (Future Consideration of Funding – Resolution E-09-85, October 2009.)  (Contributions from other sources: \$381,884.)  <u>Outcome/Output:</u> The habitat creation project will plant over three thousand oak trees in order to mitigate adverse impacts to sensitive habitat during the construction of the Willow Road extension interchange.	SLPP/12-13 CONST \$375,000 0513000097	2012-13 104-6060 SLPP 20.30.210.200	\$375,000

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Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(10a) Proposition 1B – Locally Administered State-Local Partnership Program (SLPP)</b>			<b>Resolution SLP1B-A-1213-18</b>	
6 \$1,454,000 Madera County Madera CTC 06-Madera	<b>Avenue 9 Improvements.</b> In Madera County, at the intersections of Avenue 9 & Road 36 and Avenue 9 and Road 35. Road rehabilitation and install left turn lanes.  (CEQA – CE, 09/12/2012.)  (Contributions from other sources: \$1,750,000.)  <u>Outcome/Output:</u> Improvements will provide a turn lane to improve safety for left hand turns at two intersections along this highly traveled route and reduce wear and tear on traveling vehicles.	SLPP/12-13 CONST \$1,454,000 0613000140	2012-13 104-6060 SLPP 20.30.210.200	\$1,454,000
7 \$1,000,000 City of Bakersfield Kern COG 06-Kern	<b>Mohawk Street Extension &amp; Improvements.</b> On Mohawk Street between Rosedale Highway and Siena Lane. Construction of a new two lane roadway with shoulders.  (CEQA – CE, 11/15/2012.)  (Contributions from other sources: \$1,393,000.)  <u>Outcome/Output:</u> This project will reduce vehicle miles travelled, reduce congestion, and improve transportation connectivity.	SLPP/12-13 CONST \$1,000,000 0613000161	2012-13 104-6060 SLPP 20.30.210.200	\$1,000,000
8 \$965,000 City of Fresno COFCG 06-Fresno	<b>Willow Avenue Widening.</b> On the west side of Willow Avenue between Barstow and Escalon Avenue. Construct Class I multi-purpose bicycle and pedestrian trail.  (Concurrent Consideration of Funding – Resolution E-13-19, March 2013.)  (Contributions from other sources: \$965,000.)  <u>Outcome/Output:</u> This project will improve traffic operations, reduce traffic accidents, and provide safe pedestrian access and improve circulation.	SLPP/12-13 CONST \$965,000 0613000166	2012-13 104-6060 SLPP 20.30.210.200	\$965,000
9 \$7,000,000 City of Corona RCTC 08-Riverside	<b>Foothill Parkway Westerly Extension.</b> In the city of Corona, from approximately 600 feet west of Skyline Drive to Green River Road. Construct new 4-lane roadway including landscaped medians, new traffic signals, and landscaping.  (Future Consideration of Funding – Resolution E-12-70, December 2012.)  (Contributions from other sources: \$16,518,000.)  <u>Outcome/Output:</u> This project will construct two miles of new road to accommodate planned circulation needs and improving air quality by reducing congestion.	SLPP/12-13 CONST \$7,000,000 0813000097	2012-13 104-6060 SLPP 20.30.210.200	\$7,000,000
10 \$1,550,000 City of Indian Wells RCTC 08-Riverside	<b>Highway 111 Improvements.</b> On Highway 111, from Hospitality Row to Cook Street. Widen highway from four to six lanes, landscaped medians, new synchronized traffic signals, and protected traffic turn lanes.  (Concurrent Consideration of Funding – Resolution E-13-13, March 2013.)  (Contributions from other sources: \$1,550,000.)  <u>Outcome/Output:</u> This project will widen 0.6 mile of roadway, eliminate a congestion chokepoint, and improve motorist and pedestrian safety.	SLPP/12-13 CONST \$1,550,000 0813000099	2012-13 104-6060 SLPP 20.30.210.200	\$1,550,000

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Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(10a) Proposition 1B – Locally Administered State-Local Partnership Program (SLPP)</b>			<b>Resolution SLP1B-A-1213-18</b>	
11 \$1,302,000 San Bernardino County SANBAG 08-San Bernardino	<b>Maple Lane Drainage and Slope Improvements.</b> In the community of Sugarloaf near Big Bear Lake, on Maple Lane from 0.27 mile north of Baldwin Road to State Route 38. Drainage improvements to protect the roadway and existing utilities.  (Concurrent Consideration of Funding – Resolution E-13-18, March 2013.)  (Contributions from other sources: \$1,302,000.)  <u>Outcome/Output:</u> This project will allow Maple Lane to continue as an emergency evacuation route for the mountain communities of Sugarloaf and Moonridge.	SLPP/12-13 CONST \$1,302,000 0813000094	2012-13 104-6060 SLPP 20.30.210.200	\$1,302,000
12 \$1,036,000 City of El Centro ICTC 11-Imperial	<b>City of El Centro Street Rehabilitation.</b> In the city of El Centro. Project will rehabilitate roadway segments of various city streets.  (CEQA – CE, 10/17/2012.)  (Contributions from other sources: \$1,036,000.)  <u>Outcome/Output:</u> Rehabilitation of 11.33 lane-miles of local road will maximize mobility and accessibility for all people and goods in the region by removing potholes and cracks and creating smoother driving surface. Ensure travel safety and reliability.	SLPP/12-13 CONST \$1,036,000 1112000187	2012-13 104-6060 SLPP 20.30.210.200	\$1,036,000
13 \$68,000 City of Westmoreland ICTC 11-Imperial	<b>6<sup>th</sup> Street and G Street Pavement Rehabilitation.</b> In the city of Westmoreland. Project will rehabilitate the roadway on 6 <sup>th</sup> street from N. Center Street to F Street and G Street from 7 <sup>th</sup> and 6 <sup>th</sup> Streets.  (CEQA – CE, 10/09/2012.)  (Contributions from other sources: \$68,000.)  <u>Outcome/Output:</u> This project will mitigate the existing poor pavement and drainage conditions on the roadway. The rehabilitated road will improve traffic safety for the area.	SLPP/12-13 CONST \$68,000 1113000072	2012-13 104-6060 SLPP 20.30.210.200	\$68,000
14 \$161,000 City of Holtville ICTC 11-Imperial	<b>Grape Avenue Improvements.</b> At Grape Avenue, between 4 <sup>th</sup> and 5 <sup>th</sup> Street. Bicycle and pedestrian improvements.  (CEQA – CE, 07/26/2012.)  (Contributions from other sources: \$162,000.)  <u>Outcome/Output:</u> This project will reduce hazards to motorists and pedestrians. The proposed project would also add new Class I bike lane facilities where currently none exists. This project will maximize mobility and accessibility for all people and good in the region by removing potholes and cracks and creating smoother driving surface. Ensure travel safety and reliability for all people and good in the region. Preserve and ensure sustainability regionally.	SLPP/12-13 CONST \$161,000 1113000073	2012-13 104-6060 SLPP 20.30.210.200	\$161,000

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<b>2.5g.(10a) Proposition 1B – Locally Administered State-Local Partnership Program (SLPP)</b>			<b>Resolution SLP1B-A-1213-18</b>	
15 \$133,000 City of Calipatria ICTC 11-Imperial	<b>Lake Avenue Improvements.</b> At Lake Avenue, between Elder Street and Bowles Road. Street rehabilitation. (CEQA – CE, 07/26/2012.)  (Contributions from other sources: \$138,000.)  <u>Outcome/Output:</u> This project will maximize mobility and accessibility for all people and good in the region by removing potholes and cracks and creating smoother driving surface. Ensure travel safety and reliability for all people and good in the region. Preserve and ensure sustainability regionally.	SLPP/12-13 CONST \$133,000 1113000074	2012-13 104-6060 SLPP 20.30.210.200	\$133,000
16 \$625,000 City of Brawley ICTC 11-Imperial	<b>Eastern Avenue Rehabilitation.</b> At Eastern Avenue, between Jones Street and Malan Street. Reconstructions and rehabilitation. (CEQA – CE, 10/01/2012.)  (Contributions from other sources: \$625,000.)  <u>Outcome/Output:</u> This project will reduce hazards to motorists and pedestrians. The proposed project would also add new Class I bike lane facilities where currently none exists. This project will maximize mobility and accessibility for all people and good in the region by removing potholes and cracks and creating smoother driving surface. Ensure travel safety and reliability for all people and good in the region. Preserve and ensure sustainability regionally.	SLPP/12-13 CONST \$625,000 1113000075	2012-13 104-6060 SLPP 20.30.210.200	\$625,000
17 \$384,000 City of Imperial ICTC 11-Imperial	<b>South N Street Reconstruction.</b> On South N Street, between Barioni Boulevard and 5 <sup>th</sup> Street. Install 40 foot wide roadway, curb and gutter, and sidewalks. (CEQA – CE, 09/27/2012.)  (Contributions from other sources: \$384,000.)  <u>Outcome/Output:</u> This project will allow industrial users to have access to the southeast part of the City's town core.	SLPP/12-13 CONST \$384,000 1113000084	2012-13 104-6060 SLPP 20.30.210.200	\$384,000
18 \$400,000 City of Calexico ICTC 11-Imperial	<b>Downtown Repaving – City of Calexico.</b> Resurface approximately 3,800 feet of local road. (CEQA – CE, 10/07/2012.)  (Contributions from other sources: \$400,000.)  <u>Outcome/Output:</u> This project will provide safe driving conditions and improve traffic safety for all users on the collector arterials.	SLPP/12-13 CONST \$400,000 1113000088	2012-13 104-6060 SLPP 20.30.210.200	\$400,000
19 \$515,000 City of Calexico ICTC 11-Imperial	<b>5<sup>th</sup> Street Repaving.</b> In the city of Calexico, on 5 <sup>th</sup> Street between Highway 111 and Heber Avenue. Repave existing pavement, replace sidewalk, curb, and gutter. (CEQA – CE, 10/07/2012.)  (Contributions from other sources: \$515,000.)  <u>Outcome/Output:</u> This project will greatly improve traffic safety and mobility and accessibility for all users in the area.	SLPP/12-13 CONST \$515,000 1113000089	2012-13 104-6060 SLPP 20.30.210.200	\$515,000

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<b>2.5g.(10a) Proposition 1B – Locally Administered State-Local Partnership Program (SLPP) Projects off the State Highway</b>			<b>Resolution SLP1B-A-1213-18</b>	
20 \$650,000 Imperial County ICTC 11-Imperial	<b>Willoughby Road Resurfacing.</b> From Dogwood Road to Clark Road. Street Resurfacing.  (CEQA – CE, 11/06/2012.)  (Contributions from other sources: \$650,000.)  <u>Outcome/Output:</u> This project will maximize mobility and accessibility for all people and good in the region by removing potholes and cracks and creating smoother driving surface. Ensure travel safety and reliability for all people and good in the region. Preserve and ensure sustainability regionally.	SLPP/12-13 CONST \$650,000 1113000096	2012-13 104-6060 SLPP 20.30.210.200	\$650,000
21 \$901,000 Imperial County ICTC 11-Imperial	<b>Dogwood Road Resurfacing.</b> In the county of Imperial. Project will consist of full depth reclamation of approximately 0.5 mile of roadway with the Herber Townsite. The entire 3 mile length of the project will include a 3 inch overlay of asphalt concrete.  (CEQA – CE, 0820/2012.)  (Contributions from other sources: \$901,000.)  <u>Outcome/Output:</u> This project will maximize mobility and accessibility for all people and good in the region by removing potholes and cracks and creating smoother driving surface. Ensure travel safety and reliability for all people and good in the region. Preserve and ensure sustainability regionally.	SLPP/12-13 CONST \$901,000 1113000097	2012-13 104-6060 SLPP 20.30.210.200	\$901,000
22 \$1,551,000 City of Santa Ana OCTA 12-Orange	<b>Broadway and McFadden Avenue Rehabilitation.</b> On Broadway from Civic Center Drive to 700 feet north of Santa Clara Avenue and McFadden Avenue from Newhope Street to Euclid Street. Rehabilitate asphalt concrete roadway, including replacement of curb ramps, curb and gutter, sidewalk, and adjusting utilities to grade.  (CEQA – CE, 12/05/2012.)  (Contributions from other sources: \$2,213,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 4-lane facilities and increase the useful life of 1.7 miles of roadway.	SLPP/12-13 CONST \$1,551,000 <del>1200009559</del> <b>1213000190</b>	2012-13 104-6060 SLPP 20.30.210.200	\$1,551,000
23 \$571,000 City of Buena Park OCTA 12-Orange	<b>La Palma Avenue Rehabilitation.</b> On La Palma Avenue, from Beach Boulevard to the east city limits. Rehabilitate the asphalt concrete roadway, including reconstructing curb ramps, curb and gutter, sidewalk and adjusting utilities to grade.  (CEQA – CE, 05/03/2012.)  (Contributions from other sources: \$571,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 5-lane and 6-lane facilities and increase the useful life of 1.1 miles of roadway.	SLPP/12-13 CONST \$571,000 1200020085	2012-13 104-6060 SLPP 20.30.210.200	\$571,000

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<b>2.5g.(10a) Proposition 1B – Locally Administered State-Local Partnership Program (SLPP)</b>			<b>Resolution SLP1B-A-1213-18</b>	
24 \$318,000 City of Aliso Viejo OCTA 12-Orange	<b>Aliso Creek Road Rehabilitation.</b> On Aliso Creek Road from Pacific Park Drive to Enterprise. Rehabilitate asphalt concrete roadway including the modification of traffic signals, traffic signal detection loops, pavement striping and markers, and adjusting utilities to grade.  (CEQA – CE, 07/12/2012.)  (Contributions from other sources: \$373,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 4-lane facility and increase the useful life of 0.4 mile of roadway.	SLPP/12-13 CONST \$318,000 1200020092	2012-13 104-6060 SLPP 20.30.210.200	\$318,000
25 \$479,000 City of Lake Forest OCTA 12-Orange	<b>Lake Forest Drive &amp; Rockfield Boulevard Resurfacing.</b> On Lake Forest Drive, from Dimension Drive to Rancho Parkway and on Rockfield Boulevard, from Lake Forest Drive to El Toro Road. Rehabilitate asphalt concrete roadway including the replacement of curb ramps, curb and gutter, sidewalk, and adjusting utilities to grade.  (CEQA – CE, 05/25/2012.)  (Contributions from other sources: \$556,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 4-lane facility and increase the useful life of 1.7 miles of roadway.	SLPP/12-13 CONST \$479,000 1200020139	2012-13 104-6060 SLPP 20.30.210.200	\$479,000
26 \$318,000 City of Los Alamitos OCTA 12-Orange	<b>Business Area Street Improvements.</b> In the Cerritos business area, on various local commercial and arterial streets. Rehabilitate asphalt concrete roadway and adjusting utilities to grade.  (CEQA – CE, 01/07/2013.)  (Contributions from other sources: \$318,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 2-lane and 4-lane arterial facilities to increase the useful life of 2.17 miles of roadway.	SLPP/12-13 CONST \$318,000 1200020140	2012-13 104-6060 SLPP 20.30.210.200	\$318,000
27 \$396,000 City of Fountain Valley OCTA 12-Orange	<b>Brookhurst Street Improvements.</b> On Brookhurst Street, from Slater Avenue to Warner Avenue. Rehabilitate asphalt concrete roadway, including the replacement of curb and gutter, sidewalk, curb ramps, and adjusting utilities to grade.  (CEQA – CE, 07/03/2012.)  (Contributions from other sources: \$536,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 6-lane facility and increase the useful life of 0.5 mile of roadway.	SLPP/12-13 CONST \$396,000 1200020304	2012-13 104-6060 SLPP 20.30.210.200	\$396,000
28 \$362,000 City of Brea OCTA 12-Orange	<b>Lambert Road Rehabilitation #2.</b> On Lambert Road from Tamarack Avenue to State College Boulevard. Rehabilitate asphalt concrete roadway, including the construction of ADA-compliant curb ramps, and adjusting utilities to grade.  (CEQA – CE, 10/19/2012.)  (Contributions from other sources: \$362,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 6-lane facility and increase the useful life of 1.0 mile of roadway.	SLPP/12-13 CONST \$362,000 1213000047	2012-13 104-6060 SLPP 20.30.210.200	\$362,000

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<b>2.5g.(10a) Proposition 1B – Locally Administered State-Local Partnership Program (SLPP)</b>			<b>Resolution SLP1B-A-1213-18</b>	
29 \$252,000 Orange County OCTA 12-Orange	<b>Skyline Drive Reconstruction.</b> On Skyline Drive, from Apsley Road/Equestrian Drive to Cowan Heights Drive. Reconstruct the asphalt concrete roadway including the replacement of curb ramps, curb, and gutter and adjusting utilities to grade.  (CEQA – CE, 12/05/2012.)  (Contributions from other sources: \$478,000.)  <u>Outcome/Output:</u> This project will reconstruct the existing 2-lane facility and increase the useful life of 0.25 mile of roadway.	SLPP/12-13 CONST \$252,000 1213000048	2012-13 104-6060 SLPP 20.30.210.200	\$252,000
30 \$158,000 Orange County OCTA 12-Orange	<b>Dale Street Reconstruction.</b> On Dale Street, from the Orange County Flood Control District Channel (OCFDC) to Orangewood Avenue. Reconstruct the asphalt concrete roadway including the construction of ADA compliant curb ramps.  (CEQA – CE, 12/05/2012.)  (Contributions from other sources: \$158,000.)  <u>Outcome/Output:</u> This project will reconstruct the existing 2-lane facility and increase the useful life of 0.4 mile of roadway.	SLPP/12-13 CONST \$158,000 1213000049	2012-13 104-6060 SLPP 20.30.210.200	\$158,000
31 \$764,000 Orange County OCTA 12-Orange	<b>La Colina Drive Pavement Rehabilitation.</b> On La Colina Drive, from approximately 140 feet east of Wedgewood Circle to the east Orange County Limits. Reconstruct the asphalt concrete shoulders and replace drainage facilities.  (CEQA – CE, 12/05/2012.)  (Contributions from other sources: \$915,000.)  <u>Outcome/Output:</u> This project will reconstruct the existing 2-lane facility and increase the useful life of 0.87 mile of roadway.	SLPP/12-13 CONST \$764,000 1213000050	2012-13 104-6060 SLPP 20.30.210.200	\$764,000
32 \$168,000 City of Cypress OCTA 12-Orange	<b>Cerritos Avenue East Widening.</b> On the south side of Cerritos Avenue from Walker Street to Angela Avenue. Widen to three-through lanes including the replacement of curb ramps, curb and gutter, sidewalk, construction of a retaining wall, and adjusting utilities to grade.  (CEQA – CE, 07/19/2012.)  (Contributions from other sources: \$210,000.)  <u>Outcome/Output:</u> This project will widen 0.2 mile of existing roadway to relieve traffic congestion.	SLPP/12-13 CONST \$168,000 1213000051	2012-13 104-6060 SLPP 20.30.210.200	\$168,000
33 \$180,000 City of Cypress OCTA 12-Orange	<b>Valley View Street Rehabilitation.</b> On Valley View Street from Stanton Channel to Katella Avenue and on Lincoln Avenue from Moody Street to Walker Street. Rehabilitate asphalt concrete roadway and adjusting utilities to grade.  (CEQA – CE, 09/26/2012.)  (Contributions from other sources: \$222,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 4-lane and 6-lane facility and increase the useful life of 1.0 mile of roadway.	SLPP/12-13 CONST \$180,000 1213000052	2012-13 104-6060 SLPP 20.30.210.200	\$180,000

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<b>2.5g.(10a) Proposition 1B – Locally Administered State-Local Partnership Program (SLPP)</b>			<b>Resolution SLP1B-A-1213-18</b>	
<b>Projects off the State Highway</b>				
34 \$842,000 City of Garden Grove OCTA 12-Orange	<b>Local Road Rehabilitation.</b> In the city of Garden Grove, on Garden Grove Boulevard, Westminster Avenue, and Chapman Avenue. Rehabilitate asphalt concrete roadway, including the replacement of curb and gutter, sidewalk, installing traffic detector loops, pavement striping and markings, and adjusting utilities to grade.  (CEQA – CE, 05/30/2012.)  (Contributions from other sources: \$1,736,400.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 6-lane facility and increase the useful life of 1.13 miles of roadway.	SLPP/12-13 CONST \$842,000 1213000056	2012-13 104-6060 SLPP 20.30.210.200	\$842,000
35 \$314,000 City of La Habra OCTA 12-Orange	<b>Idaho Street Rehabilitation.</b> On Idaho Street, from Lambert Road to Imperial Highway. Rehabilitate asphalt concrete roadway, including the correction of drainage deficiencies and adjusting utilities to grade.  (CEQA – CE, 11/01/2012.)  (Contributions from other sources: \$314,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 4-lane facility and increase the useful life of 0.42 mile of roadway.	SLPP/12-13 CONST \$314,000 1213000060	2012-13 104-6060 SLPP 20.30.210.200	\$314,000
36 \$318,000 City of La Palma OCTA 12-Orange	<b>La Palma Avenue Road Rehabilitation.</b> On La Palma Avenue, from the west city limits to Valley View Street. Rehabilitate asphalt concrete roadway, including the replacement of curb ramps, curb and gutter, sidewalk, and adjusting utilities to grade.  (CEQA – CE, 11/21/2012.)  (Contributions from other sources: \$318,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 4-lane facility and increase the useful life of 1.5 miles of roadway.	SLPP/12-13 CONST \$318,000 1213000061	2012-13 104-6060 SLPP 20.30.210.200	\$318,000
37 \$413,000 City of Laguna Niguel OCTA 12-Orange	<b>La Paz Road Rehabilitation.</b> On La Paz Road, from Kings Road to Aliso Creek Road. Rehabilitate asphalt concrete roadway, including the replacement of curb and gutter and adjusting utilities to grade.  (CEQA – CE, 11/07/2012.)  (Contributions from other sources: \$413,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 4-lane facility and increase the useful life of 1.25 miles of roadway.	SLPP/12-13 CONST \$413,000 1213000064	2012-13 104-6060 SLPP 20.30.210.200	\$413,000
38 \$293,000 City of Laguna Woods OCTA 12-Orange	<b>El Toro Road Reconstruction.</b> In the city of Laguna Woods, on El Toro Road from Calle Sonora to 900 feet west of Moulton Parkway. Repave roadway and construct ADA-compliant curb ramps.  (CEQA – CE, 09/21/2012.)  (Contributions from other sources: \$298,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing six-lane roadway and increase the useful life of 0.3 mile of roadway.	SLPP/12-13 CONST \$293,000 1213000082	2012-13 104-6060 SLPP 20.30.210.200	\$293,000

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<b>2.5g.(10a) Proposition 1B – Locally Administered State-Local Partnership Program (SLPP)</b>			<b>Resolution SLP1B-A-1213-18</b>	
<b>Projects off the State Highway</b>				
39 \$318,000 City of San Clemente OCTA 12-Orange	<b>Camino De Los Mares Rehabilitation.</b> On Camino De Los Mares from Avenida Vaquero to Camino Vera Cruz. Rehabilitate asphalt concrete roadway, including the replacement of curb ramps, curb and gutter and adjusting utilities to grade.  (CEQA – CE, 07/11/2012.)  (Contributions from other sources: \$1,082,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 4-lane facility and increase the useful life of 1.07 miles of roadway.	SLPP/12-13 CONST \$318,000 1213000096	2012-13 104-6060 SLPP 20.30.210.200	\$318,000
40 \$1,036,000 City of Orange OCTA 12-Orange	<b>Jamboree Road Rehabilitation.</b> On Jamboree Road, from the south city limits to Chapman Avenue. Rehabilitate asphalt concrete roadway, including the replacement of curb ramps, curb and gutter, sidewalk, and adjusting utilities to grade.  (CEQA – CE, 11/08/2012.)  (Contributions from other sources: \$1,036,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 4-lane facility and increase the useful life of 1.3 miles of roadway.	SLPP/12-13 CONST \$1,036,000 1213000107	2012-13 104-6060 SLPP 20.30.210.200	\$1,036,000
41 \$318,000 City of San Juan Capistrano OCTA 12-Orange	<b>Local Streets Rehabilitation.</b> On various local streets in the city of San Juan Capistrano. Reconstruct the asphalt concrete roadway including the replacement of curb ramps and adjusting utilities to grade.  (CEQA – CE, 11/20/2012.)  (Contributions from other sources: \$486,000.)  <u>Outcome/Output:</u> This project will reconstruct the existing 2-lane facility and increase the useful life of 0.4 mile of roadway.	SLPP/12-13 CONST \$318,000 1213000108	2012-13 104-6060 SLPP 20.30.210.200	\$318,000
42 \$318,000 City of Seal Beach OCTA 12-Orange	<b>Arterial and Local Streets Road Rehabilitation.</b> On various arterial and local streets. Rehabilitate asphalt concrete roadway and adjusting utilities to grade.  (CEQA – CE, 11/09/2012.)  (Contributions from other sources: \$337,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 2-lane facility and increase the useful life of 1.35 miles of roadway.	SLPP/12-13 CONST \$318,000 1213000109	2012-13 104-6060 SLPP 20.30.210.200	\$318,000
43 \$318,000 City of Stanton OCTA 12-Orange	<b>Citywide Street Rehabilitation.</b> On various arterial and local streets throughout the city. Rehabilitate asphalt concrete roadway, including the replacement of curb ramps, curb and gutter, sidewalk, and adjusting utilities to grade.  (CEQA – CE, 06/19/2012.)  (Contributions from other sources: \$318,000.)  <u>Outcome/Output:</u> This project will rehabilitate and increase the useful life of 1.5 miles of roadway.	SLPP/12-13 CONST \$318,000 1213000110	2012-13 104-6060 SLPP 20.30.210.200	\$318,000

2.5 Highway Financial Matters

Technically Corrected August 6, 2013  
(Project 22)

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(10a) Proposition 1B – Locally Administered State-Local Partnership Program (SLPP)</b>			<b>Resolution SLP1B-A-1213-18</b>	
<b>Projects off the State Highway</b>				
44 \$200,000 City of Tustin OCTA 12-Orange	<b>Newport Avenue Bicycle Trail Reconstruction.</b> On Newport Avenue from Main Street to Irvine Boulevard. Reconstruct the existing asphalt concrete Class I bicycle trail with concrete, constructing sidewalks, landscaping, and ADA-compliant curb ramps.  (CEQA – CE, 08/17/2012.)  (Contributions from other sources: \$200,000.)  <u>Outcome/Output:</u> This project will reconstruct 0.45 mile of existing bike trail to provide a safer and smoother riding surface.	SLPP/12-13 CONST \$200,000 1213000111	2012-13 104-6060 SLPP 20.30.210.200	\$200,000
45 \$35,000 City of Tustin OCTA 12-Orange	<b>Enderle Center Drive and Vandenberg Lane Intersections Improvements.</b> At the intersection of Enderle Center Drive and Vandenberg Lane. Construct a raised planter island on the south side of the tee-intersection with raised curb, landscaping and irrigation, and ADA-compliant curb ramps.  (CEQA – CE, 08/17/2012.)  (Contributions from other sources: \$103,000.)  <u>Outcome/Output:</u> This project will provide increased awareness and safety at the intersections to reduce traffic accidents.	SLPP/12-13 CONST \$35,000 1213000112	2012-13 104-6060 SLPP 20.30.210.200	\$35,000
46 \$358,000 City of Tustin OCTA 12-Orange	<b>Irvine Boulevard and McFadden Avenue Rehabilitation.</b> On McFadden Avenue from Mantle Lane to Pasadena Avenue and on Irvine Boulevard from Prospect Avenue to Holt Avenue. Rehabilitate asphalt concrete roadway, including the replacement of curb ramps, curb and gutter, and adjusting utilities to grade.  (CEQA – CE, 08/17/2012.)  (Contributions from other sources: \$469,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 4-lane facility and increase the useful life of 1.05 miles of roadway.	SLPP/12-13 CONST \$358,000 1213000113	2012-13 104-6060 SLPP 20.30.210.200	\$358,000
47 \$520,000 City of Westminster OCTA 12-Orange	<b>Brookhurst Street and Various Street Improvements.</b> On Brookhurst Street and various residential streets. Rehabilitate asphalt concrete roadway, including the replacement of curb ramps, curb and gutter, sidewalk, and adjusting utilities to grade.  (CEQA – CE, 06/28/2012.)  (Contributions from other sources: \$692,000.)  <u>Outcome/Output:</u> This project will rehabilitate the existing 6-lane facility and increase the useful life of 0.4 miles of roadway.	SLPP/12-13 CONST \$520,000 1213000115	2012-13 104-6060 SLPP 20.30.210.200	\$520,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.9b.  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Denix D. Anbiah  
Division Chief  
Local Assistance

Subject: **TECHNICAL CORRECTION TO PREVIOUSLY APPROVED RESOLUTION  
RESOLUTION FP-08-42**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) approve a technical correction to Resolution FP-08-42 originally approved on April 15, 2009.

## **ISSUE:**

At its April 2009 meeting, the Commission approved Resolution FP-08-42 allocating \$10,000,000 for 36 Environmental Enhancement and Mitigation (EEM) Program projects. A technical correction is need for Project 6: Middle Yuba River project, to revise the recipient from “The Trust for Public Land” to “Bear Yuba Land Trust” in the vote box on the Book Item Attachment.

There is no change to the Book Item Memorandum.

The required changes are reflected in strikethrough and bold on the attached revised vote list.

Attachment

2.5 Highway Financial Matters

Technically Corrected August 6, 2013  
(Project 6)

Project # Allocation Amount Applicant RTPA/CTC Dst-County	Location Project Description	EA Program	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(4) Locally Administered Environmental Enhancement and Mitigation Projects</b>				
<b>Resolution FP-08-42</b>				
1 \$350,000 City of Arcata <u>HcAG</u> 01-Humboldt	Stromberg Forest Acquisition (Sunny Brae Forest Addition). Acquire 25.5 acre Stromberg property to add to the 175-acre Sunny Brae Forest in the city of Arcata, to preserve a working redwood forest, contribute to protection of water quality, protect biological resources of property and lands, and expand the recreational opportunities.  (Contributions from other sources: \$25,000.)	#18-1 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
2 \$350,000 City of Redding <u>SCRTPA</u> 02-Shasta	Sacramento River Trail Enhancement in Redding, near Auditorium Drive. Widen portions of the trail system; add trail furniture, observation and way-finding signs; and install drinking fountains to meet greater capacity needs.  (Contributions from other sources: \$20,000.)	#18-2 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
3 \$200,000 City of Chico <u>BCAG</u> 03-Butte	Iron Canyon Fish Passage Project, on Big Chico Creek, an east side tributary to the Sacramento River. Rehabilitate and expand an existing set of concrete weirs that were constructed in 1958 to provide spring run salmon and steelhead passage through an area of basalt boulders that obstruct flow on Big Chico Creek in Bidwell Park to an ecological reserve.  (Contributions from other sources: \$1,926,218.)	#18-45 EEM/08-09 \$200,000	2008-09 101-0183 EEM 20.30.207.811	\$200,000
4 \$350,000 Truckee Donner Land Trust <u>Nevada LTC</u> 03-Nevada	Billy Mack Canyon-Property Acquisition. Acquire the 55 +/- acre Billy Mack Canyon property near the west end of Donner Lake, to protect the property's valuable riparian habitat and recreational resources, and to provide mitigation for the Truckee Bypass construction project.  (Contributions from other sources: \$275,000.)	#18-03 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
5 \$350,000 Sacramento County <u>SACOG</u> 03-Sacramento	Dry Creek Parkway Acquisition FY 2008-09, near Rio Linda. Acquire 66 acres to will provide space and crucial link in the Dry Creek watershed's 20 mile riparian corridor.  (Contributions from other sources: \$521,240.)	#18-04 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
6 \$350,000 <del>The Trust for Public Land</del> <b>Bear Yuba Land Trust</b> <u>SACOG</u> 03-Yuba	Middle Yuba River – Rice's Crossing Resource Lands Acquisition Project, in Nevada and Yuba Counties near Marysville Road. Acquire the 2,706 acre Rice's Crossing property to provide the public with access to 9.5 miles of the Yuba River. Protect the riparian and wildlife habitat, and provide additional mitigation for the impacts related to the State Route 20 Safety Improvement Project.  (Contributions from other sources: \$7,285,500.)	#18-5 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
7 \$350,000 The Trust for Public Land <u>SACOG</u> 03-Yuba	Yuba River: Excelsior Resource Lands Acquisition Project. Acquire the 815 acre Excelsior property adjacent to the Yuba River in Yuba and Nevada Counties to protect hundreds of acres of oak woodland, riparian and wildlife habitat, and to provide additional mitigation for the impacts related to the State Route 20 Safety Improvement Project.  (Contributions from other sources: \$4,580,000.)	#18-6 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000

Project # Allocation Amount Applicant RTPA/CTC Dst-County	Location Project Description	EA Program	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(4) Locally Administered Environmental Enhancement and Mitigation Projects</b>				
<b>Resolution FP-08-42</b>				
8 \$201,700 City of Berkeley MTC 04-Alameda	Berkeley Urban Reforestation Project. Plant 1200 trees throughout the city of Berkeley to mitigate the impacts from Interstate 80.  (Contributions from other sources: \$252,000.)	#18-7 EEM/08-09 \$201,700	2008-09 101-0183 EEM 20.30.207.811	\$201,700
9 \$350,000 Muir Heritage Land Trust MTC 04-Contra Costa	Franklin Canyon Acquisition, near Coronado and Refugio Valley Road. Acquire the 423 acre Franklin Canyon property contiguous to MHLT's 702-acre Fernandez Ranch and to close a critical gap in the 550-mile Bay area Ridge Trail and ensure regional connections to other nearby protected lands.  (Contributions from other sources: \$2,060,000.)	#18-8 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
10 \$350,000 City of Belmont MTC 04-San Mateo	US Highway 101/Ralston Avenue Pedestrian/Bike Bridge Project. Improve and mitigate pedestrian and bicycle access related to the Route 101/Ralston Avenue Interchange Modification Project.  (Contributions from other sources: \$7,835,000.)	#18-09 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
11 \$205,753 City of Morgan Hill MTC 04-Santa Clara	Sequestering Carbon Dioxide Using Urban Forestry, in Morgan Hill directly south of San Jose. Plant 325 native oak trees on 52 acres of city owned land to replace the carbon sink that was removed and offset additive vehicle emissions from the construction of Butterfield Boulevard.  (Contributions from other sources: \$15,804.)	#18-10 EEM/08-09 \$205,753	2008-09 101-0183 EEM 20.30.207.811	\$205,753
12 \$245,763 Sotoyome Resource Conservation District MTC 04-Sonoma	Arundo Removal & Riparian Enhancement – Russian River, near Highway 128 and River Road. Control invasive Arundo Donax on 200 infested riparian acres on the Russian River in stream reaches upstream of the Highway 128/Geyserville Bridge as a second phase of the Russian River Arundo Donax removal and Riparian Enhancement Program.  (Contributions from other sources: \$80,286.)	#18-11 EEM/08-09 \$245,763	2008-09 101-0183 EEM 20.30.207.811	\$245,763
13 \$150,350 Sonoma County Transportation and Public Works MTC 04-Sonoma	California Tiger Salamander Wildlife Corridor (Road Crossing) Enhancement, near Stony Point Road, 1000' south of Meacham Road, southwest of Cotati. Install a tunnel system, consisting of several small culverts/pipes under a busy roadway to provide passage to reduce mortalities of federally endangered California tiger salamanders that attempt to cross the road.  (Contributions from other sources: \$27,700.)	#18-12 EEM/08-09 \$150,350	2008-09 101-0183 EEM 20.30.207.811	\$150,350
14 \$232,950 Laguna de Santa Rosa Foundation MTC 04-Sonoma	Duer Creek Riparian Restoration Project. Restore 16 acres of riparian and oak woodland along 3,322 linear feet of Duer Creek, a tributary creek just upstream of the Occidental Road Reconstruction Project to control non-native invasive species, plant and irrigate 2,500 native trees and shrubs, and maintain the site for three years.  (Contributions from other sources: \$532,000.)	#18-13 EEM/08-09 \$232,950	2008-09 101-0183 EEM 20.30.207.811	\$232,950

Project # Allocation Amount Applicant RTPA/CTC Dst-County	Location Project Description	EA Program	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(4) Locally Administered Environmental Enhancement and Mitigation Projects</b>				
<b>Resolution FP-08-42</b>				
15 \$206,786 Atascadero Land Preservation Society SLOCOG 05-San Luis Obispo	Atascadero Creek Resource Lands Acquisition Project, near Old Morro Road/Highway 41. Acquire and permanently protect the Atascadero Creek Property in the city of Atascadero. The 103.24 acre property has multiple resource values, including oak woodlands and frontage on Atascadero Creek.  (Contributions from other sources: \$1,000,000.)	#18-15 EEM/08-09 \$206,786	2008-09 101-0183 EEM 20.30.207.811	\$206,786
16 \$110,000 Land Trust for Santa Barbara Co. SBCAG 05-Santa Barbara	Lower Refugio Creek Restoration Project. Near Refugio Canyon Road. Planting of riparian trees, and enhances scenic resources from Highway 101 and will minimize erosion on agricultural land.  (Contributions from other sources: \$456,450.)	#18-16 EEM/08-09 \$110,000	2008-09 101-0183 EEM 20.30.207.811	\$110,000
17 \$298,000 Madera County RMA MCTC 06-Madera	Watering Hole Exhibit – Fossil Discovery Center. Construct a .5 acre pond and restore 12 species of native plants to create a Pleistocene Era watering hole at the Fossil Discovery Center, adjacent to the State Route 99 Fairmead Freeway Conversion and Interchange Project in Madera County.	#18-17 EEM/08-09 \$298,000	2008-09 101-0183 EEM 20.30.207.811	\$298,000
18 \$311,400 City of Visalia TCAG 06-Tulare	Reforest Visalia. Plant 800 shade trees, 2,500 under story plants and irrigation along important transportation corridors throughout the city while providing mitigation above that required for the two related transportation facilities Route 198 Gap Closure and upgrades to State Route 63-Mooney Boulevard.  (Contribution from other sources: \$38,500.)	#18-18 EEM/08-09 \$311,400	2008-09 101-0183 EEM 20.30.207.811	\$311,400
19 \$55,000 Tree Musketeers LACMTA 07-Los Angeles	El Segundo Hometown Forest. Plant 200 street trees to mitigate air pollution in El Segundo, a small community heavily impacted by LAX traffic, directly related to the I-105 Freeway, construction and modification.  (Contribution from other sources: \$10,000.)	#18-19 EEM/08-09 \$55,000	2008-09 101-0183 EEM 20.30.207.811	\$55,000
20 \$341,300 Amigos De Los Rios LACMTA 07-Los Angeles	Emerald Necklace/Rio Hondo River Project, a greenbelt project along the Rio Hondo River corridor near Santa Anita Avenue. Landscape with trees and shrubs to mitigate the effects of carbon dioxide emission, background noise, and contaminated runoff caused by traffic increase after the I-10 HOV expansion.  (Contribution from other sources: \$50,000.)	#18-20 EEM/08-09 \$341,300	2008-09 101-0183 EEM 20.30.207.811	\$341,300
21 \$350,000 City of Pasadena LACMTA 07-Los Angeles	Native Plant Restoration of the Arroyo Seco. Improve landscape and restore habitat at various Arroyo locations, including Brookside Park, parking lots, parkways, islands, medians and adjacent slopes. Native plants and drought tolerant species will be used to establish cohesive and aesthetically pleasing landscaped areas of the Arroyo Seco.	#18-21 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000

2.5 Highway Financial Matters

Technically Corrected August 6, 2013  
(Project 6)

Project # Allocation Amount Applicant RTPA/CTC Dst-County	Location Project Description	EA Program	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(4) Locally Administered Environmental Enhancement and Mitigation Projects</b>				
<b>Resolution FP-08-42</b>				
22 \$350,000 City of Simi Valley <u>VCTC</u> 07-Ventura	118 Freeway Interchange Landscape Enhancement. Plant oak trees at various interchanges along 118 Freeway in Simi Valley to enhance and mitigate the removal of mature trees resulting from the recent 23 Freeway widening project. Phase I of this project will plant approximately 400 five-gallon oak trees at Yosemite Avenue interchange.  (Contribution from other sources: \$200,000.)	#18-22 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
23 \$350,000 City of Indio <u>RCTC</u> 08-Riverside	Monroe Street Multi-Purpose Trail. Construct a multi-purpose trail along Monroe Street between Avenue 50 and Avenue 52. The trail will be constructed of decomposed granite and will include equestrian fencing and lighting to encourage recreational use by providing a safe corridor and expand the existing trail network in the community.  (Contribution from other sources: \$25,000.)	#18-23 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
24 \$350,000 Coachella Valley Conservation Commission <u>RCTC</u> 08-Riverside	NCCP Acquisitions and Sand Dune Ecosystem Conservation. Acquire land by the Willow Hole Coachella Valley Fringe-toed Lizard Preserve in Riverside County to protect habitat values and the sand transport process that sustains the Preserve's sand dune habitat.  (Contribution from other sources: \$700,000.)	#18-24 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
25 \$350,000 City of Big Bear Lake <u>SANBAG</u> 08-San Bernardino	Boulder Bay Improvements – Phase II, on Big Bear Lake at State Route 18, PM 46.3. Improvements include grading, drainage, xeriscape-landscaping, picnic pavilion with seating, gravel walking trails, lakefront access, fishing dock, demonstration garden (xeriscape themes, explanatory signs, rock seating areas), and non-motorized boat ramp.  (Contribution from other sources: \$300,000.)	#18-25 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
26 \$522,100 Eastern Sierra Land Trust <u>Mono LTC</u> 09-Mono	Centennial Ranch Easement Addition (CREA). Conservation Easement purchase on 718.8 acres of the Centennial Ranch in the Bridgeport Valley. This easement will border Scenic Highway 395 and the 6,390 acre existing easement on Centennial Ranch, and will mitigate environmental impacts from the Sonora Junction Wildlife Passage project.  (Contribution from other sources: \$828,525.)	#18-26 EEM/08-09 \$522,100	2008-09 101-0183 EEM 20.30.207.811	\$522,100
27 \$260,000 Town of Mammoth Lakes <u>Mono LTC</u> 09-Mono	Sherwin Street Bridge Project, in the Town of Mammoth Lakes Old Mammoth neighborhood. Construct a pedestrian bridge, multi-use path, parking, and directional and interpretive signage to provide a critical link to the Towns trail system and eliminate braided trails and improvised bridges throughout the areas sensitive lands.	#18-27 EEM/08-09 \$260,000	2008-09 101-0183 EEM 20.30.207.811	\$260,000
28 \$48,000 Amador County Transportation Commission <u>Amador LTC</u> 10-Amador	State Route 49/Sutter Hill Transit Center Highway Landscaping and Recreation Project, along State Route 49. Add substantial landscaping; enhance natural and visual resources; and provide access and visibility of the open space reserve for local residents and travelers. The site was acquired by the Department for the State Route 49 Amador Bypass project.  (Contribution from other sources: \$48,000.)	#18-28 EEM/08-09 \$48,000	2008-09 101-0183 EEM 20.30.207.811	\$48,000

2.5 Highway Financial Matters

Technically Corrected August 6, 2013  
(Project 6)

Project # Allocation Amount Applicant RTPA/CTC Dst-County	Location Project Description	EA Program	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(4) Locally Administered Environmental Enhancement and Mitigation Projects</b>				
<b>Resolution FP-08-42</b>				
29 \$25,373 City of Turlock <u>StanCOG</u> 10-Stanislaus	Christoffersen Parkway/Walnut Basin Tree Mitigation, in the city of Turlock. Reforest a 20 acre storm basin with 360 trees that were removed to build Christoffersen Parkway, to help remove the pollutants emitted by the increased traffic next to the basin.  (Contribution from other sources: \$11,406.)	#18-29 EEM/08-09 \$25,373	2008-09 101-0183 EEM 20.30.207.811	\$25,373
30 \$16,125 Tuolumne County Resource Conservation District <u>Tuolumne COG</u> 10-Tuolumne	State Route 49/Stockton Road – Forest Road Ailanthus Eradication and Restoration Project. Remove dozens of non-native, invasive, Ailanthus altissima trees at the highly visible SR 49 and treat Ailanthus stumps with herbicides (multiple applications). Replant with Chinese Pistache (Pistacia Chinensis), the “official” tree of the City of Sonora, Valley oak (Quercus Lobata) and with native grasses along Woods Creek.  (Contribution from other sources: \$3,630.)	#18-30 EEM/08-09 \$16,125	2008-09 101-0183 EEM 20.30.207.811	\$16,125
31 \$350,000 River Partners <u>SANDAG</u> 11-San Diego	Otay Delta Habitat Restoration Project, a 65-acre project west of I-5 on the San Diego Bay National Wildlife Refuge. Restore riparian habitat within the Otay River floodplain. The project, which represents a partnership among River Partners, USFWS, Coastal Conservancy, and CTC-EEMP, will provide structurally diverse habitat to support migratory birds, including listed species, while also filtering storm water destined for San Diego Bay.  (Contribution from other sources: \$685,000.)	#18-31 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
32 \$269,400 San Elijo Lagoon Conservancy <u>SANDAG</u> 11-San Diego	Restoration and Enhancement of San Elijo Lagoon through Improved Tidal Circulation, near Highway 101. The three year project involves periodic dredging of the inlet to San Elijo Lagoon, ensuring proper tidal circulation throughout. A small operation each winter/early spring to breach the sand berm that forms west of Highway 101, and a larger dredging operation east of Highway 101 under the Highway 101 bridge after the winter storm cycle.  (Contribution from other sources: \$35,000.)	#18-32 EEM/08-09 \$269,400	2008-09 101-0183 EEM 20.30.207.811	\$269,400
33 \$350,000 San Dieguito River Park, JPA <u>SANDAG</u> 11-San Diego	Lake Hodges Bikeway Access Phase 2: West Bernardo Bike Path and Cantilever, a proposed Roadside Recreational Project. The Bike Path would tie directly into the south abutment of the Bridge, enabling bicyclists and pedestrians to continue from the bridge to the community park and natural area on West Bernardo Drive in an aesthetic and safe Class I facility off the main road.  (Contribution from other sources: \$1,425,862.)	#18-33 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
34 \$350,000 City of San Marcos <u>SANDAG</u> 11-San Diego	South Lake Community Park: Phase 1, near Twin Oaks Valley Road. Final design and construct the South Lake Community Park (Phase 1) which includes a scenic overlook, a native plant walk, community amphitheater, a dog run area, a park ranger house, a traffic signal, public parking and restroom facilities.  (Contribution from other sources: \$1,748,470.)	#18-34 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000

Project # Allocation Amount Applicant RTPA/CTC Dst-County	Location Project Description	EA Program	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(4) Locally Administered Environmental Enhancement and Mitigation Projects</b>				<b>Resolution FP-08-42</b>
35 \$350,000 Urban Corp of San Diego <u>SANDAG</u> 11-San Diego	State Route 94 Casa de Oro Tree Planting, from Bancroft Street to Via Mercado approximately 2.5 miles east on Route 94. Plant approximately 850 drought tolerant and native trees on the slopes and other non-landscaped areas. Maintenance, after plant establishment, will be conducted by members of the Spring Valley and Casa de Oro communities.  Project Recipient corrected through a Technical Correction at the May 2010 CTC Meeting.  (Contribution from other sources: \$60,000.)	#18-35 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000
36 \$350,000 Urban Corp of San Diego <u>SANDAG</u> 11-San Diego	State Route 54 Sweetwater Greenbelt Bike Path Tree Planting. Plant 900 trees in large unsightly bare areas prone to erosion and functioning as trash collection areas; provide planting and irrigation to the linear greenbelt and recreational corridor paralleling the Sweetwater River and along Westbound SR-54 to Interstate 805; and buffer adjoining recreational areas from additional noise created by the highway.  Project Recipient corrected through a Technical Correction at the May 2010 CTC Meeting.  (Contribution from other sources: \$20,000.)	#18-36 EEM/08-09 \$350,000	2008-09 101-0183 EEM 20.30.207.811	\$350,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.9c.  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: William D. Bronte  
Division Chief  
Rail

Subject: **TECHNICAL CORRECTION TO PREVIOUSLY APPROVED RESOLUTION  
RESOLUTION GS1B-A-1112-005**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) approve a technical correction to Resolution GS1B-A-1112-005 originally approved on May 23, 2012.

## **ISSUE:**

At the May 2012 meeting, the Commission approved Resolution GS1B-A-1112-005 allocating \$12,157,000 for two locally administered Proposition 1B Highway-Railroad Crossing Safety Account projects. A technical correction is needed for Project 2-North Spring Street Grade Separation project in the County of Los Angeles to revise the Project ID number from 0012000245 to 0013000292.

There is no change to the Book Item Memorandum

The required changes are reflected in strikethrough and bold on the attached document.

Attachment

Project #	Allocation Amount	Project Title	PPNO Program/Year Phase	Budget Year	Amount by Fund Type
Recipient	Location	Project Description	Prgm'd Amount	Item #	
RTPA/CTC District-County	Location	Project Description	Project ID	Fund Type	
			Adv Phase EA	Program Code	
<b>2.5g.(9a) Proposition 1B – Locally Administered Highway-Rail Crossing Safety Account (HRCSA) Projects</b>			<b>Resolution GS1B-A-1112-005</b>		
1	\$7,156,000	<b>Bardsley Avenue Grade Separation.</b> Construct a grade-separated underpass at Bardsley Avenue and I Street, in the City of Tulare.	75-Rail HRCSA/11-12 CONST	2010-11 104-6063 HRCSA	\$7,156,000
City of Tulare	TCAG	(Original programming resolution GS1B-P-1011-01.)	\$7,156,000	20.30.010.400	
06-Tulare		(CEQA – Categorically Exempt – CCR Sec. 15282(g).)	0012000244		
		(Contributions from other sources: \$10,799,000.)	S H023BA		
<u>Outcome/Output:</u> This project will increase safety for pedestrians and vehicles, improve emergency response time, air quality, regional circulation and public convenience.					
2	\$5,001,000	<b>North Spring Street Grade Separation.</b> Widen North Springs Street to accommodate traffic lanes, including median, sidewalks and bike lanes in each direction and seismically retrofit the historic North Spring Street Bridge within the City of Los Angeles.	75-Rail HRCSA/11-12 CONST	2010-11 104-6063 HRCSA	\$5,001,000
City of Los Angeles	LACMTA	(Original programming resolution GS1B-P-1011-01.)	\$5,001,000	20.30.010.400	
07-Los Angeles		(Concurrent Future Consideration of Funding – Resolution E-12-27, May 2012.)	0012000245		
		(Contributions from other sources: \$43,318,000.)	0013000292		
		<u>Outcome/Output:</u> The bridge widening will relieve traffic overflow to the yet to be grade-separated North Main Street Bridge.	S H011BA		

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 4.11  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Dennis Jacobs  
Acting Division Chief  
Aeronautics

Subject: **ADOPTION OF THE RATE FOR LOCAL GOVERNMENT MATCHING OF CALIFORNIA AID TO AIRPORTS PROGRAM (CAAP) GRANTS RESOLUTION G-13-09**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) approve the attached resolution to set the Acquisition and Development (A&D) matching rate at 10 percent for Fiscal Year 2013-14. This item was presented for information at the June 2013 Commission meeting.

## **ISSUE:**

Under state requirements, the Commission is required to annually establish the rate at which local governments must match A&D grants from the Aeronautics Program.

The Department proposes to continue the local match rate at 10 percent for Fiscal Year 2013-14 and recommends the Commission adopt the attached resolution.

## **BACKGROUND:**

State law (Public Utilities Code Section 21684) requires local sponsors to match A&D grants at a rate of between 10 percent and 50 percent of the project cost. This law also requires the Commission to establish the matching rate annually.

In prior years, the Commission has established the matching rate at 10 percent. The 2012 Aeronautics Program that the Commission adopted at the June 2012 meeting was developed using a 10 percent matching rate. Fiscal Year 2013-14 is the second year of the 2012 Aeronautics Program.

Attachment

**CALIFORNIA TRANSPORTATION COMMISSION**

**Establishment of Local Government Matching Rate  
for Acquisition and Development Grants  
for Fiscal Year 2013-14**

**Resolution G-13-09**

- 1.1 WHEREAS, pursuant to Section 21684 of the Public Utilities Code, the California Transportation Commission is charged with the responsibility of establishing the percentage rate of matching funds to be provided by public entities for Acquisition and Development projects under the California Aid to Airports Program; and
- 1.2 WHEREAS, a 10 percent matching rate would be compatible with the Federal Aviation Administration's grant program; and
- 1.3 WHEREAS, a 10 percent matching rate encourages timely use of funds from the Aeronautics Account; and
- 1.4 WHEREAS, a 10 percent matching rate ensures that the maximum number of airport sponsors can participate in the Aeronautics Program; and
- 1.5 WHEREAS, the 2012 Aeronautics Program was developed with a matching rate of 10 percent.
- 2.1 NOW THEREFORE BE IT RESOLVED, that the Commission hereby establishes the local government matching rate for Fiscal Year 2013-14 for Acquisition and Development projects in the Aeronautics Program at 10 percent of the non-federal portion of an airport project.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 4.12  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Dennis Jacobs  
Acting Division Chief  
Aeronautics

Subject: **APPROVAL OF THE CAPITAL IMPROVEMENT PLAN ELEMENT OF THE CALIFORNIA AVIATION SYSTEM PLAN**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) approve the Capital Improvement Plan (CIP). This item was presented for information at June 2013, Commission Meeting.

## **ISSUE:**

The CIP is the basis for the biennial Aeronautics funding program, which consists of the Airport Development and Airport Land Use Compatibility Plan (ALUCP) projects selected by the Department's Division of Aeronautics based on a priority matrix, which is then adopted by the Commission for State funding. The 2014 Aeronautics funding program will come before the Commission for adoption in late spring of 2014, if funding permits.

The CIP is an element of the overall California Aviation System Plan (CASP) as the underpinning of the California Aid to Airport Program. This CIP contains 1,986 airport development and ALUCP projects desired by airport sponsors with a fiscally unconstrained cost estimate of \$ 2.91 billion. Regional Transportation Planning Agencies and Airport Land Use Commissions are encouraged to coordinate with airport sponsors to update ALUCP documents. The funding split is 3 percent for State funding participation (\$92 million), 87 percent for federal-only funding (\$2.51 billion) and 10 percent for the local match participation (\$301 million). Of the total \$2.91 billion, 54 percent (\$1.57 billion) is for commercial service primary airports, 0.2 percent (\$5 million) is for commercial service non-primary airports, 19 percent (\$556 million) is for reliever airports and 26 percent (\$754 million) is for general aviation airports which are all part of the National Plan of Integrated Airport Systems (NPIAS). The remaining 0.8 percent (\$23 million) is for general aviation airports that are not in the NPIAS (non-NPIAS).

The CIP also integrates the General Aviation System Needs Assessment (GASNA) element as part of the CASP. The GASNA is a list of fiscally unconstrained airport improvement projects recommended from the perspective of the Department instead of airport sponsors. The recommended projects are those the Department considers to be of greatest benefit to improving the safety, capacity, and capability of the statewide system of public-use airports as well as an airport itself. A broader discussion of the GASNA is provided in the Executive Summary of this document.

**BACKGROUND:**

The CIP element of the CASP is required by Public Utilities Code, Sections 21702-21706, as a ten-year capital improvement plan for each eligible airport, and is updated every two years. The California Department of Transportation's Division of Aeronautics develops the State's CIP in collaboration with Regional Transportation Planning Agencies, airport sponsors, and the Federal Aviation Administration for projects at public-use, publicly owned airports. A priority matrix is used to select projects from the CIP based on safety first, capability improvements that enhance system capacity second, and security enhancements third. A project must be in the CIP in order to obtain State funding.

Attachment

## **EXECUTIVE SUMMARY**

The California Aviation System Plan (CASP) is a multi-element plan prepared by the California Department of Transportation (Department), Division of Aeronautics (Division), with the goal of developing and preserving a system of airports responsive to the needs of the State.

The California Public Utilities Code (PUC) Section 21702-21706 of the State Aeronautics Act (Act) requires that the CASP include as one of its elements the Capital Improvement Plan (CIP). The CIP is a ten-year compiled listing of capital projects submitted to the Department for inclusion in the CASP predominantly based on general aviation airport master plans or other comparable long-range planning documents. The CIP allows the Department's partners to actively participate and assist in the coordination of its ongoing, statewide, aviation system planning and project funding effort. The CIP is updated biennially (every two years) per PUC Section 21704.

Biennial updates to the CIP provide the basis for the development of the funding program, which consists of airport development and land use compatibility plan projects selected by the Department based on a priority matrix. The California Transportation Commission adopts the Aeronautics Program from the projects listed in the CIP, therefore projects must be in the CIP to obtain State funding. The CIP is published every odd year, and the Aeronautics Program, based on the CIP, is adopted every even year.

The list of projects shown in the CIP is contained in a database that includes the capital needs for California's publicly owned, public-use airports. The CIP serves as an unconstrained fiscal estimate for current and future airport development projects desired by the airport sponsors and for funding airport land use compatibility planning documents in California. Not all projects listed in the CIP will be programmed.

The Priority Ranking Matrix (see Appendix A) is used to rank projects for the upcoming Aeronautics Program for three fiscal years. The ranking is in order of State importance starting with the category of safety, followed by capability improvements that enhance system capacity, then security enhancements. Nearly all projects fit into these three categories.

Ground access projects, located outside of an airport's operations areas, are listed separately in the CIP and are not eligible for either federal Airport Improvement Program (AIP) or California Aid to Airports Program (CAAP) funds. Funding for these projects is typically from local agencies or the State transportation improvement program.

This CIP contains 1,986 airport development and Airport Land Use Compatibility Plan (ALUCP) projects desired by airport sponsors with a fiscally unconstrained cost estimate of \$2.91 billion. Regional transportation planning agencies and airport land use commissions are encouraged to coordinate with airport sponsors to update ALUCP documents.

### **National Connection**

The National Plan of Integrated Airport Systems (NPIAS) is a federal document that identifies airports that are significant to national air transportation and are eligible to receive grants under the Federal Aviation Administration (FAA) AIP.

Many of the projects listed in the CIP will be funded by federal AIP, CAAP AIP, and local funds. The State contribution is 5.0 percent of the federal grant amount. The broad aim of the AIP is to assist in the development of the nationwide system of public-use airports. The CIP represents California's participation in the nationwide effort. For more information and details of the AIP, refer to FAA Order 5100.38C entitled "Airport Improvement Program (AIP) Handbook." For more information on the NPIAS and FAA airport categories listed in this CIP, please refer to the FAA website:  
[http://www.faa.gov/airports/planning\\_capacity/npias/](http://www.faa.gov/airports/planning_capacity/npias/).

### **California Aid to Airports Program**

The purpose of the CAAP is to assist in establishing and improving a statewide system of safe and environmentally compatible airports for general aviation. The Department is attempting to synchronize the CAAP process with the federal programming process by creating a unified federal and State project request form. This coordination with FAA will reduce duplicative efforts and provide better service to the Department's customers, who are local airport sponsors, airport land use commissions, regional transportation planning agencies, the FAA, the aviation community, and the public.

All projects in the CIP are subject to the provisions of the State Aeronautics Act and the CAAP. In addition, the inclusion of an airport development project or an ALUCP in the CIP does not imply promise of funding or that the project complies with the National Environmental Policy Act or the California Environmental Quality Act.

Federal, State, and local sources fund airport capital improvement projects. Information on federal airport CIP funding can be found at the FAA's website:  
<http://www.faa.gov/airports/aip/>. State CAAP funding information is located in the "State Dollars for Your Airport" document found on the Division of Aeronautics' website at <http://www.dot.ca.gov/hq/planning/aeronaut/>.

## **Ground Access Projects**

The purpose of airport ground access projects is to optimize ground transportation to and from airports. Ground access to airports includes improvements to off-airport roadways, highways, public transit systems, passenger shuttle systems, parking lots, and other transportation-related modes and facilities. Enhancements to these facilities seek to provide more convenient and predictable access for passengers, employees, air cargo traffic, and general aviation users. Planning for ground access and public transportation to airports generally requires joint participation by airports, the private sector, local jurisdictions, transit agencies, the Department, congestion management agencies, and regional transportation agencies.

Airports are key assets to communities and regions for both the economy and the overall quality of life. Thus, ground access to airports is perceived to be a critical issue facing the aviation system. This includes improved access and improved intermodal connections.

This CIP contains information about ground access to airports; however, these projects are not funded by the State. Along with the requested airport projects, airport sponsors and regional transportation planning agencies provided information on various ground access projects, start dates, and costs.

## **General Aviation System Needs Assessment**

The General Aviation System Needs Assessment (GASNA) is a living document which encourages airport improvement projects. The Division considers these projects important in order to improve the overall system of aviation in California. These projects may include runway extensions, widening, and pavement repairs, as well as visual aids, instrument approach procedures, automated weather services, and fueling equipment. These unfunded projects benefit airport safety, capacity, and the capability of the statewide system of public-use airports. The Division requests that airport sponsors consider these types of projects as they compile their CIP list. Yearly updates on meeting these improvements can be found on the Division's GASNA website at: [http://www.dot.ca.gov/hq/planning/aeronaut/documents/gasna/2010\\_GASNA.pdf](http://www.dot.ca.gov/hq/planning/aeronaut/documents/gasna/2010_GASNA.pdf) and at <http://www.dot.ca.gov/hq/planning/aeronaut/documents/gasna/2012GASNAFebruary.xls>

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.4c.  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Brent L. Green  
Chief  
Division of Right of Way  
and Land Surveys

Subject: **AIRSPACE LEASE - REQUEST TO DIRECTLY NEGOTIATE WITH THE SAN DIEGO UNIFIED PORT DISTRICT**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission) approve a request to directly negotiate a ten year lease with the San Diego Unified Port District (Port) for parcels 11-SDX005-0001-03, 11-SDX005-0011, and 11-SDX005-0015. All three parcels adjoin Port property along both sides of northbound Pacific Highway in San Diego; see Exhibits "A" and "B". All three parcels are encumbered by viaducts for Interstate 5 (I-5) and are landlocked. The lease would include one five year option, as well as lease rate re-evaluations based on the consumer price index every five years. The negotiated lease rate will be based on a fair market value as determined by a Department's appraiser.

## **BACKGROUND:**

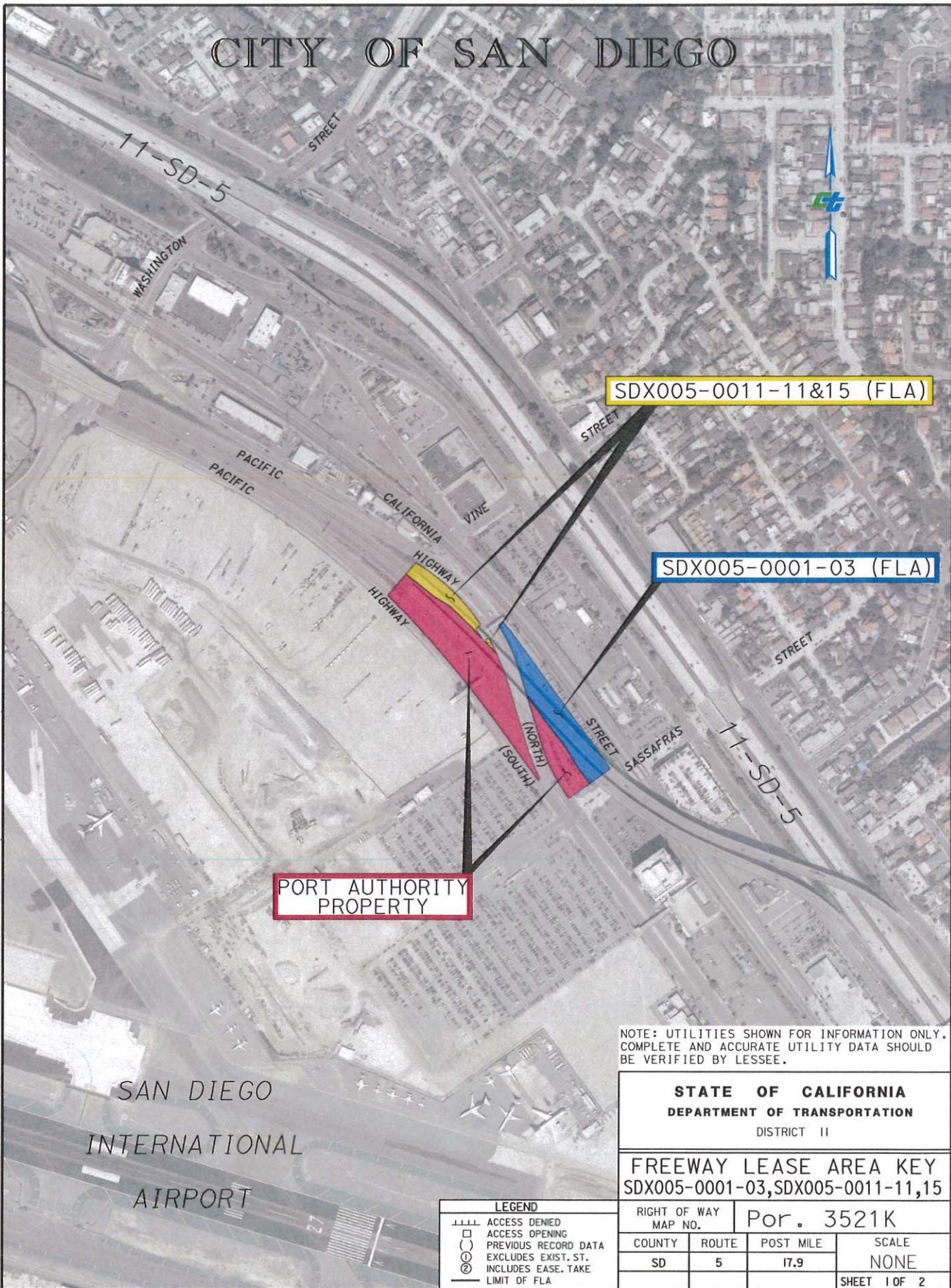
Parcels 11-SDX005-0011 and 11-SDX005-0015 are landlocked and accessible only by the adjoining Port property. Parcel 11-SDX005-0001-03 has been fenced off due to safety concerns and is functionally land-locked.

Both the Department's property and the Port's property have always been used for parking lots and they are usually leased to the same tenant. Rather than enter into a separate lease with the Port's tenant, the Department would like to lease to the Port directly. This would streamline the process and save the Department time and money.

The optimum return for these parcels will be realized through direct leases with the Port to be used as parking areas. Therefore, the Department requests that the Commission grant permission to directly negotiate with the Port for the lease of the subject parcels.

Attachments

## CITY OF SAN DIEGO



NOTE: UTILITIES SHOWN FOR INFORMATION ONLY. COMPLETE AND ACCURATE UTILITY DATA SHOULD BE VERIFIED BY LESSEE.

**STATE OF CALIFORNIA**  
**DEPARTMENT OF TRANSPORTATION**  
 DISTRICT II

**FREEWAY LEASE AREA KEY**  
 SDX005-0001-03, SDX005-0011-11, 15

RIGHT OF WAY MAP NO.		Por. 3521K	
COUNTY	ROUTE	POST MILE	SCALE
SD	5	17.9	NONE
			SHEET 1 OF 2

LEGEND	
	ACCESS DENIED
□	ACCESS OPENING
( )	PREVIOUS RECORD DATA
○	EXCLUDES EXIST. ST.
⊗	INCLUDES EASE. TAKE
—	LIMIT OF FLA

SAN DIEGO  
 INTERNATIONAL  
 AIRPORT

PORT AUTHORITY  
 PROPERTY

SDX005-0011-11&15 (FLA)

SDX005-0001-03 (FLA)



# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 3.3  
Information

From: ANDRE BOUTROS  
Executive Director

Subject: **2013 REPORT OF STATE TRANSPORTATION IMPROVEMENT PROGRAM (STIP)  
BALANCES, COUNTY AND INTERREGIONAL SHARES**

## **ISSUE:**

The Commission is required to maintain a long-term balance of shares, shortfalls, and surpluses for the regional and interregional improvement programs, and to make the balance through the preceding fiscal year available for review by all regional agencies at the time of each fund estimate, and by no later than August 15 of each year.

Commission staff transmitted this year's report to all regions and to Caltrans on July 26, 2013. The report includes both county and interregional share balances, with listings of projects programmed from those balances. The report's transmittal letter and summary table of balances are attached, and the report is available on the Commission's website ([www.catc.ca.gov](http://www.catc.ca.gov)) and from the Commission office (916) 654-4245.

## **BACKGROUND:**

Section 188.10 of the Streets and Highways Code requires that the Commission maintain a long-term balance of shares, shortfalls, and surpluses for the regional and interregional programs. This statute also requires the Commission to make the balance through the preceding fiscal year available for review at the time of each fund estimate and by not later than August 15 of each year. The balances are to include shares from the prior fund estimate, amounts programmed in the STIP, surpluses or shortfalls due to reservations or advancement, and adjustments as provided for in statute.

Attachment

July 26, 2013

To: Regional Agencies  
Caltrans

**2013 Report of STIP Balances**  
**County and Interregional Shares**

This is the California Transportation Commission's sixteenth annual report of State Transportation Improvement Program (STIP) county share and interregional share balances. Section 188.11 of the Streets and Highways Code mandates that the Commission maintain a record of balances and that it make the balances through the end of each fiscal year available for review by regional agencies not later than August 15 each year.

This year's report of share balances includes allocations approved through the Commission's June 11, 2013 meeting. The balances in this report are based on capacity identified through 2016-17 in the 2012 STIP fund estimate, adopted in August 2011. The balances also include all current cash commitments made for AB 3090 reimbursements.

Since the 2006 STIP, the Commission has broken the programming of STIP projects into three distinct categories, reflecting the restrictions of two of its major funding sources. The categories include (1) rail and transit projects; (2) enhancement projects funded from federal Transportation Enhancement (TE) funds and matching state funds; and (3) highway projects, now funded from the State Highway Account (SHA) and federal funds.

Rail and transit projects should be eligible for federal funds as Public Transportation Account funds have become more limited, to the point of being unavailable. These projects may also be eligible for SHA funds if the projects are not subject to Article 19 restrictions and there has been a county-wide vote to approve such use (as required by Proposition 5 of 1974).

Federal TE funds are no longer available due to the program's elimination under MAP-21 (Moving Ahead for Progress in the 21<sup>st</sup> Century Act), the new federal transportation act signed by the President on July 6, 2012. TE projects may be eligible for other federal funds or SHA funds. Those that are not, including TE reserves, must be deleted and the shares reprogrammed. Under law, county shares are based on the total of STIP funding estimated from all sources—there are not separate shares for each source.

This report includes STIP project listings for the primary purpose of documenting the use and availability of county and interregional shares. Although these listings may provide a useful summary of STIP projects, the project descriptions here are very brief and should not be regarded as complete and authoritative. Caltrans maintains a database with more complete project descriptions, and final authority rests with specific Commission actions and their supporting documentation.

Please direct any comments or questions regarding this report to the Commission's Deputy Director, Mitchell Weiss, at 916-654-7179, e-mail [Mitchell.Weiss@dot.ca.gov](mailto:Mitchell.Weiss@dot.ca.gov).

Sincerely,



ANDRE BOUTROS  
Executive Director

**SUMMARY OF STIP SHARE BALANCES**  
**Through June 30, 2013**  
(\$1,000's)

County	STIP County and Interregional Share Balances			
	Total Share Amount	Amount Programmed	Unprogrammed Balance	Balance Advanced
Alameda	62,512	60,512	2,000	0
Alpine	2,334	1,079	1,255	0
Amador	4,402	4,052	350	0
Butte	16,711	4,223	12,488	0
Calaveras	5,358	5,358	0	0
Colusa	7,399	6,726	673	0
Contra Costa	102,636	97,636	5,000	0
Del Norte	9,055	20,615	0	11,560
El Dorado CTC	(2,552)	6,926	0	9,478
Fresno	67,004	75,180	0	8,176
Glenn	10,362	8,560	1,802	0
Humboldt	33,839	39,494	0	5,655
Imperial	41,652	34,911	6,741	0
Inyo	44,748	34,924	9,824	0
Kern	63,937	66,648	0	2,711
Kings	6,845	24,786	0	17,941
Lake	26,159	21,494	4,665	0
Lassen	25,631	24,979	652	0
Los Angeles	606,898	624,707	0	17,809
Madera	735	14,813	0	14,078
Marin	(26,537)	13,283	0	39,820
Mariposa	7,485	5,944	1,541	0
Mendocino	22,890	21,809	1,081	0
Merced	18,115	6,460	11,655	0
Modoc	11,520	10,147	1,373	0
Mono	36,487	28,048	8,439	0
Monterey	63,355	70,199	0	6,844
Napa	13,719	11,041	2,678	0
Nevada	18,788	22,906	0	4,118
Orange	237,905	239,558	0	1,653
Placer TPA	(42,195)	3,683	0	45,878
Plumas	16,900	13,975	2,925	0
Riverside	129,432	114,052	15,380	0
Sacramento	60,876	43,246	17,630	0
San Benito	3,066	9,885	0	6,819
San Bernardino	193,301	199,270	0	5,969
San Diego	149,341	178,483	0	29,142
San Francisco	95,759	98,586	0	2,827
San Joaquin	35,540	27,583	7,957	0
San Luis Obispo	34,681	39,305	0	4,624
San Mateo	82,506	78,778	3,728	0
Santa Barbara	111,621	123,909	0	12,288
Santa Clara	61,172	80,434	0	19,262
Santa Cruz	33,382	33,993	0	611
Shasta	12,248	4,620	7,628	0
Sierra	1,983	940	1,043	0
Siskiyou	15,970	13,500	2,470	0
Solano	61,257	60,001	1,256	0
Sonoma	(2,359)	19,481	0	21,840
Stanislaus	30,051	26,759	3,292	0
Sutter	2,699	1,372	1,327	0
Tahoe RPA	6,786	5,201	1,585	0
Tehama	16,116	13,694	2,422	0
Trinity	15,928	15,342	586	0
Tulare	64,512	70,534	0	6,022
Tuolumne	10,988	2,362	8,626	0
Ventura	36,843	27,508	9,335	0
Yolo	18,478	11,739	6,739	0
Yuba	11,687	8,683	3,004	0
Statewide Regional	2,807,961	2,933,936	169,150	295,125
Interregional	1,144,252	1,157,498	0	13,246
<b>TOTAL</b>	<b>3,952,213</b>	<b>4,091,434</b>	<b>169,150</b>	<b>308,371</b>

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 3.5  
Action

From: ANDRE BOUTROS  
Executive Director

Subject: **PROPOSITION 1B SEMI-ANNUAL STATUS REPORT**

## **ISSUE:**

Should the California Transportation Commission (Commission) approve the attached Proposition 1B Semi-Annual Status Report for submittal to the Department of Finance?

## **RECOMMENDATION:**

Commission staff recommends that the Commission approve the attached proposition 1B Semi-Annual Status Report for submittal to the Department of Finance.

## **BACKGROUND:**

Senate Bill 88 designates the Commission as the administrative agency for the Corridor Mobility Improvement Account, Route 99, Trade Corridor Improvement Fund, State & Local Partnership, Local Bridge Seismic Retrofit Account, Highway Railroad Crossing Safety Account, STIP and SHOPP programs funded by Proposition 1B. As the administrative agency, the Commission is required to report on a semiannual basis to the Department of Finance on the progress of the projects in these proposition 1B programs. The purpose of the report is to ensure that the projects are being executed in a timely manner and within the approved scope and budget.

The proposition 1B Semi-Annual Status Report, issued in July of each year, and the Commission's Annual Report, issued in December, provide the reports mandated by Senate Bill 88.

Attached is the proposed Proposition 1B Semi-Annual Report. Upon Commission approval, the attached report will be submitted to the Department of Finance with the current Proposition 1B Quarterly Reports presented at the June 2013 Commission Meeting.

# **CALIFORNIA TRANSPORTATION COMMISSION**

## **Proposition 1B Highway Safety, Traffic Reduction, Air Quality and Port Security Bond Act of 2006**

### **Semi-Annual Status Report July 2013**

#### **BACKGROUND**

Proposition 1B, approved by the voters in November 2006, authorized the issuance of \$19.925 billion in State general obligation bonds for specific transportation programs intended to relieve congestion, facilitate goods movement, improve air quality, and enhance the safety of the state's transportation system. These transportation programs included the Corridor Mobility Improvement Account (CMIA), State Route 99 Corridor Account (SR 99), Trade Corridors Improvement Fund (TCIF), State and Local Partnership Program (SLPP), Local Bridge Seismic Program, Highway-Railroad Crossing Safety Account (HRCSA), Traffic Light Synchronization Program (TLSP) and the augmentation of the existing State Transportation Improvement Program (STIP) and the State Highway Operation and Protection Program (SHOPP). Consistent with the requirements of Proposition 1B, the California Transportation Commission (Commission) programs and allocates bond funds in each of the above-mentioned programs.

In clarifying legislation to Proposition 1B, Senate Bill 88 (SB 88), enacted in 2007, includes implementation and accountability requirements for Proposition 1B projects and further defines the role of the Commission as the administrative agency for the CMIA, SR 99, TCIF, STIP, SLPP, Local Bridge Seismic Account, HRCSA, TLSP, and SHOPP funded by Proposition 1B. SB 88 requires the Commission to report to the Department of Finance, on a semiannual basis, on the progress of the Proposition 1B bond projects in these programs. This report, as well as the Commission's Annual Report issued in December of each year, satisfies the reporting requirements of SB 88.

To date, the Commission has programmed all \$11.6 billion of the Proposition 1B funds within its purview. The Commission has allocated \$10.665 billion of the programmed Proposition 1B funds, to projects that were ready to commence construction.

#### **DELIVERY TRENDS & CHALLENGES**

## **Delivery Challenges**

The Buy America provisions of Section 518 of the Moving Ahead for Progress in the 21<sup>st</sup> Century Act (MAP-21), which Congress amended to existing law, broaden Buy America applicability to non FHWA-funded utility relocations. This has created significant challenges for Bond projects that are seeking an allocation. Some utility companies have refused to comply with these requirements, preventing projects from moving forward. Allocations for four bond projects were deferred in May and June as a result. Project sponsors continue to work towards a resolution with the utility companies in order to proceed with the projects. The Buy America issue has affected some already allocated projects as well.

## **Construction Cost Trends**

Since 2009, the economic downturn provided one tangible benefit for the Proposition 1B projects, that is, lower construction bids. The trend for lower construction costs continued in Fiscal Year 2012-13. The Department has received on average 6.1 bids per advertised contract, same as the prior fiscal year. The low bid for contracts was 9.1% below the Engineer's Estimate for Fiscal Year 2012-13 versus 15.6% below the Engineer's Estimate for Fiscal Year 2011-12.

## **Program Specific Issues**

### **Corridor Mobility Improvement Account (CMIA)**

The Highway Safety, Traffic Reduction, Air Quality, and Port Security Bond Act of 2006 was approved by the voters as Proposition 1B on November 7, 2006. Proposition 1B authorized \$4.5 billion in general obligation bond proceeds to be deposited in the CMIA. Funds in the CMIA are available for performance improvements on the state highway system, or major access routes to the state highway system on the local road system, that relieve congestion by expanding capacity, enhance operations, or otherwise improve travel times within these high-congestion travel corridors. Inclusion of a project in the CMIA program was conditioned on the commencement of construction no later than December 31, 2012.

At the time of adoption of the original CMIA program in February 2007, the Commission programmed 54 projects for \$4.5 billion, leveraging another \$4.6 billion in additional federal, state and local funds. As the Commission focused on assuring the delivery of the CMIA program within the statutory deadline, the Commission also worked with sponsoring agencies to recapture any cost savings at construction contract award. These contract award savings were proportioned among the mix of project funding sources. The accumulated CMIA savings were recycled to program additional CMIA projects. Through December 31, 2012, the Commission committed \$1.070 billion of CMIA savings to 45 additional projects, leveraging another \$1.73 billion in additional federal, state and local funds. What started as a program of 54 projects programmed at \$9.1 billion in total project cost grew to a program of 99 projects programmed at \$11.7 billion in total project cost, generating over 190,000 jobs and providing critical improvements to the state transportation system.

The Commission, working with the Department, was successful in capturing and utilizing all available CMIA project cost savings accrued after June 30, 2012 through the statutory deadline of December 31, 2012, by exchanging the available CMIA funds with SHOPP funds on the Gerald Desmond Bridge in Los Angeles County and the I-15 Widening and Devore Interchange Reconstruction Project in San Bernardino County.

### **Trade Corridors Improvement Fund (TCIF)**

Proposition 1B authorized \$2 billion of state general obligation bonds for the TCIF. Funds in the TCIF are available for infrastructure improvements along federally designated “Trade Corridors of National Significance” in the state or along other corridors within the state that have a high volume of freight movement. Acknowledging that the freight infrastructure needs of the state far exceed the \$2 billion provided under Proposition 1B, the Commission supported a strategy to increase TCIF funding by \$500 million from the State Highway Account to fund state-level priorities that are critical to goods movement. In April 2008 the Commission adopted a program approximately 20 percent more than the \$2.5 available. The overprogramming assumed that new revenue sources would become available and would be dedicated to funding the adopted program. Unfortunately, new revenue sources to address the overprogramming have not materialized. However, the Corridor Coalitions diligently addressed the overprogramming and all three Corridor Coalitions – the Northern California Trade Corridors Coalition (NCTCC), the Southern California Consensus Group (SCCG), and the San Diego/Border Corridor (SDBC) – achieved their programming levels by May 2013.

At the time of adoption of the TCIF program, the Commission programmed 79 projects, valued at \$3.088 billion. Since then, projects have been removed and new projects have been added. Through June 2013, 67 projects remain in the TCIF program. Out of the 67 projects, 6 projects have been completed, 54 are either under construction or about to start construction and 7 remain unallocated. The Commission continues to work with the coalitions and project sponsors to recapture any cost savings at construction contract award and/or close out and utilize the savings on new projects.

## **ACCOUNTABILITY**

In clarifying legislation to Proposition 1B, on August 24, 2007, the Governor signed into law Senate Bill 88 (SB 88) which designates the Commission as the administrative agency for the CMIA, SR 99, TCIF, STIP, SLPP, Local Bridge Seismic Account, HRCSA, and SHOPP funded by Proposition 1B. SB 88 imposes various requirements for the Commission relative to adopting guidelines, making allocations of bond funds, reporting on projects funded by the bond funds, and ensuring that the required bond project audits of expenditures and outcomes are performed.

In addition, Executive Order S-02-07, issued by Governor Arnold Schwarzenegger on January 24, 2007, significantly increases the Commission’s delivery monitoring responsibility for the bond funded projects. Specifically, the Commission is required to

develop and implement an accountability plan, with primary focus on the delivery of bond funded projects with their approved scope, cost and schedule.

A key element of the Commission's responsibility for accountability as an administrative agency for specific bond programs is submitting reports to the Department of Finance on a semiannual basis. The purpose of these reports is to ensure that projects are proceeding on schedule and within their estimated cost. As part of its Accountability Implementation Plan, the Commission requires bond fund recipients to report to the Commission on a quarterly basis. These reports are reviewed by the Commission and posted on the Bond Accountability website. In addition, the Commission prepares the Semi-Annual Proposition 1B Status Report and the Annual Report to the Legislature, which includes the status of the Proposition 1B Programs.

Another key element of bond accountability is the audit of bond project expenditures and outcomes. Specifically, the Commission is required to develop and implement an accountability plan which includes provisions for bond audits. Under the Executive Order, expenditures of bond proceeds shall be subject to audit to determine whether the expenditures made from bond proceeds:

- Were made according to the established front-end criteria and processes.
- Were consistent with all legal requirements.
- Achieved the intended outcomes.

The Commission's Accountability Implementation Plan includes provisions for the audit of bond projects. In order to ensure that the Commission is meeting the auditing requirements of an administrative agency, as mandated by SB 88 and the Governor's Executive Order, the Commission has entered into a Memorandum of Understanding with the Department of Finance to perform the required audits of Proposition 1B projects, effective July 1, 2009. In addition, the Department of Finance, in consultation with Commission staff, is currently developing the Fiscal Year 2013-14 Audit Plan for the Proposition 1B Bond Program.

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 4.10  
Action

From: ANDRE BOUTROS  
Executive Director

Subject: **TRADE CORRIDORS IMPROVEMENT FUND PROGRAM – POLICY TO UTILIZE  
PROGRAM SAVINGS  
RESOLUTION TCIF-P-1314-03**

## **ISSUE:**

Should the California Transportation Commission (Commission) approve a policy that extends the Trade Corridors Improvement Fund (TCIF) Program by one year and allows for the use of program savings on new TCIF projects?

## **RECOMMENDATION:**

Commission staff recommends that the Commission extend the current TCIF program by one year and allow programming of new TCIF projects utilizing cost savings with coalition support.

## **POLICY:**

Projects that have not received an allocation by the August 2013 Commission meeting can remain in the program at the request of their respective coalition.

New projects can be nominated with the support of their respective coalition.

All new projects will be evaluated based on the criteria set forth in the TCIF guidelines adopted by the Commission in November 2007.

All projects that remain in the program or new projects that are added to the program must begin construction by December 2014 and receive an allocation by June 2014.

No project will be considered for an allocation unless it is ready to be advertised.

All other provisions of the TCIF program Guidelines and Accountability Guidelines remain in effect.

## **BACKGROUND:**

The Highway Safety, Traffic Reduction, Air Quality, and Port Security Bond Act of 2006, passed by the voters in November 2006, included \$2.0 billion for the Proposition 1B TCIF Program. The Commission recognized that needs far exceeded the amount authorized in the Proposition 1B TCIF

program and increased the TCIF funding by approximately \$500 million from the State Highway Account. The Commission adopted guidelines that established programming targets, which included an additional over programming for each corridor and a requirement to award contracts no later than December 31, 2013.

The Commission adopted the initial TCIF Program at its April 2008 meeting. The initial program included 79 projects valued at \$3.088 billion spread amongst the four corridors: the San Francisco/Central Valley Corridor (aka Northern California Corridor), the Los Angeles/Inland Empire Corridor, the San Diego Border Region Corridor and the Other Corridor. Since then, the corridors have eliminated their over programming by utilizing award savings and by removing projects that had delivery challenges.

Through June 2013, 67 projects remain in the TCIF program. Out of the 67 projects, six did not receive an allocation by the June 2013 deadline. The Commission at its June 2013 meeting extended the allocation deadline to the August meeting. If not allocated at the August meeting, the six projects can remain in the program with the submittal of a coalition support letter, a revised schedule, an updated funding plan and an updated baseline agreement signature page. This will establish a new timeline for project delivery.

All materials should be addressed or delivered to:

Andre Boutros, Executive Director  
California Transportation Commission  
Mail Station 52, Room 2222  
1120 N Street  
Sacramento, CA 95814

Attachment: Trade Corridors Improvement Fund Guidelines, November 27, 2007

# CALIFORNIA TRANSPORTATION COMMISSION

## Trade Corridors Improvement Fund (TCIF) Program Guidelines

Adopted November 27, 2007

### General Program Policy

1. Authority and purpose of guidelines. The Highway Safety, Traffic Reduction, Air Quality, and Port Security Bond Act of 2006, approved by the voters as Proposition 1B on November 7, 2006, provided for \$2 billion to be transferred to the Trade Corridors Improvement Fund (TCIF) for infrastructure improvements along corridors that have a high volume of freight movement. The funds are available, upon appropriation by the Legislature, for allocation by the California Transportation Commission.

The TCIF program is subject to the provisions of Government Code Section 8879.23(c)(1), as added by Proposition 1B, and to Section 8879.50, as enacted through implementing legislation in 2007 (SB 88 and AB 193). The implementing legislation designated the Commission as the administrative agency responsible for programming TCIF and the agency authorized to adopt guidelines for the program.

The purpose of these guidelines is to identify the Commission's policy and expectations for the TCIF program and thus to provide guidance to the Department of Transportation (Caltrans), regional agencies, and project applicants and proponents in carrying out their responsibilities under the program. These guidelines are not intended to preclude any project nomination or any project selection that is consistent with the Bond Act.

2. Statutory program eligibility. Proposition 1B calls for the Commission to determine corridor and project eligibility, consulting:
  - the goods movement action plan (GMAP) submitted to the Commission by the Secretary of the Business, Transportation and Housing Agency and the Secretary for Environmental Protection;
  - trade infrastructure and goods movement plans adopted by regional transportation planning agencies;
  - adopted regional transportation plans required by state and federal law; and
  - the statewide port master plans prepared by the California Marine and Intermodal Transportation System Advisory Council (Cal-MITSAC).

Under Proposition 1B, eligible projects may include, but are not limited to:

- highway capacity improvements,
- freight rail system improvements,
- port capacity and efficiency projects,
- truck corridor improvements,
- improvements that maximize state access to federal border infrastructure funds, and
- airport ground access improvements.

3. Statutory programming mandates. Proposition 1B mandates that the Commission program and allocate TCIF in a manner that:

- addresses the state's most urgent needs,
- balances the demands of various ports,
- provides reasonable geographic balance between regions, and
- places emphasis on projects that improve trade corridor mobility while reducing emissions of diesel particulate and other pollutant emissions.

The Commission is also mandated to consider the following factors:

- **Velocity:** the speed by which large cargo would travel from the port through the distribution system.
- **Throughput:** the volume of cargo that would move from the port through the distribution system.
- **Reliability:** a reasonably consistent and predictable cargo travel time between points on any given day or time.
- **Congestion reduction:** the reduction in recurrent daily hours of delay to be achieved.

4. Statutory mandate for supplemental funding. Proposition 1B mandates that the Commission allocate TCIF to projects that have identified and committed supplemental funding from appropriate local, federal, or private sources. The Commission is to determine the appropriate amount of supplemental funding for each project based on a project-by-project review and an assessment of the project's benefit to the state and the program. Except for border access improvements receiving federal border infrastructure funding, the supplemental funding shall be at least equal to the TCIF contribution, and the Commission may give funding priority to projects with higher levels of committed supplemental funding.

5. Program Schedule. The Commission intends to implement the TCIF program on the following schedule:

CTC adopts TCIF guidelines	November 27, 2007
TCIF project nominations due	January 17, 2008
Hearings	Week of February 18, 2008
CTC issues staff recommendations	March 13, 2008
CTC adopts the initial TCIF program of projects	April 10, 2008

### Project Nominations

6. Eligible applicants and projects. The Commission will accept project nominations from Caltrans, regional agencies, and other public agencies, including counties, cities, and port authorities. Project proposals from railroads or other private entities should be submitted by a public agency sponsor. A nomination may identify an entity other than the nominating agency to be the project implementing agency. The implementing agency is the agency responsible for carrying out the work and completing the project.

After consulting the GMAP, Cal-MITSAC and regional transportation plans, the Commission has determined that the following corridors have a high volume of freight movement and are eligible for funding under this program:

- Bay Area Corridor
- Central Valley Corridor
- Los Angeles/Inland Empire Corridor
- San Diego/Border Corridor

The Commission acknowledges that other regions of the state may have goods movement infrastructure needs along corridors that have a high volume of freight movement that would be eligible for funding through the TCIF. The Commission would anticipate those regions would nominate their projects for consideration.

Under statute, the applicant agency must provide a project funding plan through construction that demonstrates that the supplemental funding in the plan (local, federal or private sources) is reasonably expected to be available and sufficient to complete the project. The Commission expects that TCIF project funding will be limited to the costs of construction.

The investment of public funding must be tied to public benefit as demonstrated through a public/private benefit assessment. TCIF should not supplant revenues otherwise available through existing private sector revenue streams.

The useful life of a TCIF project shall not be less than the required useful life for capital assets pursuant to the State General Obligation Bond Law, specifically subdivision (a) of Section 16727 of the Government Code. That section generally requires that projects have an expected useful life of 15 years or more.

7. Project nominations. Project nominations and their supporting documentation will form the primary basis for the Commission's TCIF program of projects. Each project nomination should include:

- A cover letter with signature authorizing and approving the nomination.
- A programming request form (Appendix A) together with a map of the project location that describes the project scope, useful life, cost, funding plan, delivery milestones, and major project benefits. Cost estimates should be escalated to the year of proposed implementation. The project delivery milestones should include the start and completion dates for environmental clearance, land acquisition, design, construction bid award, construction completion, and project closeout.
- A brief narrative that provides:
  - Project background and a purpose and need statement.
  - A concise description of the project scope and anticipated benefits (outcomes and outputs) proposed for TCIF funding.

- A specific description of non-TCIF funding (source and amount) to be applied to the project and the basis for concluding that the non-TCIF funding is expected to be available.
- A description of the project delivery plan, including a description of the known risks that could impact the successful implementation of the project and a description of the response plan for the known risks. The risks considered should include, but not be limited to, risks associated with deliverability and engineering issues, community involvement, and funding commitments.
- A description of the transportation corridor and the function of the proposed project within the corridor.
- A description and quantification of improvements in trade corridor mobility, including measures of velocity, throughput, reliability and congestion reduction for freight movement in the corridor.
- A description and quantification of the local and corridor effects of the project on diesel particulate and other pollutant emissions.
- A description of the corridor plan or other coordinated management strategy being implemented by the applicant agency and other jurisdictions within the corridor to preserve corridor mobility.
- Documentation supporting the benefits and cost estimates cited in the nomination. This should be no more than 10 pages in length, citing or excerpting, as appropriate, the project study report, environmental document, regional transportation plan, and other studies that provide quantitative measures of the project's costs and benefits, including both trade corridor mobility benefits and emission reduction benefits. Where investment of TCIF is proposed to improve private infrastructure, this documentation should include an assessment of public and private benefits to show that the share of public benefit is commensurate with the share of public funding.
- Documentation for rail investments that acknowledges and describes how the private railroads, regional agencies and appropriate state agencies will come to agreement on public and private investment levels and resulting benefits.
- Documentation supporting the availability of supplemental funding identified as part of the project funding plan. Local, federal or private funds to be expended after the adoption of the project into the TCIF program may be used for the 1:1 funding match, with the exception of Grade Separation projects where non-state funds expended since the approval of Proposition 1B may be counted for the match. The project funding plan may include other state funds but those funds may not be counted for the match. For these purposes SHOPP, STIP and TCRP are state funds which are not eligible for match, local gas tax subvention funds, and user fees will be treated as local funds, and Section 190 Grade Separation Fund revenues, RSTP and CMAQ are regarded as federal funds.

8. Submittal of project nominations. For the initial TCIF program of projects, the Commission will consider only projects for which a nomination and supporting documentation are received in the Commission office by (date to be determined), in hard copy. A nomination from a regional agency or port authority will include the signature of the Chief Executive Officer or other authorized officer of the agency. A nomination from Caltrans will include the signature of the Director of Transportation or a person authorized by the Director to submit the nomination. A nomination from a city, county, or other public agency will include the signature from an officer authorized by the city council, board of supervisors, or other agency board. Where the project is to be implemented by an agency other than the nominating agency, the nomination will also include the signature of the Chief Executive Officer or other authorized officer of the implementing agency.

The Commission requests that each project nomination include five copies of the cover letter, the project fact sheet, and the narrative description, together with two copies of all supporting documentation. All nomination materials should be addressed or delivered to:

John Barna, Executive Director  
California Transportation Commission  
Mail Station 52, Room 2222  
1120 N Street  
Sacramento, CA 95814

#### **Project Selection and Programming**

9. Program of projects based on nominations. The Commission will develop its TCIF program of projects primarily on the basis of the nominations received by the nomination due date.
10. Project nomination scoring. The Commission staff will screen and evaluate project nominations according to the following screening and evaluation criteria. The Commission will take into consideration the methods by which the corridor agencies have determined the relative priority of their nominations.

<b>Screening Criteria:</b> Screening criteria determine whether the nomination is evaluated further.
<b>1. Eligibility:</b>
<ul style="list-style-type: none"><li>• Project is included in GMAP, Cal-MITSAC, trade infrastructure and goods movement plans adopted by regional transportation planning agencies, or an adopted regional transportation plan</li></ul>
<ul style="list-style-type: none"><li>• Project can demonstrate a 1:1 funding match (local, federal or private funds)</li></ul>

<p><b>2. Deliverability:</b> Project will begin construction by December 31, 2013.</p>
<p><b>3. Air Quality:</b> Project contributes to corridor or air basin emission reduction of particulates and other pollutants</p>
<p><b>4. Economic/Jobs Growth:</b> Project will stimulate economic activity, enhance trade value, and preserve/create jobs</p>

<p><b>Evaluation Criteria:</b> Evaluation criteria are outcome oriented and customizable to each corridor. Evaluation criteria are grouped into three categories. The Commission may give priority to projects with higher levels of committed supplemental funding.</p>
<p><b>1. Freight System (Goods Movement) Factors:</b></p>
<ul style="list-style-type: none"> <li>• <b>Throughput:</b> Project provides for increased volume of freight traffic through capacity expansion or operational efficiency</li> </ul>
<ul style="list-style-type: none"> <li>• <b>Velocity:</b> Project increases the speed of freight traffic moving through the distribution system</li> </ul>
<ul style="list-style-type: none"> <li>• <b>Reliability:</b> Project reduces the variability and unpredictability of travel time</li> </ul>
<p><b>2. Transportation System (Priorities) Factors:</b></p>
<ul style="list-style-type: none"> <li>• <b>Safety:</b> Project increases the safety of the public, industry workers, and traffic</li> </ul>
<ul style="list-style-type: none"> <li>• <b>Congestion Reduction/Mitigation:</b> Project reduces daily hours of delay on the system and improves access to freight facilities</li> </ul>
<ul style="list-style-type: none"> <li>• <b>Key Transportation Bottleneck Relief:</b> Project relieves key freight system bottlenecks where forecasts of freight traffic growth rates indicate infrastructure or system needs are inadequate to meet demand</li> </ul>
<ul style="list-style-type: none"> <li>• <b>Multi-modal Strategy:</b> Project employs or supports multi-modal strategies to increase port and transportation system throughput while reducing truck vehicle miles/hour traveled (VMT/VHT)</li> </ul>
<ul style="list-style-type: none"> <li>• <b>Interregional Benefits:</b> Project links regions/corridors to serve statewide or national trade corridor needs</li> </ul>
<p><b>3. Community Impact Factors:</b></p>
<ul style="list-style-type: none"> <li>• <b>Air Quality Impact:</b> Project reduces local and regional emissions of diesel particulate, CO2, NOx, and other pollutants.</li> </ul>
<ul style="list-style-type: none"> <li>• <b>Community Impact Mitigation:</b> Project reduces negative impacts on communities (noise, localized congestions, safety, etc.)</li> </ul>
<ul style="list-style-type: none"> <li>• <b>Economic/Jobs Growth:</b> Project stimulates local economic activity, enhances trade value, and preserves/creates jobs</li> </ul>

Where a project is proposed to improve private infrastructure, the Commission's evaluation will examine the public/private benefit assessment of the project.

11. Program adoption. The Commission recognizes that statewide goods movement needs far exceed the amount that Proposition 1B authorizes for the TCIF program, that other sources of funding may and should be explored for meeting those needs, and that delivery challenges may limit the funding of identified projects by December 2013. The Commission also supports the funding strategy proposed by Caltrans and the corridor agencies to increase TCIF funding by approximately \$500 million from the State Highway Account (SHA) to fund state-level priorities that are critical to goods movement. Additionally, the Commission anticipates that it will initially program about 20% more than the resulting \$2.5 billion available from the TCIF and the SHA. This overprogramming assumes that new revenue sources (additional federal funding, user fees, tolls, etc.) will become available and will be dedicated to funding the adopted program. The Commission anticipates reviewing the programming and delivery status of all projects in 2010 on a schedule coordinated with development of the 2010 STIP. The Commission may also adopt amendments to the program to recognize the availability of other funds or changes in project delivery. The Commission may, if it finds it necessary or appropriate, advise potential applicants to submit new or revised nominations.
12. Corridor-based programming targets. The Commission supports a corridor-based programming approach to the TCIF, which recognizes and complements the goods movement planning work already done within the major trade corridors. The Commission also recognizes and supports the key role that the state has to play in project identification and supports integrating statewide goods movement priorities into the corridor approach.

To promote this corridor-based approach, the Commission has developed the following geographic programming ranges, in consultation with the Department and corridor regional agencies. The targets are neither minimums nor maximums. They do not constrain what any agency may propose or what the Commission may approve for programming and allocation within any particular corridor.

<b>TCIF Corridor Programming Ranges</b> (Range, in \$ millions)		
	Low	High
Los Angeles/Inland Empire Corridor	\$1,500	\$1,700
San Diego/International Border Corridor	250	400
San Francisco Bay/Central Valley Corridor	640	840
Other Corridors	60	80
Administration Fees	40	40
<b>Total</b>	<b>\$2,490</b>	<b>\$3,060</b>

### Project Delivery

13. Project baseline agreements. Within three months after the adoption of a project into the TCIF program of projects, the Commission, Caltrans and the implementing agency, together with the regional agency and any entity committed to providing supplemental funding for the project, will execute a project baseline agreement, which will set forth the project scope, benefits, delivery schedule, and the project budget and funding plan. The funding plan will identify the source of supplemental funding. For investments in rail projects, the Commission expects a memorandum of understanding to be in place by the time of execution of the project baseline agreements between the private railroads, appropriate regional agencies, and appropriate state agencies that details how and when public and private funding would be made available. In addition, the Commission expects the memorandum of understanding would stipulate what public benefits (i.e., additional passenger rail capacity and investments in grade separations) would accrue to the affected regions and by when those benefits would be realized. The Commission may delete a project for which no project baseline agreement is executed, and the Commission will not consider approval of a project allocation prior to the execution of a project baseline agreement.

14. Quarterly delivery reports: As a part of the project baseline agreement, the Commission will require the implementing agency to submit quarterly reports on the activities and progress made toward implementation of the project, including those project development activities taking place prior to a TCIF allocation and including the commitment status of supplemental funding identified in the project baseline agreement.

As mandated by Government Code Section 8879.50, the Commission shall forward these reports, on a semiannual basis, to the Department of Finance. The purpose of the reports is to ensure that the project is being executed in a timely fashion and is within the scope and budget identified when the decision was made to fund the project. If it is anticipated that project costs will exceed the approved project budget, the implementing agency will provide a plan to the Commission for achieving the benefits of the project by either downscoping the project to remain within budget or by identifying an alternative funding source to meet the cost increase. The Commission may either approve the corrective plan or direct the implementing agency to modify its plan. Where a project allocation has not yet been made, the Commission may amend the program of projects to delete the project.

15. Amendments to program of projects. The Commission may approve an amendment of the TCIF program in conjunction with its review of a project corrective plan as described in Section 14. The implementing agency may also request and the Commission may approve an amendment of the program at any time. An amendment need only appear on the agenda published 10 days in advance of the Commission meeting. It does not require the 30-day notice that applies to a STIP amendment.
16. Allocations from the TCIF. The Commission will consider the allocation of funds from the TCIF for a project or project component when it receives an allocation request and

recommendation from Caltrans, in the same manner as for the STIP. The recommendation will include a determination of the availability of appropriated TCIF and the availability of all identified and committed supplemental funding. The Commission will approve the allocation if the funds are available, the allocation is necessary to implement the project as included in the adopted TCIF program, and the project has the required environmental clearance.

17. Final delivery report. Within six months of the project becoming operable, the implementing agency will provide a final delivery report to the Commission on the scope of the completed project, its final costs as compared to the approved project budget, its duration as compared to the project schedule in the project baseline agreement, and performance outcomes derived from the project as compared to those described in the project baseline agreement. The Commission shall forward this report to the Department of Finance as required by Government Code Section 8879.50.

The implementing agency will also provide a supplement to the final delivery report at the completion of the project to reflect final project expenditures at the conclusion of all project activities. For the purpose of this section, a project becomes operable at the end of the construction phase when the construction contract is accepted. Project completion occurs at the conclusion of all remaining project activities, after acceptance of the construction contract.

18. Audit of project expenditures and outcomes. The Department of Transportation will ensure that project expenditures and outcomes are audited. For each TCIF project, the Commission expects the Department to provide a semi-final audit report within 6 months after the final delivery report, and a final audit report within 6 months after the final delivery report supplement. The Commission may also require interim audits at any time during the performance of the project.

Audits will be performed in accordance with Generally Accepted Government Auditing Standards promulgated by the United States Government Accountability Office. Audits will provide a finding on the following:

- Whether project costs incurred and reimbursed are in compliance with the executed project baseline agreement or approved amendments thereof; state and federal laws and regulations; contract provisions; and Commission guidelines.
- Whether project deliverables and outcomes are consistent with the project scope, schedule and benefits described in the executed project baseline agreement or approved amendments thereof.

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 4.8  
Action

From: ANDRE BOUTROS  
Executive Director

Subject: **TRADE CORRIDORS IMPROVEMENT FUND - PROGRAM AMENDMENT**  
**RESOLUTION TCIF-P-1314-02**

## **ISSUE:**

Should the California Transportation Commission (Commission) amend the Trade Corridors Improvement Fund (TCIF) Program to reflect the following actions?

### Northern California Corridor

- TCIF Project 4: *I-880 Reconstruction 29<sup>th</sup> & 23<sup>rd</sup> Avenues*, continue the project in the TCIF Program, maintain the \$73 million in TCIF funds and approve the updated schedule.
- TCIF Project 89: *Solano I-80/680/12 Connector*, continue the project in the TCIF Program, maintain the \$24 million in TCIF funds, and approve the updated schedule.
- Add *Santa Clara US-101 Freeway Performance Initiative Project* to the TCIF Program as TCIF Project 94 at a cost of \$15 million in TCIF/SHOPP funds.

### Los Angeles/Inland Corridor

- TCIF Project 21: *Washington Boulevard Widening*, continue the project in TCIF Program, maintain the \$5.8 million in TCIF funds, and approve the updated schedule.
- TCIF Project 40: *Lakeview Avenue Overcrossing*, continue the project in the TCIF Program, maintain the \$39.519 million in TCIF funds, and approve the updated schedule.
- TCIF Project 64: *Lenwood Road Grade Separation*, continue the project in the TCIF Program, maintain the \$8.855 million in TCIF funds, and approve the updated schedule.
- Add *Puente Avenue Grade Separation Project* to the TCIF Program as TCIF Project 95 at a cost of \$48 million in TCIF funds.
- Add *Fairway Drive Grade Separation Project* to the TCIF Program as TCIF Project 96 at a cost of \$56 million in TCIF funds.
- TCIF Project 48: *Avenue 56 Grade Separation Project*, increase TCIF funds by \$5.065 million.

- TCIF Project 50: *Clay Street Grade Separation*, increase TCIF funds by \$746 thousand.
- TCIF Project 53: *Magnolia Avenue Grade Separation*, increase TCIF funds by \$3.996 million.

**RECOMMENDATION:**

Commission staff recommends that the Commission approve the proposed TCIF Program Amendment to: (1) Add into the TCIF program projects 94, 95 and 96, *Santa Clara US-101 Freeway Performance Initiative Project*, *Puente Avenue Grade Separation Project* and *Fairway Drive Grade Separation Project*; (2) maintain in the TCIF program projects 4, 21, 40, 64 and 89: *I-880 Reconstruction 29<sup>th</sup> & 23<sup>rd</sup> Avenues*, *Washington Avenue Widening*, *Lakeview Avenue Overcrossing*, *Lenwood Road Grade Separation*, and *Solano I-80/680/12 Connector*; and (3) increase TCIF funds to Projects 48, 50 and 53: *Avenue 56 Grade Separation Project*; *Clay Street Grade Separation Project*; and *Magnolia Avenue Grade Separation*.

**BACKGROUND:****Project 4**

The Northern California Trade Corridor Coalition (NCTCC) and the Alameda County Transportation Commission proposes to continue Project 4, the I-880 Reconstruction 29<sup>th</sup> & 23<sup>rd</sup> Avenues Project, in the TCIF Program and maintain the \$73 million of TCIF funds on the project.

The project will construct operational and safety improvements on I-880 at the existing overcrossing of 29<sup>th</sup> and 23<sup>rd</sup> Avenues in the City of Oakland. This project was programmed in the original TCIF program in April 2008. The project was unable to be delivered by the June 2013 deadline as it was unable to meet the Buy America requirements.

The NCTCC supports continued inclusion of the project in the TCIF Program (see attached letter).

**Project 89**

The NCTCC and the Solano Transportation Authority propose to continue Project 89, the Solano I-80/680/12 Connector Project, in the TCIF Program and maintain the \$24 million of TCIF funds on the project.

The project will construct a two-lane westbound I-80 to westbound SR 12 Connector with a bridge crossing over the new westbound I-80 Green Valley Road on ramp. It will also reconstruct the I-80/Green Valley interchange. This project was programmed in the TCIF program in May 2012. The project was unable to be delivered by the June 2013 deadline as it was unable to meet the Buy America requirements.

The NCTCC supports continued inclusion of the project in the TCIF Program (see attached letter).

#### Project 94

The Metropolitan Transportation Commission (MTC) and the NCTCC propose to amend the TCIF Program by including the Santa Clara US-101 Freeway Performance Initiative Project in the Northern California Corridor element of the TCIF Program and program \$15 million of TCIF/SHOPP funds to the project.

The Santa Clara US-101 Performance Initiative Project will widen on-ramps and install ramp metering and Traffic Operations Systems at various locations along Santa Clara US-101 from San Benito County line to Route 85/101 Interchange. The total cost of the project is estimated at \$25.924 million. MTC and NCTCC propose to program \$15 million in TCIF/SHOPP funds from award savings that have been realized in the Northern California Corridor Region.

The NCTCC supports the proposed amendment to the TCIF program (see attached letter).

#### Project 21

The Southern California Consensus Group (SCCG) and the City of Commerce propose to continue Project 21, the Washington Boulevard Widening and Reconstruction Project, in the TCIF Program and maintain the \$5.8 million of TCIF funds on the project.

The project will widen and reconstruct Washington Boulevard by one additional lane in each direction from the I-5 Freeway on the east to 350' west of Indiana Street (I-710 Freeway) in the City of Commerce. This project was programmed in the original TCIF program in April 2008. The project is behind schedule and was unable to meet the June 2013 deadline for allocation.

The SCCG supports the project for continued inclusion in the TCIF Program (see attached letter).

#### Project 40

The SCCG and the Orange County Transportation Authority proposes to continue Project 40, Lakeview Avenue Overcrossing Project, in the TCIF Program and maintain the \$39.519 million of TCIF funds on the project.

The project will construct an overpass over the Burlington Northern Santa Fe (BNSF) mainline tracks and a connecting road from Orangethorpe Avenue to the new overpass of Lakeview Avenue in the City of Placentia. This project was programmed in the original TCIF program in April 2008. The project was unable to be delivered by the June 2013 deadline as it was unable to meet the Buy America requirements.

The SCCG supports continued inclusion of the project in the TCIF Program (see attached letter).

#### Project 64

The SCCG and the San Bernardino Associated Governments (SANBAG) propose to continue Project 64, Lenwood Road Grade Separation Project, in the TCIF Program and maintain the \$8.855 million of TCIF funds on the project.

The project will construct a grade separation over the BNSF track lines at Lenwood Road in the City of Barstow. This project was programmed in the TCIF program in September 2008. The project was unable to be delivered by the June 2013 deadline as it was unable to meet the Buy America requirements.

The SCCG supports continued inclusion of the project in the TCIF Program (see attached letter).

#### Project 95

The SCCG and the Alameda Corridor-East Construction Authority (ACE) propose to amend the TCIF Program by including the Puente Avenue Grade Separation Project in the Los Angeles/Inland Corridor element of the TCIF Program and program \$48 million of TCIF funds to the project.

The Puente Avenue Grade Separation Project will construct a grade separation at Puente Avenue on the Alhambra Subdivision of the Union Pacific Railroad (UPRR) located in the City of Industry. The project will eliminate the existing at-grade crossing through construction of a roadway underpass. The total cost of the project is estimated at \$96 million. SCCG and ACE propose to program \$48 million in TCIF funds from award savings that have been realized in the Los Angeles/Inland Corridor Region. Local funds will be used as the required match on the project.

The SCCG supports the proposed amendment to the TCIF program (see attached letter).

#### Project 96

The SCCG and ACE propose to amend the TCIF Program by including the Fairway Drive Grade Separation Project in the Los Angeles/Inland Corridor element of the TCIF Program and program \$56 million of TCIF funds to the project.

The Fairway Drive Grade Separation Project will construct a grade separation at Fairway drive the Los Angeles Subdivision of the UPRR located in the City of Industry. The project will eliminate the existing at-grade crossing through construction of a roadway underpass. The total cost of the project is estimated at \$119.420 million. SCCG and ACE propose to program \$56 million in TCIF funds from award savings that have been realized in the Los Angeles/Inland Corridor Region. Local funds will be used as the required match on the project.

The SCCG supports the proposed amendment to the TCIF program (see attached letter).

#### Project 48

The Riverside County Transportation Commission (RCTC) and Riverside County propose to amend the TCIF program by increasing the TCIF funds to Project 48, Avenue 56 Grade Separation Project, by \$5.066 million. The total TCIF funds programmed to this project will increase from \$10 million to \$15.066 million.

The Commission is requested to amend the TCIF program to reflect the Baseline Amendment approval and the Allocation action the Commission took at the June 11, 2013 meeting.

The SCCG supports the proposed amendment to the TCIF program (see attached letter).

Project 50

The RCTC and Riverside County propose to amend the TCIF program by increasing the TCIF funds to Project 50, Clay Street Grade Separation Project, by \$746 thousand. The total TCIF funds programmed to this project will increase from \$12.5 million to \$13.247 million.

The Commission is requested to amend the TCIF program to reflect the Baseline Amendment approval and the Allocation action the Commission took at the June 11, 2013 meeting.

The SCCG supports the proposed amendment to the TCIF program (see attached letter).

Project 53

The RCTC and Riverside County propose to amend the TCIF program by increasing the TCIF funds to Project 53, Magnolia Avenue Grade Separation Project, by \$3.996 million. The total TCIF funds programmed to this project will increase from \$13.7 million to \$17.696 million.

The Commission is requested to amend the TCIF program to reflect the Baseline Amendment approval and the Allocation action the Commission took at the June 11, 2013 meeting.

The SCCG supports the proposed amendment to the TCIF program (see attached letter).



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Cities of Contra Costa County

June 27, 2013

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Santa Clara County

*Alicia C. Aguirre*  
Cities of San Mateo County

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U.S. Department of Housing  
and Urban Development

*Tom Bates*  
Cities of Alameda County

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City and County of San Francisco

*Bill Dodd*  
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San Francisco Mayor's Appointee

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Executive Director

*Ann Flemer*  
Deputy Executive Director, Policy

*Andrew B. Fremier*  
Deputy Executive Director, Operations

Mr. Andre Boutros, Executive Director  
California Transportation Commission  
1120 N Street, MS-52  
Sacramento, CA 95814

Dear Mr. Boutros:

At the June 11, 2013 meeting of the California Transportation Commission (CTC), the CTC discussed the future of the Trade Corridor Improvement Fund (TCIF) Program. The discussion indicated that projects that are able to allocate TCIF funds in August would not need additional information; however, projects extended beyond August 2013 would require updated project documentation as well as a letter of support from the respective Coalition.

The Northern California Trade Corridors Coalition (Coalition) has four projects that have not yet been allocated in the TCIF program. These projects are discussed below.

#### **Tehachapi Rail Improvements Project**

The CTC approved an amendment for this project in May, reducing scope and funding, and updating the schedule. The project is on track to request allocation in October. The Coalition continues to support the Tehachapi Rail Improvements project.

#### **Richmond Rail Connector Project**

Caltrans was unable to secure the 401 and 404 permits from the Army Corps of Engineers in time to allocate TCIF funds at the June meeting. Caltrans expects to receive the permits at the end of this month. The Coalition believes this project will be ready to allocate at the August CTC meeting. Additionally, Caltrans requests a baseline amendment to update the cost and schedule of the project. The Coalition supports the baseline amendment request and the allocation of TCIF funds at the August meeting.

#### **Solano I-80/680/12 Interchange and Alameda I-880 Reconstruction Projects**

The remaining two projects in Solano and Alameda Counties are delayed due to the "Buy America" provision in the federal Moving Ahead for Progress in the 21st Century (MAP-21) legislation. These two projects are ready to list except they cannot provide Buy America certifications for various utility relocation work. This is a national issue that is delaying highway construction projects. The Coalition supports both projects for continued inclusion in the TCIF program, and hope that all parties involved can come to a speedy resolution of the Buy America issue. The two project sponsors and Caltrans will provide updated project baseline information to CTC staff, as requested.

Should you have any questions about the Coalition's support of our remaining TCIF projects, please contact Kenneth Kao of my staff at (510) 817-5768 or [kkao@mtc.ca.gov](mailto:kkao@mtc.ca.gov).

Sincerely,



Ross McKeown  
Principal, Programming and Allocations

cc: Northern California Coalition agencies  
Stephen Maller, CTC  
Dawn Cheser, Caltrans HQ

RJM:KK



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Deputy Executive Director, Policy

*Andrew B. Fremier*  
Deputy Executive Director, Operations

May 15, 2013

Mr. Andre Boutros, Executive Director  
California Transportation Commission  
1120 N Street, MS-52  
Sacramento, CA 95814

Dear Mr. Boutros:

The Northern California Trade Corridors Coalition (Coalition) is in support of the California Department of Transportation's (Caltrans') Trade Corridor Improvement Fund (TCIF) baseline agreement for the US-101 Freeway Performance Initiative (FPI) project in Santa Clara County.

**Santa Clara US-101 Freeway Performance Initiative Project ("Project"):**

Caltrans requests programming a new TCIF project using \$15 million in State Highway Operations and Protection Program (SHOPP) funds. The Project is eligible to use SHOPP funds, and proposes to improve traffic congestion on US-101 in southern Santa Clara County (a heavy interregional trade route) by installing ramp metering and traffic operation systems. The total construction cost is \$21.6 million, and the Project is Ready to List. Caltrans expects to advertise the project in summer 2013, with project award in fall 2013.

The Coalition met on April 30, 2013 and supports the proposed baseline agreement to program the new FPI project with \$15 million in TCIF-SHOPP funds. This amount will come from the Coalition's unprogrammed balance of TCIF-SHOPP funds. Caltrans has also submitted a concurrent allocation request for the June 2013 CTC meeting.

Should you have any questions or comments, please contact Kenneth Kao of my staff at (510) 817-5768 or [kkao@mtc.ca.gov](mailto:kkao@mtc.ca.gov).

Sincerely,

Ross McKeown  
Principal, Programming and Allocations

cc: Northern California Coalition agencies  
Stephen Maller, CTC  
Dawn Cheser, Caltrans HQ

RJM:KK





# Metro

July 1, 2013

Mr. Andre Boutros  
Executive Director  
California Transportation Commission  
1120 N Street, Mail Stop: 52  
Sacramento, CA 94273-0001

Dear Mr. Boutros:

The Southern California Consensus Group appreciates the Commissioner's recent action to streamline the process for extending the TCIF program deadlines by grandfathering existing projects. This decision reinforces our successful collaborative experience with the CTC on the TCIF program.

In that vein, we are pleased to submit the following projects for inclusion in the TCIF program in accordance with the proposal to utilize program savings.

- ❖ Project #21 - Washington Boulevard widening and reconstruction: \$5,800,000 in TCIF
- ❖ Project #40 - Lakeview Avenue overcrossing: \$39,519,000 in TCIF
- ❖ Project #64 - Lenwood grade separation: \$8,855,000 in TCIF

These existing projects were unable to receive allocation in June 2013 but are extremely valuable for goods movement in the Southern California region. Project sponsors are doing their utmost to prepare these projects for an August allocation. However, projects #40 and #64 have been delayed due to Buy America issues that are well beyond their control and project #21 is undergoing Caltrans review. We request that these projects remain on the TCIF list if allocation does not occur in August. We will work with you to follow your new processes, as appropriate.

As always we thank you for your leadership and remain strongly committed to improving the movement of goods in this region.

Sincerely,

Shahrzad Amiri  
Deputy Executive Officer

cc: Southern California Consensus Group  
Stephen Maller



**Metro**

June 12, 2013

Mr. Andre Boutros  
Executive Director  
California Transportation Commission  
1120 "N" Street, Mail Stop: 52  
Sacramento, CA 94273-0001

Dear Mr. Boutros:

The Southern California Consensus Group has discussed and approved the changes requested by our partner agency. It is our understanding that the Alameda Corridor-East Construction Authority (ACE) is requesting to seek funding of \$104,000,000 in Trade Corridor Improvement Funds (TCIF) for two new projects.

- Puente Avenue Grade Separation - \$48,000,000
- Fairway Drive Grade Separation - \$56,000,000

Please see the attached letter detailing the requested changes. Please direct any questions or comments regarding this issue to me at (213) 922-3061. We appreciate the ongoing support and guidance provided by you and your staff.

Sincerely,

Shahrzad Amiri  
Deputy Executive Officer

cc: Southern California Consensus Group  
Stephen Maller



# Metro

April 25, 2013

Mr. Andre Boutros  
Executive Director  
California Transportation Commission  
1120 "N" Street, Mail Stop: 52

Dear Mr. Boutros: *Andre!*

The Southern California Consensus Group has discussed and approved the changes requested by our partner agency. It is our understanding that the Riverside County Transportation Commission (RCTC) is requesting to amend their Baseline Agreements for Project #46 - Sunset Avenue Grade Separation, Project #48 - Avenue 56 Grade Separation, Project #50 - Clay Street Grade Separation, Project #53 - Grade Separation at Magnolia Avenue Railroad Grade Crossing, & Project #85 - Avenue 52 Grade Separation due to schedule modifications and project costs.

RCTC is also requesting to reallocate \$13.4 million from TCIF project savings.

- \$3,600,000 to Project #51 - Riverside Avenue Grade Separation.
- \$5,065,324 to Project #48 - Avenue 56 Grade Separation.
- \$746,613 to Project #50 - Clay Street Grade Separation.
- \$3,996,397 to Project #53 - Grade Separation at Magnolia Avenue Railroad Grade Crossing

The proposed modifications do not change the TCIF funds for RCTC.

Please see the attached letter detailing the requested changes. Please direct any questions or comments regarding this issue to me at (213) 922-3061. We appreciate the ongoing support and guidance provided by you and your staff.

Sincerely,

Shahrzad Amiri  
Deputy Executive Officer

cc: Southern California Consensus Group  
Stephen Maller

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 4.17  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: William D. Bronte, Chief  
Division of Rail

Subject: **PROPOSITION 1B INTERCITY RAIL IMPROVEMENT PROGRAM AMENDMENT  
RESOLUTION ICR1B-P-1314-01, AMENDING RESOLUTION ICR1B-P-1112-01**

## **RECOMMENDATION:**

The California Department of Transportation (Department) requests California Transportation Commission (Commission) consent to amend the Proposition 1B Intercity Rail Improvement Program (ICR1B) project list to delete the Fresno Mid-Route Layover Facility (FMR) project, and to reprogram the funds for the Sacramento Maintenance Facility (SMF) project.

## **ISSUE:**

The Department requests that the ICR1B project list be amended to unprogram the FMR project, to transfer the funds from FMR to the SMF project, and to update the scope of the SMF project.

### Fresno Mid-Route Layover Facility

At the time the Fresno Mid-Route Layover Facility was programmed in the ICR1B, it was estimated that the facility could be constructed by June 2014 but preliminary work has been put on hold until a determination is made on the impact that the new High-Speed Rail Authority business plan will have on this location, and the selection of a specific site for the facility.

### Sacramento Maintenance Facility

The new facility will provide maintenance and storage for existing and recently ordered Amtrak California rolling stock for both the San Joaquin and Capitol rail corridors. Phase 1 tasks include site selection and valuation, environmental clearances, remediation and property acquisition. The Department is still in the process of the site selection, valuation, as well as property acquisition.

The current project budgets and the revised SMF project budget are shown in the table below.

<b>Funding Source</b>	<b>FMR Existing</b>	<b>SMF Existing</b>	<b>FMR Amended</b>	<b>SMF Amended</b>
STIP	\$0	\$25,450,000	\$0	\$25,450,000
Proposition 1B	\$14,601,000	\$4,550,000	\$0	\$19,151,000
<b>TOTAL</b>	<b>\$14,601,000</b>	<b>\$30,000,000</b>	<b>\$0</b>	<b>\$44,601,000</b>

These benefits are consistent with the State's intercity passenger rail goals for the corridors. The SMF project is consistent with the ICR1B Program guidelines. The proposed SMF funding by component is shown in the following table.

<b>Funding Source</b>	<b>PA&amp;ED</b>	<b>PS&amp;E</b>	<b>ROW</b>	<b>CON</b>	<b>TOTAL</b>
STIP	\$0	\$6,600,000	\$18,850,000	\$0	\$25,450,000
Proposition 1B	\$0	\$0	\$900,000	\$18,251,000	\$19,151,000
<b>TOTAL</b>	<b>\$0</b>	<b>\$6,600,000</b>	<b>\$19,750,000</b>	<b>\$18,251,000</b>	<b>\$44,601,000</b>

**BACKGROUND:**

The Highway Safety, Traffic Reduction, Air Quality, and Port Security Bond Act of 2006, approved by voters as Proposition 1B, provides \$400 million, upon appropriation by the Legislature, to the Department for intercity passenger rail improvement projects. A minimum of \$125 million is designated for procurement of additional intercity passenger railcars and locomotives. This \$400 million program is part of the \$4 billion Public Transportation Modernization, Improvement, and Service Enhancement Account (PTMISEA). This account is to be used to fund public transportation projects. Pursuant to paragraph (2) of subdivision (c) of Section 8879.50 of the Government Code, the Department is the administrative agency for the PTMISEA.

At its December 2007 meeting, the Commission approved the guidelines for intercity passenger rail projects in the PTMISEA. The guidelines allow the Department, if necessary, to return to the Commission to request its consent to modify the project list.

The necessary changes are reflected in strikethrough and bold underline in the revised Proposition 1B Intercity Rail Projects list.

**PROPOSITION 1B INTERCITY RAIL IMPROVEMENT PROGRAM AMENDMENT (Proposed)**

<b>Project/Description</b>	<b>Corridor</b>	<b>Funding Request</b>
<b>Procure New Rail Cars:</b> <sup>1</sup> Purchase bi-level intercity rail cars and locomotives (est. 42 cars and 6 locomotives).	Capitol Corridor, Pacific Surfliner, San Joaquin	\$ 150,000,000
<b>Commerce/Fullerton Triple Track - Segment 6:</b> <sup>1</sup> Construct third main track from MP 154.5 to MP 157.6	Pacific Surfliner, Metrolink	\$ 32,000,000
<b>Commerce/Fullerton Triple Track - Segment 8:</b> <sup>1</sup> Construct third main track from MP 157.4 to MP 158.8	Pacific Surfliner, Metrolink	\$ 30,500,000
<b>New Station Track at LA Union Station:</b> <sup>1</sup> Build new track, platform and renovate canopies.	Pacific Surfliner, Metrolink	\$ 35,100,000
<b>San Onofre to Pulgas Double Track Project – Phase 1:</b> <sup>1</sup> Design and environmental work for Phases 1 and 2 of project, plus construction of Phase 1.	Pacific Surfliner	\$ 30,000,000
<b><u>Sacramento Maintenance Facility:</u></b> <b><u>Design and build storage track and maintenance facility.</u></b>	<b><u>Capitol Corridor,</u></b> <b><u>San Joaquin</u></b>	<del>\$ 4,550,000</del> <b>\$ 19,151,000</b>
<b>Oakley to Port Chicago:</b> <sup>1</sup> Construct double track.	San Joaquin	\$ 25,450,000
<b>Coast Daylight Track and Signal:</b> Track and signal project to allow Pacific Surfliner extension to San Francisco Bay Area.	Pacific Surfliner, Coast Daylight	\$ 25,000,000
<b><del>Mid-Route Layover Facility:</del></b> <b><del>Design and build layover facility.</del></b>	<b><del>San Joaquin</del></b>	<del>\$ 14,601,000</del>
<b>Kings Park Track and Signal Improvements:</b> <sup>1</sup> Improve track and signals along San Joaquin Intercity rail line near Hanford in Kings County.	San Joaquin	\$ 3,500,000
<b>Wireless Network for Northern California IPR Fleet:</b> <sup>1</sup> Install a wireless communication network on the Northern California IPR fleet for passenger amenity, support of safety and security, and expand ADA compliance for on-train communications.	Capitol Corridor, San Joaquin	\$ 3,750,000
<b>Raymer to Bernson Double Track:</b> Construct double track from MP 453.1 to MP 446.8 in Ventura County.	Pacific Surfliner, Metrolink	\$ 7,500,000
<b>Santa Margarita Bridge and Double Track:</b> <sup>1</sup> Replace bridge with 2-track bridge and construct additional double track.	Pacific Surfliner	\$ 16,206,000
<b>Emeryville Station and Track Improvements:</b> <sup>1</sup> Extend siding track with associated signal and other track.	Capitol Corridor, San Joaquin	\$ 6,250,000
<b>Bahia Benicia Crossover:</b> <sup>1</sup> Construct crossover between two mainline tracks and additional track improvements and upgrades including frog replacement and tie tamping on the Capitol Corridor.	Capitol Corridor	\$ 4,750,000
<b>SCRRA Sealed Corridor:</b> <sup>1</sup> Enhance safety of grade crossings and Railroad Right of Way.	Metrolink	\$ 3,000,000
<b>SUB-TOTAL ALL PROJECTS</b>		<b>\$ 392,157,000</b>
Bond Issuance Costs - Loan admin costs, arbitrage rebates, etc. <sup>2</sup>		<b>\$ 7,843,000</b>
<b>TOTAL RAIL BOND FUNDS</b>		<b>\$ 400,000,000</b>

1. Projects with CTC allocations (full or partial).  
2. Bond Issuance Cost is 2 percent of the Bond amount.

**PROPOSITION 1B INTERCITY RAIL IMPROVEMENT PROGRAM AMENDMENT (Amended)**

<b>Project/Description</b>	<b>Corridor</b>	<b>Funding Request</b>
<b>Procure New Rail Cars:</b> <sup>1</sup> Purchase bi-level intercity rail cars and locomotives (est. 42 cars and 6 locomotives).	Capitol Corridor, Pacific Surfliner, San Joaquin	\$ 150,000,000
<b>Commerce/Fullerton Triple Track - Segment 6:</b> <sup>1</sup> Construct third main track from MP 154.5 to MP 157.6	Pacific Surfliner, Metrolink	\$ 32,000,000
<b>Commerce/Fullerton Triple Track - Segment 8:</b> <sup>1</sup> Construct third main track from MP 157.4 to MP 158.8	Pacific Surfliner, Metrolink	\$ 30,500,000
<b>New Station Track at LA Union Station:</b> <sup>1</sup> Build new track, platform and renovate canopies.	Pacific Surfliner, Metrolink	\$ 35,100,000
<b>San Onofre to Pulgas Double Track Project – Phase 1:</b> <sup>1</sup> Design and environmental work for Phases 1 and 2 of project, plus construction of Phase 1.	Pacific Surfliner	\$ 30,000,000
<b>Sacramento Maintenance Facility:</b> Design and build storage track and maintenance facility.	Capitol Corridor, San Joaquin	\$ 19,151,000
<b>Oakley to Port Chicago:</b> <sup>1</sup> Construct double track.	San Joaquin	\$ 25,450,000
<b>Coast Daylight Track and Signal:</b> Track and signal project to allow Pacific Surfliner extension to San Francisco Bay Area.	Pacific Surfliner, Coast Daylight	\$ 25,000,000
<b>Kings Park Track and Signal Improvements:</b> <sup>1</sup> Improve track and signals along San Joaquin Intercity rail line near Hanford in Kings County.	San Joaquin	\$ 3,500,000
<b>Wireless Network for Northern California IPR Fleet:</b> <sup>1</sup> Install a wireless communication network on the Northern California IPR fleet for passenger amenity, support of safety and security, and expand ADA compliance for on-train communications.	Capitol Corridor, San Joaquin	\$ 3,750,000
<b>Raymer to Bernson Double Track:</b> Construct double track from MP 453.1 to MP 446.8 in Ventura County.	Pacific Surfliner, Metrolink	\$ 7,500,000
<b>Santa Margarita Bridge and Double Track:</b> <sup>1</sup> Replace bridge with 2-track bridge and construct additional double track.	Pacific Surfliner	\$ 16,206,000
<b>Emeryville Station and Track Improvements:</b> <sup>1</sup> Extend siding track with associated signal and other track.	Capitol Corridor, San Joaquin	\$ 6,250,000
<b>Bahia Benicia Crossover:</b> <sup>1</sup> Construct crossover between two mainline tracks and additional track improvements and upgrades including frog replacement and tie tamping on the Capitol Corridor.	Capitol Corridor	\$ 4,750,000
<b>SCRRRA Sealed Corridor:</b> <sup>1</sup> Enhance safety of grade crossings and Railroad Right of Way.	Metrolink	\$ 3,000,000
<b>SUB-TOTAL ALL PROJECTS</b>		<b>\$ 392,157,000</b>
Bond Issuance Costs - Loan admin costs, arbitrage rebates, etc. <sup>2</sup>		<b>\$ 7,843,000</b>
<b>TOTAL RAIL BOND FUNDS</b>		<b>\$ 400,000,000</b>

1. Projects with CTC allocations (full or partial).  
2. Bond Issuance Cost is 2 percent of the Bond amount.

**CALIFORNIA TRANSPORTATION COMMISSION**

**Commission Advice and Consent  
Proposition 1B Intercity Rail Capital Program Amendment**

**Resolution ICR1B-P-1314-01,  
Amending Resolution ICR1B-P-1112-01**

- 1.1 WHEREAS, Proposition 1B, passed by California voters on November 7, 2006, called for \$4 billion to be deposited into the Public Transportation Modernization, Improvement, and Service Enhancement Account; and
- 1.2 WHEREAS, of the \$4 billion, \$400 million was designated, to be available upon appropriation by the Legislature, for intercity rail capital projects, including at least \$125 million for the purchase of additional rail cars and locomotives; and
- 1.3 WHEREAS, the California Transportation Commission (Commission) approved at its December 2007 meeting, the “Guidelines for Intercity Passenger Rail Projects in the Public Transportation Modernization, Improvement, and Service Enhancement Account”, that provide guidance on the implementation of the Proposition 1B Intercity Passenger Rail Program; and
- 1.4 WHEREAS, the guidelines state the California Department of Transportation (Department) can return to the Commission to request formal approval to modify the project list and project scope; and
- 1.5 WHEREAS, the initial intercity rail Proposition 1B project list was approved at February 2008 Commission meeting; and
- 1.6 WHEREAS, the amended intercity rail Proposition 1B projects list includes \$392.2 million in intercity rail projects and \$7.8 million in bond issuance costs; and
- 1.7 WHEREAS, all projects on the attached amended Proposition 1B project list are consistent with the guidelines.
- 2.1 NOW THEREFORE BE IT RESOLVED, that the Commission does hereby provide its consent to the amended list of intercity rail Proposition 1B projects; and
- 2.2 BE IT FURTHER RESOLVED that the Department shall report on a quarterly basis to the Commission on the allocation status of the Proposition 1B intercity passenger rail projects as part of the Department’s quarterly delivery report.

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 4.20  
Information

From: ANDRE BOUTROS  
Executive Director

Subject: **DRAFT PROGRAM OF PROJECTS FOR THE FEDERAL FISCAL YEAR 2011-12  
FTA SECTION 5310 ELDERLY AND DISABLED TRANSIT PROGRAM**

Under Government Code Section 14055-14055.4, the Commission is responsible for allocating funds, establishing an appeals process and holding at least one public hearing prior to approving the Program of Projects for the Federal Transit Administration Section 5310 Elderly and Disabled Transit Program (Program).

## **Federal Fiscal Year (FFY) 2011-12 Program Grant Cycle**

The current estimate of available federal funding for the FFY 2011-12 Program is \$13.6 million (including the 11.47% match). Historically, Program participants have been required to use their own funds to provide a local match of 11.47%, in this cycle however, successful applicants will not be required to provide the local match as it will be funded with federal Transit Toll Credits.

Eligible agencies submitted 93 applications for 417 projects requesting a total of \$22.4 million in 5310 Program funds. The applications were scored by the Regional Transportation Planning Agencies (RTPAs). The State Review Committee reviewed the RTPA scores, and using the Program criteria adopted by the Commission, developed a draft statewide prioritized project list.

On September 4, 2013, the State Review Committee and Commission staff will hold a staff-level conference to hear any appeals by RTPAs and/or project applicants. Following the conference, the list will be revised as necessary, and Commission staff will submit the final Statewide Prioritized Project List for Commission adoption at the October 2013 meeting.

At the same meeting, prior to adopting the Program, the Commission will hold the mandated public hearing to discuss the prioritized list and overall program policy. Following the hearing, the Commission may adopt the Statewide Prioritized Project List for the FFY 2011-12 Federal Transit Administration Section 5310 Elderly and Disabled Transit Program.

Attached are the letter and the draft FFY 2011-12 Statewide Prioritized Project List as submitted to the RTPAs and to project applicants. The draft list of projects represents nearly 110% (\$15 million) of the estimated available federal funding to allow flexibility for replacement if projects within 100% of available funding are not deliverable.

**BACKGROUND**

The Program was established in 1975 and has been administered by the Department since its inception. It provides annual grants of federal funds to purchase transit capital equipment to meet the specialized needs of elderly and/or disabled persons for whom mass transportation services are unavailable, insufficient, or inappropriate.

The Program serves a variety of client groups and programs ranging from small agencies with specific clientele (e.g., dialysis and AIDS patients) to paratransit providers serving entire communities. Most of the agencies are non-profit organizations while some are public agencies where non-profit organizations are not readily available to provide the specialized service.

In 1996, AB 772 passed directing the Commission to have oversight responsibilities for the Program. AB 772 placed three mandates on the Commission regarding the Program: (1) the Commission shall direct the Department on how to allocate funds for the Program, (2) the Commission shall establish an appeals process for the Program, and (3) the Commission shall hold at least one public hearing prior to approving its Program of Projects.

In January of 1997, the Commission approved the procedures for the Program, criteria used to score the projects, and a State Review Committee consisting of representatives from the State Departments of Rehabilitation, Aging, Developmental Services and Transportation, with Commission staff acting in the role of facilitator/coordinator for the State Review Committee.

In accordance with the Commission's adopted procedures, the RTPA or its designated Regional Evaluation Committee scores projects within its jurisdiction utilizing the Commission's adopted project scoring criteria and forwards a scored list of their projects to the Department. The RTPA must notify its applicants of their project scores and provide information about the local appeals process. The State Review Committee rescores the projects to verify the scores given at the regional level. In those instances where there is a sizable difference between regional and State Review Committee project scores, the Department contacts the regions and the differences are thoroughly discussed.

A statewide prioritized list of projects is then created representing at least 110% of the estimated available funds. Typically, the Commission adopts the Program of Projects down to at least the 110% funding level to allow flexibility for replacement if projects within 100% of available funding are not deliverable.

Attachments

JAMES C. GHIEMMETTI, Chair  
CARL GUARDINO, Vice Chair  
BOB ALVARADO  
DARIUS ASSEMI  
YVONNE B. BURKE  
LUCETTA DUNN  
JAMES EARP  
DARIO FROMMER  
FRAN INMAN  
JOSEPH TAVAGLIONE

STATE OF CALIFORNIA

EDMUND G. BROWN Jr., Governor



SENATOR MARK DESAULNIER, Ex Officio  
ASSEMBLY MEMBER BONNIE LOWENTHAL, Ex Officio

Andre Boutros, Executive Director

## **CALIFORNIA TRANSPORTATION COMMISSION**

1120 N STREET, MS-52  
SACRAMENTO, CA 95814  
P. O. BOX 942873  
SACRAMENTO, CA 94273-0001  
FAX (916) 653-2134  
(916) 654-4245  
<http://www.catc.ca.gov>

July 19, 2013

**TO: REGIONAL TRANSPORTATION PLANNING AGENCIES  
APPLICANTS FOR THE ELDERLY AND DISABLED TRANSIT PROGRAM**

The State Review Committee has verified the scored applications for the Federal Fiscal Year (FFY) 2012 Elderly and Disabled Transit Program (Title 49 U.S.C. Section 5310) received from the Regional Transportation Planning Agencies. Program criteria adopted by the California Transportation Commission (Commission), was used to develop the enclosed draft statewide prioritized project list.

In those instances where there was a sizable difference between regional and State Review Committee (SRC) project scores, the regions were contacted by the Department of Transportation (Department) and the differences thoroughly discussed. The projects have been prioritized based on the Commission's adopted procedures for prioritizing projects.

Approximately \$13.6 million in federal funding (including the 11.47% local Toll Credit match) is expected to be available. Given this estimate, projects with a score of 84 and above would be candidates for funding, dependent on the outcome of the staff level conference scheduled on September 4, 2013.

Commission staff will recommend a final adopted list that represents a level of project funding down to \$15 million, nearly 110% of the estimated available federal funding, to allow flexibility for replacement if projects within 100% of available funding are not deliverable. The 110% funding line is a score of 83 and above.

The staff level conference scheduled on September 4, 2013 at 9:30 a.m. will be held in the Division of Mass Transportation's conference room located at 1120 N Street in Sacramento. The purpose of this conference is to discuss the draft statewide-prioritized project list and hear any appeals on technical issues. Only appeals based on actions that occurred at the statewide level will be considered. The appealing agency must demonstrate by using documentation from its original application that the Commission's adopted program criteria were incorrectly applied by the SRC.

Agencies planning to make a technical appeal must call the Department at their toll free number (888) 472-6816 to schedule a speaking time. Each agency will be allowed a maximum of 10 minutes for their appeal. The appeal must also be submitted in writing, and can be brought to the staff level conference, faxed, or mailed to the following address:

State Review Committee - Section 5310  
c/o Department of Transportation  
1120 N Street, Room 3300 - MS 39  
Sacramento, CA 95814  
Attention: Mr. Chuck Gunter

FAX: 916-654-9366

After the conference, the State Review Committee will revise the list as necessary, and Commission staff will submit the final statewide-prioritized list for adoption. The Commission will hold its mandated public hearing during its October 2013 meeting to discuss the prioritized list and overall program policy. Following the hearing, the Commission may adopt the Statewide Prioritized Project List as the FFY 2011-12 Program of Projects (POP) for the Federal Transit Administration Section 5310 Elderly and Disabled Transit Program.

Sincerely,

JUAN GUZMAN  
Associate Deputy Director

Enclosure

cc: Members, California Transportation Commission  
Malcolm Dougherty, Director, California Department of Transportation  
FTA Section 5310 State Review Committee

**FTA SECTION 5310 ELDERLY AND DISABLED TRANSIT PROGRAM  
FFY 2011-12 STATEWIDE PRIORITIZED LIST  
(IN STATE SCORE PRIORITY ORDER)**

Agency	Co	Urban or Rural	Project	Type	Vin	Year	Miles	Match (Toll Credits)	FTA 5310 Share	Total Project \$	Cumulative (Federal \$)	State Score
Mountain Shadows Support Group, MSSG	SD	U&R	Modified Raised Top	R	46011	2004	72,500	\$ 5,735	\$ 44,265	\$ 50,000	\$ 50,000	100
Mountain Shadows Support Group, MSSG	SD	U&R	Modified Raised Top	R	47524	2005	184,995	\$ 5,735	\$ 44,265	\$ 50,000	\$ 100,000	100
Mountain Shadows Support Group, MSSG	SD	U&R	Medium Bus	R	85134	2003	101,224	\$ 7,685	\$ 59,315	\$ 67,000	\$ 167,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	38704	2006	210,901	\$ 5,162	\$ 39,839	\$ 45,000	\$ 212,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	36198	2006	206,050	\$ 5,162	\$ 39,839	\$ 45,000	\$ 257,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	36457	2006	203,413	\$ 5,162	\$ 39,839	\$ 45,000	\$ 302,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	37672	2006	175,308	\$ 5,162	\$ 39,839	\$ 45,000	\$ 347,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	35520	2006	176,511	\$ 5,162	\$ 39,839	\$ 45,000	\$ 392,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	36553	2006	181,435	\$ 5,162	\$ 39,839	\$ 45,000	\$ 437,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	34575	2006	187,052	\$ 5,162	\$ 39,839	\$ 45,000	\$ 482,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	39045	2006	196,825	\$ 5,162	\$ 39,839	\$ 45,000	\$ 527,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	44952	2006	175,478	\$ 5,162	\$ 39,839	\$ 45,000	\$ 572,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	45953	2006	180,807	\$ 5,162	\$ 39,839	\$ 45,000	\$ 617,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	44580	2006	185,612	\$ 5,162	\$ 39,839	\$ 45,000	\$ 662,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	45066	2006	178,175	\$ 5,162	\$ 39,839	\$ 45,000	\$ 707,000	100
Outreach & Escort, Inc.	SCL	U	Minivan	R	45008	2006	178,376	\$ 5,162	\$ 39,839	\$ 45,000	\$ 752,000	100
Outreach & Escort, Inc.	SCL	U	Base Station	OE				\$ 287	\$ 2,213	\$ 2,500	\$ 754,500	100
Outreach & Escort, Inc.	SCL	U	Mobile Radio (13)	OE				\$ 1,417	\$ 10,933	\$ 12,350	\$ 766,850	100
Fresno County Economic Opportunities Commission	FRE	U	Large Bus	R	85228	2004	205,254	\$ 8,373	\$ 64,627	\$ 73,000	\$ 839,850	99
Fresno County Economic Opportunities Commission	FRE	U	Large Bus	R	85236	2004	210,212	\$ 8,373	\$ 64,627	\$ 73,000	\$ 912,850	99
Fresno County Economic Opportunities Commission	FRE	U	Large Bus	R	85237	2004	228,911	\$ 8,373	\$ 64,627	\$ 73,000	\$ 985,850	99
Fresno County Economic Opportunities Commission	FRE	U	Large Bus	R	85238	2004	213,101	\$ 8,373	\$ 64,627	\$ 73,000	\$ 1,058,850	99
Fresno County Economic Opportunities Commission	FRE	U	Large Bus	R	85239	2004	218,525	\$ 8,373	\$ 64,627	\$ 73,000	\$ 1,131,850	99
Fresno County Economic Opportunities Commission	FRE	U	Large Bus	R	85241	2004	206,210	\$ 8,373	\$ 64,627	\$ 73,000	\$ 1,204,850	99
Fresno County Economic Opportunities Commission	FRE	U	Large Bus	R	85243	2004	216,015	\$ 8,373	\$ 64,627	\$ 73,000	\$ 1,277,850	99
Fresno County Economic Opportunities Commission	FRE	U	Large Bus	R	93881	2005	232,592	\$ 8,373	\$ 64,627	\$ 73,000	\$ 1,350,850	99
Fresno County Economic Opportunities Commission	FRE	U	Computer Hardware	OE				\$ 1,815	\$ 14,009	\$ 15,824	\$ 1,366,674	99
Home of Guiding Hands Corporation	SD	U&R	Medium Bus	R	64630	2003	97,500	\$ 7,685	\$ 59,315	\$ 67,000	\$ 1,433,674	99
Home of Guiding Hands Corporation	SD	U&R	Medium Bus	R	01501	2003	65,000	\$ 7,685	\$ 59,315	\$ 67,000	\$ 1,500,674	99
Abrazar	ORA	U	Small Bus	R	00121	2001	279,648	\$ 6,882	\$ 60,000	\$ 60,000	\$ 1,560,674	98
Abrazar	ORA	U	Small Bus	R	00142	2001	276,479	\$ 6,882	\$ 60,000	\$ 60,000	\$ 1,620,674	98
Abrazar	ORA	U	40 GPS Units	OE				\$ 1,147	\$ 10,000	\$ 10,000	\$ 1,630,674	98
Abrazar	ORA	U	10 Computers	OE				\$ 1,602	\$ 13,970	\$ 13,970	\$ 1,644,644	98
Abrazar	ORA	U	10 Restraints	OE				\$ 692	\$ 6,030	\$ 6,030	\$ 1,650,674	98
Abrazar	ORA	U	Mobile Radios (10)	OE				\$ 1,147	\$ 10,000	\$ 10,000	\$ 1,660,674	98
Mountain Shadows Support Group, MSSG	SD	U&R	Modified Raised Top	SE				\$ 5,735	\$ 44,265	\$ 50,000	\$ 1,710,674	98
Home of Guiding Hands Corporation	SD	U&R	Large Bus	SE				\$ 8,373	\$ 64,627	\$ 73,000	\$ 1,783,674	97
Home of Guiding Hands Corporation	SD	U&R	Base Station (2)	OE				\$ 2,294	\$ 17,706	\$ 5,000	\$ 1,788,674	97
Home of Guiding Hands Corporation	SD	U&R	Mobile Radios (15)	OE				\$ 2,294	\$ 17,706	\$ 15,000	\$ 1,803,674	97
Home of Guiding Hands Corporation	SD	U&R	Large Bus	SE				\$ 8,373	\$ 64,627	\$ 73,000	\$ 1,876,674	97
Mountain Shadows Support Group, MSSG	SD	U&R	Modified Raised Top	SE				\$ 5,735	\$ 44,265	\$ 50,000	\$ 1,926,674	97
Tehama County Opportunity Center, Inc.	TEH	R	Large Bus	R	18798	2007	251,468	\$ 8,373	\$ 64,627	\$ 73,000	\$ 1,999,674	97
Tehama County Opportunity Center, Inc.	TEH	R	Large Bus	R	18805	2007	240,747	\$ 8,373	\$ 64,627	\$ 73,000	\$ 2,072,674	97
Home of Guiding Hands Corporation	SD	U&R	Large Bus	SE				\$ 8,373	\$ 64,627	\$ 73,000	\$ 2,145,674	96
Home of Guiding Hands Corporation	SD	U&R	Large Bus	SE				\$ 8,373	\$ 64,627	\$ 73,000	\$ 2,218,674	96
UCP/Ride-On	SLO	R	Large Bus	R	85226	2003	287,500	\$ 8,373	\$ 64,627	\$ 73,000	\$ 2,291,674	96
UCP/Ride-On	SLO	R	Small Bus	R	26584	2008	234,257	\$ 6,882	\$ 53,118	\$ 60,000	\$ 2,351,674	96
UCP/Ride-On	SLO	R	Small Bus	R	26585	2008	255,910	\$ 6,882	\$ 53,118	\$ 60,000	\$ 2,411,674	96
Work Training Center	BUT	U	Large Bus	R	75414	2006	233,506	\$ 8,373	\$ 64,627	\$ 73,000	\$ 2,484,674	95
Work Training Center	BUT	U	Mobile Radios (3)	OE				\$ 344	\$ 2,656	\$ 3,000	\$ 2,487,674	95
Work Training Center	BUT	U	W/C restraints	OE				\$ 532	\$ 4,102	\$ 4,634	\$ 2,492,308	95
Mountain Shadows Support Group, MSSG	SD	U&R	Medium Bus	R	46503	2005	95,220	\$ 7,685	\$ 59,315	\$ 67,000	\$ 2,559,308	95
Mountain Shadows Support Group, MSSG	SD	U&R	Medium Bus	R	46504	2005	98,700	\$ 7,685	\$ 59,315	\$ 67,000	\$ 2,626,308	95
El Dorado County Transit Authority	ED	R	Larger Bus	R	18235	2003	211,116	\$ 12,044	\$ 92,956	\$ 105,000	\$ 2,731,308	94
El Dorado County Transit Authority	ED	R	Larger Bus	R	19885	2007	259,959	\$ 12,044	\$ 92,956	\$ 105,000	\$ 2,836,308	94

**FTA SECTION 5310 ELDERLY AND DISABLED TRANSIT PROGRAM  
FFY 2011-12 STATEWIDE PRIORITIZED LIST  
(IN STATE SCORE PRIORITY ORDER)**

Agency	Co	Urban or Rural	Project	Type	Vin	Year	Miles	Match (Toll Credits)	FTA 5310 Share	Total Project \$	Cumulative (Federal \$)	State Score
El Dorado County Transit Authority	ED	R	Minivan	R	63299	2008	161,414	\$ 5,162	\$ 39,838	\$ 45,000	\$ 2,881,308	94
City of Fresno Department of Transportation	FRE	U	Small Bus	R	32200	2004	280,663	\$ 6,882	\$ 53,118	\$ 60,000	\$ 2,941,308	94
City of Fresno Department of Transportation	FRE	U	Small Bus	R	64003	2005	259,423	\$ 6,882	\$ 53,118	\$ 60,000	\$ 3,001,308	94
City of Fresno Department of Transportation	FRE	U	Small Bus	R	64005	2005	292,428	\$ 6,882	\$ 53,118	\$ 60,000	\$ 3,061,308	94
City of Fresno Department of Transportation	FRE	U	Small Bus	R	64006	2005	295,885	\$ 6,882	\$ 53,118	\$ 60,000	\$ 3,121,308	94
City of Fresno Department of Transportation	FRE	U	Small Bus	R	64007	2005	298,054	\$ 6,882	\$ 53,118	\$ 60,000	\$ 3,181,308	94
City of Fresno Department of Transportation	FRE	U	Small Bus	R	64008	2005	295,165	\$ 6,882	\$ 53,118	\$ 60,000	\$ 3,241,308	94
City of Fresno Department of Transportation	FRE	U	Medium Bus	R	47758	2007	262,640	\$ 7,685	\$ 59,315	\$ 67,000	\$ 3,308,308	94
City of Fresno Department of Transportation	FRE	U	Medium Bus	R	47752	2007	270,723	\$ 7,685	\$ 59,315	\$ 67,000	\$ 3,375,308	94
City of Fresno Department of Transportation	FRE	U	CAD/AVL units	OE				\$ 4,577	\$ 35,323	\$ 39,900	\$ 3,415,208	94
Inyo-Mono Association for the Handicapped (IMAH)	INY	R	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 3,482,208	94
MST	MON	R	Medium Bus	R	72418	2006	295,912	\$ 7,685	\$ 59,315	\$ 67,000	\$ 3,549,208	94
MST	MON	R	Medium Bus	R	72419	2006	293,130	\$ 7,685	\$ 59,315	\$ 67,000	\$ 3,616,208	94
MST	MON	R	Medium Bus	R	59194	2007	265,999	\$ 7,685	\$ 59,315	\$ 67,000	\$ 3,683,208	94
MST	MON	R	Medium Bus	R	59195	2007	260,233	\$ 7,685	\$ 59,315	\$ 67,000	\$ 3,750,208	94
MST	MON	R	Medium Bus	R	61208	2007	245,366	\$ 7,685	\$ 59,315	\$ 67,000	\$ 3,817,208	94
MST	MON	R	Medium Bus	R	46320	2008	226,353	\$ 7,685	\$ 59,315	\$ 67,000	\$ 3,884,208	94
MST	MON	R	Medium Bus	R	52063	2008	259,989	\$ 7,685	\$ 59,315	\$ 67,000	\$ 3,951,208	94
MST	MON	R	Mobile Radios (16)	OE				\$ 1,835	\$ 14,165	\$ 16,000	\$ 3,967,208	94
Orange County Adult Achievement Center	ORA	U	Cameras (10)	OE				\$ 2,294	\$ 20,000	\$ 20,000	\$ 3,987,208	94
Orange County Adult Achievement Center	ORA	U	Mobile Data Terminals (3)	OE				\$ 2,181	\$ 19,017	\$ 19,017	\$ 4,006,225	94
Pace Solano	SOL	U	Small Bus	R	62746	2003	154,231	\$ 6,882	\$ 53,118	\$ 60,000	\$ 4,066,225	94
Pace Solano	SOL	U	Medium Bus	R	82257	2003	109,306	\$ 7,685	\$ 59,315	\$ 67,000	\$ 4,133,225	94
Pace Solano	SOL	U	Large Bus	R	67894	2003	163,050	\$ 8,373	\$ 64,627	\$ 73,000	\$ 4,206,225	94
Pace Solano	SOL	U	Large Bus	R	70669	2003	135,595	\$ 8,373	\$ 64,627	\$ 73,000	\$ 4,279,225	94
Pace Solano	SOL	U	Large Bus	R	67892	2003	145,126	\$ 8,373	\$ 64,627	\$ 73,000	\$ 4,352,225	94
Pace Solano	SOL	U	Large Bus	R	67895	2003	139,145	\$ 8,373	\$ 64,627	\$ 73,000	\$ 4,425,225	94
Pace Solano	SOL	U	Large Bus	R	65368	2003	86,012	\$ 8,373	\$ 64,627	\$ 73,000	\$ 4,498,225	94
Pace Solano	SOL	U	Large Bus	R	3765	1998	153,652	\$ 8,373	\$ 64,627	\$ 73,000	\$ 4,571,225	94
Abrazar	ORA	U	Minivan	SE				\$ 5,162	\$ 45,000	\$ 45,000	\$ 4,616,225	93
Abrazar	ORA	U	Minivan	SE				\$ 5,162	\$ 45,000	\$ 45,000	\$ 4,661,225	93
Abrazar	ORA	U	Minivan	SE				\$ 5,162	\$ 45,000	\$ 45,000	\$ 4,706,225	93
Abrazar	ORA	U	Minivan	SE				\$ 5,162	\$ 45,000	\$ 45,000	\$ 4,751,225	93
Friends of Adult Day Health Care Centers	SD	U&R	Medium Bus	N				\$ 7,685	\$ 59,315	\$ 67,000	\$ 4,818,225	93
Friends of Children with Special Needs	ALA	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 4,885,225	92
Friends of Children with Special Needs	ALA	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 4,952,225	92
UCP of Greater Sacramento, Inc.	SAC	U	Cameras (16)	OE				\$ 4,575	\$ 35,313	\$ 39,888	\$ 4,992,113	92
HCAR	HUM	R	Small Bus	R	42516	2001	77,029	\$ 6,882	\$ 53,118	\$ 60,000	\$ 5,052,113	91
HCAR	HUM	R	Computer Hardware/Software	OE				\$ 1,096	\$ 8,457	\$ 9,553	\$ 5,061,666	91
Gold Country Telecare, Inc.	NEV	R	Small Bus	R	77452	2001	225,530	\$ 6,882	\$ 53,118	\$ 60,000	\$ 5,121,666	91
Gold Country Telecare, Inc.	NEV	R	Large Bus	R	40387	2006	140,198	\$ 8,373	\$ 64,627	\$ 73,000	\$ 5,194,666	91
Gold Country Telecare, Inc.	NEV	R	Mobile Radios (4)	OE				\$ 459	\$ 3,541	\$ 4,000	\$ 5,198,666	91
Golden Rain Foundation Laguna Woods	ORA	U	Larger Bus	R	05971	1997	449,133	\$ 12,044	\$ 105,000	\$ 105,000	\$ 5,303,666	91
Golden Rain Foundation Laguna Woods	ORA	U	Larger Bus	R	05972	1997	436,872	\$ 12,044	\$ 105,000	\$ 105,000	\$ 5,408,666	91
Golden Rain Foundation Laguna Woods	ORA	U	Larger Bus	R	00173	2000	380,095	\$ 12,044	\$ 105,000	\$ 105,000	\$ 5,513,666	91
Horizon Cross Cultural Center	ORA	U	Modified Raised Top	R	30028	2008	168,009	\$ 5,735	\$ 50,000	\$ 50,000	\$ 5,563,666	91
Peppermint Ridge	RIV	U	Modified Raised Top	R	85640	2002	100,560	\$ 5,735	\$ 44,265	\$ 50,000	\$ 5,613,666	91
Peppermint Ridge	RIV	U	Mobile Radio (1)	OE				\$ 115	\$ 885	\$ 1,000	\$ 5,614,666	91
T.E.R.I., Inc.	SD	U&R	Minivan	R	86145	2004	216,087	\$ 5,162	\$ 39,839	\$ 45,000	\$ 5,659,666	91
T.E.R.I., Inc.	SD	U&R	Medium Bus	R	64661	2005	156,434	\$ 7,685	\$ 59,315	\$ 67,000	\$ 5,726,666	91
UCP/Ride-On	SLO	R	Small Bus	R	26586	2008	209,199	\$ 6,882	\$ 53,118	\$ 60,000	\$ 5,786,666	91

**FTA SECTION 5310 ELDERLY AND DISABLED TRANSIT PROGRAM  
FFY 2011-12 STATEWIDE PRIORITIZED LIST  
(IN STATE SCORE PRIORITY ORDER)**

Agency	Co	Urban or Rural	Project	Type	Vin	Year	Miles	Match (Toll Credits)	FTA 5310 Share	Total Project \$	Cumulative (Federal \$)	State Score
UCP/Ride-On	SLO	R	Minivan	SE				\$ 5,162	\$ 39,838	\$ 45,000	\$ 5,831,666	91
UCP/Ride-On	SLO	R	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 5,898,666	91
UCP/Ride-On	SLO	R	Large Bus	SE				\$ 8,373	\$ 64,627	\$ 73,000	\$ 5,971,666	91
UCP/Ride-On	SLO	R	Large Bus	SE				\$ 8,373	\$ 64,627	\$ 73,000	\$ 6,044,666	91
Work Training Center	BUT	U	Large Bus	R	72413	2006	213,413	\$ 8,373	\$ 64,627	\$ 73,000	\$ 6,117,666	90
Work Training Center	BUT	U	Large Bus	R	18796	2007	204,562	\$ 8,373	\$ 64,627	\$ 73,000	\$ 6,190,666	90
ARC Imperial Valley	IMP	U&R	Large Bus	SE				\$ 5,373	\$ 64,627	\$ 73,000	\$ 6,263,666	90
North of the River	KER	U	Minivan	R	54613	2005	172,234	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,308,666	90
North of the River	KER	U	(1) Recall & Recording	OE				\$ 918	\$ 7,082	\$ 8,000	\$ 6,316,666	90
North of the River	KER	U	Mobile Radios (6)	OE				\$ 688	\$ 5,312	\$ 6,000	\$ 6,322,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	11149	2008	204,707	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,367,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	11188	2008	203,021	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,412,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	0-9813	2008	202,911	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,457,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	11238	2008	201,787	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,502,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	0-9857	2008	201,540	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,547,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	11190	2008	201,319	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,592,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	0-9926	2008	199,594	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,637,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	49469	2008	199,486	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,682,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	0-9427	2008	199,385	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,727,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	10927	2008	199,077	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,772,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	54638	2008	199,063	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,817,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	0-9741	2008	198,911	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,862,666	90
ACCESS SERVICES INCORPORATED	LA	U	Minivan	R	66554	2008	198,396	\$ 5,162	\$ 39,839	\$ 45,000	\$ 6,907,666	90
Brethren Hillcrest Homes	LA	U	Large Bus	R	76001	2002	120,911	\$ 8,373	\$ 64,627	\$ 73,000	\$ 6,980,666	90
Angel View, Inc.	RIV	U&R	Larger Bus	SE				\$ 12,044	\$ 92,957	\$ 105,000	\$ 7,085,666	90
Angel View, Inc.	RIV	U&R	Larger Bus	SE				\$ 12,044	\$ 92,957	\$ 105,000	\$ 7,190,666	90
Mountain Shadows Support Group, MSSG	SD	U&R	Medium Bus	R	20846	2006	96,000	\$ 7,685	\$ 59,315	\$ 67,000	\$ 7,257,666	90
WATCH Resources, Incorporated	TUO	R	Modified Raised Top	R	48679	2004	107,294	\$ 5,735	\$ 44,265	\$ 50,000	\$ 7,307,666	90
WATCH Resources, Incorporated	TUO	R	Modified Raised Top	R	48680	2004	88,325	\$ 5,735	\$ 44,265	\$ 50,000	\$ 7,357,666	90
WATCH Resources, Incorporated	TUO	R	Medium Bus	R	69928	2004	188,310	\$ 7,685	\$ 59,315	\$ 67,000	\$ 7,424,666	90
WATCH Resources, Incorporated	TUO	R	Medium Bus	R	43156	2003	246,420	\$ 7,685	\$ 59,315	\$ 67,000	\$ 7,491,666	90
WATCH Resources, Incorporated	TUO	R	Medium Bus	R	43155	2003	155,205	\$ 7,685	\$ 59,315	\$ 67,000	\$ 7,558,666	90
WATCH Resources, Incorporated	TUO	R	Mobile Radios (6)	OE				\$ 584	\$ 4,508	\$ 5,093	\$ 7,563,758	90
El Dorado County Transit Authority	ED	R	Larger Bus	R	17741	2007	229,881	\$ 12,044	\$ 92,957	\$ 105,000	\$ 7,668,758	89
Redwood Coast Seniors	MEN	R	Small Bus	R	86207	2004	113,070	\$ 6,882	\$ 53,118	\$ 60,000	\$ 7,728,758	89
MST	MON	R	Medium Bus	R	52035	2008	217,025	\$ 7,685	\$ 59,315	\$ 67,000	\$ 7,795,758	89
Age Well	ORA	U	Minivan	R	80205	2000	184,598	\$ 5,162	\$ 45,000	\$ 45,000	\$ 7,840,758	89
Age Well	ORA	U	Large Bus	R	88959	1999	131,162	\$ 8,373	\$ 73,000	\$ 73,000	\$ 7,913,758	89
Age Well	ORA	U	Large Bus	R	10828	2001	117,728	\$ 8,373	\$ 73,000	\$ 73,000	\$ 7,986,758	89
Age Well	ORA	U	Large Bus	R	54342	2002	106,251	\$ 8,373	\$ 73,000	\$ 73,000	\$ 8,059,758	89
Age Well	ORA	U	Large Bus	R	54341	2002	122,278	\$ 8,373	\$ 73,000	\$ 73,000	\$ 8,132,758	89
Age Well	ORA	U	Large Bus	R	54343	2002	120,886	\$ 8,373	\$ 73,000	\$ 73,000	\$ 8,205,758	89
Age Well	ORA	U	Large Bus	R	76562	2002	137,821	\$ 8,373	\$ 73,000	\$ 73,000	\$ 8,278,758	89
Age Well	ORA	U	Large Bus	R	76563	2002	113,921	\$ 8,373	\$ 73,000	\$ 73,000	\$ 8,351,758	89
Age Well	ORA	U	Scheduling Software	OE				\$ 4,588	\$ 40,000	\$ 40,000	\$ 8,391,758	89
PRIDE Industries One, Inc.	PLA	U&R	Medium Bus	R	19364	2005	200,908	\$ 7,685	\$ 59,315	\$ 67,000	\$ 8,458,758	89
PRIDE Industries One, Inc.	PLA	U&R	Cameras (38)	OE				\$ 4,472	\$ 34,517	\$ 38,989	\$ 8,497,747	89
UCP of Greater Sacramento, Inc.	SAC	U	Large Bus	R	10176	2007	204,396	\$ 8,373	\$ 64,627	\$ 73,000	\$ 8,570,747	89
UCP of Greater Sacramento, Inc.	SAC	U	Large Bus	R	50161	2006	219,888	\$ 8,373	\$ 64,627	\$ 73,000	\$ 8,643,747	89
UCP of Greater Sacramento, Inc.	SAC	U	Large Bus	R	50160	2006	216,804	\$ 8,373	\$ 64,627	\$ 73,000	\$ 8,716,747	89
UCP of Greater Sacramento, Inc.	SAC	U	Large Bus	R	50159	2006	212,239	\$ 8,373	\$ 64,627	\$ 73,000	\$ 8,789,747	89
UCP of Greater Sacramento, Inc.	SAC	U	Large Bus	R	56069	2005	221,724	\$ 8,373	\$ 64,627	\$ 73,000	\$ 8,862,747	89
UCP of Greater Sacramento, Inc.	SAC	U	Large Bus	R	51813	2005	202,722	\$ 8,373	\$ 64,627	\$ 73,000	\$ 8,935,747	89
Tulare County Training Center for the Handicapped (dba ABLE Industries)	TUL	U&R	Small Bus	R	1443	2003	156,000	\$ 6,882	\$ 53,118	\$ 60,000	\$ 8,995,747	89
City of Eureka	HUM	R	Mobile Radio (3)	OE				\$ 161	\$ 1,240	\$ 1,401	\$ 8,997,148	88
City of Eureka	HUM	R	Base Station (2)	OE				\$ 363	\$ 2,803	\$ 3,166	\$ 9,000,314	88
Bakersfield ARC	KER	U	Minivan	R	25573	2006	189,351	\$ 5,735	\$ 44,265	\$ 50,000	\$ 9,050,314	88
Bakersfield ARC	KER	U	Minivan	R	32319	2006	205,452	\$ 5,735	\$ 44,265	\$ 50,000	\$ 9,100,314	88
Bakersfield ARC	KER	U	Minivan	R	08874	2003	246,827	\$ 5,735	\$ 44,265	\$ 50,000	\$ 9,150,314	88
Aids Services Foundation Orange County	ORA	U	Minivan	R	43189	2005	181,756	\$ 5,162	\$ 45,000	\$ 45,000	\$ 9,195,314	88

**FTA SECTION 5310 ELDERLY AND DISABLED TRANSIT PROGRAM  
FFY 2011-12 STATEWIDE PRIORITIZED LIST  
(IN STATE SCORE PRIORITY ORDER)**

Agency	Co	Urban or Rural	Project	Type	Vin	Year	Miles	Match (Toll Credits)	FTA 5310 Share	Total Project \$	Cumulative (Federal \$)	State Score
Desert Arc	RIV	U&R	Minivan	R	12129	2000	181,837	\$ 5,162	\$ 39,839	\$ 45,000	\$ 9,240,314	88
Desert Arc	RIV	U&R	Medium Bus (CNG)	R	76578	2002	230,396	\$ 10,438	\$ 80,562	\$ 91,000	\$ 9,331,314	88
Desert Arc	RIV	U&R	Large Bus	R	03151	2000	163,336	\$ 8,373	\$ 64,627	\$ 73,000	\$ 9,404,314	88
Easter Seals Superior CA	SAC	U&R	Large Bus	R	17660	1999	149,042	\$ 8,373	\$ 64,627	\$ 73,000	\$ 9,477,314	88
On Lok Senior Health Services	SF	U	Small Bus	R	31758	2004	9,658	\$ 6,882	\$ 53,118	\$ 60,000	\$ 9,537,314	88
Solano County Transit (SolTrans)	SOL	U	Computer Software	OE				\$ 4,325	\$ 33,383	\$ 37,708	\$ 9,575,022	88
Porterville Sheltered Workshop	TUL	U&R	Computer Hardware (5)	OE	-	-	-	\$ 631	\$ 4,874	\$ 5,505	\$ 9,580,527	88
Porterville Sheltered Workshop	TUL	U&R	Computer Software (Fleet Management)	OE	-	-	-	\$ 1,093	\$ 8,433	\$ 9,526	\$ 9,590,053	88
Porterville Sheltered Workshop	TUL	U&R	Hardware Copier/Fax/Scanner	OE	-	-	-	\$ 691	\$ 5,331	\$ 6,022	\$ 9,596,075	88
Yolo Adult Day Health Center	YOL	U&R	Medium Bus	R	62656	2003	126,440	\$ 7,685	\$ 59,315	\$ 67,000	\$ 9,663,075	88
Yolo Adult Day Health Center	YOL	U&R	Medium Bus	R	62655	2003	108,046	\$ 7,685	\$ 59,315	\$ 67,000	\$ 9,730,075	88
Orange County Adult Achievement Center	ORA	U	Large Bus	SE				\$ 8,373	\$ 73,000	\$ 73,000	\$ 9,803,075	87
San Diego Center for the Blind	SD	U&R	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 9,870,075	87
San Diego Center for the Blind	SD	U&R	Medium Bus, CNG	SE				\$ 10,438	\$ 80,562	\$ 91,000	\$ 9,961,075	87
Alzheimer's Services of the East Bay	ALA	U	Small Bus	R	63071	2002	171,073	\$ 6,882	\$ 53,118	\$ 60,000	\$ 10,021,075	86
Alzheimer's Services of the East Bay	ALA	U	Small Bus	R	25106	2002	211,638	\$ 6,882	\$ 53,118	\$ 60,000	\$ 10,081,075	86
Tarzana Treatment Center	LA	U	Large Bus	R	36498	1999	140,231	\$ 8,373	\$ 64,627	\$ 73,000	\$ 10,154,075	86
SunLine Transit Agency	RIV	U&R	Mobile Radios (2)	OE				\$ 229	\$ 1,771	\$ 2,000	\$ 10,156,075	86
St. Madeleine Sophie's Center	SD	U&R	Minivan	R	04250	2001	171,421	\$ 5,162	\$ 39,839	\$ 45,000	\$ 10,201,075	86
St. Madeleine Sophie's Center	SD	U&R	Minivan	R	55483	2002	247,510	\$ 5,162	\$ 39,839	\$ 45,000	\$ 10,246,075	86
St. Madeleine Sophie's Center	SD	U&R	Minivan	R	99080	2002	142,171	\$ 5,162	\$ 39,839	\$ 45,000	\$ 10,291,075	86
St. Madeleine Sophie's Center	SD	U&R	Minivan	R	99312	2002	124,582	\$ 5,162	\$ 39,839	\$ 45,000	\$ 10,336,075	86
St. Madeleine Sophie's Center	SD	U&R	Minivan	R	98451	2002	112,485	\$ 5,162	\$ 39,839	\$ 45,000	\$ 10,381,075	86
St. Madeleine Sophie's Center	SD	U&R	Small Bus	R	02041	2003	169,291	\$ 6,882	\$ 53,118	\$ 60,000	\$ 10,441,075	86
St. Madeleine Sophie's Center	SD	U&R	Large Bus	R	06557	1997	341,184	\$ 8,373	\$ 64,627	\$ 73,000	\$ 10,514,075	86
St. Madeleine Sophie's Center	SD	U&R	Large Bus	R	11109	1999	249,577	\$ 8,373	\$ 64,627	\$ 73,000	\$ 10,587,075	86
St. Madeleine Sophie's Center	SD	U&R	Mobile Radio (4)	OE				\$ 459	\$ 3,541	\$ 4,000	\$ 10,591,075	86
UCP/Ride-On	SLO	R	Large Bus	R	49309	2007	192,902	\$ 8,373	\$ 64,627	\$ 73,000	\$ 10,664,075	86
ARC Imperial Valley	IMP	U&R	Large Bus	R	36889	2007	209,231	\$ 5,373	\$ 64,627	\$ 73,000	\$ 10,737,075	85
ARC Imperial Valley	IMP	U&R	Larger Bus	R	14687	2005	251,877	\$ 12,043	\$ 92,957	\$ 105,000	\$ 10,842,075	85
North of the River	KER	U	Modified Raised Top	R	49536	2007	162,374	\$ 5,735	\$ 44,265	\$ 50,000	\$ 10,892,075	85
North of the River	KER	U	Modified Raised Top	R	24849	2007	158,845	\$ 5,735	\$ 44,265	\$ 50,000	\$ 10,942,075	85
City of Glendale	LA	U	Larger Bus	SE				\$ 12,044	\$ 92,957	\$ 105,000	\$ 11,047,075	85
Desert Arc	RIV	U&R	Large Bus	SE				\$ 8,373	\$ 64,627	\$ 73,000	\$ 11,120,075	85
Self Help for the Elderly	SF	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 11,165,075	85
Self Help for the Elderly	SF	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 11,210,075	85
Self Help for the Elderly	SF	U	Modified Raised Top	SE				\$ 5,735	\$ 44,265	\$ 50,000	\$ 11,260,075	85
Self Help for the Elderly	SF	U	Modified Raised Top	SE				\$ 5,735	\$ 44,265	\$ 50,000	\$ 11,310,075	85
Easy Lift Transportation	SB	U&R	Software	OE				\$ 4,588	\$ 35,412	\$ 40,000	\$ 11,350,075	85
Center for Elders' Independence	ALA	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 11,417,075	84
Center for Elders' Independence	ALA	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 11,484,075	84
Center for Elders' Independence	ALA	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 11,551,075	84
Center for Elders' Independence	ALA	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 11,618,075	84
Center for Elders' Independence	ALA	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 11,685,075	84
Center for Elders' Independence	ALA	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 11,752,075	84
Center for Elders' Independence	ALA	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 11,819,075	84
Center for Elders' Independence	ALA	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 11,886,075	84
Center for Elders' Independence	ALA	U	Mobile Radios (8)	OE				\$ 459	\$ 3,541	\$ 4,000	\$ 11,890,075	84
El Dorado County Transit Authority	ED	R	Larger Bus	R	19466	2007	202,029	\$ 12,044	\$ 92,956	\$ 105,000	\$ 11,995,075	84
El Dorado County Transit Authority	ED	R	Larger Bus	R	19771	2007	202,373	\$ 12,044	\$ 92,956	\$ 105,000	\$ 12,100,075	84
ARC Imperial Valley	IMP	U&R	Large Bus	SE				\$ 5,373	\$ 64,627	\$ 73,000	\$ 12,173,075	84
ARC Imperial Valley	IMP	U&R	Large Bus	SE				\$ 5,373	\$ 64,627	\$ 73,000	\$ 12,246,075	84
AltaMed Health Services Corporation	LA	U	Medium Bus	R	45843	1999	187,668	\$ 7,685	\$ 59,315	\$ 67,000	\$ 12,313,075	84
AltaMed Health Services Corporation	LA	U	Medium Bus	R	45844	1999	210,663	\$ 7,685	\$ 59,315	\$ 67,000	\$ 12,380,075	84
AltaMed Health Services Corporation	LA	U	Medium Bus	R	26025	2013	203,701	\$ 7,685	\$ 59,315	\$ 67,000	\$ 12,447,075	84
AltaMed Health Services Corporation	LA	U	Medium Bus	R	26026	2013	206,687	\$ 7,685	\$ 59,315	\$ 67,000	\$ 12,514,075	84
AltaMed Health Services Corporation	LA	U	Medium Bus	R	26027	2013	195,554	\$ 7,685	\$ 59,315	\$ 67,000	\$ 12,581,075	84
AltaMed Health Services Corporation	LA	U	Medium Bus	R	26028	2013	192,830	\$ 7,685	\$ 59,315	\$ 67,000	\$ 12,648,075	84
Tarzana Treatment Center	LA	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 12,693,075	84
Orange County Adult Achievement Center	ORA	U	Large Bus	R	43485	2006	157,472	\$ 8,373	\$ 73,000	\$ 73,000	\$ 12,766,075	84

**FTA SECTION 5310 ELDERLY AND DISABLED TRANSIT PROGRAM  
FFY 2011-12 STATEWIDE PRIORITIZED LIST  
(IN STATE SCORE PRIORITY ORDER)**

Agency	Co	Urban or Rural	Project	Type	Vin	Year	Miles	Match (Toll Credits)	FTA 5310 Share	Total Project \$	Cumulative (Federal \$)	State Score
Orange County Adult Achievement Center	ORA	U	Large Bus	R	43486	2006	131,019	\$ 8,373	\$ 73,000	\$ 73,000	\$ 12,839,075	84
Orange County Adult Achievement Center	ORA	U	Large Bus	R	43488	2006	146,811	\$ 8,373	\$ 73,000	\$ 73,000	\$ 12,912,075	84
Orange County Adult Achievement Center	ORA	U	Large Bus	R	43489	2006	169,394	\$ 8,373	\$ 73,000	\$ 73,000	\$ 12,985,075	84
Orange County Adult Achievement Center	ORA	U	Large Bus	R	43483	2006	173,155	\$ 8,373	\$ 73,000	\$ 73,000	\$ 13,058,075	84
Orange County Adult Achievement Center	ORA	U	Large Bus	R	01271	2006	179,826	\$ 8,373	\$ 73,000	\$ 73,000	\$ 13,131,075	84
SunLine Transit Agency	RIV	U&R	Medium Bus CNG	SE				\$ 10,438	\$ 80,562	\$ 91,000	\$ 13,222,075	84
SunLine Transit Agency	RIV	U&R	Medium Bus CNG	SE				\$ 10,438	\$ 80,562	\$ 91,000	\$ 13,313,075	84
Paratransit, Inc..	SAC	U	Rangers	OE				\$ 1,377	\$ 10,623	\$ 12,000	\$ 13,325,075	84
Laguna Honda Hospital and Rehabilitation Center	SF	U	Medium Bus	R	20106	1994	153,190	\$ 7,685	\$ 59,315	\$ 67,000	\$ 13,392,075	84
Laguna Honda Hospital and Rehabilitation Center	SF	U	Medium Bus	R	17650	1999	153,683	\$ 7,685	\$ 59,315	\$ 67,000	\$ 13,459,075	84
Laguna Honda Hospital and Rehabilitation Center	SF	U	Larger Bus	R	92466	2000	38,146	\$ 12,044	\$ 92,957	\$ 105,000	\$ 13,564,075	84
Self Help for the Elderly	SF	U	Computer Hardware (10)	OE				\$ 2,390	\$ 18,450	\$ 20,840	\$ 13,584,915	84
Self Help for the Elderly	SF	U	Computer Software (10)	OE				\$ 486	\$ 3,754	\$ 4,240	\$ 13,589,155	84
Self Help for the Elderly	SF	U	Base Station	OE				\$ 287	\$ 2,213	\$ 2,500	\$ 13,591,655	84
Self Help for the Elderly	SF	U	Mobile Radio (11)	OE				\$ 1,262	\$ 9,738	\$ 11,000	\$ 13,602,655	84
											<b>100% Line</b>	
Lamorinda Spirit Van, City of Lafayette	CC	U	Medium Bus	R	23929	2002	240,353	\$ 7,685	\$ 59,315	\$ 67,000	\$ 13,669,655	83
City of Eureka	HUM	R	Small Bus	R	93809	2005	156,049	\$ 6,882	\$ 53,118	\$ 60,000	\$ 13,729,655	83
City of Eureka	HUM	R	Small Bus	R	93810	2005	167,212	\$ 6,882	\$ 53,118	\$ 60,000	\$ 13,789,655	83
Project Independence	ORA	U	Minivan	R	59545	2004	81,223	\$ 5,162	\$ 45,000	\$ 45,000	\$ 13,834,655	83
Project Independence	ORA	U	Minivan	R	58891	2004	123,314	\$ 5,162	\$ 45,000	\$ 45,000	\$ 13,879,655	83
Project Independence	ORA	U	Minivan	R	58211	2004	130,260	\$ 5,162	\$ 45,000	\$ 45,000	\$ 13,924,655	83
Project Independence	ORA	U	Minivan	R	56203	2004	121,482	\$ 5,162	\$ 45,000	\$ 45,000	\$ 13,969,655	83
Care-A-Van Transit	RIV	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 14,014,655	83
Care-A-Van Transit	RIV	U&R	Modified Raised Top	SE				\$ 5,735	\$ 44,265	\$ 50,000	\$ 14,064,655	83
Desert Arc	RIV	U&R	Large Bus CNG	SE				\$ 11,126	\$ 85,874	\$ 97,000	\$ 14,161,655	83
Friends of Moreno Valley Senior Center Inc.	RIV	U	Medium Bus	R	85802	2006	203,163	\$ 7,685	\$ 59,315	\$ 67,000	\$ 14,228,655	83
Paratransit, Inc..	SAC	U	Large Bus CNG	SE				\$ 11,126	\$ 85,874	\$ 97,000	\$ 14,325,655	83
Paratransit, Inc..	SAC	U	Large Bus CNG	SE				\$ 11,126	\$ 85,874	\$ 97,000	\$ 14,422,655	83
Paratransit, Inc..	SAC	U	Large Bus CNG	SE				\$ 11,126	\$ 85,874	\$ 97,000	\$ 14,519,655	83
Paratransit, Inc..	SAC	U	Large Bus CNG	SE				\$ 11,126	\$ 85,874	\$ 97,000	\$ 14,616,655	83
Paratransit, Inc..	SAC	U	Large Bus CNG	SE				\$ 11,126	\$ 85,874	\$ 97,000	\$ 14,713,655	83
Paratransit, Inc..	SAC	U	Large Bus CNG	SE				\$ 11,126	\$ 85,874	\$ 97,000	\$ 14,810,655	83
Friends of Adult Day Health Care Centers	SD	U&R	Minivan	N				\$ 5,162	\$ 39,839	\$ 45,000	\$ 14,855,655	83
Institute on Aging	SF	U	Wheelchair Tie Down Kit (5)	OE				\$ 3,206	\$ 24,744	\$ 27,950	\$ 14,883,605	83
Milestones Adult Development Center	SOL	U	Minivan	R	15723	1998	191,436	\$ 5,162	\$ 39,839	\$ 45,000	\$ 14,928,605	83
Milestones Adult Development Center	SOL	U	Small Bus	R	40576	2004	114,755	\$ 6,882	\$ 53,118	\$ 60,000	\$ 14,988,605	83
Milestones Adult Development Center	SOL	U	Wheelchair Tie Down Kit	OE				\$ 963	\$ 7,437	\$ 8,400	\$ 14,997,005	83
United Cerebral Palsy of Stanislaus County	STA	U	Modified Raised Top	SE				\$ 5,735	\$ 44,265	\$ 50,000	\$ 15,047,005	83
											<b>110% Line</b>	
City of Santa Ana	ORA	U	Medium Bus CNG	R	15636	2006	178,426	\$ 10,438	\$ 91,000	\$ 91,000	\$ 15,138,005	82
City of Santa Ana	ORA	U	Medium Bus CNG	R	28458	2006	156,239	\$ 10,438	\$ 91,000	\$ 91,000	\$ 15,229,005	82
City of Elk Grove/e-van	SAC	U	Minivan	R	31909	2006	239,601	\$ 5,162	\$ 39,838	\$ 45,000	\$ 15,274,005	82
City of Elk Grove/e-van	SAC	U	Minivan	R	51209	2006	261,604	\$ 5,162	\$ 39,838	\$ 45,000	\$ 15,319,005	82
City of Elk Grove/e-van	SAC	U	Minivan	R	31385	2006	279,516	\$ 5,162	\$ 39,838	\$ 45,000	\$ 15,364,005	82
Community Bridges	SCR	U&R	Minivan	R	27595	2003	128,783	\$ 5,162	\$ 39,839	\$ 45,000	\$ 15,409,005	82
Community Bridges	SCR	U&R	Minivan	R	71941	2000	146,915	\$ 5,162	\$ 39,839	\$ 45,000	\$ 15,454,005	82
Community Bridges	SCR	U&R	Mobile Data Computers (14)	OE				\$ 4,586	\$ 35,394	\$ 39,980	\$ 15,493,985	82
The Arc of Ventura County	VEN	U&R	Minivan	R	10303	2000	98,645	\$ 5,162	\$ 39,839	\$ 45,000	\$ 15,538,985	82
The Arc of Ventura County	VEN	U&R	Minivan	R	27065	2002	98,645	\$ 5,162	\$ 39,839	\$ 45,000	\$ 15,583,985	82
Bay Area Outreach and Recreation Program, Inc.	ALA	U	Large Bus	SE				\$ 8,373	\$ 64,627	\$ 73,000	\$ 15,656,985	81
City of Fortuna	HUM	R	Medium Bus	R	90417	2003	117,738	\$ 7,685	\$ 59,315	\$ 67,000	\$ 15,723,985	81
Desert Area Resource Training	KER	R	Mobile Radios (6)	OE				\$ 688	\$ 5,312	\$ 6,000	\$ 15,729,985	81

**FTA SECTION 5310 ELDERLY AND DISABLED TRANSIT PROGRAM  
FFY 2011-12 STATEWIDE PRIORITIZED LIST  
(IN STATE SCORE PRIORITY ORDER)**

Agency	Co	Urban or Rural	Project	Type	Vin	Year	Miles	Match (Toll Credits)	FTA 5310 Share	Total Project \$	Cumulative (Federal \$)	State Score
Desert Area Resource Training	KER	R	Computer Software	OE				\$ 918	\$ 7,082	\$ 8,000	\$ 15,737,985	81
Kings Rehabilitation Center Inc.	KIN	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 15,782,985	81
Kings Rehabilitation Center Inc.	KIN	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 15,827,985	81
Brethren Hillcrest Homes	LA	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 15,872,985	81
Institute for the Redesign of Learning	LA	U	Small Bus	R	34209	2006	124,825	\$ 6,882	\$ 53,118	\$ 60,000	\$ 15,932,985	81
Gold Country Telecare, Inc.	NEV	R	Larger Bus	R	18791	2007	189,107	\$ 12,043	\$ 92,957	\$ 105,000	\$ 16,037,985	81
Gold Country Telecare, Inc.	NEV	R	Large Bus	R	40388	2006	153,997	\$ 8,373	\$ 64,627	\$ 73,000	\$ 16,110,985	81
St. Madeleine Sophie's Center	SD	U&R	Small Bus	R	07261	2005	123,073	\$ 6,882	\$ 53,118	\$ 60,000	\$ 16,170,985	81
Milestones Adult Development Center	SOL	U	Computer Hardware	OE				\$ 229	\$ 1,770	\$ 1,999	\$ 16,172,984	81
Catholic Charities of the Diocese of Stockton	STA	U&R	Minivan	R	19360	2004	128,516	\$ 5,162	\$ 39,839	\$ 45,000	\$ 16,217,984	81
ARC Imperial Valley	IMP	U&R	Larger Bus	R	13345	2005	227,216	\$ 12,043	\$ 92,957	\$ 105,000	\$ 16,322,984	80
North of the River	KER	U	Modified Raised Top	R	O2909	2005	132,765	\$ 5,735	\$ 44,265	\$ 50,000	\$ 16,372,984	80
North of the River	KER	U	Small Bus	R	30121	2003	135,520	\$ 6,882	\$ 53,118	\$ 60,000	\$ 16,432,984	80
Kings Rehabilitation Center Inc.	KIN	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 16,477,984	80
City of Vista	SD	U&R	Large Bus	R	13592	2005	103,747	\$ 8,373	\$ 64,627	\$ 73,000	\$ 16,550,984	80
St. Madeleine Sophie's Center	SD	U&R	Small Bus	SE				\$ 6,882	\$ 53,118	\$ 60,000	\$ 16,610,984	80
WATCH Resources, Incorporated	TUO	R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 16,655,984	80
Ukiah Senior Center, Inc.	MEN	R	Medium Bus	R	85788	2006	92,622	\$ 7,685	\$ 59,315	\$ 67,000	\$ 16,722,984	79
Town of Truckee	NEV	R	Medium Bus	R	06133	2007	148,685	\$ 7,685	\$ 59,315	\$ 67,000	\$ 16,789,984	79
Town of Truckee	NEV	R	Medium Bus	R	06992	2007	157,266	\$ 7,685	\$ 59,315	\$ 67,000	\$ 16,856,984	79
PRIDE Industries One, Inc.	PLA	U&R	Larger Bus	R	13687	2007	207,013	\$ 12,044	\$ 92,957	\$ 105,000	\$ 16,961,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,006,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,051,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,096,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,141,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,186,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,231,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,276,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,321,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,366,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,411,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,456,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,501,984	79
Full Access & Coordinated Transportation, Inc.	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,546,984	79
St. Madeleine Sophie's Center	SD	U&R	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 17,591,984	79
HOPE Rehabilitation Services	SCL	U	Small Bus	R	11941	1999	58,827	\$ 6,882	\$ 53,118	\$ 60,000	\$ 17,651,984	79
HOPE Rehabilitation Services	SCL	U	Small Bus	R	29530	1999	81,070	\$ 6,882	\$ 53,118	\$ 60,000	\$ 17,711,984	79
HOPE Rehabilitation Services	SCL	U	Small Bus	R	10329	2002	96,946	\$ 6,882	\$ 53,118	\$ 60,000	\$ 17,771,984	79
HOPE Rehabilitation Services	SCL	U	Small Bus	R	93904	1999	117,361	\$ 6,882	\$ 53,118	\$ 60,000	\$ 17,831,984	79
HOPE Rehabilitation Services	SCL	U	Small Bus	R	42431	2004	115,507	\$ 6,882	\$ 53,118	\$ 60,000	\$ 17,891,984	79
HOPE Rehabilitation Services	SCL	U	Small Bus	R	10458	1992	83,891	\$ 6,882	\$ 53,118	\$ 60,000	\$ 17,951,984	79
HOPE Rehabilitation Services	SCL	U	Small Bus	R	29207	1996	107,155	\$ 6,882	\$ 53,118	\$ 60,000	\$ 18,011,984	79
HOPE Rehabilitation Services	SCL	U	Small Bus	R	92284	1999	132,699	\$ 6,882	\$ 53,118	\$ 60,000	\$ 18,071,984	79
HOPE Rehabilitation Services	SCL	U	Small Bus	R	19799	2001	117,780	\$ 6,882	\$ 53,118	\$ 60,000	\$ 18,131,984	79
HELP of Ojai	VEN	U&R	Small Bus	R	32724	2003	93,588	\$ 6,882	\$ 53,118	\$ 60,000	\$ 18,191,984	79
Napa County Transportation and Planning Agency	NAP	U	Medium Bus	R	13821	1999	122,861	\$ 7,685	\$ 59,315	\$ 67,000	\$ 18,258,984	78
Napa County Transportation and Planning Agency	NAP	U	Medium Bus	R	12461	1999	168,870	\$ 7,685	\$ 59,315	\$ 67,000	\$ 18,325,984	78
Napa County Transportation and Planning Agency	NAP	U	Medium Bus	R	13818	1999	95,758	\$ 7,685	\$ 59,315	\$ 67,000	\$ 18,392,984	78

**FTA SECTION 5310 ELDERLY AND DISABLED TRANSIT PROGRAM  
FFY 2011-12 STATEWIDE PRIORITIZED LIST  
(IN STATE SCORE PRIORITY ORDER)**

Agency	Co	Urban or Rural	Project	Type	Vin	Year	Miles	Match (Toll Credits)	FTA 5310 Share	Total Project \$	Cumulative (Federal \$)	State Score
Easter Seals Superior CA	SAC	U&R	Large Bus	SE				\$ 8,373	\$ 64,627	\$ 73,000	\$ 18,465,984	78
Institute on Aging	SF	U	Large Bus	R	65972	2005	81,157	\$ 8,373	\$ 64,627	\$ 73,000	\$ 18,538,984	78
Institute on Aging	SF	U	Large Bus	R	65970	2005	71,522	\$ 8,373	\$ 64,627	\$ 73,000	\$ 18,611,984	78
Institute on Aging	SF	U	Large Bus	R	63998	2005	74,902	\$ 8,373	\$ 64,627	\$ 73,000	\$ 18,684,984	78
Institute on Aging	SF	U	Large Bus	R	65971	2005	82,836	\$ 8,373	\$ 64,627	\$ 73,000	\$ 18,757,984	78
Institute on Aging	SF	U	Large Bus	R	63997	2005	70,780	\$ 8,373	\$ 64,627	\$ 73,000	\$ 18,830,984	78
Institute on Aging	SF	U	Video Surveillance System (12)	OE				\$ 1,170	\$ 9,030	\$ 10,200	\$ 18,841,184	78
North and South of Market Adult Day Health Corp. (SteppingStone)	SF	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 18,908,184	78
North and South of Market Adult Day Health Corp. (SteppingStone)	SF	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 18,975,184	78
On Lok Senior Health Services	SF	U	Small Bus	R	23264	2006	85,860	\$ 6,882	\$ 53,118	\$ 60,000	\$ 19,035,184	78
On Lok Senior Health Services	SF	U	Small Bus	SE				\$ 6,882	\$ 53,118	\$ 60,000	\$ 19,095,184	78
On Lok Senior Health Services	SF	U	Small Bus	SE				\$ 6,882	\$ 53,118	\$ 60,000	\$ 19,155,184	78
On Lok Senior Health Services	SF	U	Small Bus	SE				\$ 6,882	\$ 53,118	\$ 60,000	\$ 19,215,184	78
On Lok Senior Health Services	SF	U	Small Bus	SE				\$ 6,882	\$ 53,118	\$ 60,000	\$ 19,275,184	78
On Lok Senior Health Services	SF	U	Small Bus	SE				\$ 6,882	\$ 53,118	\$ 60,000	\$ 19,335,184	78
On Lok Senior Health Services	SF	U	Small Bus	SE				\$ 6,882	\$ 53,118	\$ 60,000	\$ 19,395,184	78
NCI Affiliates, Inc.	SLO	R	Minivan	R	69550	2004	76,311	\$ 5,162	\$ 39,838	\$ 45,000	\$ 19,440,184	78
NCI Affiliates, Inc.	SLO	R	Minivan	R	68655	2004	84,767	\$ 5,162	\$ 39,838	\$ 45,000	\$ 19,485,184	78
Milestones Adult Development Center	SOL	U	Small Bus	R	06317	2005	181,790	\$ 6,882	\$ 53,118	\$ 60,000	\$ 19,545,184	78
Milestones Adult Development Center	SOL	U	Large Bus	R	06318	2007	152,549	\$ 8,373	\$ 64,627	\$ 73,000	\$ 19,618,184	78
Milestones Adult Development Center	SOL	U	Larger Bus	R	73528	2007	258,216	\$ 12,044	\$ 92,957	\$ 105,000	\$ 19,723,184	78
OPARC	SBD	U	Small Bus	SE				\$ 6,882	\$ 53,118	\$ 60,000	\$ 19,783,184	77
OPARC	SBD	U	Small Bus	SE				\$ 6,882	\$ 53,118	\$ 60,000	\$ 19,843,184	77
OPARC	SBD	U	Small Bus	SE				\$ 6,882	\$ 53,118	\$ 60,000	\$ 19,903,184	77
San Diego Center for the Blind	SD	U&R	Medium Bus	R	61207	2007	39,827	\$ 7,685	\$ 59,315	\$ 67,000	\$ 19,970,184	77
San Diego Center for the Blind	SD	U&R	Medium Bus	R	61211	2007	32,948	\$ 7,685	\$ 59,315	\$ 67,000	\$ 20,037,184	77
City of Glendale	LA	U	Minivan	R	61996	2008	129,731	\$ 5,162	\$ 39,839	\$ 45,000	\$ 20,082,184	76
Institute for the Redesign of Learning	LA	U	Small Bus	R	36073	2006	129,721	\$ 6,882	\$ 53,118	\$ 60,000	\$ 20,142,184	76
Institute for the Redesign of Learning	LA	U	Small Bus	R	59607	2006	119,721	\$ 6,882	\$ 53,118	\$ 60,000	\$ 20,202,184	76
ARC Imperial Valley	IMP	U&R	Larger Bus	R	13171	2005	217,901	\$ 12,043	\$ 92,957	\$ 105,000	\$ 20,307,184	75
North of the River	KER	U	Small Bus	R	18738	2008	137,485	\$ 6,882	\$ 53,118	\$ 60,000	\$ 20,367,184	75
Speech & Language Development Center	ORA	U	Minivan	SE				\$ 5,162	\$ 45,000	\$ 45,000	\$ 20,412,184	75
San Benito County LTA	SBD	R	Mobile Radios (3)	OE				\$ 344	\$ 2,656	\$ 3,000	\$ 20,415,184	75
United Christian Centers of Greater Sacramento	VAR	U	Small Bus	SE				\$ 6,882	\$ 53,118	\$ 60,000	\$ 20,475,184	75
Tarzana Treatment Center	LA	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 20,520,184	74
Speech & Language Development Center	ORA	U	Modified Raised Top	SE				\$ 5,735	\$ 50,000	\$ 50,000	\$ 20,570,184	74
Institute on Aging	SF	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 20,615,184	72
Institute on Aging	SF	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 20,682,184	72
Manteca CAPS	SJ	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 20,749,184	72
Manteca CAPS	SJ	U	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 20,816,184	72
Desert Area Resource Training	KER	R	Minivan	R	14666	2007	116,952	\$ 5,162	\$ 39,839	\$ 45,000	\$ 20,861,184	71
Desert Area Resource Training	KER	R	Minivan	R	28032	2007	143,291	\$ 5,162	\$ 39,839	\$ 45,000	\$ 20,906,184	71
Desert Area Resource Training	KER	R	Minivan	R	21466	2007	109,978	\$ 5,162	\$ 39,839	\$ 45,000	\$ 20,951,184	71
Desert Area Resource Training	KER	R	Minivan	R	40334	2007	138,789	\$ 5,162	\$ 39,839	\$ 45,000	\$ 20,996,184	71
Desert Area Resource Training	KER	R	Modified Raised Top	R	40872	2007	125,890	\$ 5,735	\$ 44,265	\$ 50,000	\$ 21,046,184	71
Desert Area Resource Training	KER	R	Modified Raised Top	R	40629	2007	134,526	\$ 5,735	\$ 44,265	\$ 50,000	\$ 21,096,184	71
City of Glendale	LA	U	Minivan	R	62031	2008	124,678	\$ 5,162	\$ 39,839	\$ 45,000	\$ 21,141,184	71
City of Glendale	LA	U	Minivan	R	62479	2008	120,173	\$ 5,162	\$ 39,839	\$ 45,000	\$ 21,186,184	71
City of Glendale	LA	U	Minivan	R	60656	2008	104,530	\$ 5,162	\$ 39,839	\$ 45,000	\$ 21,231,184	71
Edgewood Center for Children and Families	SF	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 21,276,184	71
Edgewood Center for Children and Families	SF	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 21,321,184	71
Edgewood Center for Children and Families	SF	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 21,366,184	71
Edgewood Center for Children and Families	SF	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 21,411,184	71
Institute on Aging	SF	U	Modified Raised Top	SE				\$ 5,735	\$ 44,265	\$ 50,000	\$ 21,461,184	71
ARC San Joaquin	SJ	U	Modified Raised Top	R	67,482	2006	142,181	\$ 5,735	\$ 44,265	\$ 50,000	\$ 21,511,184	71

**FTA SECTION 5310 ELDERLY AND DISABLED TRANSIT PROGRAM  
FFY 2011-12 STATEWIDE PRIORITIZED LIST  
(IN STATE SCORE PRIORITY ORDER)**

Agency	Co	Urban or Rural	Project	Type	Vin	Year	Miles	Match (Toll Credits)	FTA 5310 Share	Total Project \$	Cumulative (Federal \$)	State Score
Easter Seals Southern California	ORA	U	Modified Raised Top	SE				\$ 5,735	\$ 44,265	\$ 50,000	\$ 21,561,184	69
Easter Seals Southern California	ORA	U	Modified Raised Top	SE				\$ 5,735	\$ 44,265	\$ 50,000	\$ 21,611,184	69
Laguna Honda Hospital and Rehabilitation Center	SF	U	Minivan	SE				\$ 5,162	\$ 39,839	\$ 45,000	\$ 21,656,184	69
HOPE Rehabilitation Services	SCL	U	Small Bus	R	03316	2006	129,212	\$ 6,882	\$ 53,118	\$ 60,000	\$ 21,716,184	69
Milestones Adult Development Center	SOL	U	Large Bus	R	73534	2007	116,906	\$ 8,373	\$ 64,627	\$ 73,000	\$ 21,789,184	68
Milestones Adult Development Center	SOL	U	Large Bus	R	73531	2007	101,117	\$ 8,373	\$ 64,627	\$ 73,000	\$ 21,862,184	68
San Benito County LTA	SBD	R	Minivan	R	38515	2008	114,841	\$ 5,162	\$ 39,839	\$ 45,000	\$ 21,907,184	67
San Benito County LTA	SBD	R	Medium Bus	R	1432	2008	155,713	\$ 7,685	\$ 59,315	\$ 67,000	\$ 21,974,184	67
San Benito County LTA	SBD	R	Medium Bus	R	52880	2007	158,095	\$ 7,685	\$ 59,315	\$ 67,000	\$ 22,041,184	67
Catholic Charities of the Diocese of Stockton	STA	U&R	MDT (5)	OE				\$ 356	\$ 2,744	\$ 3,100	\$ 22,044,284	66
Catholic Charities of the Diocese of Stockton	STA	U&R	Computer Software	OE				\$ 4,232	\$ 32,668	\$ 36,900	\$ 22,081,184	66
Golden Age Center	TRI	R	Small bus	R	11344	1995	270,000	\$ 6,882	\$ 53,118	\$ 60,000	\$ 22,141,184	66
Napa County Transportation and Planning Agency	NAP	U	Mobile Radio (3)	OE				\$ 344	\$ 2,656	\$ 3,000	\$ 22,144,184	64
SHIELDS for Families	LA	U	Larger Bus	SE				\$ 12,044	\$ 92,957	\$ 105,000	\$ 22,249,184	63
Milestones Adult Development Center	SOL	U	Large Bus	R	09506	2008	122,348	\$ 8,373	\$ 64,627	\$ 73,000	\$ 22,322,184	63
City of Chowchilla	MAD	U&R	Computers (2)	OE				\$ 317	\$ 2,446	\$ 2,763	\$ 22,324,947	60
City of Chowchilla	MAD	U&R	Hardware Printer/Fax/Scanner	OE				\$ 23	\$ 176	\$ 199	\$ 22,325,146	60
City of Chowchilla	MAD	U&R	Mobile Radios (3)	OE				\$ 122	\$ 942	\$ 1,064	\$ 22,326,211	60
City of Chowchilla	MAD	U&R	Medium Bus	SE				\$ 7,685	\$ 59,315	\$ 67,000	\$ 22,393,211	58

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.2c.(2)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Katrina Pierce  
Division Chief  
Environmental Analysis

Subject: **APPROVAL OF PROJECT FOR FUTURE CONSIDERATION OF FUNDING**  
**07-LA-5; PM R45.4/R59.0**  
**RESOLUTION E-13-64**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission), as a responsible agency, approve the attached Resolutions E-13-64.

## **ISSUE:**

The attached resolution proposes to approve for future consideration of funding the following project for which a Supplemental Final Environmental Impact Report (SFEIR) has been completed:

- Interstate 5 (I-5) in Los Angeles County. Addition of a High Occupancy Toll Lane to a portion of I-5 in and near the city of Valencia.

This project in Los Angeles County will construct High Occupancy Toll lanes on Interstate 5 from State Route 14 to Parker Road. The project is fully funded with local funds. The total estimated cost is \$365,000,000 for capital and support. Construction is estimated to begin in Fiscal Year 2014-15.

A copy of the SFEIR has been provided to Commission staff. The decision to prepare a Supplemental Environmental Impact Report was made due to changes to the original project that may have resulted in additional environmental impacts.

The analysis resulted in no additional impacts associated with the project changes. As a result, a SFEIR was prepared for the project.

Attachments

## **CALIFORNIA TRANSPORTATION COMMISSION**

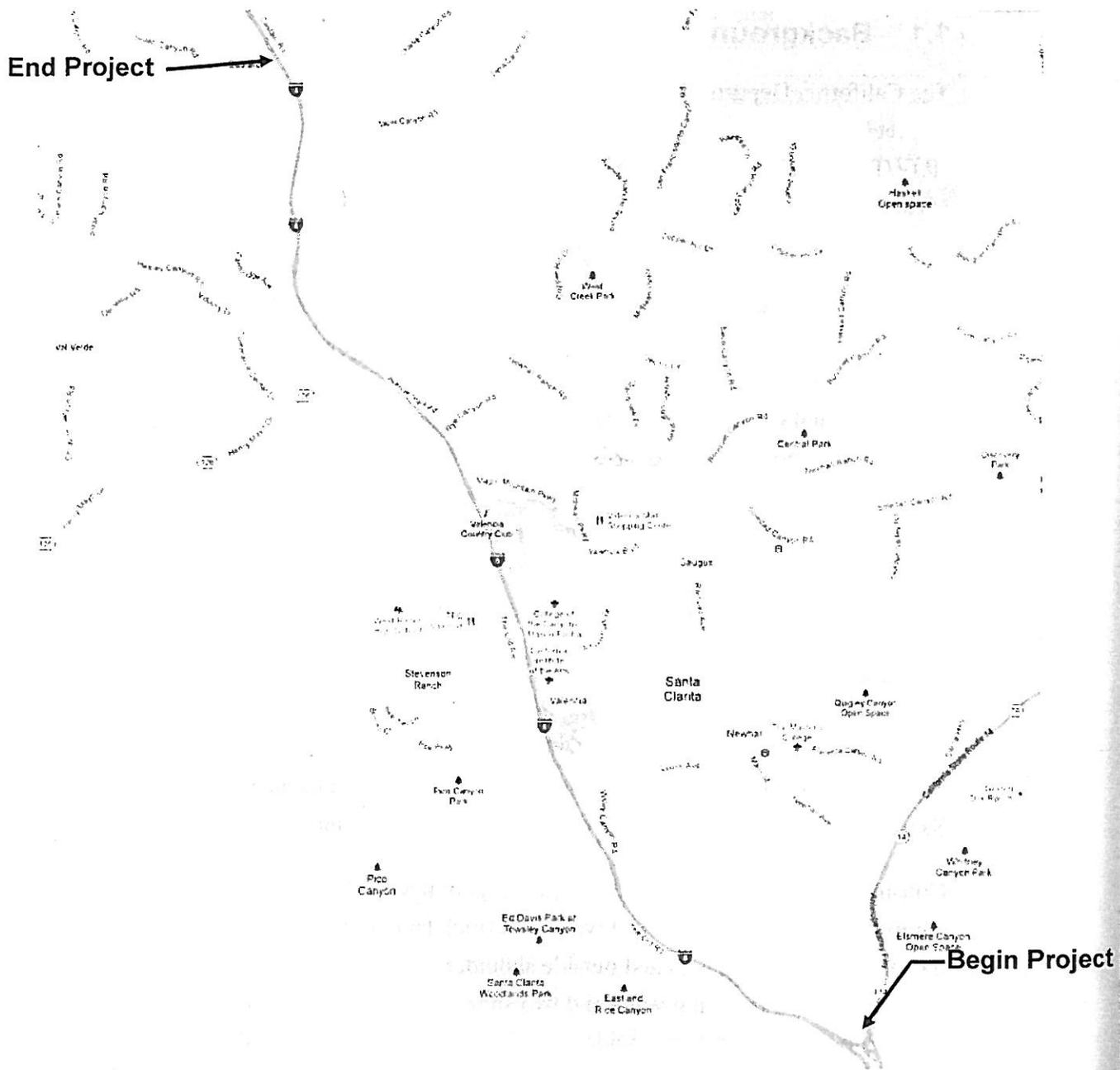
### **Resolution for Future Consideration of Funding**

**07-LA-5, PM R45.4/R59.0**

**Resolution E-13-64**

- 1.1** **WHEREAS**, the California Department of Transportation (Department) has completed an Environmental Impact Report pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- Interstate 5 (I-5) in Los Angeles County. Addition of a High Occupancy Toll Lane to a portion of I-5 in and near the city of Valencia.
- 1.2** **WHEREAS**, the Department has certified that the Environmental Impact Report has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3** **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Supplemental Final Environmental Impact Report.
- 1.4** **WHEREAS**, the project will have a significant effect on the environment.
- 1.5** **WHEREAS**, a Statement of Overriding Considerations was prepared; and
- 1.6** **WHEREAS**, Findings were made pursuant to the State CEQA Guidelines; and
- 2.1** **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby support approval of the above referenced project to allow for consideration of funding.

Figure 1.1 Project Location



**CALIFORNIA DEPARTMENT OF TRANSPORTATION**  
**CEQA FINDINGS**  
**FOR THE**  
**IINTERSTATES5 HOV/TRUCK LANES PROJECT**  
**STATE ROUTE 14 TO PARKER ROAD**

The following information is presented to comply with California Environmental Quality Act Guidelines (Title 14 California Code of Regulations, Chapter 3, Section 15091) and the Department of Transportation Environmental Regulations (Title 21, California Code of Regulations, Chapter 11, Section 1501). Reference is made to the Final Environmental Impact Report (EIR) for the project, which is the basic source for the information.

The following adverse effects have been identified in the Final EIR as resulting from the project. Only effects found to be significant have been included.

**AESTHETICS**

**Adverse Environmental Effect:**

The project would result in the removal of trees and vegetation that constitute a scenic view and would require construction of retaining walls and noise barriers. The loss of these visual resources and/or construction of walls/barriers would adversely affect the visual character and quality at some locations in the project area. **Findings:**

Changes or alterations have been required in, or incorporated into, the project, which avoid or substantially lessen the significant environmental effect as identified in the Final EIR.

**Statement of Facts:**

As mitigation, the Final EIR recommended avoidance of mature trees and/or replacement of mature trees at a 1:1 ratio, preparation of a landscape plan, application of wall treatments at Key View 4, and consideration of the use of transparent materials in noise barriers adjacent to residential uses with views of protected open space or of the adjacent Santa Monica or Santa Susan Mountains to reduce visual impacts (Section 3.2.2.1 of the Final EIR).

**AIR QUALITY**

**Adverse Environmental Effect:**

Short-term impacts to air quality are expected during construction due to types of work-performed, construction equipment, and motor vehicles used. The potential exists for air pollutant emissions to be generated in quantities that would exceed the current air quality standards.

**Findings:**

Changes or alterations have been required in, or incorporated into, the project, which avoid or substantially lessen the significant environmental effect as identified in the Final EIR.

**Statement of Facts:**

Construction activities have the potential to produce combustion emissions from various sources such as site grading, utility engines, on-site heavy-duty construction vehicles, equipment hauling materials to and from the site, and motor vehicles transporting the construction crew. During construction, mitigation and control measures included in Appendix IV-A of the 2007 Air Quality Management Plan (AQMP) will be implemented to reduce the impact of these emissions (Section 3.2.2.3 of the Final EIR).

**BIOLOGICAL RESOURCES**

**Significant Environmental Effect:**

The project would result in permanent impacts to 109 oak trees (20 heritage oaks and 89 other oak trees). Because the development of mature trees requires 60-80 years, temporal impacts (i.e., the time between planting and regrowth of trees to the size of existing trees) to the oak woodlands would result in unavoidable long term loss of habitat. This temporal and/or cumulative loss of oak woodlands would be considered significant.

**Findings:**

While measures have been incorporated to substantially lessen the significant environmental effects as identified in the Final EIR, these measures would not fully mitigate the long-term loss of habitat.

**Statement of Facts:**

As mitigation, the Final EIR recommended that any oak trees within Caltrans right-of-way (ROW) with a trunk sizes above 8 inches (in) diameter at breast height (dbh) will be replaced at a mitigation-to-impact ratio of 3:1, if possible. Heritage oaks (oaks with dbh greater than 36 in) will be replaced at a mitigation-to-impact ratio of 10:1, if possible. The Final EIR recommended that the County of Los Angeles Oak Tree Protection Ordinances and the City of Santa Clarita Municipal Code conditions be implemented to ensure that oak trees are protected outside of Caltrans ROW. These conditions may involve, but are not limited to, replacement of oak trees and an oak tree protection plan within the project area to protect

replacement oaks and those oak trees not impacted by the proposed project. Caltrans finds that these mitigation measures are feasible.

However, even with implementation of these conditions, the temporal loss of oak woodlands would not be fully mitigable. Thus, the removal of oak trees is still considered to be a significant and unavoidable impact. (Section 3.2.3 of the Final EIR)

### **MANDATORY FINDINGS OF SIGNIFICANCE**

#### **Significant Environmental Effect:**

The project would have the potential to degrade the quality of the environment. The temporal loss of oak woodlands would be a long-term impact of the project based on the regrowth rate of these species.

#### **Findings:**

While measures have been incorporated to substantially lessen the significant environmental effects as identified in the Final EIR, because the development of mature large trees requires 60–80 years, the direct removal of this habitat type would result in unavoidable long-term temporal loss of habitat.

#### **Statement of Facts:**

As mitigation, the Final EIR recommended that any oak trees within Caltrans ROW with a trunk sizes above 8 in dbh will be replaced at a mitigation-to-impact ratio of 3:1, if possible. Heritage oaks (oaks with dbh greater than 36 in) will be replaced at a mitigation-to-impact ratio of 10:1, if possible. The Final EIR recommended that the County of Los Angeles Oak Tree Protection Ordinances and the City of Santa Clarita Municipal Code conditions be implemented to ensure that oak trees are protected outside of Caltrans ROW. These conditions may involve, but are not limited to, replacement of oak trees and an oak tree protection plan within the project area to protect replacement oaks and those oak trees not impacted by the proposed project.

Even with implementation of these conditions, the temporal loss of oak woodlands (i.e., the time between planting and regrowth of trees to the size of existing trees) is considered to be significant and unavoidable (Section 3.2.5.1 of the Final EIR).

#### **Significant Environmental Effect:**

The project would result cumulatively considerable long-term impacts related to oak woodlands.

#### **Findings:**

While measures have been incorporated to substantially lessen the significant environmental effects as identified in the Final EIR, because the development of mature large trees requires 60–80 years, the direct removal of this habitat type would result in unavoidable long-term loss of habitat. Coupled with the removal of oak woodlands in other parts of southern California, the project’s contribution to the loss of oak woodlands (permanent or temporary) would be considered cumulatively considerable.

**Statement of Facts:**

As mitigation, the Final EIR recommended that any oak trees within Caltrans ROW with a trunk sizes above 8 in dbh will be replaced at a mitigation-to-impact ratio of 3:1, if possible. Heritage oaks (oaks with dbh greater than 36 in) will be replaced at a mitigation-to-impact ratio of 10:1, if possible. The Final EIR recommended that the County of Los Angeles Oak Tree Protection Ordinances and the City of Santa Clarita Municipal Code conditions be implemented to ensure that oak trees are protected outside of Caltrans ROW. These conditions may involve, but are not limited to, replacement of oak trees and an oak tree protection plan within the project area to protect replacement oaks and those oak trees not impacted by the proposed project.

Even with implementation of these conditions, the temporal loss of oak woodlands (i.e., the time between planting and regrowth of trees to the size of existing trees) is considered to be cumulatively considerable (Section 3.2.5.2 of the Final EIR).

**CALIFORNIA DEPARTMENT OF TRANSPORTATION**  
**STATEMENT OF OVERRIDING CONSIDERATIONS**  
**FOR THE**  
**INTERSTATE 5 HOV/TRUCK LANES PROJECT**  
**STATE ROUTE 14 TO PARKER ROAD**

The following information is presented to comply with the California Environmental Quality Act (CEQA) Guidelines (Title 14 CCR Chapter 3, Section 15903) and the Department of Transportation and California Transportation Commission Environmental Regulations (Title 21 CCR Chapter 11, Section 1501). Reference is made to the Final Environmental Impact Report (EIR) for the project, which is the basic source for the information.

The implementation of the Selected Alternative (Alternative 2 – Reduced Median) would result in the following impacts have been identified as significant and not fully mitigable:

- **Biological Resources:** Temporal and cumulative loss of oak woodlands (Final EIR Section 3.2.4)

Overriding considerations that support approval of this recommended project were identified in the Purpose and Need section of the Final EIR and are as follows:

**Insufficient Level of Service (LOS).** I-5 is experiencing greater automobile and truck congestion as a result of population growth in north Los Angeles County and goods movement into and out of the Ports of Los Angeles and Long Beach. Freeway traffic volumes are expected to approximately double by 2030, which will continue to cause substantial delays (Final EIR, Chapter 1). Without the project, if capacity improvements are not made, traffic volumes in the project area are forecast to increase by 2030, resulting in a decrease in LOS. The proposed project would reduce existing and forecast traffic congestion on the project segment of I-5 to accommodate planned growth within the study area.

**Operational Deficiencies.** In the existing condition, due to the grades within the project area, slow-moving trucks and vehicles must share existing travel lanes with other vehicles and can obstruct the flow of traffic, thereby increasing congestion and reducing mobility. Given the high percentage of trucks and the conflict and inefficient operations as mentioned above, there is a need to separate trucks from passenger vehicles to improve congestion and delay associated with the interaction of these vehicle types. Without the project, the operational deficiencies would not be addressed. The project would reduce delays to vehicles caused by slower-moving trucks through the hilly southern portion of this segment of I-5. In addition, the

project would improve operational and safety design features to facilitate the movement of people, freight, and goods on the project segment.

Adoption of the Selected Alternative will: (1) reduce delays to vehicles caused by slower-moving trucks through the hilly southern portion of this segment of I-5; (2) improve operational and safety design features to facilitate the movement of people, freight, and goods on the project segment; and (3) reduce existing and forecast traffic congestion on the project segment of I-5 to accommodate planned growth within the study area.

Caltrans concludes, based upon the whole of the record, that the economic, social, and environmental benefits of improved mobility and reduced travel times of the selected alternative outweigh the unavoidable environmental impacts associated with its construction and operation and determines that said benefits override the significance of their associated adverse impacts.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.2c.(3)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Katrina Pierce  
Division Chief  
Environmental Analysis

Subject: **APPROVAL OF PROJECT FOR FUTURE CONSIDERATION OF FUNDING  
05-Mon-156; PM R1.6/T5.2, 05-Mon-101, PM 94.6/96.8  
RESOLUTION E-13-65**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission), as a responsible agency, approve the attached Resolutions E-13-65.

## **ISSUE:**

The attached resolution proposes to approve for future consideration of funding the following project for which a Final Environmental Impact Report (FEIR) has been completed:

- State Route 156 (SR 156) and United States Route 101 (US-101) in Monterey County. Construct roadway improvements including lane additions and interchange improvements along portions of SR 156 and US-101 near the city of Prunedale. (PPNO 0057C)

This project in Monterey County will widen a portion of State Route 156 from two lanes to four lanes and convert a portion of United States Route 101 from an expressway to a freeway near the city of Castroville. The project is not fully funded. Design and Right of Way are programmed in the 2012 State Transportation Improvement Program. The total estimated cost is \$104,194,000 for capital and support. Construction is estimated to begin in Fiscal Year 2018-19 or later. The scope, as described for the preferred alternative, is consistent with the project scope programmed by the Commission in the 2012 State Transportation Improvement Program.

A copy of the FEIR has been provided to Commission staff. Resources that may be impacted by the project include: farmland, residential and commercial displacements, aesthetics and

visual, water quality and stormwater runoff, hazardous waste, air quality noise, and biological resources. Potential impacts associated with the project can all be mitigated to below significance through proposed mitigation measures with the exception of farmlands, which has been determined to be an unavoidable significant environmental effect. As a result, a Final Environmental Impact Report was prepared for the project.

Attachments

## **CALIFORNIA TRANSPORTATION COMMISSION**

### **Resolution for Future Consideration of Funding 05-Mon-156, PM R1.6/T5.2, 05-Mon-101, PM 94.6/96.8 Resolution E-13-65**

- 1.1** **WHEREAS**, the California Department of Transportation (Department) has completed an Environmental Impact Report pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- State Route 156 (SR 156) and United States Route 101 (US-101) in Monterey County. Construct roadway improvements including lane additions and interchange improvements along portions of SR-156 and US-101 near the city of Prunedale. (PPNO 0057C)
- 1.2** **WHEREAS**, the Department has certified that the Environmental Impact Report has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3** **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Final Environmental Impact Report.
- 1.4** **WHEREAS**, the project will have a significant effect on the environment.
- 1.5** **WHEREAS**, a Statement of Overriding Considerations was prepared; and
- 1.6** **WHEREAS**, Findings were made pursuant to the State CEQA Guidelines; and
- 2.1** **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby support approval of the above referenced project to allow for consideration of funding.

# Project Vicinity Map

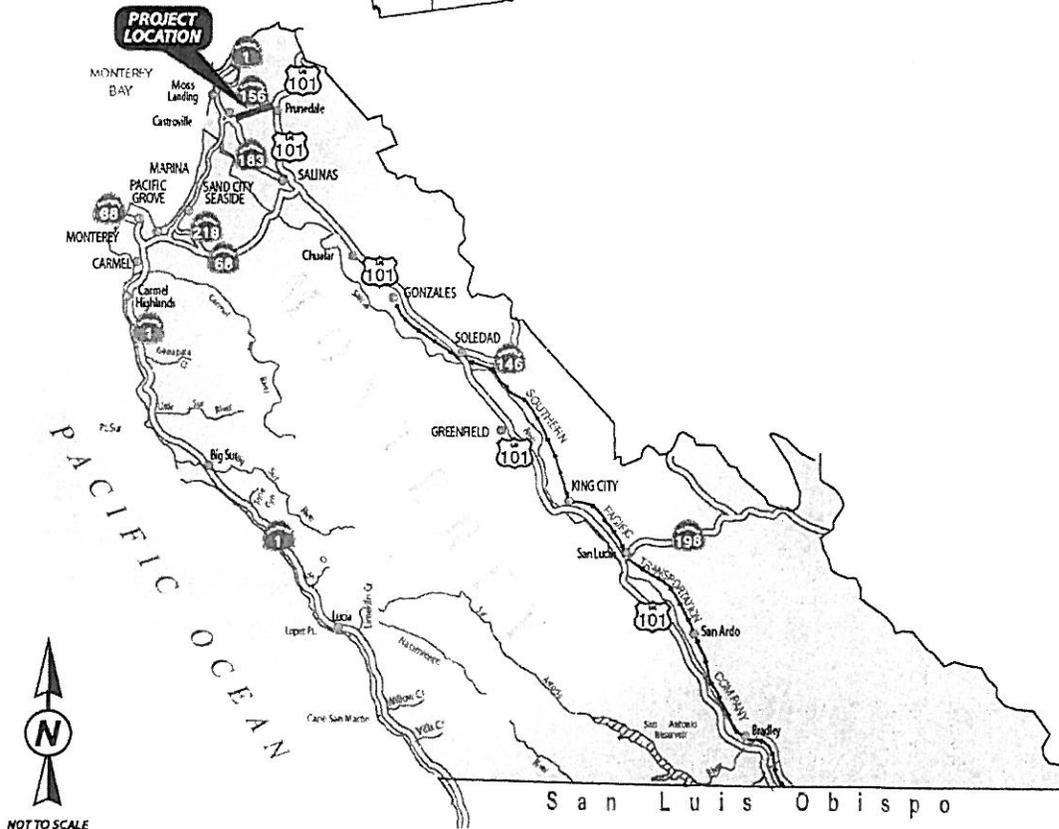
## West Corridor Project

State Route 156/US 101

05-MON-156-PM R1.6/T5.2

05-MON-101-PM 94.6/96.8

EA 05-316000



# Project Location Map

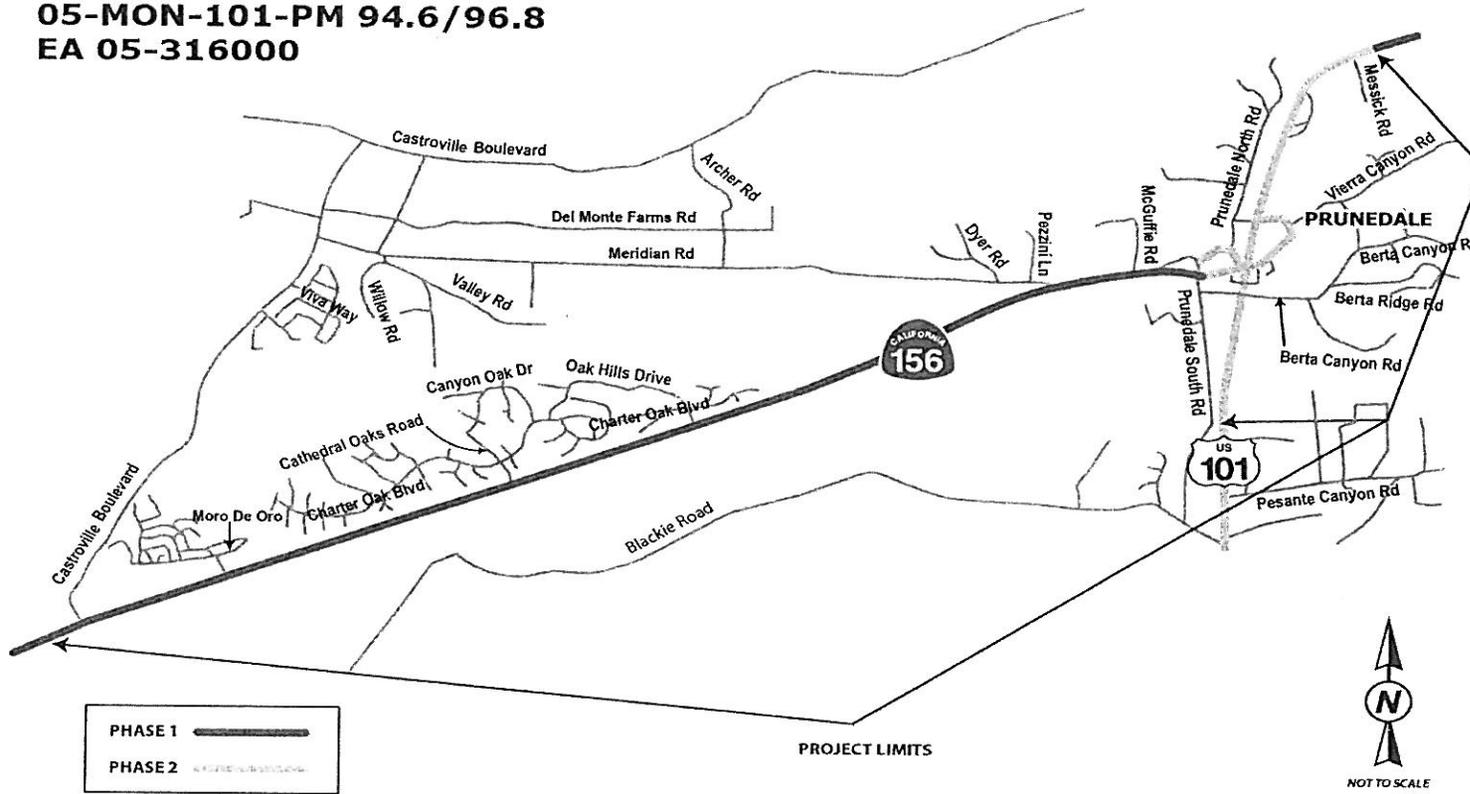
## West Corridor Project

State Route 156/US 101

05-MON-156-PM R1.6/T5.2

05-MON-101-PM 94.6/96.8

EA 05-316000



## FINDINGS

### CALIFORNIA DEPARTMENT OF TRANSPORTATION FINDINGS FOR ROUTE 156 WEST CORRIDOR

The following information is presented to comply with State CEQA Guidelines (Title 14 California Code of Regulations, Chapter 3, Section 15901) and the Department of Transportation and California Transportation Commission Environmental Regulations (Title 21, California Code of Regulations, Chapter 11, Section 1501). Reference is made to the Final Environmental Impact Report (FEIR) for the project, which is the basic source for the information.

The following effects have been identified in the EIR as resulting from the project. Effects found not to be significant have not been included.

#### Significant Impact to Farmland

The project would require conversion of 165 acres of farmland, 85.5 acres are of statewide or local importance and 147 acres of coastal agricultural preserve land. The proposed project would add two new lanes in both eastbound and westbound directions, south of the existing State Route 156. Farmland is located south of the existing State Route 156.

#### Findings

Changes or alterations have been required in or incorporated into the project which avoid or substantially lessen the significant environmental effect as identified in the final EIR.

#### Statement of Facts

The proposed project would add two new lanes in both eastbound and westbound directions, south of the existing State Route 156. Farmland is located south of the existing State Route 156. The project cannot avoid farmland impacts: only the No-Build alternative would avoid farmland impacts. Farmland impacts are directly related to the amount of right of way needed for the project. Other alternatives considered to lessen farmland impacts were withdrawn because they either did not lessen environmental impacts or achieve the project's objectives.

Caltrans has reduced the project's farmland impacts from 165 acres to 118 acres and reduced impacts to coastal agricultural land to 105 acres by reducing the width of the median and by reducing the footprint of the Castroville Boulevard interchange. Caltrans' Relocation Advisory Assistance will be available for displacement of onsite investments, such as wells and irrigation systems. During construction, farmers will be provided property access easements, access to irrigation and adequate access to ensure that agricultural operations would not be impaired. This project lies within the coastal zone, and mitigation for farmland impacts would be a condition of the local coastal permit for this project.

#### Adverse Environmental Effects to Visual Resources

The entire length of State Route 156 throughout the project limits is an officially designated State Scenic Highway.

The interchanges and structures will contrast with the existing conditions. Vegetation loss and the introduction of man-made structures could result in an overall loss of rural character. The proposed Moro Cojo Slough bridge, structures and related ramps proposed at Castroville Boulevard, the State Route 156/U.S. Route 101 overcrossing at Messick Road would dramatically change the visual scale of the highway setting at those locations. The proposed State Route 156/U.S. Route 101 connector flyover would also cause an urbanizing effect.

Retaining walls along eastbound State Route 156 west of Prunedale South Road across from McGuffie Road, northbound U.S. Route 101 north of Vierra Canyon Road, and northbound San Miguel Canyon Road would have a distinctly urbanizing effect on the setting. Retaining walls proposed for southbound U.S. Route 101 would generally be below the roadway and would not be readily seen by the highway traveler. These walls would be seen, however, from local roadways such as Prunedale North Road.

Soundwalls are proposed for southbound U.S. Route 101 across from Vierra Canyon Road, along the northbound lanes of U.S. Route 101 near the proposed Messick Road overcrossing and westbound State Route 156 near McGuffie Road. These walls would contribute to a more engineered built appearance of the corridor.

The extent of visual impact caused by the project would be a factor of how these physical changes are perceived by the viewing public. Viewer sensitivity is likely to be moderately high based on the Scenic Highway designation of State Route 156, review of Monterey County planning policy, and potential viewer activity.

#### Findings

Changes or alterations have been required in or incorporated into the project which avoid or substantially lessen the significant environmental effect as identified in the final EIR.

#### Statement of Facts

Measures will be taken to reduce potential impacts include: grading for a natural appearance, minimizing structure profiles, using materials and special treatments that enhance necessary additional to the built environment, and planting trees and landscaping to control erosion and improve the aesthetics of the surrounding visual resources that contribute most to the scenic quality of the corridor. New lights on structures would be shielded to keep light at downcast.

#### Adverse Environmental Effects on Biological Resources

Project activities would impact central maritime chaparral and coast live oak woodland natural communities. Central maritime chaparral and coast live oak woodland are considered habitats of special concern by the California Department of Fish and Game.

Project activities associated with construction staging and incidental runoff would result in temporary and permanent impacts to wetlands and other waters of United States requiring a permit from the U.S. Army Corps of Engineers pursuant to Section 404 of the Clean Water Act.

Project activities associated with temporary dewatering of aquatic habitat for culvert and bridge work and construction activities such as moving equipment, grading, clearing, removing vegetation, stockpiling, dredging, and filling would impact:

- Santa Cruz long-toed salamander (federally and state listed as endangered, fully protected species under the California Endangered Species Act)
- California tiger salamander (federally and state listed as threatened)
- California red-legged frog (federally listed as threatened and California species of special concern)

#### Findings

Changes or alterations have been required in or incorporated into the project which avoid or substantially lessen the significant environmental effect as identified in the final EIR.

#### Statement of Facts

Measures to reduce impacts will be used both on and off the project site. Mitigation land will be purchased in the area that would provide the opportunity for preservation, restoration, and enhancement. The acreage required for compensating for impacts based on resource agency recommendations, as well as the function and quality of aquatic and upland habitat that needs to

be replaced. The measures to reduce impacts during construction will include where appropriate and where feasible the following: preconstruction surveys and biological monitoring; establishment of environmentally sensitive areas; revegetation inside the right-of-way; tree removal would occur outside of the nesting season.

The Santa Cruz long-toed salamander is a fully protected species and "take" of any fully protected species is prohibited. Additionally, the Department of Fish and Game cannot authorize their "take" for development. Discussions with the Department of Fish and Game and the California Department of Transportation regarding the Santa Cruz long-toed salamander are ongoing.

#### Adverse Environmental Effects on Water Quality

The project would have minimal short-term impacts to surface and groundwater quality. No long-term impacts to water quality are anticipated. The project would not have substantial impacts to groundwater quality.

#### Findings

Changes or alterations have been required in or incorporated into the project which avoid or substantially lessen the significant environmental effect as identified in the final EIR.

#### Statement of Facts

Potential temporary impacts to water quality during construction would be addressed in the project plans by including measures to ensure that there would be no detrimental discharge into any bodies of water. Basins and vegetated drainage ditches will likely be the treatments implemented. During the construction phase, Caltrans would require the contractor to prepare and implement a program to control water pollution during construction. Before the start of project construction, the contractor would be required to prepare a Storm Water Pollution Prevention Plan that satisfies the requirements of the Caltrans statewide National Pollutant Discharge Elimination Systems Permit and the General Construction Permit. The permits require the following: A Storm Water Pollution Prevention Plan is to be prepared and implemented during construction.

## STATEMENT OF OVERRIDING CONSIDERATIONS

### **CALIFORNIA DEPARTMENT OF TRANSPORTATION STATEMENT OF OVERRIDING CONSIDERATION FOR THE ROUTE 156 WEST CORRIDOR PROJECT WHICH PROPOSES TO WIDEN STATE ROUTE 156 BETWEEN U.S. ROUTE 101 AND WEST OF CASTROVILLE BOULEVARD AND REBUILD THE U.S. ROUTE 101/STATE ROUTE 156 INTERCHANGE IN MONTEREY COUNTY**

The following information is presented to comply with State CEQA Guidelines (Title 14 California Code of Regulations, Chapter 3, Section 15093, and the Department of transportation and California Transportation Commission Environmental Regulations (Title 21 California Code of Regulations, Chapter 11, Section 1501). Reference is made to the Final Environmental Impact Report for the project, which is the basic source of information.

The following impacts have been identified as significant and not fully mitigable:

- Farmland

Overriding considerations that support approval of this recommended project area as follows:

The purpose of the project is to:

- Improve safety and operations
- Improve local road access to State Route 156
- Improve interregional traffic flow along State Route 156
- Relieve existing congestion and provide capacity for future increases in traffic volume

The need for the project is based on the following:

- An increase in congestion
- Separation of interregional and local traffic
- Lack of route continuity

Monterey County's population was approximately 415,057 in 2010 and is expected to increase to 602,732 in 2030 according to the Association of Monterey Bay Area Governments. The county will see the largest growth in the Fort Ord area, Marina and Salinas. Castroville and the Fort Ord area, Seaside and Marina combined will compose roughly 92 percent of the total projected population increase between the years 2005 and 2030.

In general, the employment centers closest to the project area are growing faster than those farther away in the San Francisco Bay Area. According to projections by the Association of Monterey Bay Area Governments, between the years 2000 and 2030, 65 percent of employment growth will occur in the Silicon Valley, Coyote Valley and Gilroy. All employment centers south of San Jose except Santa Cruz are projected to grow more than or close to 50 percent by 2030 according to Association of Monterey Bay Area Governments. Relative to population or housing, jobs are also projected to grow faster in the Monterey County coastal areas.

According to the Monterey County Agricultural Commissioner, agriculture contributed \$8.2 billion and more than 73,000 jobs to the County's economy in 2010. The county supplies 80 percent of the nation's lettuces and nearly the same percentage of artichokes, in addition to other vegetables. Monterey County's crop production was valued at \$3.85 billion in 2011. Agricultural land use within the project limits

is on the south side of State Route 156 between Castroville Boulevard and Meridian Road on the north side of State Route 156 next to Castroville Boulevard. Strawberries and artichokes are produced on the south side of State Route 156. Land for livestock grazing is on the north and south sides of State Route 156. Castroville, the Artichoke Capital of the World, has been producing artichokes since the early to mid-1900s. Artichokes are harvested year-round, mostly for the fresh market.

The existing State Route 156 is a two-lane conventional highway with 12-foot lanes and 6-foot to 8-foot outside shoulders. There are at-grade intersections and several private driveways along the route. Left-turn lanes at the intersections allow motorists to turn onto county roads from State Route 156. State Route 156 serves interregional and recreational traffic linking the Monterey Peninsula to the Bay Area and the Central Valley. State Route 156 goes through agricultural land for much of the route. State Route 156 within the project limits is located in the coastal zone. State Route 156 throughout the project limits is an officially designated State Scenic Highway.

The existing U.S. Route 101 within the project limits is a four-lane expressway with 12-foot-wide lanes, 4- to 8-foot-wide outside shoulders, a 16- to 22-foot-wide median and three at-grade intersections that allow right-in, right-out turns. Two interchanges—one at the State Route 156 junction and one at San Miguel Canyon Road—sit in the project limits. U.S. Route 101 is a four-lane divided expressway serving interregional traffic, much of it tourist, trucking and commuting traffic. State Route 156 and U.S. Route 101 come together and share the same roadway for 8 miles between Prunedale and San Juan Bautista.

Construction of a new alignment for State Route 156 would divert interregional traffic away from the residential communities next to State Route 156 and U.S. Route 101. This would affect the existing access provided from State Route 156 to the Oak Hills subdivision; currently Oak Hills residents or visitors must turn left, crossing in front of oncoming traffic to enter or exit the development. These turns are particularly difficult during the higher traffic volumes in summer, vacation and harvest season, and the weekday peak commuting hours.

Residents of the Monte del Lago mobile home park face a similar situation competing with recreational, truck and commuter traffic when traveling to Prunedale or Castroville for shopping, services, medical appointments and jobs.

State Route 156 within the project limits is designated as a Terminal Access Route to the National Truck Network. Commodity exports, including agricultural products and quarry materials generate significant truck traffic along State Route 156 and U.S. Route 101. Almost the entire global artichoke crop is produced in California within the Castroville area. These commodities are moved mainly by truck to the San Francisco or Los Angeles areas via U.S. Route 101 or to the interstate system. State Route 156 is a key statewide connector because, in conjunction with State Route 152, it is an important east-west corridor south of the Bay Area and north of San Luis Obispo.

Within the project limits, State Route 156 is a two-lane roadway. State Route 156 just west of Castroville Boulevard and next to the U.S. Route 101/State Route 156 interchange is a four-lane roadway. In Monterey County, the two-lane roadway of State Route 156 totals less than 4 miles. The lanes to be added under the Route 156 West Corridor project would make the roadway four lanes—for a continuous four-lane route to and from the Monterey Peninsula.

## **Project Alternatives**

Twelve build alternatives were developed and studied by the Project Development Team (comprised of Caltrans personnel from different functional branches, local and state agency representatives and other stakeholders). Ten of these alternatives were rejected because they did not reduce environmental impacts or they were not feasible to construct. Two alternatives, Alternative 11 and Alternative 12, and the No Build Alternative were considered in the environmental document. Alternative 11 would add two new lanes in both eastbound and westbound directions, while the existing highway would essentially function as a frontage road. Alternative 12 would use the existing highway for part of the alignment and would add two new lanes south of the existing highway. Both build alternatives would include improvements at the U.S. Route 101/State Route 156 and San Miguel Canyon interchanges, including new connectors to northbound and southbound U.S. Route 101.

## **Preferred Alternative**

Alternative 11 was chosen as the preferred alternative based on engineering and environmental analysis, and community and agency input. While all build alternatives would meet the purpose and need of the project, Alternative 11 was selected because:

- Fewer permanent impacts to aquatic and upland habitats of California tiger salamander, Santa Cruz long-toed salamander and California red-legged frog
- Fewer permanent impacts to Coast live oak woodland and Riparian habitat
- Fewer permanent impact to seasonal jurisdiction wetlands
- Construction can occur for the project without disrupting through traffic on the existing Route 156

Alternative 11 provides a safer route for pedestrians and bicyclists by providing a frontage road, separating interregional traffic from local traffic, while maintaining residential access for property owners.

Under the No-Build Alternative, State Route 156 and the U.S. Route 101/State Route 156 interchange would stay in their present conditions. No improvements would be made to State Route 156 or the U.S. Route 101/State Route 156 interchange. No measures would be taken to increase capacity, reduce congestion, or improve safety and operations.

## **Conclusion**

Farmland impact cannot be avoided but with mitigation measures adopted, the impact has been substantially lessened. Caltrans has reduced the project's farmland impacts from 165 acres to 118 acres, including impacts to coastal agricultural land (105 acres) by reducing the width of the median and by reducing the footprint of the Castroville Boulevard interchange. Caltrans' Relocation Advisory Assistance will be available for displacement of onsite investments, such as wells and irrigation systems. During construction, farmers will be provided property access easements, access to irrigation and adequate access to ensure that agricultural operations would not be impaired. This project lies within the coastal zone, and mitigation for farmland impacts would be a condition of the local coastal permit for this project. Caltrans would partially compensate the direct loss of agricultural land and will protect a portion of California's remaining land resources in accordance with California Environmental Quality Act Guideline 15370.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.2c.(4)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Katrina Pierce  
Division Chief  
Environmental Analysis

Subject: **APPROVAL OF PROJECT FOR FUTURE CONSIDERATION OF FUNDING  
04-SCI-152; PM 0.14/5.20  
RESOLUTION E-13-66**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission), as a responsible agency, approve the attached Resolutions E-13-66.

## **ISSUE:**

The attached resolution proposes to approve for future consideration of funding the following project for which a Final Environmental Impact Report (FEIR) has been completed:

- State Route 152 (SR 152) in Santa Clara County. Construct roadway improvements at five locations along portions of SR-152 near the city of Gilroy. (PPNO 0483J)

This project in Santa Clara County will construct roadway improvements in five locations on State Route 152 near the city of Gilroy. The project is programmed in the 2012 State Highway Operation and Protection Program. The total estimated cost is \$37,141,000 for capital and support. Construction is estimated to begin in Fiscal Year 2013-14. The scope, as described for the preferred alternative, is consistent with the project scope programmed by the Commission in the 2012 State Highway Operation and Protection Program.

A copy of the FEIR including the addendum has been provided to Commission staff. An addendum to the original FEIR was prepared to address changes to the original project design. Resources that may be impacted by the project include: visual, water quality and stormwater runoff, hazardous waste, geology and soils, and biological resources. Potential impacts associated with the project can all be mitigated to below significance through proposed mitigation measures with the exception of visual impacts, which have been determined to be an unavoidable significant environmental effect. As a result, a Final Environmental Impact Report with addendum was prepared for the project.

Attachments

## **CALIFORNIA TRANSPORTATION COMMISSION**

### **Resolution for Future Consideration of Funding**

**04-SCI-152, PM 0.14/5.20**

**Resolution E-13-66**

- 1.1** **WHEREAS**, the California Department of Transportation (Department) has completed an Environmental Impact Report pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- State Route 152 (SR 152) in Santa Clara County. Construct roadway improvements at five locations along portions of SR 152 near the city of Gilroy. (PPNO 0483J)
- 1.2** **WHEREAS**, the Department has certified that the Environmental Impact Report has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3** **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Final Environmental Impact Report.
- 1.4** **WHEREAS**, the project will have a significant effect on the environment.
- 1.5** **WHEREAS**, a Statement of Overriding Considerations was prepared; and
- 1.6** **WHEREAS**, Findings were made pursuant to the State CEQA Guidelines; and
- 2.1** **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby support approval of the above referenced project to allow for consideration of funding.

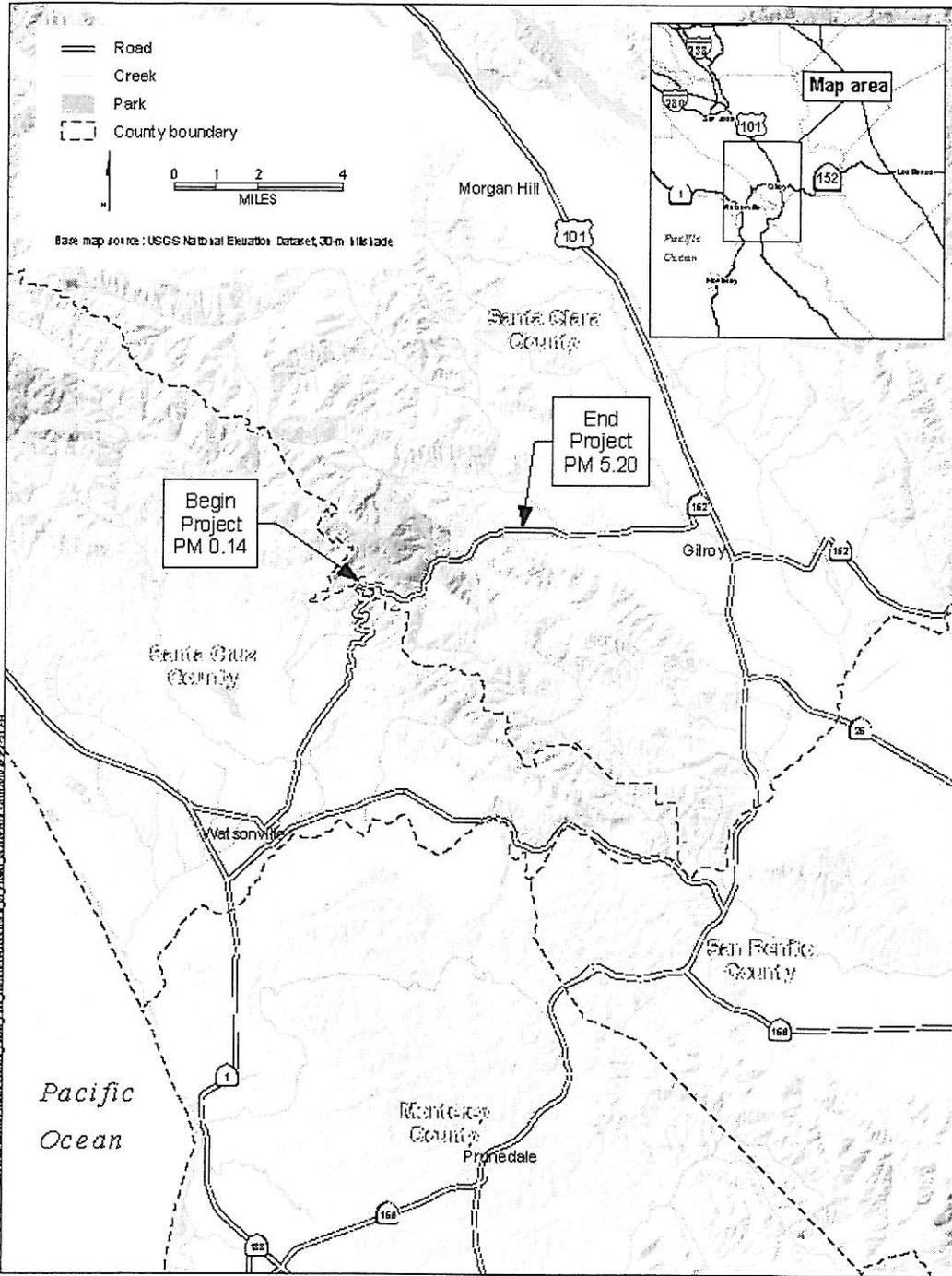


Figure 1: Project Vicinity Map

## FINDINGS

### CALIFORNIA DEPARTMENT OF TRANSPORTATION FINDINGS FOR THE HECKER PASS SAFETY IMPROVEMENT PROJECT

The following information is presented to comply with State CEQA Guidelines (Title 14 California Code of Regulations, Chapter 3, Section 15901) and the Department of Transportation and California Transportation Commission Environmental Regulations (Title 21, California Code of Regulations, Chapter 11, Section 1501). Reference is made to the Final Environmental Impact Report (FEIR) for the project, which is the basic source for the information.

The following effects have been identified in the EIR as resulting from the project. Effects found not to be significant have not been included.

#### Visual/Aesthetics

##### Adverse Environmental Effects

The introduction of the proposed retaining walls, with all recommended mitigation measures, would still have significant adverse impacts to the visual character and quality of the SR 152 scenic highway corridor. In all, up to approximately 0.75 mile (approximately 3,963 ft) of new retaining walls would be constructed in the corridor; approximately 1.6 miles of the highway overall would be affected by the project.

##### Findings:

Changes or alterations have been required in, or incorporated into, the project, which avoid or substantially lessen the significant environmental effect as identified in the final EIR.

#### Statement of Facts

To address the potential impacts of visual intrusion from the new retaining walls, the following mitigation measures are recommended:

- Minimizing overall wall height to the greatest extent feasible.
- Using context-sensitive wall texture and color treatment, in consultation with local agencies, to reduce visual contrast and enhance compatibility of visual character to the greatest extent feasible.
- Staining of bottom, safety-barrier portion of walls to reduce overall color contrast and visual intrusion.

- If feasible, walls shall be gutterless and without chain-link safety fence in order to reduce visual contrast.
- Wherever feasible and consistent with safety, the use of crash cushions at retaining walls shall be avoided to reduce the visual contrast with the natural environment.

In addition to those measures, the following measures also apply to Location 4: To minimize the contrast in visual character and decline in visual quality as a result of Wall 4E as seen by park visitors, the Department will use context-sensitive wall color and texture treatment. Color shall be dark to minimize contrast and reflectivity; texture treatment such as stacked stone, carved rock or other similar treatment shall be used to articulate the wall surface and provide a more naturalistic, context-compatible visual character.

## STATEMENT OF OVERRIDING CONSIDERATIONS

### CALIFORNIA DEPARTMENT OF TRANSPORTATION STATEMENT OF OVERRIDING CONSIDERATIONS FOR THE HECKER PASS SAFETY IMPROVEMENTS PROJECT

The following information is presented to comply with State CEQA Guidelines (Title 14 California Code of Regulations, Chapter 3, Section 15903), and the Department of Transportation and California Transportation Commission Environmental Regulations (Title 21 California Code of Regulations, Chapter 11, Section 1501). Reference is made to the Final Environmental Impact Report (FEIR) for the project, which is the basic source for the information.

The following impacts have been identified as significant and not fully mitigable: The introduction of the proposed retaining walls, with all recommended mitigation measures, would still have significant adverse impacts to the visual character and quality of the SR 152 scenic highway corridor.

Overriding considerations that support approval of this recommended project are as follows:

This is a safety improvement project, of which the purpose is to reduce the number of cross-centerline and run-off-the road accidents along this highway corridor. The roadway currently has many deficiencies. The existing alignment has sharp turns, narrow shoulders in many locations, and steep embankments and vegetation adjacent to the shoulders of the roadway. Truck-trailers over 45 ft long have already been banned from SR 152 between Watsonville and Gilroy since 1986, based on, among other factors, the limited roadway width, winding alignment of the highway, and the number of truck-involved accidents. Adding to the available pullout areas would increase refuge opportunities for disabled vehicles. The inability of emergency response vehicles to use shoulders and bypass stalled traffic to reach disabled vehicles delays their response time.

A number of accidents on this segment of the highway have involved vehicles that cross the roadway centerline. The Department developed a list in 2004 of candidate major collision-reduction projects to the Headquarters of Traffic Safety program coordinator based on results from the Two- and Three-Lane Safety Monitoring program. Because a high number of cross-centerline accidents (CCAs) occurred within this portion of SR 152 during the study period, the Project was submitted and the District's recommendation was approved by District Headquarters on June 21, 2004. The study found that 29 of the 176 accidents involved vehicles that crossed the centerline. Constructing the improvements proposed as part of this project would create an upgraded facility that would be better able to assist out-of-control motorists from crossing the centerline and would reduce CCAs.

Improving safety along the present location of the highway cannot be accomplished without removal of vegetation, additional side slope grading, and construction of retaining walls and other slope stability measures.

This project had two alternatives, a build and no build. The build alternative was chosen as it meets the project's purpose and need. There was no public controversy, and the Department received comments supporting the build alternative, but none supporting the no build alternative.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.1a.  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **STIP AMENDMENT 12S-049**

## **SUMMARY:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) approve the requested State Transportation Improvement Program (STIP) Amendment 12S-049. This item was noticed at the Commission's June 11, 2013 meeting.

## **ISSUE:**

The Department and the Siskiyou County Local Transportation Commission (SCLTC) propose to amend the 2012 STIP to program \$300,000 in Regional Improvement Program (RIP) funds from Siskiyou County's unprogrammed share balance for construction on a new project along Route 97, Angel Maple Operational Improvements (PPNO 3530), in Siskiyou County.

## **BACKGROUND:**

At the request of Siskiyou County Local Transportation Commission, the Department proposes to program a new project for construction in Fiscal Year 2015-16 to widen the shoulders along Route 97 from Angel Valley Road to Maple Avenue. A new development along Route 97 has triggered the need for left-turn channelization in order to mitigate expected safety and operational concerns along the corridor. The owners of the development have committed to contribute \$50,000 towards the construction costs.

The proposed programming is shown in the table on the following page.

**REVISE: Angel Maple Operational Improvements Project (PPNO 3530)**

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Siskiyou	2	3530	4G000	CO	2015-16	2.1	2.5	97					
<b>Implementing Agency: (by component)</b>	<b>PA&amp;ED</b>	Caltrans				<b>PS&amp;E</b>	Caltrans						
	<b>R/W</b>	Caltrans				<b>CON</b>	Caltrans						
<b>RTPA/CTC:</b>	Siskiyou County Transportation Commission												
<b>Project Title:</b>	Angel Maple Operational Improvements												
<b>Location</b>	In Siskiyou County near Weed from Angel Valley Road to Maple Avenue.												
<b>Description:</b>	Widen shoulders.												
<b>(DOLLARS IN THOUSANDS)</b>													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	12/13	13/14	14/15	15/16	16/17	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
<b>RIP</b>													
Existing	0					0			0				
Change	300					300			300				
Proposed	300					300			300				
<b>Local Funds</b>													
Existing	0					0			0				
Change	50					50			50				
Proposed	50					50			50				
<b>Other State - SHOPP - Minor</b>													
Existing	0		0	0	0		0	0	0	0	0	0	
Change	494		92	151	251		5	153	92	133	13	98	
Proposed	494		92	151	251		5	153	92	133	13	98	
<b>Total</b>													
Existing	0		0	0	0		0	0	0	0	0	0	
Change	844		92	151	601		5	503	92	133	13	98	
Proposed	844		92	151	601		5	503	92	133	13	98	

**RESOLUTION:**

Be it Resolved, that the California Transportation Commission does hereby amend the State Transportation Improvement Program to amend the 2012 STIP to program \$300,000 in Regional Improvement Program (RIP) funds from Siskiyou County’s unprogrammed share balance for construction on a new project along Route 97, Angel Maple Operational Improvements (PPNO 3530), in Siskiyou County as described above.

# Memorandum

**To:** CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

**CTC Meeting:** August 6, 2013

**Reference No.:** 2.1c.(5)  
Action Item

**From:** STEVEN KECK  
Acting Chief Financial Officer

**Prepared by:** Katie Benouar  
Division Chief  
Transportation Planning

**Subject:** TRADE CORRIDORS IMPROVEMENT FUND-PROJECT BASELINE AMENDMENT  
RESOLUTION TCIF-P-1314-01, AMENDING RESOLUTION TCIF-P-1112-45

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) amend the Trade Corridors Improvement Fund (TCIF) Project Baseline Agreement for Project 15, San Gabriel Valley Grade Separation Program (PPNO TC15) in Los Angeles County. The Southern California Consensus Group concurs with this amendment and the requested changes.

## **ISSUE:**

The Alameda Corridor-East Construction Authority (ACE) proposes to amend the TCIF Project Baseline Agreement for Project 15, San Gabriel Valley Grade Separation Program (San Gabriel Program) to identify the eight grade separation projects, which are being used to provide the required match for the TCIF funds, update the delivery schedule, cost, and funding plan for the entire program of projects.

ACE is requesting removal of the Puente Avenue and Fairway Drive Projects as they are seeking concurrent programming of separate TCIF Funds for these eligible projects at the August 6, 2013 Commission meeting. ACE is also requesting the addition of three new grade separation projects: Hamilton Boulevard, Fullerton Road and Durfee Avenue.

## **BACKGROUND:**

The San Gabriel Program was adopted into the Proposition 1B TCIF Program by the Commission on April 10, 2008. The Baseline Agreement was executed between ACE, the Department, and the Commission, and approved on October 29, 2008, under Resolution TCIF-P-0809-04B. An amendment to the Baseline Agreement was approved on June 23, 2011, under Resolution TCIF-P-1011-29, to split the project into two phases and update the schedule and cost. Allocations for Phase I and II were approved on October 26, 2011, under Resolution TCIF-A-1112-05. In addition, a Project Baseline Agreement Amendment was approved June 27, 2012, under Resolution TCIF-P-1112-45, to identify seven grade separation projects, which are required match for the TCIF

funds, update the project delivery schedule, move a portion of Phase 1 work to Phase 2, and the cost for the entire program of projects.

The San Gabriel Program consists of multiple grade separation projects where major urban arterials cross freight mainlines in eastern Los Angeles County. The scope includes the 2.2 mile San Gabriel Trench project, and a series of grade separation projects within the San Gabriel Program. The completed project will increase efficiency, reliability, and throughput on the Union Pacific Railroad (UPRR) Alhambra Subdivision mainline east-west corridor to accommodate the existing freight train traffic, as well as projected increases in train traffic. It will also reduce roadway traffic delays, enhance traffic safety, and reduce air and noise pollution in the local area.

Revised Schedule for Phase I and II

The “End Construction,” “Begin Closeout,” and “End Closeout” Phases have been extended by 12 months due to ACE extending the bid period due to the volume and complexity of questions posed by prospective bidders. ACE also extended the End Construction Phase in response to UPRR’s request for an additional six months to complete UPRR work.

Identification of Match Projects

The San Gabriel Program consists of Phase I and II of the San Gabriel Trench project along with eight additional non-TCIF funded grade separation projects that are contributing to the required 1:1 match for the TCIF funds. This amendment will identify the eight grade separation projects, along with their delivery schedules and funding plans.

ACE is requesting removal of the Puente Avenue and Fairway Drive Projects as they are seeking concurrent programming of separate TCIF Funds for these eligible projects at the August 6, 2013 Commission meeting. ACE is also requesting the addition of three new grade separation projects: Hamilton Boulevard, Fullerton Road, and Durfee Avenue.

The following table provides a summary of the amount each project is contributing for match:

<b>Grade Separation Program of Projects</b>	<b>Non-State Match</b>
Brea Canyon Road	\$38,922,000
Fairway Drive	\$105,365,000
Puente Avenue	\$86,388,000
Ramona Boulevard	\$14,965,000
Reservoir Street	\$12,480,000
Sunset Avenue	\$35,208,000
Temple Avenue	\$45,177,000
San Gabriel Trench – Phases 1 & 2	\$68,554,000
Hamilton Boulevard	\$76,326,000
Fullerton Road	\$99,052,000
Durfee Avenue	\$73,568,000
<b>TOTAL</b>	<b>\$464,252,000</b>

Delivery Schedules and Funding Plans

The following tables summarize the delivery schedules and funding plans for Phase I and II, and the eight grade separation projects which are contributing to the match funds:

Milestone	Current Approved Phase I	Proposed Phase I	Change Phase I	Current Approved Phase II	Proposed Phase II	Change Phase II
Begin Environmental	Jul-03	Jul-03	No Change	Jul-03	Jul-03	No Change
End Environmental	Nov-10	Nov-10	No Change	Nov-10	Nov-10	No Change
Begin Design	Jun-09	Jun-09	No Change	Jun-09	Jun-09	No Change
End Design	Feb-12	Feb-12	No Change	Feb-12	Feb-12	No Change
Begin Right of Way	Dec-10	Dec-10	No Change	Dec-10	Dec-10	No Change
End Right of Way	Jun-11	Jun-11	No Change	Jun-11	Jun-11	No Change
Begin Construction	Aug-11	Aug-11	No Change	Aug-12	Aug-12	No Change
End Construction	Sep-16	Sep-17	12 months	Sep-16	Sep-17	12 months
Begin Closeout	Oct-16	Oct-17	12 months	Oct-16	Oct-17	12 months
End Closeout	Oct-17	Oct-18	12 months	Oct-17	Oct-18	12 months

Non-State Match Grade Separation Program of Projects

Milestone	Proposed Brea Canyon Rd	Proposed Ramona Blvd	Proposed Reservoir Street	Proposed Sunset Avenue	Proposed Temple Avenue	Proposed Hamilton Blvd	Proposed Fullerton Road	Proposed Durfee Avenue
Begin Environmental	Aug-01	Jan-01	Feb-00	May-01	Oct-00	Sep-12	Nov-12	Oct-12
End Environmental	Aug-01	Dec-02	Feb-01	Jul-01	Mar-01	Jul-13	Jul-13	Jan-14
Begin Design	Mar-02	Mar-01	Dec-00	May-02	Jul-01	Sep-12	Nov-12	Oct-12
End Design	Sep-04	Dec-04	May-03	Jan-05	Sep-03	Jun-14	Jun-14	Sep-14
Begin Right of Way	Sep-02	Mar-07	Mar-03	Oct-02	Dec-04	Jul-13	Apr-14	Feb-14
End Right of Way	Jan-06	Feb-11	Jul-04	Dec-05	Jan-09	Apr-15	Apr-15	Oct-15
Begin Construction	Jul-06	Dec-04	Jun-04	Nov-06	Nov-08	Jul-15	Sep-15	Jan-16
End Construction	Aug-08	Apr-08	Jul-08	Dec-10	Mar-10	Nov-17	Sep-17	May-18
Begin Closeout	Jan-10	Apr-09	Oct-10	Feb-12	May-10	Nov-17	Sep-17	May-18
End Closeout	Aug-10	May-10	Sep-11	Jun-12	Dec-14	May-18	Nov-17	Nov-18

Program Overall Funding Plan

( DOLLARS IN THOUSANDS )

FUND SOURCE	TOTAL	Totals by Fiscal Year						Totals by Project Phase			
		Prior	13/14	14/15	15/16	16/17	17/18	PA&ED	PS&E	R/W	CONST
<b>State Bond - Trade Corridors Improvement Fund (TCIF)</b>											
Current Approved	336,600	336,600									336,600
Change	-68,662	-68,662									-68,662
Proposed	267,938	267,938									267,938
<b>Existing Federal Appropriations</b>											
Current Approved	47,255	47,255	0						15,059	8,184	24,012
Change	13,675	5,449	8,226						5,839	7,836	0
Proposed	60,930	52,704	8,226						20,898	16,020	24,012
<b>MTA Committed Funds</b>											
Current Approved	149,411	120,922	28,489	0	0				19,124	11,759	118,528
Change	18,119	-6,065	12,396	10,000	1,788				-8,319	59,116	-32,678
Proposed	167,530	114,857	40,885	10,000	1,788				10,805	70,875	85,850
<b>PUC Section 190</b>											
Current Approved	35,000	25,000	10,000								35,000
Change	-30,000	-20,000	-10,000								-30,000
Proposed	5,000	5,000	0								5,000
<b>Railroad Contribution</b>											
Current Approved	23,506	23,506									23,506
Change	-13,008	-13,008									-13,008
Proposed	10,498	10,498									10,498
<b>Measure R</b>											
Current Approved	176,844	70,000	106,844	0	0	0	0		15,075	86,016	75,753
Change	41,265	-28,000	-78,293	35,981	31,864	50,033	29,680		13,050	2,255	25,960
Proposed	218,109	42,000	28,551	35,981	31,864	50,033	29,680		28,125	88,271	101,713
<b>Cities</b>											
Current Approved	2,185	2,185								2,185	
Change	0	0								0	
Proposed	2,185	2,185								2,185	
<b>TOTAL</b>											
Current Approved	770,801	625,468	145,333	0	0	0	0		49,258	108,144	613,399
Change	-38,611	-130,286	-67,671	45,981	33,652	50,033	29,680		10,570	69,207	-118,388
Proposed	732,190	495,182	77,662	45,981	33,652	50,033	29,680		59,828	177,351	495,011

CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

Reference No.: 2.1c.(5)  
August 6, 2013  
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San Gabriel Trench Project – Phase I

Reference Allocation Amendment: TCIF-AA-1213-03, 08/22/12, reduced allocation to \$4,000,000

<b>(DOLLARS IN THOUSANDS)</b>										
FUND SOURCE	TOTAL	Totals by Fiscal Year					Totals by Project Phase			
		Prior	10/11	11/12	12/13	13/14	PA&ED	PS&E	R/W	CONST
<b>State Bond - Trade Corridors Improvement Fund (TCIF)</b>										
Current Approved	8,000			8,000					4,000	4,000
Change	-4,000			-4,000					-4,000	0
Proposed	4,000			4,000					0	4,000
<b>TOTAL</b>										
Current Approved	8,000			8,000					4,000	4,000
Change	-4,000			-4,000					-4,000	0
Proposed	4,000			4,000					0	4,000

San Gabriel Trench Project – Phase II

Reference Allocation Amendment: TCIF-AA-1213-04, 08/22/12, revised allocation to \$332,600,000

<b>(DOLLARS IN THOUSANDS)</b>										
FUND SOURCE	TOTAL	Totals by Fiscal Year					Totals by Project Phase			
		Prior	10/11	11/12	12/13	13/14	PA&ED	PS&E	R/W	CONST
<b>State Bond - Trade Corridors Improvement Fund (TCIF)</b>										
Current Approved	332,600			332,600						332,600
Change	-68,662			-68,662						-68,662
Proposed	263,938			263,938						263,938
<b>Existing Federal Appropriations</b>										
Current Approved	14,414	12,105	466	1,343	500			13,914	500	
Change	0	0	0	0	0			0	0	
Proposed	14,414	12,105	466	1,343	500			13,914	500	
<b>MTA Committed Funds</b>										
Current Approved	4,398	3,033		1,365				4,398	0	
Change	5,557	6,922		-1,365				2	5,555	
Proposed	9,955	9,955		0				4,400	5,555	
<b>PUC Section 190</b>										
Current Approved	20,000				20,000					20,000
Change	-20,000				-20,000					-20,000
Proposed	0				0					0
<b>Railroad Contribution</b>										
Current Approved	8,600			8,600						8,600
Change	-8,600			-8,600						-8,600
Proposed	0			0						0
<b>Measure R</b>										
Current Approved	38,000	8,823	15,251	13,926				15,075	22,925	
Change	4,000	33,177	-15,251	-13,926				69	3,931	
Proposed	42,000	42,000	0	0				15,144	26,856	
<b>Cities</b>										
Current Approved	2,185	2,185							2,185	
Change	0	0							0	
Proposed	2,185	2,185							2,185	
<b>TOTAL</b>										
Current Approved	420,197	26,146	15,717	357,834	20,500			33,387	25,610	361,200
Change	-87,705	40,099	-15,251	-92,553	-20,000			71	9,486	-97,262
Proposed	332,492	66,245	466	265,281	500			33,458	35,096	263,938

Brea Canyon Grade Separation

<b>(DOLLARS IN THOUSANDS)</b>										
<b>FUND SOURCE</b>	<b>TOTAL</b>	<b>Totals by Fiscal Year</b>					<b>Totals by Project Phase</b>			
		<b>Prior</b>	<b>10/11</b>	<b>11/12</b>	<b>12/13</b>	<b>13/14</b>	<b>PA&amp;ED</b>	<b>PS&amp;E</b>	<b>R/W</b>	<b>CONST</b>
<b>Existing Federal Appropriations – SAFETEA-LU</b>										
Current Approved	5,521	4,998	523						5,521	
Change	0	0	0						0	
Proposed	5,521	4,998	523						5,521	
<b>MTA Committed Funds</b>										
Current Approved	26,397	26,397						538	4,187	21,672
Change	0	0						0	0	0
Proposed	26,397	26,397						538	4,187	21,672
<b>PUC Section 190</b>										
Current Approved	5,000	5,000								5,000
Change	0	0								0
Proposed	5,000	5,000								5,000
<b>Railroad Contribution</b>										
Current Approved	2,004	2,004								2,004
Change	0	0								0
Proposed	2,004	2,004								2,004
<b>Total</b>										
Current Approved	38,922	38,399	523					538	9,708	28,676
Change	0	0	0					0	0	0
Proposed	38,922	38,399	523					538	9,708	28,676

Fairway Drive Grade Separation

<b>(DOLLARS IN THOUSANDS)</b>										
<b>FUND SOURCE</b>	<b>TOTAL</b>	<b>Totals by Fiscal Year</b>					<b>Totals by Project Phase</b>			
		<b>Prior</b>	<b>10/11</b>	<b>11/12</b>	<b>12/13</b>	<b>13/14</b>	<b>PA&amp;ED</b>	<b>PS&amp;E</b>	<b>R/W</b>	<b>CONST</b>
<b>Existing Federal Appropriations</b>										
Current Approved	240		240					240		
Change	-240		-240					-240		
Proposed	0		0					0		
<b>MTA Committed Funds</b>										
Current Approved	19,691		53	5,582	1,905	12,151		7,540		12,151
Change	-19,691		-53	-5,582	-1,905	-12,151		-7,540		-12,151
Proposed	0		0	0	0	0		0		0
<b>PUC Section 190</b>										
Current Approved	5,000					5,000				5,000
Change	-5,000					-5,000				-5,000
Proposed	0					0				0
<b>Measure R</b>										
Current Approved	80,434				12,000	68,434			35,434	45,000
Change	-80,434				-12,000	-68,434			-35,434	-45,000
Proposed	0				0	0			0	0
<b>TOTAL</b>										
Current Approved	105,365		293	5,582	13,905	85,585		7,780	35,434	62,151
Change	-105,365		-293	-5,582	-13,905	-85,585		-7,780	-35,434	-62,151
Proposed	0		0	0	0	0		0	0	0

Puente Avenue Grade Separation

<b>(DOLLARS IN THOUSANDS)</b>										
<b>FUND SOURCE</b>	<b>TOTAL</b>	<b>Totals by Fiscal Year</b>					<b>Totals by Project Phase</b>			
		<b>Prior</b>	<b>10/11</b>	<b>11/12</b>	<b>12/13</b>	<b>13/14</b>	<b>PA&amp;ED</b>	<b>PS&amp;E</b>	<b>R/W</b>	<b>CONST</b>
<b>Existing Federal Appropriations</b>										
Current Approved	905		216	689				905		
Change	-905		-216	-689				-905		
Proposed	0		0	0				0		
<b>MTA Committed Funds</b>										
Current Approved	22,073		29	4,941	765	16,338		5,735		16,338
Change	-22,073		-29	-4,941	-765	-16,338		-5,735		-16,338
Proposed	0		0	0	0	0		0		0
<b>PUC Section 190</b>										
Current Approved	5,000					5,000				5,000
Change	-5,000					-5,000				-5,000
Proposed	0					0				0
<b>Measure R</b>										
Current Approved	58,410				20,000	38,410			27,657	30,753
Change	-58,410				-20,000	-38,410			-27,657	-30,753
Proposed	0				0	0			0	0
<b>TOTAL</b>										
Current Approved	86,388		245	5,630	20,765	59,748		6,640	27,657	52,091
Change	-86,388		-245	-5,630	-20,765	-59,748		-6,640	-27,657	-52,091
Proposed	0		0	0	0	0		0	0	0

Ramona Boulevard Grade Separation

<b>(DOLLARS IN THOUSANDS)</b>										
<b>FUND SOURCE</b>	<b>TOTAL</b>	<b>Totals by Fiscal Year</b>					<b>Totals by Project Phase</b>			
		<b>Prior</b>	<b>10/11</b>	<b>11/12</b>	<b>12/13</b>	<b>13/14</b>	<b>PA&amp;ED</b>	<b>PS&amp;E</b>	<b>R/W</b>	<b>CONST</b>
<b>Existing Federal Appropriations – TEA-21 / STP</b>										
Current Approved	8,680	8,289	391							8,680
Change	0	0	0							0
Proposed	8,680	8,289	391							8,680
<b>MTA Committed Funds</b>										
Current Approved	5,303	5,303						34	2,959	2,310
Change	0	0						0	0	0
Proposed	5,303	5,303						34	2,959	2,310
<b>Railroad Contribution</b>										
Current Approved	982	882	100							982
Change	0	0	0							0
Proposed	982	882	100							982
<b>TOTAL</b>										
Current Approved	14,965	14,474	491					34	2,959	11,972
Change	0	0	0					0	0	0
Proposed	14,965	14,474	491					34	2,959	11,972

Reservoir Street Grade Separation

<b>(DOLLARS IN THOUSANDS)</b>										
FUND SOURCE	TOTAL	Totals by Fiscal Year					Totals by Project Phase			
		Prior	10/11	11/12	12/13	13/14	PA&ED	PS&E	R/W	CONST
<b>MTA Committed Funds</b>										
Current Approved	11,127	11,109	18						1,125	10,002
Change	0	0	0						0	0
Proposed	11,127	11,109	18						1,125	10,002
<b>Railroad Contribution</b>										
Current Approved	1,353	1,353								1,353
Change	0	0								0
Proposed	1,353	1,353								1,353
<b>TOTAL</b>										
Current Approved	12,480	12,462	18						1,125	11,355
Change	0	0	0						0	0
Proposed	12,480	12,462	18						1,125	11,355

Sunset Avenue Grade Separation

<b>(DOLLARS IN THOUSANDS)</b>										
FUND SOURCE	TOTAL	Totals by Fiscal Year					Totals by Project Phase			
		Prior	10/11	11/12	12/13	13/14	PA&ED	PS&E	R/W	CONST
<b>Existing Federal Appropriations – TEA-21/SAFETEA-LU</b>										
Current Approved	2,163	1,757	11	395					2,163	
Change	-390	16	-11	-395					-390	
Proposed	1,773	1,773	0	0					1,773	
<b>MTA Committed Funds</b>										
Current Approved	37,032	24,216	7,529	5,287	0			339	565	36,128
Change	-3,597	9,150	-7,529	-5,287	69			0	888	-4,485
Proposed	33,435	33,366	0	0	69			339	1,453	31,643
<b>TOTAL</b>										
Current Approved	39,195	25,973	7,540	5,682	0			339	2,728	36,128
Change	-3,987	9,166	-7,540	-5,682	69			0	498	-4,485
Proposed	35,208	35,139	0	0	69			339	3,226	31,643

Temple Avenue Train Diversion

<b>(DOLLARS IN THOUSANDS)</b>										
<b>FUND SOURCE</b>	<b>TOTAL</b>	<b>Totals by Fiscal Year</b>					<b>Totals by Project Phase</b>			
		<b>Prior</b>	<b>10/11</b>	<b>11/12</b>	<b>12/13</b>	<b>13/14</b>	<b>PA&amp;ED</b>	<b>PS&amp;E</b>	<b>R/W</b>	<b>CONST</b>
<b>Existing Federal Appropriations</b>										
Current Approved	15,332	15,332		0						15,332
Change	0	0		0						0
Proposed	15,332	15,332		0						15,332
<b>MTA Committed Funds</b>										
Current Approved	23,390	21,744	1,646		0			540	2,923	19,927
Change	296	1,699	-1,646		243			0	0	296
Proposed	23,686	23,443	0		243			540	2,923	20,223
<b>Railroad Contribution</b>										
Current Approved	10,567	5,283		3,950	1,334					10,567
Change	-4,408	876		-3,950	-1,334					-4,408
Proposed	6,159	6,159		0	0					6,159
<b>TOTAL</b>										
Current Approved	49,289	42,359	1,646	3,950	1,334			540	2,923	45,826
Change	-4,112	2,575	-1,646	-3,950	-1,091			0	0	-4,112
Proposed	45,177	44,934	0	0	243			540	2,923	41,714

Hamilton Boulevard Grade Separation

<b>( DOLLARS IN THOUSANDS )</b>											
<b>FUND SOURCE</b>	<b>TOTAL</b>	<b>Totals by Fiscal Year</b>						<b>Totals by Project Phase</b>			
		<b>Prior</b>	<b>13/14</b>	<b>14/15</b>	<b>15/16</b>	<b>16/17</b>	<b>17/18</b>	<b>PA&amp;ED</b>	<b>PS&amp;E</b>	<b>R/W</b>	<b>CONST</b>
<b>MTA Committed Funds</b>											
Current Approved	0	0	0	0	0			0	0		
Change	18,288	1,500	5,000	10,000	1,788			1,500	16,788		
Proposed	18,288	1,500	5,000	10,000	1,788			1,500	16,788		
<b>Measure R</b>											
Current Approved	0		0	0	0	0	0		0	0	
Change	58,038		4,551	2,375	8,000	24,000	19,112		5,375	1,551	
Proposed	58,038		4,551	2,375	8,000	24,000	19,112		5,375	1,551	
<b>TOTAL</b>											
Current Approved	0	0	0	0	0	0	0		0	0	
Change	76,326	1,500	9,551	12,375	9,788	24,000	19,112		6,875	18,339	
Proposed	76,326	1,500	9,551	12,375	9,788	24,000	19,112		6,875	18,339	

Fullerton Road Grade Separation

<b>( DOLLARS IN THOUSANDS )</b>											
FUND SOURCE	TOTAL	Totals by Fiscal Year						Totals by Project Phase			
		Prior	13/14	14/15	15/16	16/17	17/18	PA&ED	PS&E	R/W	CONST
<b>MTA Committed Funds</b>											
Current Approved	0	0	0						0	0	
Change	7,549	1,700	5,849						1,700	5,849	
Proposed	7,549	1,700	5,849						1,700	5,849	
<b>Measure R</b>											
Current Approved	0		0	0	0	0			0	0	0
Change	91,503		24,000	33,606	19,864	14,033			7,606	59,864	24,033
Proposed	91,503		24,000	33,606	19,864	14,033			7,606	59,864	24,033
<b>TOTAL</b>											
Current Approved	0	0	0	0	0	0			0	0	0
Change	99,052	1,700	29,849	33,606	19,864	14,033			9,306	65,713	24,033
Proposed	99,052	1,700	29,849	33,606	19,864	14,033			9,306	65,713	24,033

Durfee Avenue Grade Separation

<b>( DOLLARS IN THOUSANDS )</b>											
FUND SOURCE	TOTAL	Totals by Fiscal Year						Totals by Project Phase			
		Prior	13/14	14/15	15/16	16/17	17/18	PA&ED	PS&E	R/W	CONST
<b>Existing Federal Appropriations</b>											
Current Approved	0	0	0						0	0	
Change	15,210	6,984	8,226						6,984	8,226	
Proposed	15,210	6,984	8,226						6,984	8,226	
<b>MTA Committed Funds</b>											
Current Approved	0	0	0						0	0	
Change	31,790	1,754	30,036						1,754	30,036	
Proposed	31,790	1,754	30,036						1,754	30,036	
<b>Measure R</b>											
Current Approved	0				0	0	0				0
Change	26,568				4,000	12,000	10,568				26,568
Proposed	26,568				4,000	12,000	10,568				26,568
<b>TOTAL</b>											
Current Approved	0	0	0		0	0	0		0	0	0
Change	73,568	8,738	38,262		4,000	12,000	10,568		8,738	38,262	26,568
Proposed	73,568	8,738	38,262		4,000	12,000	10,568		8,738	38,262	26,568

**RESOLUTION TCIF-P-1314-01**

Be it Resolved, that the California Transportation Commission does hereby amend the Trade Corridors Improvement Fund Baseline Agreement for Project 15, San Gabriel Valley Grade Separation Program (PPNO TC15), in accordance with the changes described and illustrated above.



**Metro**

June 12, 2013

Mr. Andre Boutros  
Executive Director  
California Transportation Commission  
1120 "N" Street, Mail Stop: 52  
Sacramento, CA 94273-0001

Dear Mr. Boutros:

The Southern California Consensus Group has discussed and approved the changes requested by our partner agency. It is our understanding that the Alameda Corridor-East Construction Authority (ACE) is requesting to seek funding of \$104,000,000 in Trade Corridor Improvement Funds (TCIF) for two new projects.

- Puente Avenue Grade Separation - \$48,000,000
- Fairway Drive Grade Separation - \$56,000,000

Please see the attached letter detailing the requested changes. Please direct any questions or comments regarding this issue to me at (213) 922-3061. We appreciate the ongoing support and guidance provided by you and your staff.

Sincerely,

Shahrzad Amiri  
Deputy Executive Officer

cc: Southern California Consensus Group  
Stephen Maller

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5b.(1)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR SHOPP PROJECTS**  
**RESOLUTION FP-13-01**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission allocate \$117,422,000 for 27 projects programmed in the 2012 State Highway Operation and Protection Program (SHOPP) and \$12,076,000 for five additional projects amended into the SHOPP by Department action.

## **ISSUE:**

The attached vote list describes 32 SHOPP projects totaling \$129,498,000. The Department is ready to proceed with these projects and is requesting an allocation at this time.

## **FINANCIAL RESOLUTION:**

Resolved, that \$129,498,000 be allocated from the Budget Act of 2012, Budget Act Items 2660-302-0042 and 2660-302-0890, for 32 SHOPP projects described on the attached vote list.

The Department has complied with the National Environmental Policy Act and the California Environmental Quality Act requirements in preparing these projects.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID AdvPhase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(1) SHOPP Projects</b>				
<b>Resolution FP-13-01</b>				
1 \$5,000,000 Mendocino 01-Men-128 14.3/40.6	Near Boonville, from west of Mill Creek Bridge to east of Beebe Creek Bridge. <u>Outcome/Output:</u> Rehabilitate existing culverts, replace deteriorated culverts and place standard drainage inlet and outlet structures at 51 locations to improve drainage.	01-0200 SHOPP/12-13 \$5,000,000 0100000136 4 378164	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.151	\$100,000 \$4,900,000
2 \$4,500,000 Mendocino 01-Men-128 34.5/35.5	Near Boonville, from Shearing Creek Bridge to 0.7 mile west of Maple Creek Bridge. <u>Outcome/Output:</u> Stabilize embankment, install cast-in-place steel reinforced ground anchor wall system and rock slope protection (RSP) damaged by heavy rainfall.	f 01-4463 SHOPP/12-13 \$10,329,000 0100000351 4 476604	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.031	\$90,000 \$4,410,000
3 \$4,700,000 Humboldt 01-Hum-169 13.6/23.4	Near Redwood National Park, from 0.8 mile west of Peewan Creek Bridge to 1.0 mile east of Cappell Creek Bridge at various locations. <u>Outcome/Output:</u> Widen roadway and install metal beam guardrail at eight locations to reduce the potential for collisions and reduce the severity of run-off-road collisions.	01-2028 SHOPP/12-13 \$5,636,000 0100000219 4 450904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$94,000 \$4,606,000
4 \$27,314,000 Placer 03-Pla-80 8.1/37.8	In and Near Loomis, at various locations from Brace Road to Margra Road. <u>Outcome/Output:</u> Raise six overcrossing structures and replace roadway structural section beneath one underpass and two overcrossing structures to meet vertical clearance requirements for permit vehicles	03-5095 SHOPP/12-13 \$27,134,000 0300000473 4 3E1004	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.322	\$546,000 \$26,768,000
5 \$7,996,000 Alameda 04-Ala-580 46.0/46.5	In Oakland, at MacArthur Boulevard Bridge No. 33-0061L/R. <u>Outcome/Output:</u> Remove unsound concrete and patch with rapid set concrete, construct polyester concrete overlay and replace joint seal assemblies to maintain structural integrity, and reduce the risk to lives and properties.	04-0159N SHOPP/12-13 \$49,290,000 0412000346 4 1A6714	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.110	\$160,000 \$7,836,000
6 \$475,000 Marin 04-Mrn-1 0.9/1.0	Near the city of Marin, from Ross Drive to Tennessee Avenue. <u>Outcome/Output:</u> Replace corrugated metal pipe culverts with plastic pipe culverts, construct new drainage inlets, fill-in existing drainage ditch and replace with paved curb and gutter with drainage inlets and wing-wall damaged by heavy rainfall.	04-0312L SHOPP/12-13 \$880,000 0400020144 4 4S7704	2012-13 302-0042 SHA 20.20.201.131	\$475,000
7 \$1,150,000 Marin 04-Mrn-1 31.2	Near Point Reyes Station, at Petaluma Road. <u>Outcome/Output:</u> Stabilize roadway embankment and construct a continuous tangent cast in drilled hope pile wall to prevent further erosion problems to the embankment as a result of high-water rain events during the winter rainy seasons.	04-0322C SHOPP/12-13 \$1,100,000 0400001238 4 4S4504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$23,000 \$1,127,000
8 \$2,102,000 San Francisco 04-SF-Var. Var.	In San Francisco and Santa Clara Counties on various routes and various locations. <u>Outcome/Output:</u> Construct curb ramps and island passageways to meet the Americans with Disabilities Act (ADA) requirements.	04-0273M SHOPP/12-13 \$11,619,000 0400001152 4 4A6304	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.361	\$42,000 \$2,060,000
9 \$6,994,000 San Francisco 04-SF-280 R5.2/R6.0	In San Francisco, at Southern Freeway Viaduct Bridge No. 34-0046. <u>Outcome/Output:</u> Replace deteriorating hinges to maintain structural integrity, and reduce the risk to lives and properties.	04-0609K SHOPP/12-13 \$9,004,000 0400001138 4 4A5104	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.110	\$140,000 \$6,854,000

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prm'd Amount Project ID AdvPhase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(1) SHOPP Projects</b>		<b>Resolution FP-13-01</b>		
10 \$1,939,000 Santa Clara 04-SCI-9 4.2	Near Saratoga, 0.7 mile west of Sanborn Road. <u>Outcome/Output:</u> Stabilize slope, construct a tie-back retaining wall and install new drainage system damaged by heavy rainfall.	04-0392C SHOPP/12-13 \$2,780,000 0400001202 4 4S0504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$39,000 \$1,900,000
11 \$2,936,000 Solano 04-Sol-12 22.7/23.7	Near Rio Vista, at Currie, McCloskey and Azevedo Roads; also from Azevedo Road to Liberty Island Road. <u>Outcome/Output:</u> Construct left turn pockets and widen shoulders to reduce collisions and minimize collision severity involving fixed objects and provide a clear recovery zone off the traveled way.  Additional contributions: \$8,000,000 Office of Traffic Safety (OTS) federal grant.	04-8085A SHOPP/12-13 \$9,116,000 0400000832 4 2A6204	2012-13 302-0890 FTF 20.20.201.015	\$2,936,000
12 \$10,578,000 Sonoma 04-Son-12 9.6	In Sebastopol, at the Laguna de Santa Rosa Bridge (No. 20-0035). <u>Outcome/Output:</u> Replace one bridge that is rapidly deteriorating to maintain structural integrity, reduce the risk to lives and properties, and to comply with the Bridge Inspection Report recommendation.	04-0756F SHOPP/12-13 \$11,167,000 0400000482 4 1A2904	2012-13 302-0890 FTF 20.20.201.111	\$10,578,000
13 \$17,240,000 Santa Barbara 05-SB-101 22.3/23.0	In Goleta from 0.2 mile east to 0.7 mile west of the Fairview Avenue Overcrossing. <u>Outcome/Output:</u> Replace existing concrete culverts with bridges so as to increase the hydraulic flow capacity for two creeks and to minimize the possibility of future roadway flooding.	05-0707 SHOPP/12-13 \$17,169,000 0500000055 4 0G0704	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.150	\$345,000 \$16,895,000
14 \$1,077,000 Kern 06-Ker-119 Var	In Kern, Fresno, and Tulare Counties on Routes 33, 46, 63, 119, 216, and 223 at various locations. <u>Outcome/Output:</u> Extend culverts to outside highway clear recovery zone and upgrade guardrail at six different locations to reduce the frequency and severity of traffic collisions.	06-6462 SHOPP/12-13 \$2,097,000 0600000206 4 0J9304	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$22,000 \$1,055,000
15 \$319,000 Tulare 06-Tul-245 20.4	Near Badger, at Cottonwood Creek. <u>Outcome/Output:</u> Place rock slope protection at one location to stop and prevent further erosion of the embankment supporting the roadway.	06-6569 SHOPP/12-13 \$406,000 0600020698 4 0N3904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$37,000 \$282,000
16 \$2,678,000 Los Angeles 07-LA-1 8.5/8.8	In the city of Los Angeles, near Wilmington, at 0.1 mile west of the Dominguez Channel Bridge. <u>Outcome/Output:</u> Replace retaining wall system to restore storm damage.	07-4437 SHOPP/12-13 \$3,500,000 0712000064 4 3X3904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$54,000 \$2,624,000
17 \$1,500,000 Los Angeles 07-LA-5 14.9/16.8	In the city of Los Angeles, from Indiana Street to Boyle Avenue. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and inert material to reduce erosion and stabilize slopes over six acres of treatment area.	07-4060 SHOPP/12-13 \$2,033,000 0700000506 4 272404	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$30,000 \$1,470,000
18 \$1,000,000 Los Angeles 07-LA-5 25.2/27.0	In the city of Los Angeles, near Glendale, from 0.5 mile south of the Colorado Freeway Extension to Zoo Drive. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and other storm water quality measures to reduce erosion and stabilize slopes over 13 acres of treatment area.  (EA 27750, PPNO 4197 combined with EA 27760, PPNO 4198 and with EA 27770, PPNO 4199 for construction under EA 2777U, Project ID 0712000151)	07-4197 SHOPP/12-13 \$1,100,000 0700000533 4 277504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$20,000 \$980,000

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID AdvPhase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(1) SHOPP Projects</b>		<b>Resolution FP-13-01</b>		
19 \$1,000,000 Los Angeles 07-LA-5 25.2/27.0	In the cities of Los Angeles and Glendale, northbound from Zoo Drive to Route 134, also westbound Route 134 from the Los Angeles River to Forest Lawn Drive. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and other storm water quality measures to reduce erosion and stabilize slopes over 13 acres of treatment area.  (EA 27760, PPNO 4198 combined with EA 27750, PPNO 4197 and with EA 27770, PPNO 4199 for construction under EA 2777U, Project ID 0712000151)	07-4198 SHOPP/12-13 \$1,200,000 0700000534 4 277604	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$20,000 \$980,000
20 \$1,100,000 Los Angeles 07-LA-5 25.2/27.0	In the cities of Los Angeles and Glendale, southbound from Zoo Drive to Route 134, also eastbound Route 134 from Los Angeles River to Forest Lawn Drive. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and other storm water quality measures to reduce erosion and stabilize slopes over 13 acres of treatment area  (EA 27770, PPNO 4199 combined with EA 27750, PPNO 4197 and with EA 27760, PPNO 4198 for construction under EA 2777U, Project ID 0712000151)	07-4199 SHOPP/12-13 \$1,200,000 0700000535 4 277704	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$22,000 \$1,078,000
21 \$1,928,000 Los Angeles 07-LA-5 R54.4/R54.8	In Santa Clarita, from Rye Canyon Road to Route 126 at the Castaic Truck Inspection Facility. <u>Outcome/Output:</u> Upgrade weigh station by replacing signs, CCTV, public address system, electrical equipment, weight and height gage equipment, signal lights, parking lot lights, and computer room cooling system. Upgrade plumbing, replace leaking roofs, replace damaged concrete driveway and apron slabs, and reconstruct asphalt shoulders.	07-4219 SHOPP/12-13 \$1,931,000 0700020197 4 278804	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.321	\$39,000 \$1,889,000
22 \$2,211,000 Los Angeles 07-LA-60 20.6	Near Industry and Rowland Heights, at the westbound off ramp to Nogales Street. <u>Outcome/Output:</u> Widen westbound off-ramp from two lanes to three lanes to improve traffic operations and reduce congestion.	07-3592 SHOPP/12-13 \$2,606,000 0700021079 4 4H9004	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.310	\$44,000 \$2,167,000
23 \$950,000 Los Angeles 07-LA-110 23.5/23.9	In the city of Los Angeles, at the northbound interchange with Route 101. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and other storm water quality measures to reduce erosion and stabilize slopes over 4.4 acres of treatment area.  (EA 27490, PPNO 4151 combined with EA 27590, PPNO 4172 for construction under EA 2759U, Project ID 0713000205)	07-4151 SHOPP/12-13 \$1,250,000 0700000517 4 274904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$19,000 \$931,000
24 \$1,000,000 Los Angeles 07-LA-110 23.5/23.9	In the city of Los Angeles, at the southbound interchange with Route 101. <u>Outcome/Output:</u> Install plants, mulch, irrigation system, and other storm water quality measures to reduce erosion and stabilize slopes over 4.4 acres of treatment area.  (EA 27590, PPNO 4172 combined with EA 27490, PPNO 4151 for construction under EA 2759U, Project ID 0713000205)	07-4172 SHOPP/12-13 \$1,300,000 0700000524 4 275904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$20,000 \$980,000
25 \$4,389,000 Riverside 08-Riv-111 1.5	At 1.5 miles north of the Imperial County line near the Salton Sea, at Salton Creek Bridge No. 56-0236. <u>Outcome/Output:</u> Replace bridge.	08-0086A SHOPP/12-13 \$7,398,000 0800000714 4 449104	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.112	\$88,000 \$4,301,000
26 \$486,000 San Bernardino 08-SBd-40 R18.0	Near Newberry Springs, 0.4 mile west of the National Trails Highway at Crest Wash Bridge No.54-0717L/R. <u>Outcome/Output:</u> Regrade streambed and place rock slope protection for both right and left bridges.	08-0207H SHOPP/12-13 \$489,000 0800020179 4 435414	2012-13 302-0042 SHA 20.20.201.119	\$486,000

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID AdvPhase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(1) SHOPP Projects</b>		<b>Resolution FP-13-01</b>		
27 \$4,860,000	In the cities of Orange and Anaheim, at Santiago Boulevard and Nohl Ranch Road. <u>Outcome/Output:</u> Improve slope stability by removing soil from top of the slope.	12-3577A SHOPP/12-13 \$4,780,000 120000188 4 0H2084	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$97,000 \$4,763,000
Orange 12-Ora-55 17.0/R17.7				

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(1) Projects Amended into the SHOPP by Department Action</b>		<b>Resolution FP-13-01</b>		
28 \$1,300,000	Near Auburn and Colfax, on Routes 49, 80, and 193 at various locations. <u>Outcome/Output:</u> Replace metal beam guardrail with concrete barrier at 9 locations to reduce repair costs and exposure to traffic of maintenance personnel.	03-4783 SHOPP/12-13 \$1,680,000 0312000283 4 3F4104	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$26,000 \$1,274,000
Placer 03-Pla-80 0.9/1.0				
29 \$7,000,000	In La Mirada, Santa Fe Springs, Norwalk, and Downey, from Orange County line to 0.7 mile north of Route 605. <u>Outcome/Output:</u> Replace median barrier, signs, lighting, and ramp meters, and improve drainage.	07-4156 SHOPP/12-13 \$7,000,000 0700001835 4 215954	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.121	\$140,000 \$6,860,000
Los Angeles 07-LA-5 0.0/7.6				
30 \$308,000	In Long Beach and Signal Hill, from Atlantic Avenue to Wardlow Road. <u>Outcome/Output:</u> Construct or upgrade pedestrian facilities at 20 locations to comply with Americans with Disabilities Act (ADA) standards.	07-4561 SHOPP/12-13 \$400,000 0700021239 4 1W3204	2012-13 302-0042 SHA 20.20.201.378	\$308,000
Los Angeles 07-LA-405 6.0/6.5				
31 \$3,000,000	At various locations in Long Beach, Compton, Paramount and Lynwood, from 0.1 mile south of Route 1 to 0.2 mile north of Route 105. <u>Outcome/Output:</u> Construct sand filters, infiltration basins and other Best Management Practices (BMPs) to remove storm water pollutants and meet permit requirements issued by the regulatory agencies.	07-3868A SHOPP/12-13 \$3,000,000 0713000045 4 259014	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.335	\$60,000 \$2,940,000
Los Angeles 07-LA-710 6.7/R15.9				
32 \$468,000	Near Lake Elsinore, east of El Cariso Road. <u>Outcome/Output:</u> Install metal beam guardrail at steep embankment areas to enhance safety and reduce the number and severity of collisions.	08-0050K SHOPP/12-13 \$566,000 0800020127 4 0M4204	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$11,000 \$457,000
Riverside 08-Riv-74 6.9/10.2				

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5b.(2)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR SHOPP PROJECTS**  
**RESOLUTION FP-13-02**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission allocate \$39,792,000 for nine projects programmed in the 2012 State Highway Operation and Protection Program (SHOPP) and \$14,000,000 for two additional projects amended into the SHOPP by Department action.

## **ISSUE:**

The attached vote list describes 11 SHOPP projects totaling \$53,792,000. The Department is ready to proceed with these projects and is requesting an allocation at this time.

## **FINANCIAL RESOLUTION:**

Resolved, that \$53,792,000 be allocated from the Budget Act of 2012, Budget Act Items 2660-302-0042 and 2660-302-0890, for 11 SHOPP projects described on the attached vote list.

The Department has complied with the National Environmental Policy Act and the California Environmental Quality Act requirements in preparing these projects.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID AdvPhase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(2) SHOPP Projects</b>				
<b>Resolution FP-13-02</b>				
1 \$1,525,000 Mendocino 01-Men-101 46.2/R84.6	Near Willits, from 0.3 mile north of Baechtel Creek Bridge to 0.1 mile south of Cummings Road. <u>Outcome/Output:</u> Rehabilitate drainage system components at 33 locations that have reached the end of their useful lives to reduce maintenance costs and personnel exposure to traffic.	01-0181A SHOPP/13-14 \$3,175,000 0100000156 4 402804	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.151	\$30,000 \$1,495,000
2 \$27,000,000 Shasta 02-Sha-299 30.3/40.7	Near Bella Vista, from 0.3 mile east of Intermountain Road to 0.3 mile west of Backbone Ridge Road. <u>Outcome/Output:</u> Rehabilitate 22.1 lane miles of roadway to improve the ride quality, prevent further deterioration of the road surface, minimize the costly roadway repairs and extend the pavement service life.	02-3017 SHOPP/13-14 \$27,000,000 0200000262 4 360704	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.120	\$540,000 \$26,460,000
3 \$1,250,000 El Dorado 03-ED-49 31.1	Near Auburn, at the Rattlesnake Bar Road Intersection. <u>Outcome/Output:</u> Construct left-turn lanes to reduce collisions and improve safety to allow through traffic to pass left-turn vehicles.	03-3118 SHOPP/13-14 \$1,250,000 0300020538 4 2F1504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.010	\$25,000 \$1,225,000
4 \$990,000 San Mateo 04-SM-35 13.1	Near Skylonda, at north Bear Gulch Road. <u>Outcome/Output:</u> Stabilize embankment and construct soldier pile wall damaged by heavy rainfall.	04-0636R SHOPP/13-14 \$1,158,000 0412000015 4 3G0204	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$20,000 \$970,000
5 \$1,713,000 Kern 06-Ker-5 10.4/R15.8	Near Grapevine, from 0.2 mile north of Grapevine Undercrossing to 0.3 mile north of Route 99 . <u>Outcome/Output:</u> Repair damaged pavement by replacing broken slabs to maintain traffic operation and safety.	06-6571 SHOPP/13-14 \$1,764,000 0600020697 4 0N3604	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$34,000 \$1,679,000
6 \$4,792,000 Riverside 08-Riv-10 R57.6/R60.9	In and near Coachella, from Route 86S to 0.5 mile east of the Coachella Canal. <u>Outcome/Output:</u> Provide preventative rehabilitation treatments to 13.2 lane miles and construct two ramp terminal sections. Project will extend pavement service life and ride quality.	08-0015K SHOPP/13-14 \$6,778,000 0800000337 4 0K2304	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.121	\$96,000 \$4,696,000
7 \$668,000 San Bernardino 08-SBd-18 53.5	Near Big Bear City, at Big Tree Drive. <u>Outcome/Output:</u> Improve safety by constructing left-turn lanes in both directions and widen westbound shoulder to reduce the number and severity of traffic collisions.	08-0188C SHOPP/13-14 \$803,000 0800000278 4 0J0104	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.010	\$26,000 \$642,000
8 \$1,178,000 San Joaquin 10-SJ-12 R4.4	Near Terminous, at Little Potato Slough Bridge, and near Manteca, at the westbound Route 120 to northbound Route 5 Overhead Connector. <u>Outcome/Output:</u> Replace bridge bearing pads and joint seals to maintain bridge operation and extend bridge service life.	10-7364 SHOPP/13-14 \$2,745,000 1000000043 4 0G3504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.110	\$24,000 \$1,154,000
9 \$676,000 San Diego 11-SD-94 32.8/40.3	Near Barrett Junction, at various locations from 1.0 mile west of Cochera via Drive to 1.0 mile west of Potrero Valley Road. <u>Outcome/Output:</u> Enhance safety by reconstructing guardrail and upgrading end treatments to reduce the number and severity of collisions.	11-0659 SHOPP/13-14 \$1,351,000 1100000346 4 287904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$14,000 \$662,000

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(2) Projects Amended into the SHOPP by Department Action</b>		<b>Resolution FP-13-02</b>		
10 \$7,000,000 Fresno 06-Fre-Var Var	In Fresno and Madera Counties, on Routes 5, 41, 99, and 145 at various locations. <u>Outcome/Output</u> : Repair electrical systems damaged by theft and vandalism to restore traffic operations including highway lighting, traffic signals, pumping plants and irrigation.	06-6664 SHOPP/13-14 \$7,000,000 0613000265 4 0Q5404	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$140,000 \$6,860,000
11 \$7,000,000 Fresno 06-Fre-Var Var	In Fresno County, on Routes 168 and 180 at various locations. <u>Outcome/Output</u> : Repair electrical systems damaged by theft and vandalism to restore traffic operations including highway lighting, traffic signals, pumping plants and irrigation.	06-6663 SHOPP/13-14 \$7,000,000 0613000266 4 0Q5504	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.131	\$140,000 \$6,860,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5b.(4)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR SHOPP PROJECTS - ADVANCEMENTS**  
**RESOLUTION FP-13-13**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission allocate \$1,369,000 for one project programmed in the 2012 State Highway Operation and Protection Program (SHOPP) in Fiscal Year (FY) 2014-15.

## **ISSUE:**

The attached vote list describes one SHOPP project for \$1,369,000 programmed in FY 2014-15. The Department is ready to proceed with this project and is requesting an allocation at this time.

## **FINANCIAL RESOLUTION:**

Resolved, that \$1,369,000 be allocated from the Budget Act of 2012, Budget Act Items 2660-302-0042 and 2660-302-0890, for one SHOPP project described on the attached vote list.

The Department has complied with the National Environmental Policy Act and the California Environmental Quality Act requirements in preparing these projects.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(4) SHOPP Projects (ADVANCEMENTS)</b>			<b>Resolution FP-13-13</b>	
1 \$1,369,000 Los Angeles 07-LA-210 R6.8/R7.2	In the city of Los Angeles, from 0.2 mile east of Van Nuys Boulevard to 0.1 mile west of Terra Bella Street. <u>Outcome/Output:</u> Enhance safety by replacing existing metal beam guardrail at eastbound shoulder with concrete barrier to reduce the number and severity of collisions.	07-4495 SHOPP/14-15 \$1,500,000 0712000067 4 290904	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.015	\$27,000 \$1,342,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5b.(3a)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR SHOPP PROJECTS**  
**RESOLUTION FP-13-03**  
**RESOLUTION TCIF-A-1314-01**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission allocate \$73,433,000 for the 04-Alameda-880 - Overcrossing Reconstruction State Highway Operation and Protection Program/Trade Corridors Improvement Fund (SHOPP/TCIF) project in Alameda County programmed in the 2012 SHOPP.

## **ISSUE:**

The attached vote list describes one SHOPP/TCIF project totaling \$73,433,000. The Department is ready to proceed with this project and is requesting an allocation at this time.

## **FINANCIAL RESOLUTION:**

Resolved, that \$62,133,000 be allocated from the Budget Act of 2012, Budget Act Items 2660-302-0042 and 2660-302-0890 for construction and \$11,300,000 for construction engineering for the SHOPP/TCIF project described on the attached vote list.

The Department has complied with the National Environmental Policy Act and the California Environmental Quality Act requirements in preparing these projects.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(3a) Allocation for Multi-Funded Projects Funded with SHOPP and Proposition 1B TCIF</b>			<b>Resolution FP-13-03 Resolution TCIF-A-1314-01</b>	
1 \$73,433,000  Alameda 04-Ala-880 28.4/29.2	<b>I-880 Reconstruction – 29<sup>th</sup> and 23<sup>rd</sup> Avenues Overcrossing.</b> In Oakland, from 0.2 mile south of 29 <sup>th</sup> Avenue Overcrossing to 0.3 mile north of 23 <sup>rd</sup> Avenue Overcrossing. <u>Outcome/Output:</u> Reconstruct Overcrossings, improve on and off ramps, and construct sound walls to improve traffic flow during peak hours and enhance safety. (TCIF Project 4)  (The TCIF/SHOPP allocation is split as follows: \$10,687,000 for construction engineering and \$62,133,000 for construction capital. The SHOPP allocation for construction engineering is \$433,000.)  (SHOPP funded Trade Corridor Improvement Fund (TCIF) project.)	04-0044C SHOPP/12-13 CON ENG \$11,300,000 CONST \$62,133,000 0400000160 4 0A7104	001-0042 SHA  2012-13 302-0042 SHA 302-0890 FTF 20.20.201.310	\$11,300,000   \$1,243,000 \$60,890,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5b.(3b)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR MULTI-FUNDED PROJECTS FUNDED WITH SHOPP  
AND PROPOSITION 1B TCIF  
RESOLUTION FP-13-04  
RESOLUTION TCIF-A-1314-02**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission allocate \$15,000,000 for the 04-Santa Clara-101 - San Benito County Line to Route 85 State Highway Operation and Protection Program/Trade Corridors Improvement Fund (SHOPP/TCIF) project in Santa Clara County programmed in the 2012 SHOPP.

## **ISSUE:**

The attached vote list describes one SHOPP/TCIF project totaling \$15,000,000. The Department is ready to proceed with this project and is requesting an allocation at this time.

## **FINANCIAL RESOLUTION:**

Resolved, that \$15,000,000 be allocated from the Budget Act of 2012, Budget Act Items 2660-302-0042 and 2660-302-0890, for the SHOPP/TCIF project described on the attached vote list.

The Department has complied with the National Environmental Policy Act and the California Environmental Quality Act requirements in preparing these projects.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5b.(3b) Allocation for Multi-Funded Projects Funded with SHOPP and Proposition 1B TCIF</b>			<b>Resolution FP-13-04 Resolution TCIF-A-1314-02</b>	
1 \$15,000,000 Santa Clara 04-SCI-101 26.7	<b>Santa Clara – US 101 Freeway Performance Initiative Project.</b> Near Edenvale, from San Benito County line to Route 85. <u>Outcome/Output:</u> Install ramp metering and traffic operation system to minimize gridlock of the highway system, decrease travel time and improve mobility. (TCIF Project 94)  (SHOPP funded Trade Corridor Improvement Fund (TCIF) project.)	04-0449R SHOPP/12-13 \$18,349,000 0400020304 4 153304	2012-13 302-0042 SHA 302-0890 FTF 20.20.201.315	\$300,000 \$14,700,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5c.(1)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR STATE ADMINISTERED STIP PROJECTS ON THE STATE HIGHWAY SYSTEM RESOLUTION FP-13-06**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission allocate \$29,935,000 for three State administered State Transportation Improvement Program (STIP) projects, on the State Highway System.

## **ISSUE:**

The attached vote list describes three State administered STIP projects on the State Highway System totaling \$29,935,000, plus \$648,000 from other sources. The Department is ready to proceed with these projects and is requesting an allocation at this time.

## **FINANCIAL RESOLUTION:**

Resolved, that \$25,356,000 be allocated from Budget Act of 2012, Budget Act Items 2660-301-0042 and 2660-301-0890 for construction and \$4,579,000 for construction engineering for three State administered STIP projects described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> County Dist-Co-Rte Postmile	Project Title Location Project Description Project Support Expenditures	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(1) State Administered STIP Projects on the State Highway System</b>			<b>Resolution FP-13-06</b>	
1 \$14,900,000 Department of Transportation <u>DNLTC</u> Del Norte 01-DN-199 20.5/25.7	<p>Realignment &amp; Widening at Patrick Creek Narrows. Near Gasquet, on Route 199 at Patrick Creek. Shoulder widening and bridge Replacement.</p> <p>Final Project Development (RIP) Support Estimate: \$4,080,000 Programmed Amount: <u>\$3,512,000</u> Adjustment: \$ 0 (&lt; 20%)</p> <p>Final Right of Way (RIP) Right of Way Estimate: \$ 350,000 Programmed Amount: <u>\$1,346,000</u> Adjustment: \$ 996,000 (Credit)</p> <p>(RIP CON ENG increase because of \$334,000 to come from Del Norte regional shares balance.)</p> <p>(Future Consideration of Funding under Resolution E-13-46; June 2013.)</p> <p>(Time extension for FY 12-13 CON expires on August 31, 2013.)</p> <p>(Contributions from other sources: \$648,000.)</p> <p><u>Outcome/Output:</u> Roadway will be upgraded to STAA Route to accommodate the larger trucks traveling from the Oregon border to Route 101.</p>	<p>01-1047 RIP / 12-13 CON ENG <del>\$1,566,000</del> <b>\$1,900,000</b> CONST \$13,000,000 0100000371 4 479404</p>	<p>001-0042 SHA 001-0890 FTF 2012-13 301-0042 SHA 301-0890 FTF 20.20.075.600</p>	<p>\$38,000 \$1,862,000 \$260,000 \$12,740,000</p>
2 \$1,018,000 Department of Transportation <u>SBCAG</u> Santa Barbara 05-SB-101 83.2/83.8	<p>Union Valley Parkway Interchange Planting. In Santa Maria, from 0.9 mile north of Clark Avenue to 0.7 mile south of Santa Maria Way. Install mitigation landscaping.</p> <p>Final Project Development Support Estimate: \$341,000 Programmed Amount: <u>\$305,000</u> Adjustment: \$0 (&lt; 20%)</p> <p>Final Right of Way Right of Way Estimate: \$5,000 Programmed Amount: <u>\$5,000</u> Adjustment: \$0 (&lt; 20%)</p> <p>(CONST savings of \$32,000 to return to Santa Barbara County regional shares.)</p> <p>(Future Consideration of Funding under Resolution E-12-12; March 2012.)</p> <p><u>Outcome/Output:</u> Install 2.9 acres of mitigation planting.</p>	<p>05-4638Y RIP/13-14 CON ENG \$400,000 CONST <del>\$650,000</del> <b>\$618,000</b> 0512000105 4 46381</p>	<p>001-0042 SHA 2012-13 301-0042 SHA 20.20.075.600</p>	<p>\$400,000 \$618,000</p>

2.5 Highway Financial Matters

Project #	Allocation Amount	Recipient	Project Title	PPNO	Budget Year	Amount by
RTPA/CTC	County	Location	Project Description	Program/Year	Item #	Fund Type
Dist-Co-Rte	Postmile	Project Support Expenditures	Adv Phase	EA	Fund Type	Program Code
<b>2.5c.(1) State Administered STIP Projects on the State Highway System</b>						
						<b>Resolution FP-13-06</b>
3	\$14,017,000	Department of Transportation	12th Avenue Interchange on SR 198. In Hanford at 12th Avenue. Reconstruct interchange.	06-4348	001-0042	\$46,000
KCAG		Final Project Development		RIP/13-14	SHA	
Kings		Support Estimate: \$4,799,000		CON ENG		
06-Kin-198		Programmed Amount: <u>\$3,715,000</u>		\$2,279,000	001-0890	\$2,233,00
R16.5/R17.2		Adjustment: \$1,084,000 (Debit)		CONST	FTF	
				<del>\$15,491,000</del>		
		Final Right of Way		<b>\$11,738,000</b>	2012-13	
		Right of Way Estimate: \$2,297,000		0600000488	301-0042	\$235,000
		Programmed Amount: <u>\$1,608,000</u>		4	SHA	
		Adjustment: \$ 689,000 (Debit)		487504	301-0890	\$11,503,000
					FTF	
					20.20.075.600	
		(CONST saving for \$3,753,000 to return to Kings County regional shares.)				
		(Future Consideration of Funding under Resolution E-10-22; April 2010.)				
		<u>Outcome/Output:</u> Reconstruct interchange to increase capacity, reduce congestion, and improve safety.				

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5c.(3a)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR LOCALLY ADMINISTERED STIP PROJECTS  
OFF THE STATE HIGHWAY SYSTEM  
RESOLUTION FP-13-07**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission allocate \$16,925,000 for 24 locally administered State Transportation Improvement Program (STIP) projects off the State Highway System, as follows:

- o \$4,768,000 for eight STIP projects; and
- o \$7,342,000 for three STIP Transportation Enhancement projects; and
- o \$4,815,000 for 13 STIP Programming, Planning, and Monitoring projects.

## **ISSUE:**

The attached vote list describes 24 locally administered STIP projects off the State Highway System totaling \$16,925,000. The local agencies are ready to proceed with these projects and are requesting an allocation at this time.

## **FINANCIAL RESOLUTION:**

Resolved, that \$16,925,000 be allocated from the Budget Act of 2012, Budget Act Items 2660-101-0042 and 2660-101-0890 for 24 locally administered STIP projects described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(3a) Locally Administered STIP Projects off the State Highway System</b>				<b>Resolution FP-13-07</b>
1 \$10,000 County of Humboldt HCAOG 01-Humboldt	Construction of Traffic Signal System at Fern Street and Walnut Drive. In Cutten, at Walnut Drive and Fern Street. Construct traffic signal system.  <u>Outcome/Output:</u> Improve safety to motorist, pedestrians, and bicyclist who travel through the intersection from nearby schools, parks, and residential areas.	01-2258 RIP/13-14 PA&ED \$10,000 0100020175	2012-13 101-0042 SHA 20.30.600.620	\$10,000
2 \$225,000 City of Portola Plumas CTC 02-Plumas	Route A15 Reconstruction-Phase II. In Portola, on Route A15 from Colorado Street to Commercial Street.  (Future Consideration of Funding – Resolution E-13-48, April 2013.)  <u>Outcome/Output:</u> Reconstruct 0.6 existing lane mile according to an approved rehabilitation plan.	02-2480 RIP/13-14 PS&E \$148,000 R/W \$77,000 0200020123	2012-13 101-0042 SHA 20.30.600.621	\$225,000
3 \$2,050,000 City of Mount Shasta SCLTC 02-Siskiyou	Alma Street Rehabilitation. In Mount Shasta City, from Chestnut Street to Rockfellow Drive. Remove failed concrete and asphalt roadway, and replace with new asphalt roadway.  <u>Outcome/Output:</u> Rehabilitate to extend the useful life of the roadway by 10-15 years and improve vehicular pedestrian and bicyclist safety.	02-2453 RIP/13-14 CONST \$2,050,000 0200000417	2012-13 101-0042 SHA 20.30.600.621	\$2,050,000
4 \$501,000 Town of Fort Jones SCLTC 02-Siskiyou	Town of Fort Jones Roadway Rehabilitation. In Fort Jones, on Fort Jones, on Marble View Avenue, Oak Mill Drive, Diggles Street, Douglas Street and Hamilton Street. Rehabilitate roadway.  <u>Outcome/Output:</u> Construct and rehabilitate failed and deteriorated sections of roadway and help prevent most costly full reconstruction later. Extend the useful lives of the facilities by at least 10 years and improve vehicular safety.	02-2454 RIP/13-14 CONST \$501,000 0200000422	2012-13 101-0042 SHA 20.30.600.621	\$501,000
5 \$600,000 City of Tulelake SCLTC 02-Siskiyou	Tulelake Street Rehabilitation. In Tulelake, on F Street from 4th Street to Main Street and Modoc Street and from A Street to 1st Street. Rehabilitate roadway.  (Construction increase of \$200,000 to come from Siskiyou County unprogrammed share balance.)  <u>Outcome/Output:</u> Rehabilitate approximately 4,000 linear feet of roadway using a one-inch leveling course with geotextile fabric and two-inch AC overlay. Extend the life of the facility by an expected 10 or more years.	02-2471 RIP/13-14 CONST <del>\$400,000</del> \$600,000 0200000423	2012-13 101-0042 SHA 20.30.600.621	\$600,000
6 \$1,200,000 City of Weed SCLTC 02-Siskiyou	Black Butte Drive and Vista Drive Rehabilitation. In Weed, on Black Butte Drive between Shastina Drive and Vista Drive, and on Vista Drive between Shastina Drive and Black Butte Drive. Rehabilitate roadway.  (Future Consideration of Funding – Resolution E-09-82, August 2008.)  <u>Outcome/Output:</u> Increase turn lane storage from frontage road and rehabilitate failed and cracked roadways.	02-2448 RIP/13-14 CONST \$1,200,000 0200000426	2012-13 101-0042 SHA 20.30.600.621	\$1,200,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(3a) Locally Administered STIP Projects off the State Highway System</b>				<b>Resolution FP-13-07</b>
7 \$60,000 City of Yreka SCLTC 02-Siskiyou	Foothill Drive Project. In Yreka, on Foothill Drive from Kleaver Street easterly to city limit. Rehabilitate existing pavement, and install new asphalt pavement surface.  <u>Outcome/Output:</u> This project will reconstruct failed pavement structure and rehabilitate roadways to improve vehicular safety and extend roadway life by at least 10 years. This project will also extend bike lanes in accordance with the City of Yreka Bicycle Transportation Plan to improve bicycle safety along foothill drive.	02-2452 RIP/13-14 PS&E \$60,000 0213000109	2012-13 101-0042 SHA 20.30.600.621	\$60,000
8 \$122,000 Inyo County Inyo LTC 09-Inyo	West Bishop Roadway Reconstruction. In West Bishop, on various residential streets. Reconstruct roadways.  (SB 184 Submittal effective July 1, 2013.)  <u>Outcome/Output:</u> Provide greater motorist safety. Extend the life of 3 miles of existing roadway.	09-2599 RIP/13-14 PS&E \$122,000 0913000022	2012-13 101-0042 SHA 20.30.600.621	\$122,000

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(3a) Locally Administered STIP Transportation Enhancement Projects off the State Highway System</b>				<b>Resolution FP-13-07</b>
9 \$6,963,000 City of Sacramento SACOG 03-Sacramento	Sacramento City College Pedestrian Overcrossing. In the City of Sacramento, between the Sacramento City College RT light rail station and the Curtis Park Village development: construct a bicycle and pedestrian overcrossing over the railroad tracks.  (Future Consideration of Funding – Resolution E-13-49, July 2011.)  <u>Outcome/Output:</u> Construction of pedestrian and bicycle overcrossing to facilitate access to light rail station from neighborhoods currently restricted by railroad barrier and to allow light rail passengers to access neighborhoods to the east.	03-6577 RIP TE/13-14 CONST \$6,963,000 0300020206	2012-13 101-0890 FTF 20.30.600.731	\$6,963,000
10 \$329,000 County of Marin MTC 04-Marin	Sir Francis Drake Blvd Westbound Class II Bike Lane. Near the town of Fairfax in Marin County. Reconstruct and/or widen westbound shoulder on Sir Francis Drake Blvd from intersection of Baywood Canyon to top of White's Hill Road.  <u>Outcome/Output:</u> Increased bicycle use on Sir Francis Drake Blvd.	04-2127Q (Marin) RIP TE/13-14 CONST \$35,000  (Solano) RIP TE/13-14 CONST \$294,000 0400021116	2012-13 101-0890 FTF 20.30.600.731	\$329,000
11 \$50,000 Inyo County Inyo LTC 09-Inyo	Ed Powers Bike Lanes. Near West Bishop, on Ed Powers Road, from Route 168 to Route 395. Construct Class II bike lanes.  <u>Outcome/Output:</u> Provide a safer and less-congested alternative bicycle route along 2.4 miles of roadway.	09-2598 RIP TE/13-14 PS&E \$50,000 0913000021	2012-13 101-0890 FTF 20.30.600.731	\$50,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(3a) Local STIP Planning, Programming and Monitoring Projects</b>			<b>Resolution FP-13-07</b>	
12 \$150,000 Humboldt County Association of Governments <u>HCAOG</u> 01-Humboldt	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	01-2002P RIP/13-14 CONST \$150,000 0113000131	2012-13 101-0042 SHA 20.30.600.670	\$150,000
13 \$64,000 Lake County/City Area Planning Council <u>Lake CCAPC</u> 01-Lake	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	01-3002P RIP/13-14 CONST \$64,000 0100020431	2012-13 101-0042 SHA 20.30.600.670	\$64,000
14 \$140,000 Mendocino Council of Governments <u>MCOG</u> 01-Mendocino	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	01-4002P RIP/13-14 CONST \$140,000 0113000129	2012-13 101-0042 SHA 20.30.600.670	\$140,000
15 \$81,000 Nevada County Transportation Commission <u>Nevada CTC</u> 03-Nevada	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	03-0L83 RIP/13-14 CONST \$81,000 0313000289	2012-13 101-0042 SHA 20.30.600.670	\$81,000
16 \$750,000 Alameda County Transportation Commission <u>ACTC</u> 04-Alameda	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	04-2179 RIP/13-14 CONST \$750,000 0413000390	2012-13 101-0042 SHA 20.30.600.670	\$750,000
17 \$259,000 Transportation Agency for Monterey County <u>TAMC</u> 05-Monterey	Planning, Programming and Monitoring	05-1165 RIP/13-14 CONST \$259,000 0513000170	2012-13 101-0042 SHA 20.30.600.670	\$259,000
18 \$350,000 Santa Barbara County Association of Governments <u>SBCAG</u> 05-Santa Barbara	Planning, Programming and Monitoring	05-1914 RIP/13-14 CONST \$350,000 0513000172	2012-13 101-0042 SHA 20.30.600.670	\$350,000
19 \$413,000 Ventura County Transportation Commission <u>VCTC</u> 07-Ventura	Planning, Programming and Monitoring	07-9002 RIP/13-14 CONST \$413,000 0713000465	2012-13 101-0042 SHA 20.30.600.670	\$ 413,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5c.(3a) Local STIP Planning, Programming and Monitoring Projects</b>		<b>Resolution FP-13-07</b>		
20 \$1,200,000 San Bernardino Associated Governments <u>SANBAG</u> 08-San Bernardino	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	08-9811 RIP/13-14 CONST \$1,200,000 0813000219	2012-13 101-0042 SHA 20.30.600.670	\$1,200,000
21 \$54,000 Calaveras Council of Governments <u>Calaveras COG</u> 10-Calaveras	Planning, Programming and Monitoring	10-C1950 RIP/13-14 CONST \$54,000 1013000230	2012-13 101-0042 SHA 20.30.600.670	\$54,000
22 \$200,000 San Joaquin Council of Governments <u>SJCOG</u> 10-San Joaquin	Planning, Programming and Monitoring	10-7952 RIP/13-14 CONST \$200,000 1013000233	2012-13 101-0042 SHA 20.30.600.670	\$200,000
23 \$300,000 Imperial County Transportation Commission <u>ICTC</u> 11-Imperial	Planning, Programming and Monitoring	11-7200 RIP/13-14 CONST \$300,000 1113000166	2012-13 101-0042 SHA 20.30.600.670	\$300,000
24 \$854,000 San Diego Association of Governments <u>SANDAG</u> 11-San Diego	Planning, Programming and Monitoring  (SB 184 Submittal effective July 1, 2013.)	11-7402 RIP/13-14 CONST \$854,000 1113000174	2012-13 101-0042 SHA 20.30.600.670	\$854,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5c.(3b)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Denix D. Anbiah  
Division Chief  
Local Assistance

Subject: **ALLOCATION FOR SUPPLEMENTAL FUNDS FOR PREVIOUSLY VOTED LOCALLY ADMINISTERED PROJECT RESOLUTION FP-13-08**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission) allocate an additional \$183,000 of State Transportation Improvement Program (STIP) funds for the locally administered Evergreen Road Bridge at South Fork Cottonwood Creek Replacement project in Tehama County (County), as identified below.

## **ISSUE:**

Additional funds are needed for the previously voted locally administered Evergreen Road Bridge at South Fork Cottonwood Creek replacement project (PPNO 2379) in order to complete this project. The previous allocation of \$65,000 was as “state-only” STIP funds which were used to provide a federal match; therefore, this project could not be federalized. It is requested that these additional supplemental funds for \$183,000 also be as “state-only” STIP funds.

## **RESOLUTION:**

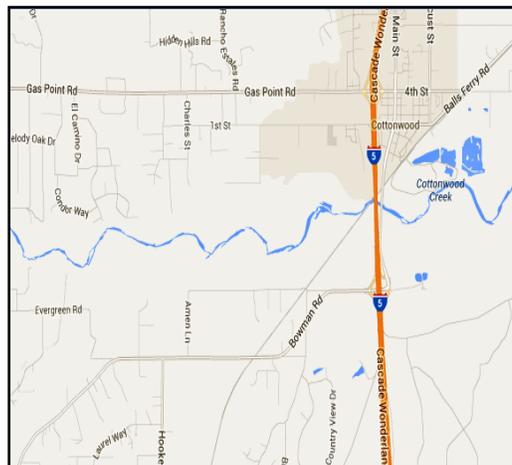
Resolved, that \$183,000 be allocated from the Budget Act of 2012, Budget Act Item 2660-101-0042 to provide additional funds for the project identified below.

<u>Project</u>	<u>Dist-Co-Rte</u>	<u>Original allocated Amount</u>	<u>Original Award Amount</u>	<u>Current Allocation</u>	<u>Allocation Adjustment</u>	<u>Revised Allocation</u>	<u>% Increase Above Current Allocation</u>
1	02-Teh-CR	\$65,000	\$65,000	\$65,000	\$183,000	\$248,000	282%

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program Phase Funding Year Budget Item # Fund Type Program Codes Project ID	State Federal Current Amount by Fund Type	State Federal Additional Amount by Fund Type	State Federal Revised Amount by Fund Type
<b>2.5c.(3b) Supplemental Financial Allocation for Locally Administered STIP Projects off the State Highway System</b>			<b>Resolution FP-13-08</b>		
\$183,000 Tehama County <u>Tehama CTC</u> 02-Tehama	Evergreen Road Bridge at South Fork Cottonwood Creek. Near Red Bluff, on Evergreen Road at Cottonwood Creek, Bridge No. 8C-008. Replace Bridge (HBP Match) (SB184 Submittal)  (Future Consideration of Funding – Resolution E-12-71, November 2012.)  <u>Outcome/Output</u> This project replaces a seismically deficient, functionally obsolete structure with one that meets current standards.  Supplemental funds needed to complete construction.  Total Revised Amount: \$248,000	02-2379 RIP PS&E 2012-13 101-0042 SHA 20.30.600.620 0200000352	\$65,000	\$183,000	\$248,000

**RECOMMENDATION:**

The Department recommends that this request for \$183,000 be approved to allow the County to complete this project.



**PROJECT DESCRIPTION:**

Evergreen Road Bridge at South Fork Cottonwood Creek (Near Red Bluff, on Evergreen Road at Cottonwood Creek), Bridge No. 08C-0008.

This bridge replacement project will replace a seismically deficient, functionally obsolete structure with a new bridge that meets current standards.

**FUNDING STATUS:**

On December 6, 2012, the Commission allocated \$65,000 in “state-only” STIP funding. Additional Plans, Specification and Estimate work will be required and the County endeavors to use an additional \$324,000 in federal bridge funds, and \$183,000 of state-funded STIP included in this request.

**REASONS FOR COST INCREASE:**

Additional Plan Specification and Estimate (PS&E) funds are needed to pay for the consultant’s final design, which is consistent with the amended contract on December 6, 2012 and for staff oversight of the design.

The initial scope of the project is for Evergreen Road Bridge project. However, west of the original project site is another bridge, the Anderson-Cottonwood Irrigation District (ACID) Bridge. The County needs to address this ACID bridge as part of the overall Evergreen Road Bridge project. The ACID bridge is narrow 20 foot wide bridge, which will also required work as it is locate within a few hundred feet of the seismically deficient Evergreen Road Bridge touchdown.

Another added element to this project is that the Evergreen Road Bridge replacement will now require the appropriate touchdown lengths, as well as corridor safety improvements, at the intersection of Bowman Road and Evergreen Road, which is located at east approach to the bridge.

**FUNDING OPTIONS:**

**OPTION A:** Approve this request for \$183,000 and allow the County to complete this project.

**OPTION B:** Deny this request and the County would lose \$324,000 in federal bridge funds for the PS&E phase.

**RECOMMENDED OPTION:**

The Department recommends that this request for \$183,000, as presented in Option A above, be approved to allow for the completion of this project.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5c.(3c)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Denix D. Anbiah  
Division Chief  
Local Assistance

Subject: **ALLOCATION FOR SUPPLEMENTAL FUNDS FOR PREVIOUSLY VOTED LOCALLY ADMINISTERED PROJECT RESOLUTION FP-13-09**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission) allocate an additional \$50,000 to Mendocino County for the locally administered State Transportation Improvement Program (STIP) Branscomb Road Pedestrian Bridge project as identified below.

## **ISSUE:**

Additional funds are needed for the previously voted locally administered Branscomb Road Pedestrian Bridge STIP project (PPNO 4517) to complete Project Approval and Environmental Document (PA&ED) work.

## **RESOLUTION:**

Resolved, that \$50,000 be allocated from the Budget Act of 2012, Budget Act Item 2660-101-0890 to provide additional funds for the project identified below.

<u>Project</u>	<u>Dist-Co</u>	<u>Original allocated Amount</u>	<u>Current Allocation</u>	<u>Allocation Adjustment</u>	<u>Revised Allocation</u>	<u>% Increase Above Current Allocation</u>
1	01-Men	\$160,000	\$160,000	\$50,000	\$210,000	31.25%



**FUNDING STATUS:**

PA&ED has been funded under two different allocations. On December 15, 2011, the Commission allocated \$50,000, and on August 22, 2012, the Commission allocated \$110,000.

**REASONS FOR COST INCREASE:**

The Branscomb Road Bridge project experienced a \$50,000 cost increase to the environmental phase due to the need for more extensive archaeological studies and an additional floodplain report.

The initial scope of work for preliminary engineering did not anticipate any significant archeological or historical impacts. It has been determined that there is a need to perform a higher level of investigation.

The elevation of the pedestrian multi-use bridge needs to match the new vehicle bridge elevation when it is replaced. Additional engineering studies and design are required for this accommodation.

**FUNDING OPTIONS:**

**OPTION A:** Approve this request for supplemental funds, as presented above, for \$50,000 and allow the project to complete environmental phase.

**OPTION B:** Deny this request and direct the County to revise the project to remain within the allocated amount.

**RECOMMENDED OPTION:**

The Department recommends that this request for \$50,000, as presented in Option A above, be approved to allow the completion of the environmental work.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5c.(4)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR LOCALLY ADMINISTERED STIP TRANSPORTATION ENHANCEMENT PROJECTS OFF THE STATE HIGHWAY SYSTEM (ADVANCEMENTS) RESOLUTION FP-13-**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) defer an allocation of \$2,468,000 for two locally administered State Transportation Improvement Program (STIP) Transportation Enhancement (TE) projects, off the State Highway System because these projects are advanced from future program years.

## **ISSUE:**

The attached vote list describes two locally administered STIP TE projects off the State Highway System programmed in Fiscal Year (FY) 2015-16, totaling \$2,468,000 plus \$3,700,000 from other sources. Although the local agencies are ready to proceed with these projects, it is recommended that the Commission defer this allocation until it is known that sufficient allocation capacity is available once all the projects programmed in FY 2013-14 are funded.

Attachment

2.5 Highway Financial Matters

Project #	Allocation Amount	Project Title	PPNO	Budget Year	
Recipient	RTPA/CTC	Location	Program/Year	Item #	
District-County	District-County	Project Description	Phase	Fund Type	Amount by
					Resolution FP-13-__
<b>2.5c.(4) Locally Administered STIP Transportation Enhancement Projects off the State Highway System (ADVANCEMENTS)</b>					
1	\$1,897,000	Sunnyside/Shepherd Trail Head Rest Area. In Clovis, at the southwest corner of Sunnyside Avenue and Shepherd Avenues. Construct a trail head/rest area.	06-B002S RIP TE/15-16 CONST <b>\$1,897,000</b> 0613000156	2012-13 101-0890 FTF 20.30.600.731	\$1,897,000
City of Clovis FCOG 06-Fresno		(Future Consideration of Funding – Resolution E-13-26, May 2013.)  (Funded from Fresno County FY 2015-16 TE Reserve PPNO B002.)  <u>Outcome/Output:</u> This project is designed as a streetscape improvement benefiting bicyclists and pedestrians.			
<b>THE DEPARTMENT RECOMMENDS THIS ITEM BE DEFERRED AT THIS TIME.</b>					
2	\$571,000	Downtown Long Beach Pine Avenue Streetscape Improvements. In Long Beach on Pine Avenue between <del>Seaside Way and Anaheim Street.</del> Shoreline Drive and 8th Street, 3rd Street between Pacific Avenue and Long Beach Boulevard, Broadway between Pacific Avenue and Long Beach Boulevard, and 1st Street between Pacific Avenue and Elm Avenue. Streetscape improvements.	07-4542 RIP TE/15-16 CONST <del>\$2,889,000</del> <b>\$571,000</b> 0713000405	2012-13 101-0890 FTF 20.30.600.731	\$571,000
City of Long Beach LACMTA 07-Los Angeles		(NEPA - CE, 05/02/2013.)  (CONST allocation funded from FY 2015-16 TE Reserve PPNO B002.)  (RIP TE Construction savings of \$2,318,000 to be returned to Los Angeles County regional share balance.)  (Contributions from other sources: \$3,700,000.)  <u>Outcome/Output:</u> The improvements will include pedestrian lighting, crosswalk enhancements, diagonal crosswalks, street furniture, bike racks, street trees, landscaping and bollards to facilitate street closure for community events. It also includes removal of obstructions from the walkway to improve pedestrian mobility. The project will support local and Metro transit stations, employment areas, business districts, and major activity nodes.			
<b>THE DEPARTMENT RECOMMENDS THIS ITEM BE DEFERRED AT THIS TIME.</b>					

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5c.(5)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR LOCAL ALTERNATIVE TRANSPORTATION  
IMPROVEMENT PROGRAM PROJECTS ON THE STATE HIGHWAY SYSTEM  
RESOLUTION FP-13-11**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) allocate \$8,100,000 for the Local Alternative Transportation Improvement Program Route 238 Corridor Improvements (PPNO 0095E) project in Alameda County, on the State Highway System.

## **ISSUE:**

The attached vote list describes one locally administered project on the State Highway System for \$8,100,000.

This project is included in the State Route 238 Local Alternative Transportation Improvement Program (LATIP) approved by the Commission at its May 2010 meeting. Projects included in the LATIP will be funded from proceeds of the sale of the properties purchased for the construction of the Hayward Bypass Project. The Agency is requesting an allocation as the project is 95 percent complete and has accrued expenditures in excess of \$8,100,000.

At this time, the amount of funds accumulated from the sale of properties are not sufficient to make the full allocation. However, the Department is expecting that sufficient funds will be available by the Commission meeting. Allocation is contingent on availability of funds.

## **FINANCIAL RESOLUTION:**

Resolved, that \$8,100,000 be allocated from the Budget Act of 2010, Budget Act Item 2660-501-0942 for the locally administered Local Alternative Transportation Improvement Program project described in the attached vote box.

Attachment

2.5 Highway Financial Matters

Project #	Allocation Amount	Recipient	Project Title	PPNO Program/Year	Budget Year	Amount by Fund Type
RTPA/CTC	County	Dist-Co-Rte	Location	Project ID	Item #	
Postmile	Project Description	Adv Phase	Project Support Expenditures	EA	Fund Type	Program Code
<b>2.5c.(5) Local Alternative Transportation Improvement Program Projects on the State Highway System Resolution FP-13-11</b>						
1	\$8,100,000	City of Hayward	Route 238 Corridor Improvements. In Hayward, on Route 238 from Industrial Parkway to the I-580 ramp near Apple Avenue. Construct street improvements including pavement, curb, gutter, sidewalk, medians, streetlights, signals and utilities.	04-0095E XXXX / 12-13 CONST \$8,100,000 0400000427	2010-11 501-0942 SHA 20.20.XXX.XXX	\$8,100,000
MTC	Alameda	04-Ala-238		4CONL		
9.31/14			(This project was included in the State Route 238 Local Alternative Transportation Improvement Program [LATIP]) approved by the Commission in May 2010.)	155312		
			(Future Consideration of Funding – Resolution E-10-41, May 2010.)			
			(Agency has accrued expenditures in excess of \$8,100,000.)			
			<u>Outcome/Output:</u> Construct various street improvements.			
			<b>ALLOCATION IS CONTINGENT ON AVAILABILITY OF SUFFICIENT FUNDS</b>			

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5e.(2)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **ALLOCATION FOR SUPPLEMENTAL FUNDS FOR PREVIOUSLY VOTED PROJECT  
RESOLUTION FA-13-04**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission allocate an additional \$260,000 for one State Highway Operation and Protection Program (SHOPP) project identified below.

## **ISSUE:**

Additional funds are needed for one previously approved project in order to close-out the construction contract.

## **RESOLUTION:**

Resolved, that \$260,000 be allocated from the Budget Act of 2013, Budget Act Item 2660-302-0042 to provide additional funds to allow the following project to be closed-out.

<u>Project</u>	<u>Dist-Co-Rte</u>	<u>Original Allocated Amount</u>	<u>Current Allocation</u>	<u>Allocation Adjustment</u>	<u>Revised Allocation</u>	<u>% Increase Above Current Allocation</u>
1	11-Imp-111	\$1,400,000	\$1,400,000	\$260,000	\$1,660,000	18.6%

Project # Allocation Amount Recipient County Dist-Co-Rte Postmile	Location Project Description Reason for Supplemental Funds	PPNO Program Funding Year Item # Fund Type Program Codes Project ID Adv Phase EA	State Federal Current Amount by Fund Type	State Federal Additional Amount by Fund Type	State Federal Revised Amount by Fund Type
<b>2.5e.(2) Supplemental Funds for Previously Voted Projects</b>					<b>Resolution FA-13-04</b>
1 \$260,000 Department of Transportation Imperial 11-Imp-111 44.7	Near Calipatria at the "Z" Drain Bridge (Bridge No. 58-0153). A heavy rainstorm on July 13, 2012 caused severe erosion and undermining of the supporting bridge piles. Temporary falsework is necessary to stabilize the bridge and water has to be diverted away from the bridge. <u>Outcome/Outputs:</u> This project is to build two box culverts with wing walls on both the inlet and outlet sides and place rock slope protection on the upstream and downstream sides of the new culverts.  Supplemental funds needed to close-out the contract.  Total Revised Amount: \$1,660,000.	11-0527 SHOPP 2012-13 302-0042 SHA 20.20.201.130 1113000031 4 415804 20.20.201.130	\$1,400,000	\$260,000	\$1,660,000

**PROJECT LOCATION:**



**PROJECT DESCRIPTION:**

This project is located in Imperial County on Route 111 adjacent to the Salton Sea and near the city of Calipatria, at the 'Z' Drain Bridge (Bridge No. 58-0153). The project was a result of the July 13, 2012 rainstorm event that caused severe erosion and undermining of the supporting bridge piles. An Emergency Director's Order was approved to construct temporary falsework to stabilize the bridge and restore the roadway. Water was diverted away from the bridge allowing construction of two concrete box culverts under the existing bridge. Wingwalls were constructed at both ends of the new box culvert structures and rock slope protection was placed at the inlets and outlets. The existing bridge deck was then removed during an extended road closure and roadway embankment was placed on the completed box culverts. The roadway pavement section was then placed, allowing the roadway to be re-opened.

**FUNDING STATUS:**

The project was constructed under an Emergency Director's Order approved on September 21, 2012 for \$1,400,000. Construction began immediately under an Emergency Force Account contract that was completed on December 17, 2012. An additional \$260,000 in supplemental funds is needed to close-out the contract and results in an overall increase of 18.6 percent over the current allocation.

**REASONS FOR COST INCREASE:**

This request for \$260,000 in supplemental funds to close-out the contract is due to the urgent nature of the work to restore the damaged roadway. The project schedule allowed the contractor few working days, most of which used 10 to 12 hour shifts and included weekends. Work included public outreach during full roadway closure.

The limited work-time meant that invoices were not provided by the contractor until after the contract work was completed. Although detailed daily cost accounting records were maintained by the Department, the unit costs associated with the contract work were underestimated. The Department has since reached agreement with the contractor on the higher unit costs. If the project schedule allowed for interim invoices, the unit costs could have been adjusted accordingly and the overrun would have been identified earlier allowing for an amended Emergency Director's Order request.

As a result of this incident, and to improve the Department's current business practice, work done under an Emergency Director's Order has been modified. Now when construction work is approximately 75 percent complete, field staff will develop an estimate to complete. At that point a decision can be made to proceed with an amended Director's Order before the field work has finished.

**FUNDING OPTIONS:**

**OPTION A:** Approve this request as presented above for \$260,000 to allow the close-out of this construction contract.

**OPTION B:** Deny this request and direct the Department to find another source of funding. The Department considered this option and determined that there are no other known sources of State funds available to cover the requested amount.

**RECOMMENDED OPTION:**

The Department recommends that this request of \$260,000, as presented in Option A above, be approved to allow the close-out of this project.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5e.(3)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **ALLOCATION FOR SUPPLEMENTAL FUNDS FOR PREVIOUSLY VOTED PROJECT  
RESOLUTION FA-13-05**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission allocate an additional \$50,000 for one State Highway Operation and Protection Program (SHOPP) project identified below.

## **ISSUE:**

Additional funds are needed for one previously approved project in order to close-out the construction contract.

## **RESOLUTION:**

Resolved, that \$50,000 be allocated from the Budget Act of 2013, Budget Act Item 2660-302-0042 to provide additional funds to allow the following project to be closed-out.

<u>Project</u>	<u>Dist-Co-Rte</u>	<u>Original Allocated Amount</u>	<u>Current Allocation</u>	<u>Allocation Adjustment</u>	<u>Revised Allocation</u>	<u>% Increase Above Current Allocation</u>
1	11-SD-78	\$450,000	\$450,000	\$50,000	\$500,000	11.1%

Project # Allocation Amount Recipient County Dist-Co-Rte Postmile	Location Project Description Reason for Supplemental Funds	PPNO Program Funding Year Item # Fund Type Program Codes Project ID Adv Phase EA	State Federal Current Amount by Fund Type	State Federal Additional Amount by Fund Type	State Federal Revised Amount by Fund Type
<b>2.5e.(3) Supplemental Funds for Previously Voted Projects</b>			<b>Resolution FA-13-05</b>		
1 \$50,000 Department of Transportation San Diego 11-SD-78 2.3	Near Oceanside, at 0.8 mile east of El Camino Real. On March 16, 2012, a sinkhole occurred in the median adjacent to the Number 1 lane. The sinkhole was filled with slurry to stabilize the travel lane. Subsequent close circuit video and ground penetrating radar survey of a metal drainage pipe at this location revealed that due to severe corrosion of the pipe, the slurry inadvertently flowed into the pipe reducing its capacity by 60 percent. <u>Outcome/Outputs:</u> This project is to excavate the old metal culvert pipe and replace it with a new reinforced concrete pipe (RCP).  Supplemental funds needed close-out the contract.  Total Revised Amount: \$500,000	11-1082 SHOPP 2012-13 302-0042 SHA 20.20.201.130 1113000028 4 415904 20.20.201.130	\$450,000	\$50,000	\$500,000

**PROJECT LOCATION:**



**PROJECT DESCRIPTION:**

This project is located in San Diego County on Route 78 near the city of Oceanside, at 0.8 mile east of El Camino Real. In March 2012 a sink hole developed in the roadway median immediately adjacent to the inside westbound travel lane. An Emergency Director's Order was executed at that time to fill the sinkhole with slurry and stabilize the travel lanes. However, subsequent use of closed-circuit video and ground penetrating radar equipment showed the slurry had inadvertently flowed into the nearby drainage pipe and significantly reduced its capacity. Furthermore, the culvert was found to be severely corroded and imminent failure expected under heavy rains. Therefore, an additional Director's Emergency Order was prepared to excavate and remove the damaged culvert, place a new reinforced concrete pipe culvert, and reconstruct the impacted roadway section.

**FUNDING STATUS:**

The project was constructed under an Emergency Director's Order approved on October 12, 2012 for \$450,000. The emergency limited bid construction contract work started November 15 and was completed on December 31, 2012. An additional \$50,000 in supplemental funds is needed to close-out the contract and results in an overall increase of 11.1 percent over the current allocation.

**REASONS FOR COST INCREASE:**

This request for \$50,000 in supplemental funds to close-out the contract is due to the urgent nature of the work to restore the impacted drainage pipe and roadway before inclement rainy weather. Increased costs include additional nights worked to complete before anticipated rains; higher than estimated costs for nighttime concrete delivery; delays due to trenching cave-ins; and the need to temporarily backfill failed trenches in order to re-open lanes for morning peak commute traffic.

The limited work-time meant that invoices were not provided by the contractor until after the contract work was completed. Although detailed daily cost accounting records were maintained by the Department, the unit costs associated with the contract work were underestimated. The Department has since reached agreement with the contractor on the higher unit cost. If the project schedule allowed for interim invoices, the unit costs could have been adjusted accordingly and the overrun would have been identified earlier allowing for an amended Emergency Director's Order request.

As a result of this incident, and to improve the Department's current business practice, work done under an Emergency Director's Order has been modified. Now when construction work is approximately 75 percent complete, field staff will develop an estimate to complete. At that point a decision can be made to proceed with an amended Director's Order before the field work has finished.

**FUNDING OPTIONS:**

**OPTION A:** Approve this request as presented above for \$50,000 to allow the close-out of this construction contract.

**OPTION B:** Deny this request and direct the Department to find another source of funding. The Department considered this option and determined that there are no other known sources of State funds available to cover the requested amount.

**RECOMMENDED OPTION:**

The Department recommends that this request of \$50,000, as presented in Option A above, be approved to allow the close-out of this project.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5e.(4)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **APPROVAL OF SUPPLEMENTAL FUNDS FOR PREVIOUSLY VOTED PROJECT  
RESOLUTION FA-13-06**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission) allocate \$2,796,000 in American Recovery and Reinvestment Act of 2009 (Recovery Act) - State funds for the State Route 24 Caldecott Tunnel Fourth Bore project (PPNO 0057A) in Alameda and Contra Costa Counties.

## **ISSUE:**

Additional funds are needed for one previously voted multi-funded project in order to complete construction engineering activities.

## **PROJECT DESCRIPTION:**

The project is located on State Route 24 in Alameda and Contra Costa Counties. The main bore project will construct a two-lane fourth bore for the Caldecott Tunnel.

## **BACKGROUND AND FUNDING STATUS:**

At its May 2009 meeting, the Commission allocated Construction funds to the project. The funding plan for Construction Support reported to the Commission at the time of allocation is shown below.

Fund Type	x1,000
	Original Budget
Interregional Improvement Program (IIP)	\$2,700
American Recovery and Reinvestment Act of 2009 (Recovery Act) - Regional	\$17,600
Local funds (CCTA)	\$21,700
Total	<b>\$42,000</b>

In April 2012, the Commission approved supplemental funds to cover a funding shortfall in Construction Capital. To cover a portion of that shortfall, \$3,000,000 Recovery Act- Regional funds were transferred from Construction Support to Construction Capital. This transfer was based upon anticipated savings in Construction Support at that time. However, those anticipated savings failed to

materialize. Therefore, these Recovery Act-Regional funds are being transferred back to Construction Capital. Local funds will be used to cover that \$3,000,000 shortfall in Construction Capital.

Though the Department is the implementing agency for administering the construction contract, the project co-sponsor Contra Costa Transportation Authority (CCTA) has hired consultants to augment the Department's construction oversight staff and to provide design support during construction. This arrangement was agreed to between the Department and CCTA and documented in the Cooperative Agreement. Funding that supports the Department's Construction Support costs come from the ITIP and the Recovery Act-Regional funds. The costs for the CCTA consultants are being funded directly by the CCTA; the funds are drawn directly from their local funding contribution to the project Construction Support budget. In effect, all of the CCTA construction support funds are not directly available to the Department, rather the equivalent dollar amounts of services are being provided through the CCTA to the project through its consultants. Although the Department has a staffing plan that shows the CCTA consultant staff and their tasks, under this arrangement no verifiable billing or invoicing is processed through the Department.

At this time, there is an overall funding shortfall of \$12,311,000 in Construction Support. Out of the \$12,311,000 shortfall, \$5,796,000 is needed for the activities which will be performed by the Department personnel and \$6,515,000 is needed for activities to be performed by the CCTA consultants.

Out of that \$5,796,000 amount that is needed for the Department, \$3,000,000 will be funded by transferring Recovery Act-Regional funds from Construction Capital as described above. The remaining \$2,796,000 is proposed to be covered with Recovery Act –State funds.

Local funds from the project reserve will be used to cover \$6,515,000 that is needed for the activities which will be performed by the CCTA consultants.

**RECOMMENDATION:**

The Department recommends that this request for \$2,796,000 be approved to allow the Department to complete construction engineering activities.



**REASONS FOR COST INCREASE**

The shortfall in Construction Support is primarily due to the following reasons:

- Delays experienced during the excavation and installation of the tunnel support systems have resulted in increased design and construction oversight costs.
- The need to add a training budget for Department personnel. The Department maintenance personnel will be trained to use the newly-installed equipment to maintain, control and operate four tunnels and also to respond in the case of an emergency or other traffic incidents.

**Update the Construction Capital funding plan**

Furthermore, the project has experienced increase in Construction Capital costs. As a result of encountering additional locations of differing site conditions, revisions to various electrical systems, and additional costs associated with the Operations and Maintenance (OMC) Building, the project construction cost has increased from \$266,401,000 to \$282,491,000. This \$16,090,000 shortfall is being covered using local funds from the project reserve which currently stands at \$31,029,615.

**FUNDING OPTIONS:**

**OPTION A:** Approve this request for \$2,796,000 in supplemental funds that are needed to complete construction engineering activities.

**OPTION B:** Deny this request. Without a budget, the Department will cease conducting construction oversight activities.

**RECOMMENDED OPTION:**

The Department recommends that this request of \$2,796,000, as presented in Option A above, be approved to complete construction engineering activities.

**RESOLUTION:**

Resolved, that \$2,786,000 be allocated from the Budget Item 001-0042, to provide additional construction engineering funds for the project identified below.

Project	Dist-Co-Rte	Funds	Original Approved Budget	Current Approved Budget	Additional Allocation	Revised Budget	% Increase Above Current Budget
1	04-Ala/	IIP	\$2,700,000	\$2,700,000	\$0	\$2,700,000	0%
	CC-24	Recovery Act-Regional	\$17,600,000	\$14,600,000	\$3,000,000	\$17,600,000	20.5%
		Recovery Act-State	\$0	\$0	<b>\$2,796,000</b>	<b>\$2,796,000</b>	N/A
		<u>Local funds</u>	<u>\$21,700,000</u>	<u>\$21,700,000</u>	<u>\$6,515,000</u>	<u>\$28,215,000</u>	<u>30.0%</u>
		Total	\$42,000,000	\$39,000,000	\$12,311,000	\$51,311,000	31.6%

Note:

- \$3,000,000 Recovery Act-Regional is a simple transfer back from Construction Capital. Please see Page 2 for additional explanation.

Project # Allocation Amount Recipient RTPA/CTC Dist-Co-Rte Postmile	Project Title Location Project Description	PPNO Program/ Budget Item # Fund Type Program Codes Project ID Adv Phase EA	State Federal Current Amount by Fund Type	State Federal Additional Amount by Fund Type	State Federal Revised Amount by Fund Type
<b>2.5e.(4) Supplemental Funds for Previously Voted Projects</b>		<b>Resolution FA-13-06</b>			
1 \$2,796,000 Department of Transportation MTC Alameda/Contra Costa 04N-Ala/CC-24 Ala 5.3/ CC 1.3	On Route 24 in Alameda and Contra Costa Counties. Route 24/Caldecott Tunnel Fourth Bore (Segment 1) – Construct 2-lane fourth bore north of existing bores.  <u>Outcome/Outputs:</u> When combined with other segments (PPNO 0057G and 0057I), the overall Route 24/Caldecott Tunnel Fourth Bore project will result in daily vehicle hours of delay savings of about 10,368 hours.  Supplemental Funds needed to complete construction engineering activities.  Contribution from other sources: \$45,815,000 (\$17,600,000 [ARRA-Regional], \$28,215,000 [Local funds]).  Total Revised Amount for construction engineering: \$51,311,000.	04-0057A IIP 001-0042 SHA 501-0890 ARRA 0400002022 3 264134	\$2,700,000   \$0	\$0  \$2,796,000	\$2,700,000  \$2,796,000

**REVISE: State Route 24 Caldecott Tunnel Fourth Bore project (PPNO 0057A)**

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Alameda	4	0057A	29491	CO	2008-09	5.3 (Ala)	1.3 (CC)	24					
<b>Implementing Agency: (by component)</b>		<b>PA&amp;ED</b>	Department			<b>PS&amp;E</b>	Department						
		<b>R/W</b>	Department			<b>CON</b>	Department						
<b>RTPA/CTC:</b>		Metropolitan Transportation Commission											
<b>Project Title:</b>		State Route 24 Caldecott Tunnel Fourth Bore											
<b>Location:</b>		Near Oakland, on Route 24 from Route 13 in Alameda County to Gateway Boulevard in Contra Costa County.											
<b>Description:</b>		Construct 2-lane fourth bore for the Caldecott Tunnel, north of existing third bore.											
<b>(DOLLARS IN THOUSANDS)</b>													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	10/11	11/12	12/13	13/14	14/15	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
<b>Regional Improvement Program (RIP)</b>													
Existing	2,000	2,000									2,000		
Change	0	0									0		
Proposed	2,000	2,000									2,000		
<b>Interregional Improvement Program (IIP)</b>													
Existing	17,965	17,965				0		795		9,000	5,200	270	2,700
Change	0	0				0		0		0	0	0	0
Proposed	17,965	17,965						795		9,000	5,200	270	2,700
<b>State Bond - Corridor Mobility Improvement Account (CMIA)</b>													
Existing	11,043	11,043							11,043				
Change	0	0							0				
Proposed	11,043	11,043							11,043				
<b>ARRA - State</b>													
Existing	73,439	73,439				0			73,439				0
Change	2,796	0				2,796			0				2,796
Proposed	76,235	73,439				2,796			73,439				2,796
<b>Traffic Congestion Relief Program (TCRP)</b>													
Existing	19,978	19,978								9,993	9,985		
Change	0	0								0	0		
Proposed	19,978	19,978								9,993	9,985		
<b>Federal Demonstration (Demo)</b>													
Existing	1,440	1,440							1,440				
Change	0	0							0				
Proposed	1,440	1,440							1,440				
<b>Regional Measure 2</b>													
Existing	42,342	42,342							25,367	1,490	15,485		
Change	0	0							0	0	0		
Proposed	42,342	42,342							25,367	1,490	15,485		
<b>Local Measure J</b>													
Existing	91,755	91,755				0			70,055				21,700
Change	9,515					9,515			3,000				6,515
Proposed	101,270	91,755				9,515			73,055				28,215
<b>ARRA - Regional</b>													
Existing	99,657	99,657							85,057				14,600
Change	0	0							(3,000)				3,000
Proposed	99,657	99,657							82,057				17,600
<b>Total</b>													
Existing	359,619	359,619				0		795	266,401	20,483	32,670	270	39,000
Change	12,311	0				12,311		0	0	0	0	0	12,311
Proposed	371,930	359,619				12,311		795	266,401	20,483	32,670	270	51,311

NOTES: a) With the above described transfer of \$25,605,000, the project reserve has been reduced from \$31,029,615 to \$5,424,615. The funding plan shown above does not include this remaining reserve.  
b) The funding plan above reflects the fact that the original \$73,439,000 of CMIA Construction Capital was allocated using ARRA-State funds.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5g.(5a)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR LOCALLY ADMINISTERED TRADE CORRIDOR  
IMPROVEMENT FUND PROJECTS OFF THE STATE HIGHWAY SYSTEM  
RESOLUTION TCIF-A-1314-04**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) allocate \$8,855,000 for the locally administered Proposition 1B Trade Corridor Improvement Fund (TCIF) Lenwood Road Railroad Grade Separation (PPNO 1135) project in San Bernardino County, off the State Highway System.

## **ISSUE:**

The attached vote list describes one locally administered Proposition 1B TCIF project off the State Highway System for \$8,855,000, plus \$22,878,000 from other sources. The local agency is ready to proceed with this project and is requesting an allocation at this time. Allocation is contingent upon approval of a budget revision by the Department of Finance.

## **FINANCIAL RESOLUTION:**

Resolved, that \$8,855,000 be allocated from the Budget Act of 2012, Budget Act Item 2660-104-6056 for one locally administered Proposition 1B Trade Corridor Improvement Fund project described in the attached vote box.

Be it further resolved, that as a condition of allocation of these funds and to perform its administrative role established by Senate Bill 88, the Commission requests that the Department perform the functions necessary to ensure proper accountability measures are employed and reporting requirements are met for the Proposition 1B Trade Corridor Improvement Fund.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(5a) Proposition 1B – Locally Administered TCIF Projects off the State Highway System</b>			<b>Resolution TCIF-A-1314-04</b>	
1 \$8,855,000 San Bernardino Associated Governments <u>SANBAG</u> 08-San Bernardino	<p><b>Lenwood Road Railroad Grade Separation.</b> In the city of Barstow. Construct a grade separation for BNSF lines at Lenwood Road (TCIF Project 64).</p> <p>(The TCIF allocation is split as follows: \$500,000 for construction engineering and \$8,355,000 for construction capital.)</p> <p>(Contributions from other sources: \$22,878,000.)</p> <p><u>Outcome/Output:</u> This project will eliminate the at-grade crossing, mitigate the impact of freight movement in the communities, eliminate gate down time, increase travel reliability, eliminate potential conflicts between vehicular and train traffic, increase safety and improve air quality.</p> <p><b>ALLOCATION IS CONTINGENT UPON APPROVAL OF A BUDGET REVISION BY THE DEPARTMENT OF FINANCE.</b></p>	08-1135 TCIF/13-14 CONST \$8,855,000 0800020269	2012-13 104-6056 TCIF 20.30.210.300	\$8,855,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5g.(5b)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR LOCALLY ADMINISTERED TRADE CORRIDOR  
IMPROVEMENT FUND PROJECTS OFF THE STATE HIGHWAY SYSTEM  
RESOLUTION TCIF-A-1314-05**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) allocate \$39,519,000 for the locally administered Proposition 1B Trade Corridor Improvement Fund (TCIF) Lakeview Avenue Grade Separation (PPNO TC40) project in Orange County, off the State Highway System.

## **ISSUE:**

The attached vote list describes one locally administered Proposition 1B TCIF project off the State Highway System for \$39,519,000, plus \$60,244,000 from other sources. The local agency is ready to proceed with this project and is requesting an allocation at this time. Allocation is contingent upon approval of a budget revision by the Department of Finance.

## **FINANCIAL RESOLUTION:**

Resolved, that \$39,519,000 be allocated from the Budget Act of 2013, Budget Act Item 2660-104-6056 for one locally administered Proposition 1B Trade Corridor Improvement Fund project described in the attached vote box.

Be it further resolved, that as a condition of allocation of these funds and to perform its administrative role established by Senate Bill 88, the Commission requests that the Department perform the functions necessary to ensure proper accountability measures are employed and reporting requirements are met for the Proposition 1B Trade Corridor Improvement Fund.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(5b) Proposition 1B – Locally Administered TCIF Projects off the State Highway System</b>			<b>Resolution TCIF-A-1314-05</b>	
1 \$39,519,000 Orange County Transportation Authority OCTA 12-Orange	<p><b>Lakeview Avenue Grade Separation.</b> In Placentia at the Lakeview Avenue at-grade crossing. Construct overpass of the BNSF mainline tracks, including a connection road from Orangethorpe Avenue to the new overpass of Lakeview Ave. (TCIF Project 40)</p> <p>(Future Consideration of Funding – Resolution E-10-74, July, 2010.)</p> <p>(Related TCIF Programming Amendment under Resolution TCIF-P-1213-42; March 2013.)</p> <p>(Related TCIF Baseline Amendment under Resolution TCIF-P-1213-44; March 2013.)</p> <p>(The TCIF allocation is split as follows: \$6,241,000 for construction engineering and \$33,278,000 for construction capital.)</p> <p>(Contributions from other sources: \$60,244,000.)</p> <p><u>Outcome/Output:</u> This project will decrease in traffic congestion and travel time. The elimination of potential collision points will improve goods movement and provide greater driver safety.</p> <p><b>ALLOCATION IS CONTINGENT UPON APPROVAL OF A BUDGET REVISION BY THE DEPARTMENT OF FINANCE.</b></p>	12-TC40 TCIF/13-14 CONST \$39,519,000 1212000004	2013-14 104-6056 TCIF 20.30.210.300	\$39,519,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5g.(5c)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR LOCALLY ADMINISTERED TRADE CORRIDOR  
IMPROVEMENT FUND RAIL PROJECTS  
RESOLUTION TCIF-A-1314-06**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) allocate \$10,880,000 for the locally administered Proposition 1B Trade Corridor Improvement Fund (TCIF) Richmond Rail Connector (PPNO 0241B) Rail project in Contra Costa County.

## **ISSUE:**

The attached vote list describes one locally administered TCIF Rail project totaling \$10,880,000, plus \$11,770,000 from other sources. The local agency is ready to proceed with this project and is requesting an allocation at this time.

## **FINANCIAL RESOLUTION:**

Resolved, that \$10,880,000 be allocated from the Budget Act of 2012, Budget Act Item 2660-304-6056 for the locally administered Proposition 1B Trade Corridor Improvement Fund project described in the attached vote box.

Be it further resolved that as a condition of allocation of these funds and to perform its administrative role established by Senate Bill 88, the Commission requests that the Department perform the functions necessary to ensure proper accountability measures are employed and reporting requirements are met for the Proposition 1B Trade Corridor Improvement Fund.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(5c) Proposition 1B – Locally Administered TCIF Rail Projects</b>			<b>Resolution TCIF-A-1314-06</b>	
1 \$10,880,000 Department of Transportation MTC 04-Contra Costa	<p><b>Richmond Rail Connector.</b> (TCIF Project 2) Located between the cities of San Pablo and Richmond. The project will construct a rail connector on BNSF's Stockton Subdivision and UP's Martinez Subdivision. The at-grade rail connector between the two lines will allow BNSF trains access to UP's Martinez Subdivision rather than travel through the center of the city of Richmond for a more direct route to and from the Port of Oakland.</p> <p>(Original programming under Resolution TCIF-P-0708-01; April 2008.)</p> <p>(Future Consideration of Funding - Resolution E-13-41; May 2013.)</p> <p>(The TCIF allocation is split as follows: \$880,000 for construction engineering and \$10,000,000 for construction capital.)</p> <p>(Contribution from other sources: \$11,770,000)</p> <p><u>Outcome/Output:</u> Project will improve freight velocity to and from the Port of Oakland with reduced traffic delay in the city of Richmond</p>	04-0241B TCIF/12-13 CONST \$10,880,000 0012000218 S	2012-13 304-6056 TCIF 30.20.723.000	\$10,880,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.5g.(5d)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Ron Sheppard  
Acting Division Chief  
Budgets

Subject: **FINANCIAL ALLOCATION FOR STATE ADMINISTERED MULTI-FUNDED  
TCIF/STIP/SHOPP PROJECTS ON THE STATE HIGHWAY  
RESOLUTION TCIF-A-1314-07  
RESOLUTION FP-13-12**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) allocate \$35,412,000 for the State administered multi-funded Proposition 1B Trade Corridor Improvements Fund (TCIF)/State Transportation Improvement Program (STIP)/State Highway Operation and Protection Program (SHOPP) Project 89 - WB I-80 to SR12 (West) Connector and Green Valley Road Interchange Improvements (PPNO 5301L) project in Solano County, on the State Highway System.

## **ISSUE:**

The attached vote list describes one State administered Proposition 1B TCIF/STIP/SHOPP project on the State Highway System totaling \$35,412,000, plus \$29,448,000 from other sources. The Department is ready to proceed with this project and is requesting an allocation at this time.

## **FINANCIAL RESOLUTION:**

Resolved, that \$26,952,000 be allocated from the Budget Act of 2012, Budget Act Items 2660-304-6056, 2660-301-0042, 2660-301-0890, 2660-302-0042 and 2660-302-0890 for construction and \$8,460,000 for construction engineering, for the State administered Proposition 1B Trade Corridor Improvement Fund/State Transportation Improvement Program/State Highway Operation and Protection Program project described on the attached vote list.

Be it further resolved, that as a condition of allocation of these funds and to perform its administrative role established by Senate Bill 88, the Commission requests that the Department perform the functions necessary to ensure proper accountability measures are employed and reporting requirements are met for the Proposition 1B Trade Corridor Improvement Fund.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> County Dist-Co-Rte Postmile	Project Title Location Project Description	PPNO Program/Year Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
<b>2.5g.(5d) Proposition 1B – State Administered Multi-Funded STIP/TCIF/SHOPP Project on the State Highway Resolution</b>		<b>Resolution FP-13-12_</b> <b>Resolution TCIF-A-1314-07_</b>		
1 \$35,412,000 Department of Transportation <u>MTC</u> Solano 04-Sol-12,80 <del>12.0/13.1</del> <del>R2.1/R2.8</del> <b>12.0/12.9</b> <b>2.5/R2.8</b> 12.0/13.1	<b>WB I-80 to SR 12 (West) Connector and Green Valley Road Interchange Improvements.</b> In Fairfield at I-80 and SR 12. Construct a two-lane WB I-80 to WB SR 12 connector and reconstruct the I-80/Green Valley Road interchange (TCIF Project 89).  Final Project Development: N/A  Final Right of Way: N/A  (Future Consideration of Funding under Resolution E-13-02; January 2013.)  (Time extension for FY 11-12 CONST expires on July 31, 2013.)  (Contributions from other sources: \$29,448,000.)  (Project limits are being slightly modified to reflect revised conforms consistent with as-built conditions.)  <u>Outcome/Output:</u> Construct I-80/Green Valley Road interchange and I-80/ SR 12 connector.	04-5301L RIP / 11-12 CONST \$11,412,000  TCIF/12-13 CON ENG \$8,460,000 CONST \$7,040,000  SHOPP/12-13 \$8,500,000 0400021131 4 0A5344	2012-13 301-0042 SHA 301-0890 FTF 20.20.075.600  2012-13 304-6056 TCIF 20.20.723.000  2012-13 302-0042 SHA 302-0890 FTF 20.20.201.310	\$228,000 \$11,184,000  \$8,460,000  \$7,040,000  \$170,000 \$8,330,000

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: August 6, 2013

Reference No.: 2.1d  
Action

From: ANDRE BOUTROS  
Executive Director

Subject: **PROPOSITION 116 RAIL PROGRAM APPLICATION AMENDMENT**  
**RESOLUTION PA-13-01, AMENDING RESOLUTION PA-10-03**

## **ISSUE**

The Transportation Agency for Monterey County (TAMC) has determined that an amendment to the \$6,247,813 in Proposition 116 funds programmed for the Rail Extension to Monterey County project will enable TAMC to expedite delivery of the project.

## **RECOMMENDATION**

Staff recommends that the Commission approve the amended Proposition 116 application for the \$6,247,813 currently programmed for right of way activities to allow TAMC to reprogram \$300,000 for PA&ED; reprogram \$300,000 for PS&E; leave \$729,976 programmed for R/W; and reprogram the remaining \$4,917,837 for the CONST phase.

## **BACKGROUND**

Proposition 116 (PUC Section 99638) authorizes \$17,000,000 to TAMC for either the (a) extension of Caltrain service to Monterey County; or (b) another rail project within Monterey County.

TAMC programmed \$9,820,000 of the \$17,000,000 for project development and right of way acquisition of the Monterey Branch Line. The other \$7,180,000 is available for the Rail Extension to Monterey County project.

On April 8, 2010, the Commission approved Resolution PA-10-03, programming \$7,180,000 for the CalTrain Extension to Monterey County project, of which, \$932,187 was programmed and allocated for project development activities. The remaining \$6,247,813 was programmed for right of way.

TAMC has submitted a Proposition 116 application amendment for the \$6,247,813 to reprogram \$300,000 for PA&ED; \$300,000 for PS&E; leave \$729,976 programmed for R/W; and reprogram the remaining \$4,917,837 for the CONST phase of the Rail Extension to Monterey County project. This will enable TAMC to expeditiously use these funds to continue the planning, design and right of way acquisition phases for this project.

Attachment

CALIFORNIA TRANSPORTATION COMMISSION

Commission Project Application Approval Amendment  
Transportation Agency for Monterey County  
Rail Extension to Monterey County

Resolution PA-13-01  
Amending Resolution PA-10-03

- 1.1 WHEREAS, in June 1990 the voters approved the Clean Air and Transportation Improvement Act, Proposition 116, for \$1.99 billion for rail and mass transportation purposes; and
- 1.2 WHEREAS, the California Transportation Commission is designated in Proposition 116 to oversee the five grant programs over the 20-year term of the Proposition; and
- 1.3 WHEREAS, Proposition 116 calls for the Commission to establish an application process and to develop and adopt guidelines to implement those programs; and
- 1.4 WHEREAS, Proposition 116 establishes as a purpose of the application process that it "facilitate implementation of improved cost-effective transit service to the maximum number of Californians and to prevent the funds provided for by this part from being spent on needlessly costly features"; and
- 1.5 WHEREAS, Proposition 116 requires applications to specify full and complete capital plans, financial plans, and operating plans, including schedules and funding sources, and should the project exceed the cost approved by the Commission, the increased cost shall be covered by funds other than Proposition 116; and
- 1.6 WHEREAS, in December 1990 the Commission adopted the policy and application guidelines (#G-90-23) for the Proposition 116 rail program; and
- 1.7 WHEREAS, the Commission has established a Hazardous Waste Identification and Clean-up Policy (#G-91-2) that requires the local agency to have performed full due diligence in identifying the hazardous waste in the right-of-way and easements and properties as well as clean-up, and that the state has been indemnified from clean-up liability of damages, both present and future; and
- 1.8 WHEREAS, Proposition 116 (PUC Section 99638) authorizes \$17,000,000 to the Transportation Agency of Monterey County for the following: (a) extension of Caltrain service; or (b) other rail project within Monterey County; and
- 1.9 WHEREAS, on September 22, 1998, the Commission approved PA-98-19, programming \$450,000 for PA&ED on the Monterey Branch Line project; and

Resolution PA-13-01, Amending  
Resolution PA-10-03

- 1.10 WHEREAS, on October 20, 1998, pursuant to authority delegated by the Commission, the Department approved Resolution G25-95-05 approving the allocation of the \$450,000 for PA&ED on the Monterey Branch Line project; and
  - 1.11 WHEREAS, on August 14, 2003, the Commission approved PA-03-06 to program, and also approved BFP-03-02 to allocate, \$9,370,000 for right of way acquisition on the Monterey Branch Line project; and
  - 1.12 WHEREAS, on April 8, 2004, the Commission approved PA-04-02, programming \$3,000,000 for the Caltrain Extension to Monterey project; and
  - 1.13 WHEREAS, also on April 8, 2004, the Commission approved BFP-03-04 approving a partial allocation of \$932,187 out of the \$3,000,000 for preliminary activities on the Caltrain Extension to Monterey project; and
  - 1.14 WHEREAS, on April 8, 2010, the Commission approved PA-10-03, programming \$7,180,000 for the CalTrain Extension to Monterey County project, including the \$932,187 allocated via BFP-03-04 for project development activities. The remaining \$6,247,813 of the \$7,180,000 in Proposition 116 funds available for this project was programmed for right of way; and
  - 1.15 WHEREAS, in June 2013, TAMC submitted an amended application for the \$6,247,813 of the \$7,180,000 in Proposition 116 funds to reprogram \$300,000 for PA&ED; \$300,000 for PS&E; leave \$729,976 programmed for R/W; and reprogram the remaining \$4,917,837 for the CONST phase of the Rail Extension to Monterey County project; and
  - 1.15 WHEREAS, the amended application from TAMC, including all supplemental information, has been reviewed by Commission staff, and appears to meet all the basic requirements as specified in Proposition 116 and the Commission's policies and guidelines.
- 2.1 BE IT THEREFORE RESOLVED, that the Commission hereby approves the amended application from the Transportation Agency of Monterey County; and
  - 2.2 BE IT FURTHER RESOLVED, that the Commission hereby acknowledges that a concurrent request will be submitted to reallocate \$1,329,976 of the \$6,247,813 in Proposition 116 funds previously allocated for right of way acquisition on the Commuter Rail Extension to Monterey County project; and
  - 2.3 BE IT FURTHER RESOLVED, that the Commission hereby allows the partial reallocation for \$1,329,976 of the \$6,247,813 in Proposition 116 funds previously allocated, and reallocates \$300,000 for PA&ED; \$300,000 for PS&E; leaves \$729,976 allocated for R/W; and preserves the remaining \$4,917,837 for future allocation for construction phase of the Rail Extension to Monterey County project; and
  - 2.4 BE IT FURTHER RESOLVED, that Resolution PA-10-03 is hereby amended.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.6b.  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Jane Perez  
Division Chief  
Mass Transportation

Subject: **FINANCIAL ALLOCATION AMENDMENT FOR LOCAL PROPOSITION 116  
PROGRAM PROJECTS  
RESOLUTION BFA-13-01, AMENDING RESOLUTION BFP-09-03**

## **RECOMMENDATION:**

The Department of Transportation (Department) recommends the California Transportation Commission (Commission) approve an amendment to the original allocation of \$6,247,813 in Proposition 116 Bond funds under Resolution BFP-09-03, originally approved April 7, 2010, for the Rail Extension to Monterey County project, as described on the attached vote list and consistent with the concurrent Proposition 116 project amendment.

## **ISSUE:**

The attached vote list describes a regional rail project funded from Proposition 116 bond proceeds authorized under Public Utilities Code (PUC) Section 99638(a). The implementing agency, Transportation Agency for Monterey County (TAMC), was unable to expend the total amount allocated within the Commission's timely use of funds time frame due to additional work required under the preliminary phases. TAMC is requesting a reduction to the original allocation of \$6,247,813 by \$4,917,837 to \$1,329,976 and reallocation of \$300,000 for Project Approval and Environmental Document (PA&ED), \$300,000 for Plans, Specifications and Estimates (PS&E), and \$729,976 for Right of Way. (A concurrent Programming Amendment is also on this month's Commission agenda for approval.)

## **FINANCIAL RESOLUTION:**

### **Resolved That:**

- 1.1 WHEREAS, in April 7, 2010, the California Transportation Commission (Commission) passed Resolution BFP-09-03, which approved \$6,247,813 in Proposition 116 Bond funds from PUC 99638(a), for the Rail Extension to Monterey County project; and
- 1.2 WHEREAS, the Transportation Agency for Monterey County was unable to expend the total amount allocated for right of way work for the Rail Extension to Monterey County project in accordance with the Commission's timely use of funds policy due to additional work required under the preliminary phases; and

- 1.3 WHEREAS, a balance of \$6,017,837 in Proposition 116 Rail Bond funds allocated under Resolution BFP-09-03 remain unexpended and should be de-allocated and made available for re-allocation.
- 2.1 NOW THEREFORE BE IT RESOLVED, that the California Transportation Commission hereby adjusts the approved funding amount allocated under Resolution BFP-09-03 for the Rail Extension to Monterey County project from \$6,247,813 for Right of Way to \$300,000 for PA&ED; \$300,000 for PS&E and \$729,976 for Right of Way, and de-allocating \$4,917,837 for future Construction allocation.
- 2.2 BE IT FURTHER RESOLVED, that Resolution BFA-13-01 is hereby approved amending Resolution BFP-09-03.

**FINANCIAL RESOLUTION:**

- 1.1 WHEREAS, the California Transportation Commission (Commission) has adopted an annual program for mass transportation capital purposes, and the electorate enacted both Proposition 108, Passenger Rail and Clean Air Bond Act of 1990 and Proposition 116, Clean Air and Transportation Improvement Bond Act of 1990; authorizing the sale of general obligation rail bonds for rail transit purposes; and
- 1.2 WHEREAS, the Project further detailed on the attached vote list, as component phases or in its entirety, appears on the necessary State capital projects funding list and is entitled to participate in this allocation; and
- 1.3 WHEREAS, the Commission has established a “Hazardous Waste identification and Clean-up Policy” (Resolution G-91-2) which requires the Recipient to perform, with diligence, the process of identification and remediation of any hazardous waste in the Right of Way, easements and properties.
- 2.1 NOW THEREFORE BE IT RESOLVED, that \$1,329,976 in Proposition 116 Clean Air and Transportation Improvement Bond proceeds be allocated to the recipient for the project detailed on the attached vote list; and
- 2.2 BE IT FURTHER RESOLVED, that the transfer of funds for each project shall be governed by the program supplement, and subsequent amendments to the same if required; and
- 2.3 BE IT FURTHER RESOLVED, that each recipient shall provide the Department’s Division of Mass Transportation with an updated expenditure plan on a quarterly basis by category including any proposed changes for the balance of all funded Project allocations commencing with the first quarter; and
- 2.4 BE IT FURTHER RESOLVED, that in any instance of rail bond financing of a project, the Commission, acting on behalf of the State, by this Resolution intends:
  - A. To cause and approve the issuance of taxable or tax-exempt State general obligation bonds under Proposition 116, as appropriate, to reimburse the Recipient for the Project identified on attached vote list;

- B. To reimburse the Recipient for expenditures that shall not have been paid from the proceeds of any other tax-exempt indebtedness unless such prior indebtedness is retired with the proceeds of such State monies;
- C. That this Resolution be a declaration of official intent of the State within the meaning of U.S. Treasury Regulations Section 1.103-17© with respect to the Project; and

2.5 BE IT FURTHER RESOLVED, that in any instance of rail bond financing, an allocation for a project is subject to the following conditions and assurance:

- A. Completed bond sales authorized by the Office of State Treasurer;
- B. Receive bond certification from the Department, preferably prior to the Commission's allocation action but, certification must be obtained prior to execution of the fund transfer agreement;
- C. The Recipient's certification that will not adopt new increased current development taxes, fees, exactions or permit fees for the purpose of providing local matching funds; and the certification of this delivered to this Commission, preferably by the time of Commission allocation action but not later than prior to execution of a fund transfer agreement;
- D. A formal resolution by the Recipient's Board stating that when utilizing State funding for acquisition of property or for capital improvements on the Project, Recipient has exercised all due diligence in the discovery of hazardous wastes; that Recipient will enter into enforceable agreement(s) with any and all owners of to-be acquired properties for clean-up of hazardous wastes pursuant to the requirements of Resolution G-91-2 regarding Hazardous Waste Identification and Clean-up for Right of Way;
- E. A formal resolution by the Recipient's Board stating that when utilizing state-provided and other-than-state funding for acquisition of property or for capital improvements on the Project, that no additional State funds will be requested for clean-up, damages, or liability associated with hazardous wastes on or below the acquired property, delivered to this Commission;
- F. That in any instance of rail bond financing with Proposition 116 funds, eligible costs may be incurred for project development after the project application approval, and all reimbursements of eligible costs are subject to an executed fund transfer agreement;
- G. The Recipient shall provide the Commission with an "Evaluation of Property report" by the time of the Commission allocation action, in compliance with Commission Policy G-95-09, Rail Right of Way Policy, to be verified by the Department or its Agent;
- H. The Recipient shall post on the Project construction site at least one sign, visible to the public, stating that the Project is partially funded with Proposition 116 Clean Air and Transportation Improvement Bond Act of 1990 proceeds; and

- 2.6 BE IT FURTHER RESOLVED, that this Commission shall be entitle to a then present value refund, or credit, at State's option, equivalent to the proportionate funding participation by the State towards, property acquisition and project construction in the event that Recipient, or successor public entitles, fail or cease to utilize the Project for the intended public passenger rail purposes or sells or transfers title to the Project. The credit for future purchases or condemnation of all or portions of the Project by the State, and the refund or credit due the Commission in each instance, will be measured by the ratio of State and other market value of the Project property; and
- 2.7 BE IT FURTHER RESOLVED, that if the Recipient receives any revenues or profits from any non-governmental use of property allowed pursuant to bond certification (whether approved at this time or hereafter approved by the State), the Recipient agrees that such revenues or profits shall be used exclusively for the public transportation services for which the project was initially approved, either for capital improvements or maintenance and operational costs. If the Recipient does not so dedicate the revenues or profits, a proportionate shall (unless disapproved by the State's Bond Council) be paid to the State equivalent to the State's percentage participation in the Project.
- 2.8 BE IT FURTHER RESOLVED, that an allocation for the project is subject to the following conditions and assurances:
- A. Reimbursements of eligible costs are subject to the terms and conditions of the executed Program Supplement;
  - B. The grant recipient must complete the work to be reimbursed and the actual reimbursement by December 31, 2016, unless the Commission authorizes a waiver that extends, if permitted by statute, the period of availability of the funds.

Attachment

2.6 Mass Transportation Financial Matters

Project #	Allocation Amount	Recipient	Project Title	Location	Project Description	Dist-PPNO Program/YearP A#	PUC Code	Prgm'd Amount	Project ID	Budget Year	Fund Type	Program Code	Allocation Amount
<b>2.6b. Allocation Amendment - Proposition 116 - Locally Administered Rail Projects</b>													
<b>Resolution BFA-13-01, Amending Resolution BFP-09-03</b>													
1	<del>\$6,247,813</del>		<b>Rail Extension to Monterey County.</b>			05-1155				2013-14			<del>\$6,247,813</del>
	<b>\$1,329,976</b>	Transportation Agency for Monterey County TAMC 05-Monterey	Extend Capital Corridor passenger rail service from San Jose to Salinas and make improvements at three stations.		(Future Consideration of Funding – Resolution E-06-28; August 2006.)	P116/13-14			PA-13-01	P116		30.10.070.625	<b>\$1,329,976</b>
			(Concurrent Programming Amendment under Resolution PA-13-01; August 2013.)			PUC 99638(a)			<b>PA&amp;ED</b>				
									<b>\$300,000</b>				
									<b>PS&amp;E</b>				
									<b>\$300,000</b>				
									R/W				
									<del>\$6,247,813</del>				
									<b>\$729,976</b>				
			<u>Outcome/Output:</u> Complete environmental document, final engineering and property acquisition.			0014000001							
						R1316C							
						S							
			<u>Amend Resolution BFP-09-03 to reallocate \$300,000 to PA&amp;ED; \$300,000 to PS&amp;E, leave \$729,976 for R/W and de-allocate \$4,917,837 for future Proposition 116 CONST allocation.</u>						P116/15-16				
									CON				
									<b>\$4,917,837</b>				

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.6e.(1)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **FINANCIAL ALLOCATION FOR TCRP PROJECTS**  
**RESOLUTION TFP-13-01**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission) allocate \$38,142,000 in Traffic Congestion Relief Program (TCRP) funds for three projects in Los Angeles County.

## **ISSUE:**

The attached vote list describes three TCRP projects totaling \$38,142,000 for allocation. These are Tier 1 Projects in the approved TCRP Allocation Plan, and scheduled for allocation in Fiscal Year 2013-14.

## **FINANCIAL RESOLUTION:**

Resolved, that the projects, as component phases or in their entirety, appear under Government Code Section 14556.40(a) and are entitled to participate in this allocation.

Reimbursement of eligible costs is subject to the policies, restrictions and assurances as set forth in the Commission's policy for allocating, monitoring, and auditing Traffic Congestion Relief Program projects, and is governed by the terms and conditions of the Fund Transfer Agreement, Program Supplement or Cooperative Agreement, and subsequent amendments to the same if required, as executed between the implementing agency and the California Department of Transportation.

Attachment

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Implementing Agency District-County	BREF # and Project Description Description of Allocation	Item # Fund Type	Amount by Fund Type
<b>2.6e.(1) Traffic Congestion Relief Program Allocations</b>			<b>Resolution TFP-13-01</b>
1 \$8,000,000 Los Angeles County Metropolitan Transportation Authority 07- Los Angeles	<b>Project #39 – Route 405 – Add Carpool Lane from Route 10 to Route 101 (Northbound) (PPNO 0851G)</b>  Allocate \$8,000,000 per approved TCRP Allocation Plan.  <u>Output/Outcome:</u> Construct 10 miles of HOV lane Northbound from Route 10 to Route 101.  <b>This is a Tier 1 project.</b>	Chapter 91 of the Statutes of 2000  889-3007 TCRF	   \$8,000,000
2 \$10,309,000 Los Angeles County Metropolitan Transportation Authority 07 – Los Angeles	<b>Project 40 – Interstate 10; between Route 605 and Route 57 project (PPNO 0306H)</b>  Allocate \$10,309,000 per approved TCRP allocation plan.  <u>Output/Outcome:</u> Construction for Interstate 10 between Route 605 and Route 57.  <b>This is a Tier 1 project.</b>	Chapter 91 of the Statutes of 2000  889-3007 TCRF	   \$10,309,000
3 \$19,833,000 Los Angeles County Metropolitan Transportation Authority 07 – Los Angeles	<b>Project 42 – I-5 Widening; Orange County Line to Route 605 (PPNO 2808)</b>  Allocate \$19,833,000 per approved TCRP allocation plan.  <u>Output/Outcome:</u> Widen the I-5 Corridor from the Orange County Line to the Route 5/Route 605 junction: add HOV and Mixed-flow lanes from .02 miles south of Artesia Avenue to 0.2 mile north of the Florence Avenue Overcrossing to eliminate the northbound bottleneck.  <b>This is a Tier 1 project.</b>	Chapter 91 of the Statutes of 2000  889-3007 TCRF	   \$19,833,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.6e.(2)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **FINANCIAL ALLOCATION FOR TCRP PROJECTS**  
**RESOLUTION TFP-13-02**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission) allocate \$40,000,000 in Traffic Congestion Relief Program (TCRP) funds for TCRP Project 1.2 – BART to San Jose - Phase 1; Extend BART from Warm Springs to Berryessa (PPNO 2147D) project in Santa Clara County.

## **ISSUE:**

The attached vote list describes one TCRP project for \$40,000,000 for construction. This is a Tier 1 Project in the approved TCRP Allocation Plan, and scheduled for allocation in Fiscal Year 2013-14.

## **FINANCIAL RESOLUTION:**

Resolved, that the project(s), as component phases or in their entirety, appear under Government Code Section 14556.40(a) and are entitled to participate in this allocation.

Reimbursement of eligible costs is subject to the policies, restrictions and assurances as set forth in the Commission's policy for allocating, monitoring, and auditing Traffic Congestion Relief Program projects, and is governed by the terms and conditions of the Fund Transfer Agreement, Program Supplement or Cooperative Agreement, and subsequent amendments to the same if required, as executed between the implementing agency and the California Department of Transportation.

Attachment

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Implementing Agency District-County	BREF # and Project Description Description of Allocation	Item # Fund Type	Amount by Fund Type
<b>2.6e.(2) Traffic Congestion Relief Program Allocations</b>		<b>Resolution TFP-13-02</b>	
1 \$40,000,000 Santa Clara Valley Transportation Authority 04 – Santa Clara	<b>Project 1.2 – BART to San Jose – Phase 1; extend BART from Warm Springs to Berryessa.</b>  Allocate \$40,000,000 for Construction.  (Future Consideration of Funding – Resolution E-08-19, October 2008.) <u>Output/Outcome:</u> Construct BART extension from Warm Springs (Fremont) to Berryessa (San Jose).  <b>This is a Tier 1 project.</b>	Chapter 91 of the Statutes of 2000  601-3007 TCRF	\$40,000,000

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.7  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Dennis Jacobs  
Acting Division Chief  
Aeronautics

Subject: **FINANCIAL ALLOCATION FOR FISCAL YEAR 2013-14 AERONAUTICS SET-ASIDE TO MATCH FEDERAL AIRPORT IMPROVEMENT PROGRAM GRANTS RESOLUTION FDOA-2013-01**

## **RECOMMENDATION:**

The California Department of Transportation (Department) recommends the California Transportation Commission (Commission) approve Resolution FDOA-2013-01, allocating \$550,000 for Fiscal Year (FY) 2013-14 to match federal Airport Improvement Program (AIP) grants.

## **ISSUE:**

The Aeronautics program includes an annual set-aside used to match federal AIP grants. The attached resolution proposes to renew the delegation for the allocation of state funds to match the federal AIP grants.

## **BACKGROUND:**

Each year the Commission approves a set-aside to match AIP grants. This allocation provides the authority for the Department to subvent matching funds to individual projects as requested by airport sponsors.

The Department provides Commission staff with monthly reports on allocations for AIP Matching funds. The reports show the sponsor name, the airport name, a project description, the AIP grant amount, the state matching amount, and an explanation on any portions of the AIP grant that were not matched. Because the Aeronautics Account is continuously appropriated, any unused remainder of this allocation would be available in FY 2013-14 to fund additional Aeronautics Program projects.

Attachment

# **CALIFORNIA TRANSPORTATION COMMISSION**

## **Allocation of Funds to Match Federal Airport Improvement Program (AIP) Grants**

### **Resolution FDOA-2013-01**

- 1.1 WHEREAS, pursuant to Section 21683.1 of the Public Utilities Code (PUC), the California Transportation Commission (Commission) is authorized to allocate funds for a portion of the local match for federal Airport Improvement Program (AIP) grants to certain airports; and
- 1.2 WHEREAS, the Commission sponsored the legislation that enacted PUC Section 21683.1 in order to maximize the amount of federal airport funds that can be allocated to California; and
- 1.3 WHEREAS, the Federal Aviation Administration requires that airport sponsors certify that matching funds are available as a condition of accepting a federal grant; and
- 1.4 WHEREAS, on September 15, 2011, the Commission approved the Capital Improvement Program, which is an element of the California Aviation System Plan, and lists needed federal airport improvement projects from all funding sources.
- 2.1 NOW, THEREFORE BE IT RESOLVED, that, regarding federal Airport Improvement Program grants made by the Federal Aviation Administration to public entities in this State received by the Department of Transportation (Department) from July 1, 2013 through June 30, 2014, with the approval of the Department, at such time as the federal grant offer is accepted by the public entity applicant, there is allocated to each applicant from the Aeronautics Account, 5 percent (5%) of that portion of the grant whose primary benefit is for general aviation in order to provide a part of the local match for the grant in accordance with the provisions of Public Utilities Code Section 21683.1 (b) until the total of all such allocations equals \$550,000; and
- 2.2 BE IT FURTHER RESOLVED, that the Department shall provide Commission staff with monthly status reports on sponsors' matching fund applications that have been approved by the Department.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.8a.  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR STATE HIGHWAY OPERATION AND PROTECTION PROGRAM (SHOPP) PROJECT WAIVER 13-33**

## **RECOMMENDATION:**

The Department of Transportation (Department) recommends the California Transportation Commission (Commission) approve allocation extension requests for 9 State Highway Operation and Protection Program (SHOPP) projects totaling \$60,224,000 as shown in the attachment.

## **ISSUE:**

The Department experienced challenges in bringing 9 SHOPP projects for allocation and was not ready to request funds at the June 2013 Commission meeting. Therefore, the Department is requesting extensions to the allocation deadline for projects as shown in the attachment.

## **BACKGROUND:**

Resolution G-03-19, State Transportation Improvement Program (STIP) Guidelines, adopted by the Commission on December 11, 2003, stipulates that the Commission may approve waivers to the timely use of funds deadline one time only for up to 20 months in accordance with Government Code section 14529.8. Although the STIP Guidelines do not apply to SHOPP projects, it is prudent to apply the same management practices relative to timely use of funds to SHOPP projects.

Attachment

**Time Extension/Waiver – Project Allocation Deadline**

Project #	Dist-Co-Rte PPNo  Project Description  Reason for Delay	Program / Year Extension Amount (\$X,000) Con (Construction)	Number of Months Requested Extended Deadline
1	01-Men-1 PPNO: 01-4459  Near Westport, from 0.8 mile north of Ten Mile River Bridge to 4.2 miles south of Blue Slide Gulch Bridge. Repair slipout.  The Department is requesting an eight-month time extension for construction allocation. This project is programmed in the 2012 SHOPP for \$4,600 for construction. Due to constraints for settling the jurisdictional matter and conflict resolution policy with the Mendocino County Planning and Building Department and the California Coastal Commissions the permits could not be completed in a timely manner.	SHOPP – Bridge / 2012-13 \$4,600	8 months February 2014
2	01-Men-1 PPNO: 01-4492  Near Albion, from 0.1 mile south to 0.6 mile north of Navarro River Bridge; also on Route 128, from 0.0 mile to 0.2 mile east of Navarro River Bridge. Install metal beam guardrail and rumble strips.  The Department is requesting a six-month time extension for construction allocation. This project is programmed in the 2012 SHOPP for \$2,500 for construction. Additional time is requested due to public opposition to the elements of the project scope and the uncertainty of the timeline to develop proposals acceptable to the California Coastal Commissions.	SHOPP – Collision / 2012-13 \$2,500	6 months December 2013
3	04-SM-101 PPNO: 04-0685P  Near East Palo Alto, at the Santa Clara County line. Replace the San Francisquito Creek Bridge.  The Department is requesting a four-month time extension for construction allocation. This project is programmed in the 2012 SHOPP for \$9,320 for construction. Utility agreements from the utility companies for overhead electrical and fiber optic lines were not secured due to the Buy America provisions. Revised agreements were sent to the utility companies and the final agreements have not yet been received. It is anticipated that the agreements will be on hand by the October CTC meeting.	SHOPP - Bridge/ 2012-13 \$9,320	4 months October 2013
4	04-Son-116 PPNO : 04-0816K  Near Guerneville, from 1.1 miles east of Mays Canyon Road to 0.1 mile west of Odd Fellows Park Road. Reconstruct retaining wall.  The Department is requesting a four-month time extension for construction allocation. This project is programmed in the 2012 SHOPP for \$680 for construction. A modification to the final estimate was necessary after delivery to include additional permit costs and additional time is needed to update the contract documents to comply with the updated 2010 standards.	SHOPP – Major Damage /2012-13 \$680	4 months October 2013
5	07-LA-1 PPNO: 07-4446  In Malibu, one mile north of Topanga Canyon Road. Reconstruct roadway embankment, shoulder and repair failed drainage system.  The Department is requesting a 19-month time extension for construction allocation. This project is programmed in the 2012 SHOPP for \$3,500 for construction. Additional time is requested due to a California Coastal Commissions (CCC) permit requirement, the Department is performing a wave uprush study which will be completed in September 2013. After the updates to the Plan, Specifications & Estimates from the California Coastal Commissions, it is expected to be submitted to Division of Engineering Services in November 2014 and ready to list in January 2015.	SHOPP - Major Damage/ 2012-13 \$3,500	19 months March 2015

**Time Extension/Waiver – Project Allocation Deadline**

Project #	Dist-Co-Rte PPNo  Project Description  Reason for Delay	Program / Year Extension Amount (\$X,000) Con (Construction)	Number of Months Requested Extended Deadline
6	07-LA-10 PPNO : 07-4456  In West Covina and San Dimas, from South Meadow Road to Route 57. Rehabilitate pavement.  The Department is requesting a 12-month time extension for construction allocation. This project is programmed in the 2012 SHOPP for \$33,040 for construction.  This project to rehabilitate the existing pavement is planned to be combined for construction with a High Occupancy Vehicle (HOV) project funded predominantly (94 percent) by the Los Angeles County Metropolitan Transportation Authority (LACMTA, or Metro). Since accomplishing both the HOV and SHOPP roadway rehabilitation together will result in less disruption to the traveling public, design modifications were needed to accomplish both improvements together. The alignment of the HOV project, on which the design was nearly complete, had to be shifted to avoid right-of-way impacts to properties on which negotiations were contentious. Additional time is required to modify the design of this project and the HOV project to stay within both the capital and support budgets, while still meeting the purpose and need of the respective projects.	SHOPP – Roadway/2012-13 \$33,040	12 months June 2014
7	07-LA-10 PPNO: 07-3870  In the city of Los Angeles, at Ballona Creek drainage system. Construct sand filters and infiltration devices Phase 2 of 10.  The Department is requesting a four-month time extension for construction allocation. This project is programmed in the 2012 SHOPP for \$5,654 for construction. The project included work at 5 locations. After careful review and field visit, maintenance crew determined one location may not be safe enough to maintain and need to be removed from the project. Additional time is needed to remove the location from the project plans and update the cost estimate to reflect the change.	SHOPP – Mandates / 2012-13 \$5,654	4 months October 2013
8	07-LA-105 PPNO: 07-4433  In Hawthorne, at Inglewood Avenue. Reconstruct failed slope and hydro seed.  The Department is requesting a four-month time extension for construction allocation. This project is programmed in the 2012 SHOPP for \$500 for construction. An additional four months is requested to incorporate the 2010 construction contract standards into the project.	SHOPP – Major Damage / 2012-13 \$500	4 months December 2013
9	10-SJ-0 PPNO: 10-0123  In Stockton, at the Stockton Yard Maintenance Station (L8721). Install hazardous waste remediation system.  The Department is requesting a six-month time extension for construction allocation. This project is programmed in the 2012 SHOPP for \$430 for construction. The San Joaquin County Environmental Health Department recently contacted the Department to report the Central Valley Regional Water Quality Control Board (CVRWQCB) may not require remediation at this location. Additional time is needed for the CVRWQCB to reach a final determination.	SHOPP – Mandates / 2012-13 \$430	6 months December 2013

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.8b.(1)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Denix D. Anbiah  
Division Chief  
Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF CONTRACT AWARD FOR LOCALLY-ADMINISTERED STIP PROJECTS, PER RESOLUTION G-06-08 WAIVER 13-34**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) extend the period of contract award for the time periods identified for each project on the attached document.

## **ISSUE:**

The Commission allocated \$1,227,000 for the construction of three locally-administered State Transportation Improvement Program (STIP) projects identified on the attachment. The responsible agencies have been unable to award the contracts within six months of allocation. The attachment describes the details of the projects and the explanations for the delays. The respective agencies request extensions, and the planning agencies concur.

## **BACKGROUND:**

In June 2006, the Commission adopted Resolution G-06-08, which requires the agency implementing a project to request a time extension if the project will not be awarded within six months of the allocation. STIP Guidelines stipulate that the Commission may approve a waiver to the contract award deadline one time only for up to 20 months in accordance with Section 14529.8 of the Government Code.

Attachment

**Time Extension/Waiver – Project Contract Award Deadline  
Local Streets and Roads Projects**

<b>Project #</b>	<b>Applicant County PPNO Project Description Reason for Project Delay</b>	<b>Extension Amount  <i>Construction Only</i></b>	<b>Allocation Date Resolution Number Number of Months Requested Extended Deadline CT Recommendation</b>
1	City of Concord Contra Costa County PPNO 04-2119A Monument Corridor Pedestrian and Bikeway Network, TE Project	\$944,000	3/5/2013 FP-12-40 2 months 11/30/2013 Support
<p>The City of Concord (City) is requesting a two-month extension to the period of contract award for the Monument Corridor Pedestrian and Bikeway Network Transportation Enhancement project. The City has experienced delays in advancing and advertising the project due to coordination efforts with the California Department of Toxic Substance Control (DTSC).</p> <p>The City completed the Plan Specification &amp; Estimate (PS&amp;E) package and submitted the Authorization to proceed with Construction (E-76) request to the Department in late January 2013. The Department reviewed the PS&amp;E package and determined that due to the presence of Arsenic on the project and DTSC involvement, Work and Risk plans and letters of concurrence from the Contra Costa County and DTSC were required. The City coordinated with DTSC and Contra Costa County, completed the additional items, and then re-submitted to the Department on April 30, 2013. The City received the E-76 approval on June 18, 2013 with a three-week advertising period. However, the City Council needs to approve the contractor selection and the meeting is scheduled for July 23, 2013, therefore, the City is requesting a two-month extension, to allow for any unforeseen issues that may delay the project award.</p>			
2	City of American Canyon Napa County PPNO 04-2130G Napa Junction Elementary School Pedestrian Improvement, TE Project	\$197,000	12/6/2012 FP-12-30 3 months 9/30/2013 Support
<p>The City of American Canyon (City) is requesting a three-month extension to the period of contract award for the Napa Junction Elementary School Pedestrian Improvement TE project. The City experienced delays in advancing the project towards advertisement and award due to unobligated project funds.</p> <p>The project was allocated on December 6, 2012. At the time of allocation, the Department noted that the preliminary engineering (PE) funds had not yet been obligated. The Department worked with the City to obligate the PE funds. The City was unable to submit the Request for Authorization to proceed with Construction (E-76) until the Authorization to Proceed with PE had been issued by the Department.</p> <p>This delay set the project schedule back by approximately six weeks. The City submitted an E-76 to the Department and received authorization from the Department and FHWA on May 30, 2013. The City advertised the project on June 6, 2013. The City anticipates awarding the project by mid August 2013. In order to allow sufficient time for any unforeseen delays, the City is requesting an additional three months to September 30, 2013.</p>			
3	Town of Yountville Napa County PPNO 04-2130H North Yountville Bike Route, Sidewalk Extension, Enhancements, TE Project	\$86,000	12/6/2012 FP-12-30 4 months 10/31/2013 Support
<p>The Town of Yountville (Town) is requesting a four-month extension to the period of contract award for the North Yountville Bike Route, Sidewalk Extension, Enhancements, TE project. The Town experienced delays in obtaining encroachment permits and complying with the new Disadvantaged Business Enterprise (DBE) Race Conscious (RC) requirements.</p> <p>The Town initially submitted the project to the Department for encroachment permits in June 2011. After several rounds of comments, revisions and re-submittals, the Right of Way certification was finalized in January 2013. The Town requested the Authorization to Proceed with Construction (E-76) on January 16, 2013. The Town had to revise the E-76 package to address the new DBE RC requirements. The Town revised their request, resubmitted the package and the Department approved the E-76 in mid-June, 2013. The Town anticipates advertising and awarding the project by October 2013. Therefore, the Town is requesting a four-month extension to October 31, 2013.</p>			

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.8b.(2)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF CONTRACT AWARD FOR STATE  
ADMINISTERED PROJECTS ON THE STATE HIGHWAY SYSTEM, PER  
RESOLUTION G-06-08  
WAIVER 13-35**

## **RECOMMENDATION:**

The Department of Transportation (Department) recommends the California Transportation Commission (Commission) extend the period of contract award for six months for one State Highway Operation and Protection Program (SHOPP) project described below.

## **ISSUE:**

On January 8, 2013, the Commission allocated \$11,273,000 for one SHOPP project (PPNO 0176F) to upgrade the Safety Roadside Rest Area along Highway 15 in San Bernardino County. In accordance with Resolution G-06-08, the deadline to award contracts for projects allocated in January 2013 is July 31, 2013. The Department will not be able to meet the deadline and is requesting a six-month time extension for the period of contract award.

Bids for this project were opened on May 9, 2013. The Department was not able to award the contract to the lowest bidder as the lowest bid was over the Engineer's Estimate and the amount of funding available for the project. However, it was decided that it would be feasible to down-scope the project to match the available funding. This six-month time extension will allow the Department sufficient time to re-scope, re-advertise, and award the project contract.

## **BACKGROUND:**

In June 2006, the Commission adopted Resolution G-06-08, making the six-month period to award a permanent requirement under the State Transportation Improvement Program Guidelines.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.8b.(3)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF CONTRACT AWARD FOR LOCALLY ADMINISTERED PROJECT ON THE STATE HIGHWAY SYSTEM, PER RESOLUTION G-06-08 WAIVER 13-36**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) approve the request by El Dorado County (County) to extend the period of contract award for six months for the construction of new interchange on Highway 50 at Silva Valley Parkway Project (PPNO 3302) in El Dorado County.

## **ISSUE:**

On January 8, 2013, the Commission allocated \$1,000,000 for one locally administered Proposition 1B State-Local Partnership Program project. In accordance with Resolution G-06-08, the deadline to award contracts for projects allocated in January 2013 is July 31, 2013. However, the County will not be able to meet the deadline and is requesting a six-month time extension to award the contract.

At the time of allocation, it was assumed and expected that all Right of Way issues would have been resolved and that the County would have had order of possession of all parcels required for this project. However, one of the fourteen parcels needed for this project underwent the eminent domain process for acquisition. Order of possession was then followed and executed on June 8, 2013. The project was advertised on July 12, 2013. However, due to large size and complexity of the project, the advertisement period will be longer than usual. This time extension will allow the County sufficient time to address any bidder inquiries, open and analyze bids and award the project contract.

## **BACKGROUND:**

In June 2006, the Commission adopted Resolution G-06-08, making the six-month period to award a permanent requirement under the State Transportation Improvement Program Guidelines.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.8b.(4)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Rachel Falsetti  
Division Chief  
Transportation Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF CONTRACT AWARD FOR LOCALLY ADMINISTERED PROJECT ON THE STATE HIGHWAY SYSTEM, PER RESOLUTION G-06-08 WAIVER 13-37**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) approve the request by San Bernardino Associated Governments (SANBAG) to extend the period of contract award for four months for the Sidewalk and Median Improvement project along Route 62 (PPNO 2112) in San Bernardino County.

## **ISSUE:**

On January 8, 2013, the Commission allocated \$778,000 for one locally administered Proposition 1B State-Local Partnership Program (SLPP) project. In accordance with Resolution G-06-08, the deadline to award contracts for projects allocated in January 2013 is July 31, 2013. However, SANBAG will not be able to meet the deadline and is requesting a four-month time extension, to November 30, 2013 to award the contract. The delay to award is due to federal Authorization to Proceed (E-76). The E-76 process was delayed for three months for the Highway Safety Improvement Program portion of the project funds. This four-month time extension will allow SANBAG sufficient time to acquire the E-76 authorization, advertise and award the project contract.

## **BACKGROUND:**

In June 2006, the Commission adopted Resolution G-06-08, making the six-month period to award a permanent requirement under the State Transportation Improvement Guidelines.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.8d.(1)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Jane Perez  
Division Chief  
Mass Transportation

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT DEVELOPMENT  
EXPENDITURES PER RESOLUTION G-99-25, GUIDELINES FOR ALLOCATING,  
MONITORING, AND AUDITING OF FUNDS FOR LOCAL ASSISTANCE PROJECTS  
WAIVER 13-39**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) approve a six-month extension for the period of project development expenditures, to December 31, 2013, for the Lathrop Road Westerly Grade Separation project in San Joaquin County.

## **ISSUE:**

In November 2010, the Commission approved Resolution MFP-10-09 allocating \$3,032,000 to the City of Lathrop (City) for the Lathrop Road Westerly Grade Separation project. Completion of the Right of Way acquisition has been delayed due to additional easements and pending acquisition of two remaining properties needed in order to complete the final utilities relocation alignment. The first property requires ratification of the sales agreement by the City Council and completion of escrow. The second acquisition requires entering into a purchase and sales agreement with the property owner prior to transfer of ownership. The City has hired a private firm to administer and handle all land and Right of Way acquisition services.

As of March 2013, plans, specifications and estimates are 100 percent complete while Right of Way acquisition is 90 percent complete. The remaining balance of \$978,906 will be expended over the next six months.

## **BACKGROUND:**

Resolution G-99-25, Guidelines for Allocating, Monitoring, and Auditing of Funds for Local Assistance Projects, adopted by the Commission on August 18, 1999, stipulates that funds allocated for local project development or Right of Way costs must be expended by the end of the second fiscal year following the fiscal year in which the funds were allocated. The Commission may approve a waiver to the timely use of funds deadline one time only for up to 20 months in accordance with Section 14529.8 of the Government Code.

# Memorandum

To: CHAIR AND COMMISSIONERS  
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: August 6, 2013

Reference No.: 2.8d.(3)  
Action Item

From: STEVEN KECK  
Acting Chief Financial Officer

Prepared by: Denix D. Anbiah  
Division Chief  
Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT DEVELOPMENT EXPENDITURE FOR LOCALLY-ADMINISTERED STIP PROJECTS PER RESOLUTION-06-08 WAIVER 13-41**

## **RECOMMENDATION:**

The California Department of Transportation recommends the California Transportation Commission (Commission) approve the request by Mendocino County (County) to extend the period of project development expenditure for Project Approval and Environmental Document (PA&ED) for the State Transportation Improvement Program (STIP) Brooktrails Second Access project (PPNO 4099P) by 14 months, from June 30, 2013 to August 31, 2014, per STIP Guidelines.

## **ISSUE:**

On July 1, 2010, the Commission approved Resolution FP-09-55, allocating \$459,000 for the PA&ED component of the Brooktrails Second Access project. The County anticipates that they will be unable to complete the preliminary engineering and environmental studies to obtain environmental clearances by the June 30, 2013 expenditure deadline and is requesting a 14-month extension to August 31, 2014.

## **BACKGROUND:**

The project will provide a two-lane road between Brooktrails Township and State Route 101 to improve traffic circulation and provide a second access during emergency conditions. This new access will significantly affect the Brooktrails area thus requiring additional effort beyond that typically expected for a project of similar magnitude. Preliminary engineering and environmental studies have been delayed due to resistance from principal landowners to grant access to the proposed project site. The County has diligently attempted to gain voluntary rights of entry and has not yet pursued legal access through the courts.

The County has completed a few of the required environmental studies and continues to seek cooperation of all the applicable property owners in conducting the remaining environmental and engineering studies. The County anticipates obtaining rights of entry and completing the PA&ED phase by August 2014. Therefore, the County is requesting a 14-month time extension to August 31, 2014.

Current STIP Guidelines stipulate that funds allocated for local project development or Right of Way costs must be expended by the end of the second fiscal year following the fiscal year in which the funds were allocated. The Commission may approve a waiver to the timely use of funds deadline one time only for up to 20 months in accordance with Section 14529.8 of the Government Code.